



**METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY**

**JOHN COOPER  
MAYOR**

**NASHVILLE DEPARTMENT OF TRANSPORTATION  
AND MULTIMODAL INFRASTRUCTURE**

**INSTRUCTIONAL BULLETIN NO. 2022-3 (Revised)**  
**Multimodal Access Closure Policy Update**

Instructional Bulletin No. 2022-3 is hereby revised effective on 9/20/2022. These revisions are being made to clarify the previous policy bulletin as a result of questions and to address internal Department operations and workflow. The following list outlines the substantive portion of this revision:

- The intent of this policy is to prohibit the continuous closure of multimodal pathways in the public rights of way for a period of seven (7) days or more. The word “continuous” was added to clarify that this policy does not apply to permit requests for discrete locations where the total amount of time the closure will be in place is less than seven (7) days.
- The terminology describing the process in which those seeking an exemption from this policy was changed from “variance” to “exception”. This revision was made to prevent confusion and/or false assumption between this and other processes referred to as variances.
- An additional paragraph was added to provide more guidance to those seeking a permit for work less than seven (7) days. These permit request will require traffic control plans, however only those submitted through the exception process will require plans stamped and signed by a licensed engineer.
- The body established by the Director to support the review of those seeking an exemption for closures seven (7) days or more is being clarified as an advisory committee. All policy exceptions are at the sole discretion of the Director. The advisory committee will provide technical support and industry feedback.

**Effective immediately**, all newly permitted construction activities closing a multimodal pathway of travel in Metro Nashville’s right-of-way for a continuous period of seven (7) days or more will be explicitly prohibited. All ongoing currently permitted work will be under review by the Nashville Department of Transportation & Multimodal Infrastructure (NDOT).

To provide for the safety, health, and welfare of the citizens of Davidson County, this policy update seeks to ensure all modes of transportation including traffic lanes, sidewalks, bikeways, pedestrian crossing and bridges, and bus stops remain open and in good condition. NDOT will limit right-of-way closures to keep its infrastructure in a state free from obstruction and in good working order.

For work that impacts multimodal pathway travel for a period of less than 7 days, a traffic control plan must be submitted at the time of the permit application. This plan should outline the layout of required the MUTCD traffic control signage and an alternate travel pathway that meets ADA standards. Traffic control plans for closures less than 7 days are not required to be stamped by a licensed engineer.

The Nashville Department of Transportation and Multimodal Infrastructure (NDOT) has the discretion to move the effective date to the end of the year after review on a case-by-case basis. If the applicant believes that the inability to utilize Metro right-of-way for more than seven days will constitute an undue burden to completion of a construction project, the applicant may submit a request for an exception. Under the authority and discretion of the Director, NDOT may grant exceptions on a case-by-case basis. The Director will create an advisory committee to support providing industry feedback during the exception process. Please refer to the enclosed exception request guidelines and application template for further process details.

A handwritten signature in black ink, appearing to read "Brad Freeze", is written over a horizontal line.

Brad Freeze, PE

Chief Engineer/Assistant Director

## FAQ

### **Who does this new policy apply to?**

The Multimodal Access Closure Policy applies to all new permit applicants seeking to close multimodal paths of travel in the public right-of-way for more than 7 days. The policy also applies to current permit holders looking to renew right-of-way permits for more than 7 days. For example, if a current permit holder has a 30 day sidewalk closure permit, once that permit expires, the permit holder will work with NDOT to assess closure needs and will either receive a 7 day closure permit, or if necessary, may request an exception as outlined in this policy.

### **How does the policy affect construction projects that have been permitted but have not yet begun?**

Permits that have been issued prior to August 19, 2022 will be honored for the permitted amount of time. Once the permit has expired, closures will be subject to the new policy.

### **How does this policy affect construction projects that are seeking a permit for future construction?**

This policy will impact future contractors seeking to obtain a construction easement permit in that they would not be allowed to close a multimodal path of travel without the approval of an exception.

### **What recourse exists if a construction firm maintains they need to use the public right-of-way to implement a project?**

Any entity maintaining a need to close multimodal access in the public right-of-way for more than 7 days may apply for an exception through the process described herein. The exception will be considered by NDOT and approved or denied by the Director on a case-by-case basis.

## **Guidelines for Submitting a Multimodal Access Closure Exception For Public Right-of-Way**

### **Section 1 – General Notes**

Prior to any permit approval that requires temporary right-of-way closures impacting multimodal access facilities within Davidson County, the project owner shall submit a construction package to the Nashville Department of Transportation and Multimodal Infrastructure (NDOT) for review. If any parts listed below are missing and/or incomplete, the package will be immediately rejected. It is therefore recommended that the owner prepare the exception requirements at the conceptual stage of the project. This will allow both NDOT and the owner to understand all requirements needed for the project before any permitting is granted.

NDOT will have the authority to reject any exception application for any reason deemed necessary. Under such circumstances, the fee will not be reimbursed, and a second, and possible subsequent, submittal will be required until all requirements are met. Applying for an exception does not guarantee that the proposed project closures will be approved by NDOT.

See Section 2 for fees and the following Application Form and Checklist.

### **Section 2 – Fee Requirements**

1<sup>st</sup> Submittal: \$500

2<sup>nd</sup> and Subsequent Submittals: \$250

# MULTIMODAL ACCESS CLOSURE EXCEPTION APPLICATION FORM AND CHECKLIST

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Submittal Date: 1/15/24  New Submittal  Re-Submittal No: 00

Related Building Permit No: NA

Project Name: MWS Main Street Gallatin AVE Water Replacement

Street Name Location: Main Street to Gallatin AVE

Between: S 5<sup>th</sup> Street And: Eastland Ave

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Applicant Name: Garney Companies Inc

Address: 200 Crutchfield Ave Nashville TN 37214

Phone: 615 300 3287 Fax: \_\_\_\_\_ Contact: Tony.Naredo

Email: tony.naredo@garney.com

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Project Description: Installation of new Water line from 5<sup>th</sup> Street and Main Street to Gallatin AVE and

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Start Date: 1/29/24 End Date: 12/31/2024 Project Length: 360 cal day

Describe Type of Closure: One lane (#1 lane) will be closed during the life of the project 24/7. It will be starting at 5<sup>th</sup> Street and progress eastbound along Main street towards Gallatin and Eastland. During work hours the lane closure will be changed to a two lane closure. One lane will be maintained in each direction.

Provide Reasons why Project cannot be completed without closures and what other options were considered (attach documents as needed): The new water line is located in the first and second lane. All of the water services are to be replaced along with the Main street alignment. There will be two excavators, trucks and pipe in the street to install the new line.

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## PROJECT INFORMATION CHECKLIST:

### Included Not Applicable

- X  Project Vicinity Map with Project Area shown, street names, property information, existing pavement and striping, gutter and building locations, north arrow, and scale.  
**See Attachment 1A project Vicinity Map**
- Planned work hours included.  
**Planned Work Hours are 6:00 AM -6:00 PM. Additional Night work is required for certain tie ins and will be coordinated with NDOT prior to doing so.**
- Exact location and dimensions of the construction work zone shown.
- If multiple phases are necessary, include perimeter impact of each phase, phase number, anticipated work hours and phase duration.  
**It is a moving lane closure**
- Details on construction activity and equipment being used as part of construction included for each phase.  
**Trenching Equipment includes Cat 320 Excavator with Hoe Ram attachment, John Deere 624k Loader, 300 Class Skidsteer Loader, 335 Cat Excavator.**
- Specify if any on-street parking, and/or metered parking, is to be restricted and if bus zone will need to be relocated.
- Specify if trash pickup will be impacted.  
**Any trash route requiring north bound access will follow detour**
- Provide information on all utility work and utility connections.  
**Attached is Planned utilities-8" water line**
- List all affected residents, businesses, agencies, and schools and any conversations/agreements taken place.  
**Businesses from S. 5<sup>th</sup> Street to Gallatin and Eastland**
- Show ongoing construction projects within vicinity of proposed project impact.  
**See Attachment NDOT Street, Lane, and Sidewalk Closures**
- Provide plan to address conflicts with other nearby projects.  
**Current proposed detours do not impede existing detours.**
- Provide traffic control plan for each phase of construction (see

traffic control checklist for more information).

See Attached Standard Control Traffic Control Plan for Single lane, double lane closure and road Closure plan.

- Provide information on work vehicle parking locations.  
Worker's trucks will be parked at stadium parking and coordinated with the Titans.
- Show construction trucks ingress/egress to project location.  
Access will come thru road closure and at both ends of S. 5<sup>th</sup> Street
- Provide information on any traffic signals, traffic signal loops, and traffic signal cabinets in close proximity to project.  
Road is to be Milled and overlayed in separate contract.

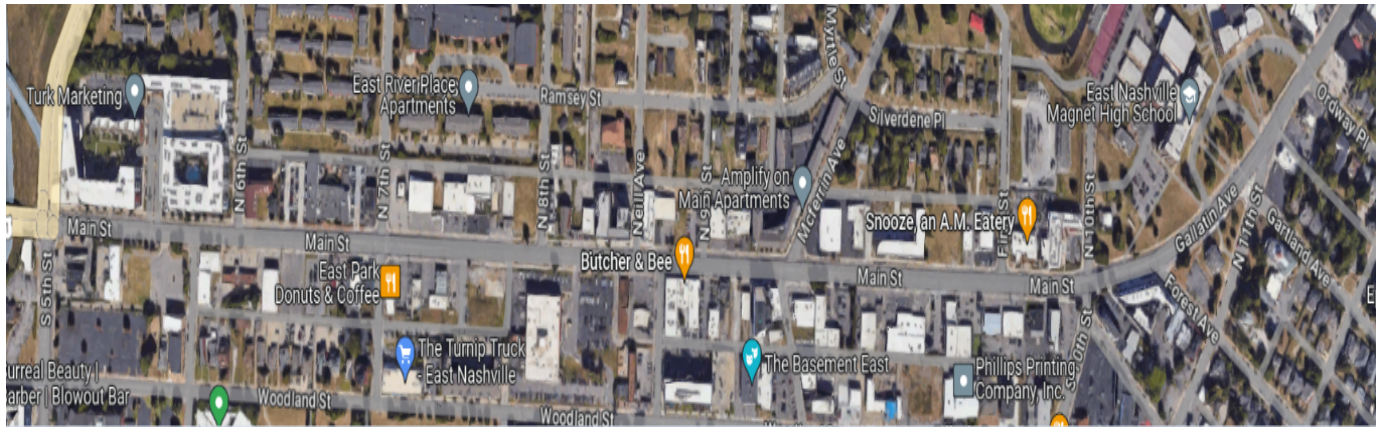
## TRAFFIC CONTROL PLAN CHECKLIST:

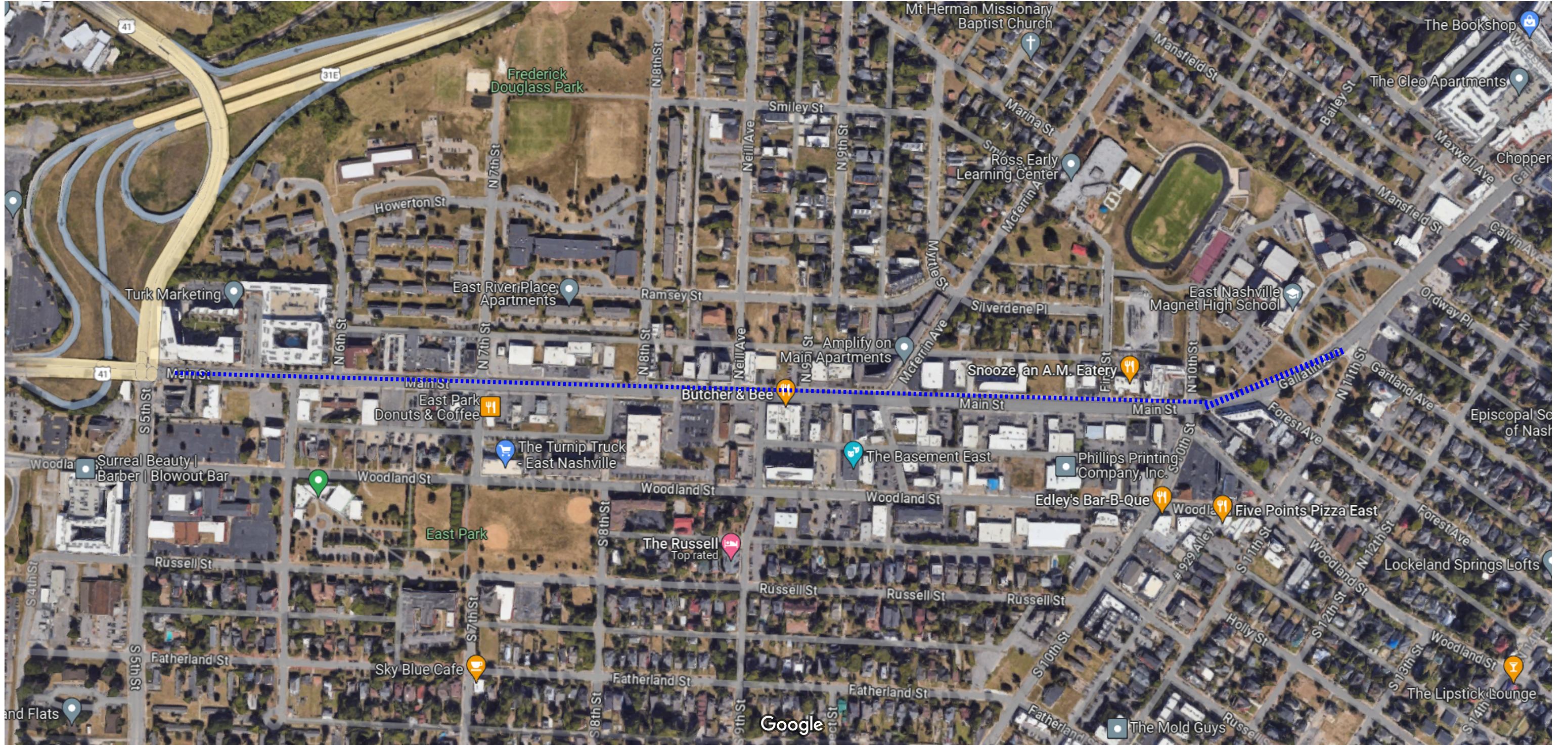
### Included Not Applicable

- |                                     |                                     |   |
|-------------------------------------|-------------------------------------|---|
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | All temporary traffic control plans shall be designed in accordance with the most recent ADA regulations and requirements of the Manual of Uniform Traffic Control Devices. |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Clearly show the locations of all existing signs (including speed limit) as well as the proposed signs for each construction phase.   |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | Show the location of all existing pedestrian paths and pedestrian detour route of each stage of construction.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Show dimensions of travel lane width, shoulder width, sidewalk of each phase, and overall roadway width along the length of affected area.                                  |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | Show all existing striping and markings to remain, to be removed, and all proposed striping and markings for each construction stage.                                       |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Provide detour plan clearly showing detour route for any roadway or pedestrian/bike path closures.  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Specify placement of all temporary traffic control devices.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Specify spacing of all temporary traffic control devices.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Show all existing traffic signals and streetlights in the workzone location.  |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | Lighting provided for all pedestrian detour routes.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Provide minimum eleven (11) foot travel lanes at all times.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Show size, height, and location of all channelizing devices, warning lights, flag trees, barriers, etc.<br><b>See Attachment 1E Channelizing Device plan</b>                |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Label all taper lengths and widths.   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Provide locations of police officers for each phase as needed.<br><b>Two Police officers will be onsite. One stationed at the beginning of each lane closure</b>            |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Temporary Traffic Control Plan has been stamped and signed by a TN licensed Civil Engineer.   |



1A: PROJECT VICINITY MAP shown, street names, property information, existing pavement and striping, gutter and building locations, north arrow, and scale.

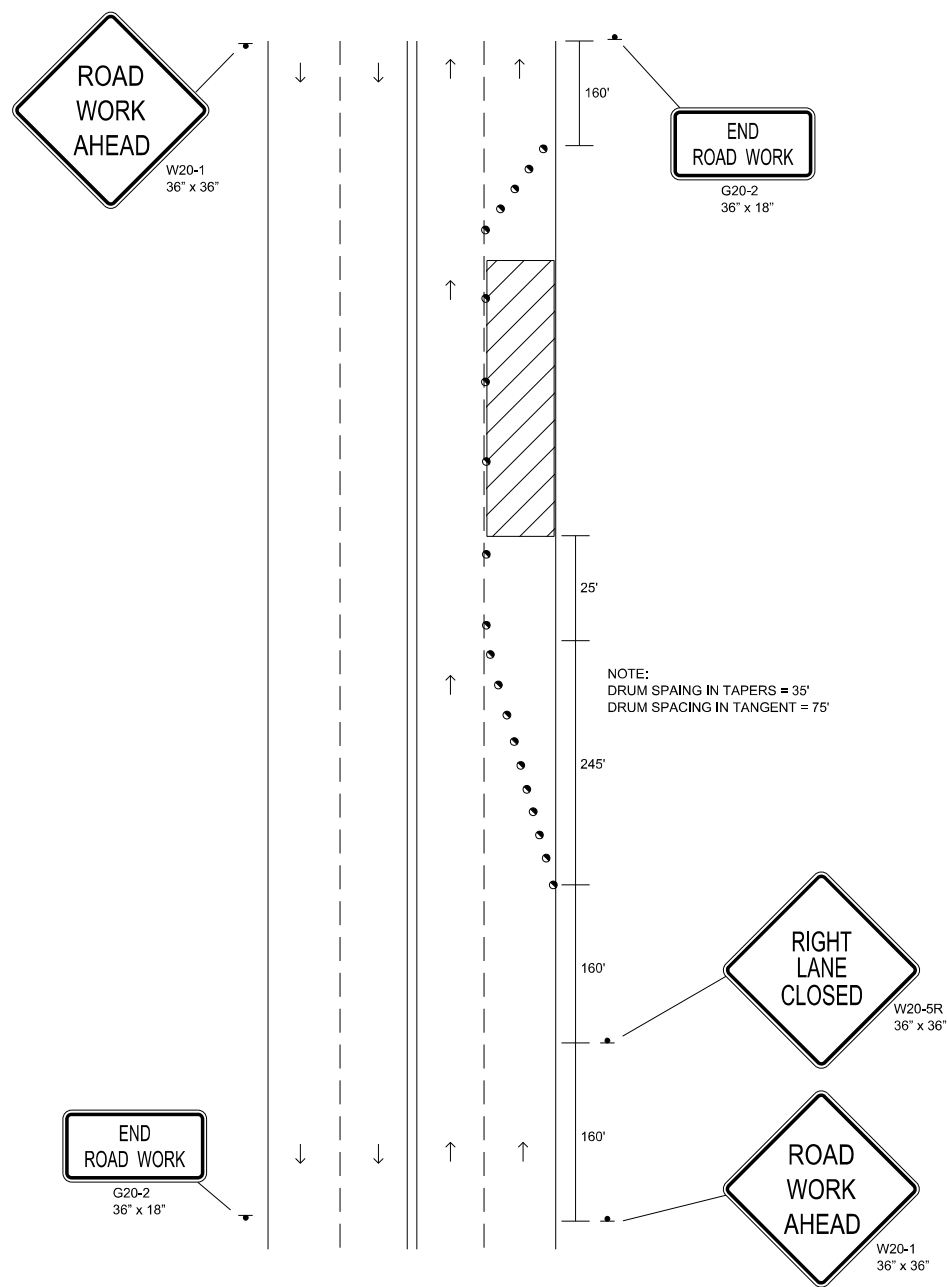




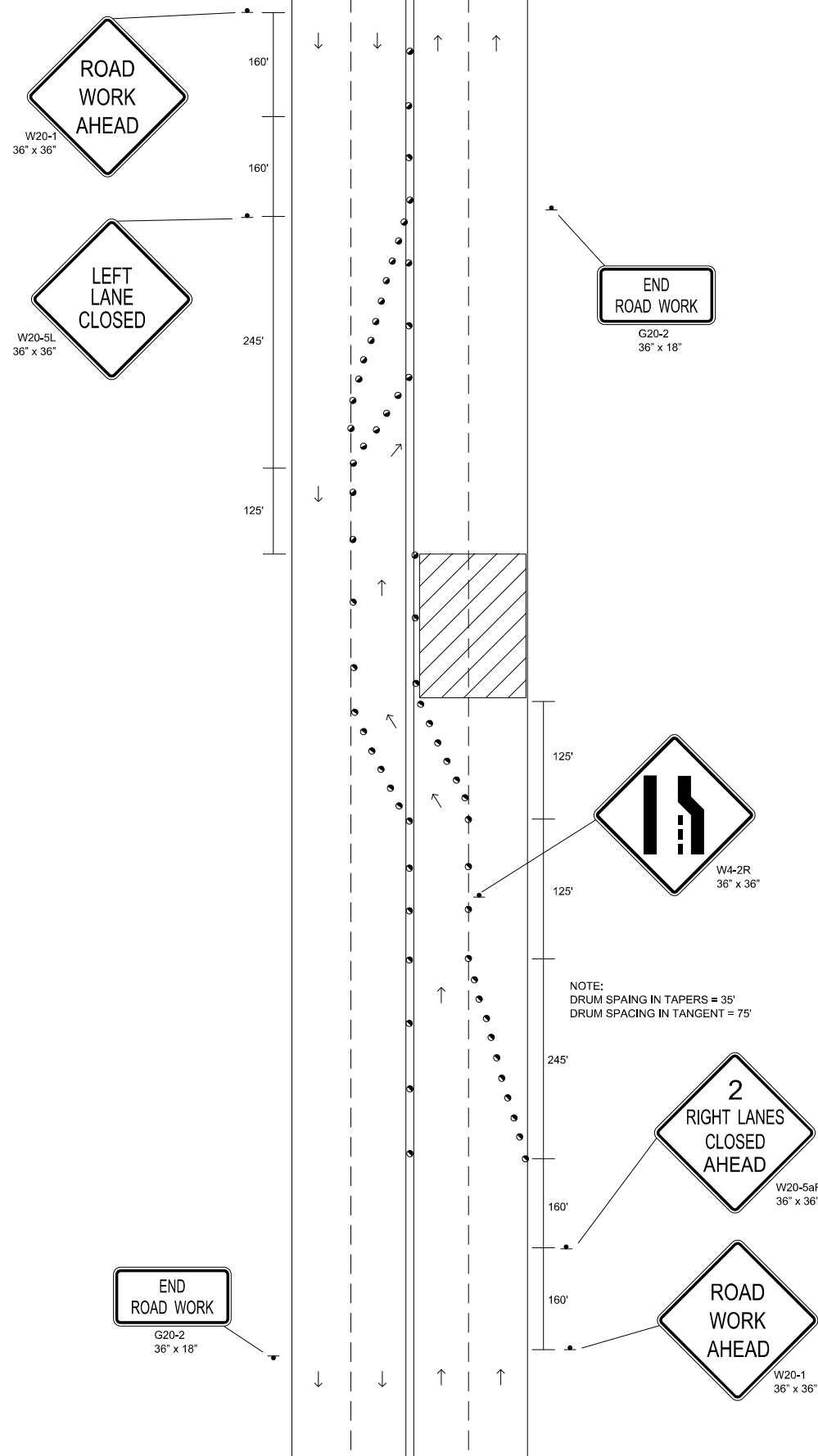
Imagery ©2024 Google, Imagery ©2024 Airbus, CNES / Airbus, Maxar Technologies, USDA/FPAC/GEO, Map data ©2024 Nashville Davidson County 500 ft

TYPE	YEAR	PROJECT NO.	SHEET NO.
TCP	2024	PO 3423-850-19	1

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW



TYPICAL  
1-OUTSIDE LANE CLOSURE  
DETAIL



TYPICAL  
2-OUTSIDE LANE CLOSURE  
DETAIL

SEE SHEET 2  
FOR SIDEWALK TRAFFIC  
CONTROL DETAIL AND  
TRAFFIC CONTROL NOTES.

**SEALED BY**

**thompson**  
ENGINEERING

**GARNEY CONSTRUCTION**

NASHVILLE - MAIN ST.

**TEMPORARY TRAFFIC CONTROL PLAN**

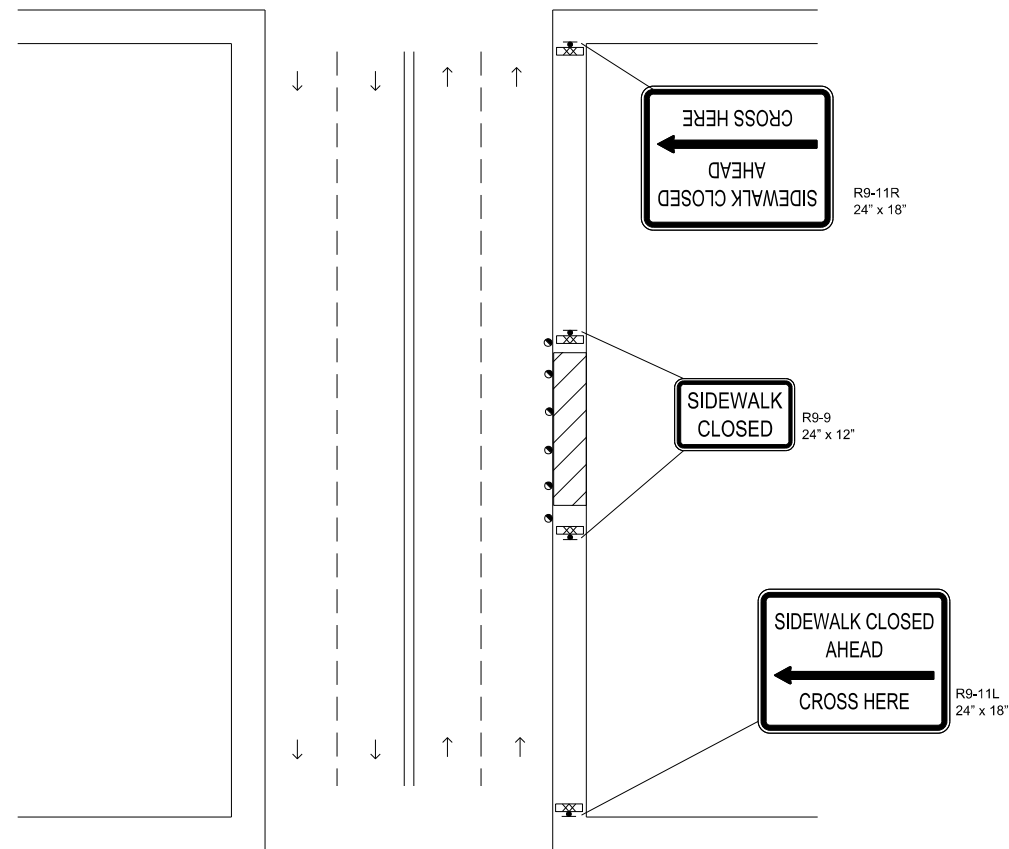
NOT TO SCALE

TYPE	YEAR	PROJECT NO.	SHEET NO.
TCP	2024	PO 3423-850-19	2

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	TEMPORARY BARRICADE (TYPE II)

**TRAFFIC CONTROL NOTES:**

1. THIS TRAFFIC CONTROL PLAN IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE NEEDED DURING CONSTRUCTION.
2. THIS TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
3. TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING. DEVICES USED SPORADICALLY MAY REMAIN IN PLACE PROVIDED THE SIGN FACE IS COVERED.
4. PROVISIONS SHALL BE MADE AT ALL TIMES TO EXPEDITE THE MOVEMENT OF EMERGENCY VEHICLES THROUGH THE WORK ZONE.
5. ACCESS TO ALL PROPERTY OWNERS SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
6. FLAGGERS MAY BE USED AS DIRECTED WITH APPROPRIATE FLAGGER SIGNS (W20-7) IF TRAFFIC OPERATIONS WARRANT.
7. BARRICADES SHALL BE PLACED ACROSS FULL WIDTH OF CLOSED SIDEWALK.
8. SIDEWALK DETOUR SHALL BE DETECTABLE AND ADA ACCESSIBLE.



**TYPICAL  
SIDEWALK CLOSURE  
DETAIL**

**SEALED BY**

**thompson**  
ENGINEERING

**GARNEY CONSTRUCTION**

NASHVILLE - MAIN ST.

**TEMPORARY  
TRAFFIC CONTROL  
PLAN**

NOT TO SCALE

TPCAD 9 PM  
Phase 1 N 6th Street Intersection

Google Maps



Imagery ©2024 Google, Imagery ©2024 Airbus, CNES / Airbus, Maxar Technologies, USDA/FPAC/GEO, Map data ©2024 Nashville Davidson County 200 ft





METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY

JOHN COOPER  
MAYOR

NASHVILLE DEPARTMENT OF TRANSPORTATION  
AND MULTIMODAL INFRASTRUCTURE

### **Chapter 13.20.020 Excavations and Obstruction Permissions and Notice Required**

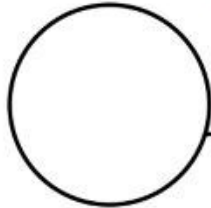
According to the referenced Code on Excavations and Obstructions in Metro Right of Way: “no excavation may be undertaken in any street, road, alley or right of way or of any utility or temporary construction easement of the Metropolitan Government or other government entity by any department of the metropolitan government or any other entity unless notice as required herein has been given”.

**All contractors performing excavations in Metro right of way are required to provide 1 to 2 weeks of advance notice to the homeowners, residents, and businesses along the streets where the work is being performed.** Your company must provide some form of documentation or proof that you have given adequate advance notice of your project. *Failure to do so could result in a triple fee fine to your permit.*

Attached is an example of what the door hangers or mailers should look like.

Please contact [PWPermits@Nashville.gov](mailto:PWPermits@Nashville.gov) or 615-862-8782 if you have any questions.

# NDOT



**Mayor  
John Cooper**

*NASHVILLE DEPARTMENT OF TRANSPORTATION  
& MULTIMODAL INFRASTRUCTURE*

Please be informed that a project for Utility Work will begin in approximately 1-2 weeks depending on weather conditions.

FOR ADDITIONAL INFORMATION CONTACT

Contractor \_\_\_\_\_

Contact Person \_\_\_\_\_


Phone # \_\_\_\_\_

Type of Work \_\_\_\_\_

**NDOT (Formerly Public Works)**

Phone #  
615-862-8782

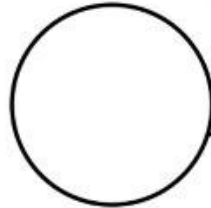
We appreciate your cooperation.

 To Request this information in an alternate format or ADA accommodations, please contact

Rick Kirkpatrick  
615-862-8750

**PROJECT NOTIFICATION**

# NDOT



**Mayor  
John Cooper**

*NASHVILLE DEPARTMENT OF TRANSPORTATION  
& MULTIMODAL INFRASTRUCTURE*

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FOR ADDITIONAL INFORMATION CONTACT

Contractor \_\_\_\_\_

Contact Person \_\_\_\_\_


Phone # \_\_\_\_\_

Type of Work \_\_\_\_\_

**NDOT (Formerly Public Works)**

Phone #  
615-862-8782

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Rick Kirkpatrick  
615-862-8750

**PROJECT NOTIFICATION**



## Nashville Department of Transportation

### Change from Lane Closure to Street Closure Policy

#### Change from Lane Closure to Street Closure after Permit is Issued:

A Street Closure Application and Traffic Plan from a Licensed Traffic Engineer must be submitted for approval to the NDOT Permit Office to change the Permit issued for a Lane Closure to a Street Closure. All requests are assessed based on the scope of work, time required, and impacts to pedestrian and vehicular traffic. Street Closure request is reviewed and restrictions such as work times and need for Safety Officers/Traffic Management are based on the location, knowledge of traffic flows, existing permits already issued, and impacts to the public.

If a Street Closure is requested by the contractor for a permit with an existing Lane Closure, a meeting will be scheduled by the inspector with the contractor and the Permit Administrator to review the conditions and requirements. At that time if there is a need to adjust the Lane Closure parameters the inspector directs the contractor to request the change via e-mail to the Permit Office. The inspector will notify the Permit Office via email of their agreement with the request. If there is a need for a Street Closure, the contractor shall submit a traffic plan from a licensed Traffic Engineer that includes a detour plan with Safety Officers, traffic control and notification of the Street Closure to all Metro entities such as OEM, MNPS, MNPD, NFD, and WeGo prior to the Street Closure. When the Street Closure Application and the Traffic Plan for the Street Closure is approved by the Permit Administrator, the original Lane Closure Permit will be revised to a Street Closure Permit and reissued. The contractor closes the road, and the inspector confirms that the plan is being followed and is working safely.

Intermittent Traffic Control (ITC) measures, in which a street is temporarily closed for a very short period (less than thirty minutes) must be documented on a Lane Closure Permit. The contractor should request this during their initial application. If during the initial site visit with the assigned inspector, if the need for ITC is identified but not clearly indicated on the permit, the contractor will need to request this addition via e-mail to the Permit Office. The inspector will notify the Permit Office via email of their agreement to add the ITC. If during the project, the need for ITC measures is identified, the contractor should notify the assigned inspector to discuss.

#### Change from Lane Closure to Street Closure after Permit is Issued due to Safety Conditions:

Changing a Lane Closure to a Street Closure requires following specific protocols. At any time during work if conditions at the site deteriorate such that there is an immediate traffic safety issue, the road should be immediately closed, and the contractor immediately contact the area inspector and the Permit Office. The area inspector will visit the site immediately to assess the situation associated with a potential Street Closure.

Based upon the revised approved Traffic Plan, it will be determined if variable message boards or other elements of a traffic control plan are required.

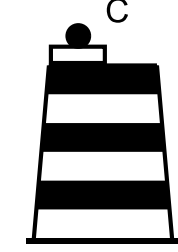
# Nashville Department of Transportation

## Change from Lane to Street Closure Procedures

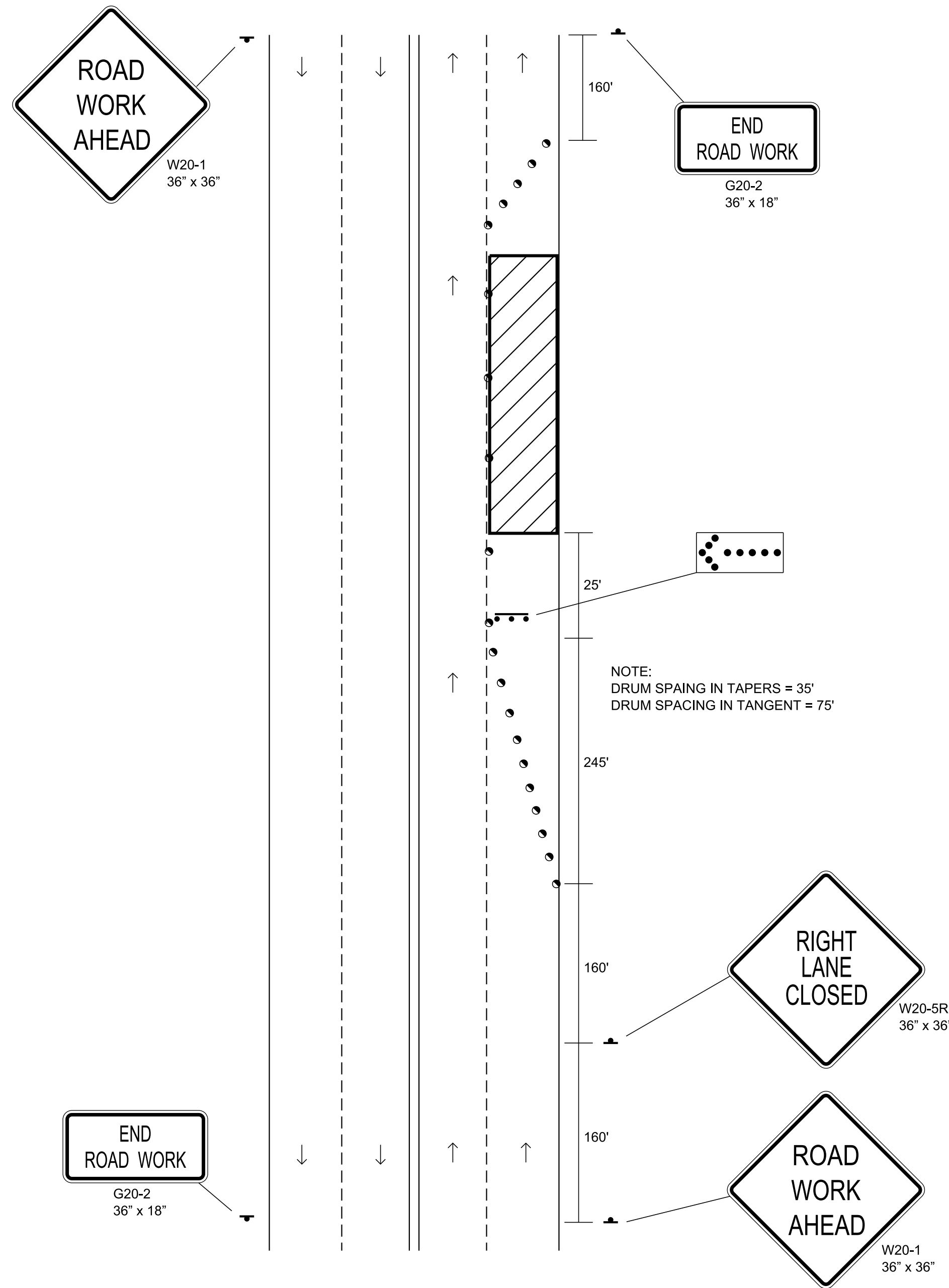
1. Contractor emails Permit Office to request a change from Lane Closure to a Street closure.  
Contractor submits a Traffic Plan from a Licensed Engineer and explains in detail why the change is needed.
2. Office staff receives the email from Contractor requesting a Street Closure and will send to Permit Administrator and NDOT Traffic Engineer for approval.
3. NDOT Permit Office and Traffic Engineer require a minimum of 5 days to review and approve the Traffic Plan for the Street Closure.
4. If the Street Closure is approved by Permit Administrator and Traffic Engineer, area inspector and Permit Office will receive a copy of approved traffic plans and approved Street Closure Permit.
5. The area inspector will contact contractor with approval and conditions of the permit. Inspector will also inform the contractor that it is their responsibility to contact and update the permit with the permit office.
6. Per emailed request to the permit office, referencing Lane closure permit #, the permit office can process a new Street Closure permit at no charge and showing it was paid for on a previous Lane Closure permit.
7. Prior to construction, contractor shall meet with area inspector on site to verify traffic control measures are in place. Inspector will document approval.

TYPE	YEAR	PROJECT NO.	SHEET NO.
TCP	2024	PO 3423-850-19	1

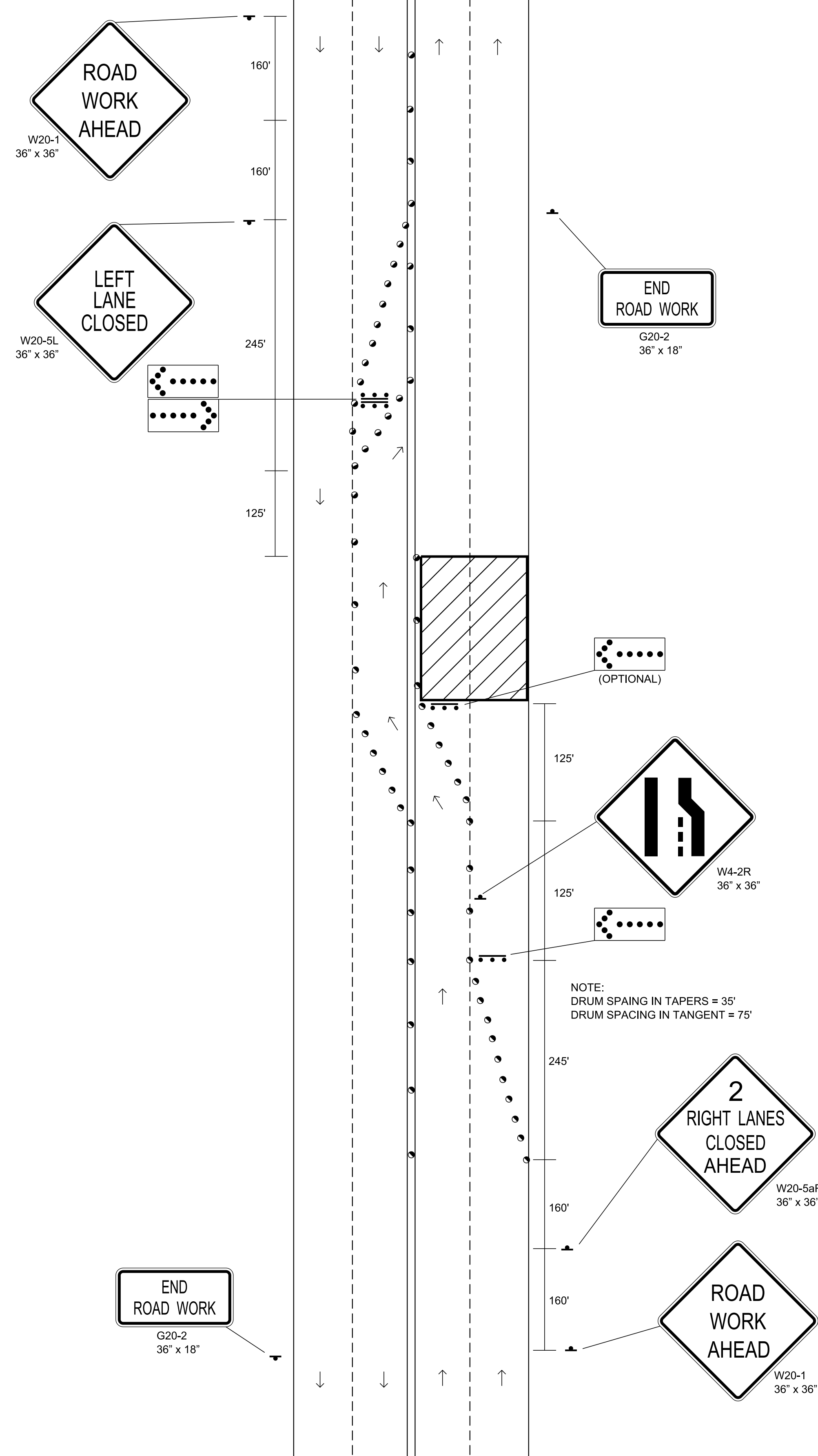
TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	* FLEXIBLE DRUM (OR OTHER CHANNELIZING DEVICE AS DIRECTED BY THE FIELD SUPERVISOR)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	* WARNING LIGHT (TYPE C) (STEADY-BURN)
	TYPE 'A' FLASHING ARROWBOARD



\* WARNING LIGHTS MAY BE ADDED TO EVERY THIRD CHANNELIZING DEVICE OR AS DIRECTED BY THE FIELD SUPERVISOR.



TYPICAL  
1-OUTSIDE LANE CLOSURE  
DETAIL



TYPICAL  
2-OUTSIDE LANE CLOSURE  
DETAIL

SEE SHEET 3  
FOR SIDEWALK TRAFFIC  
CONTROL DETAIL AND  
TRAFFIC CONTROL NOTES.

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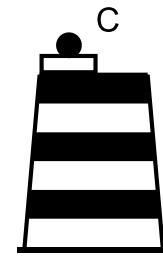
NASHVILLE - MAIN ST.

TEMPORARY  
TRAFFIC CONTROL  
PLAN

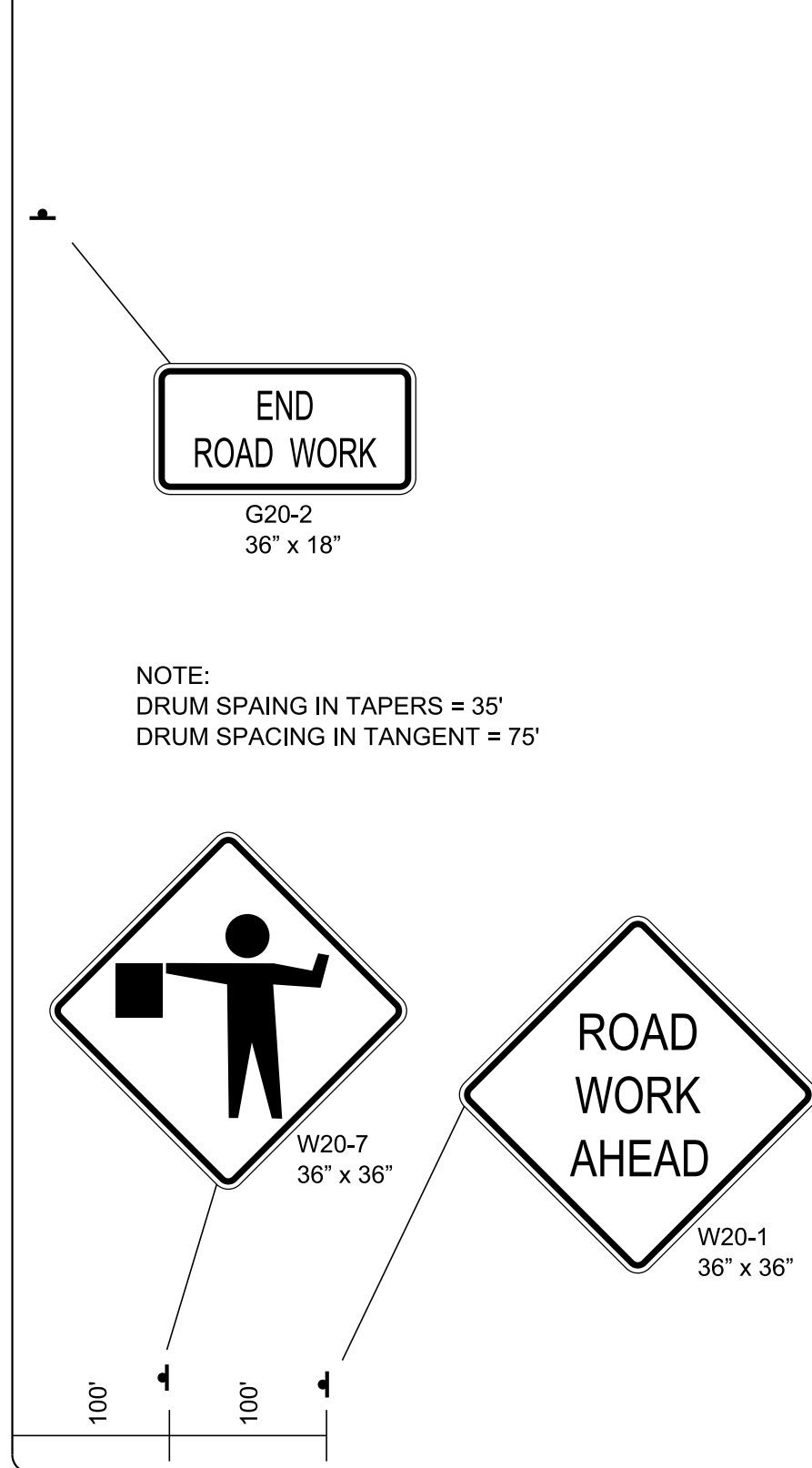
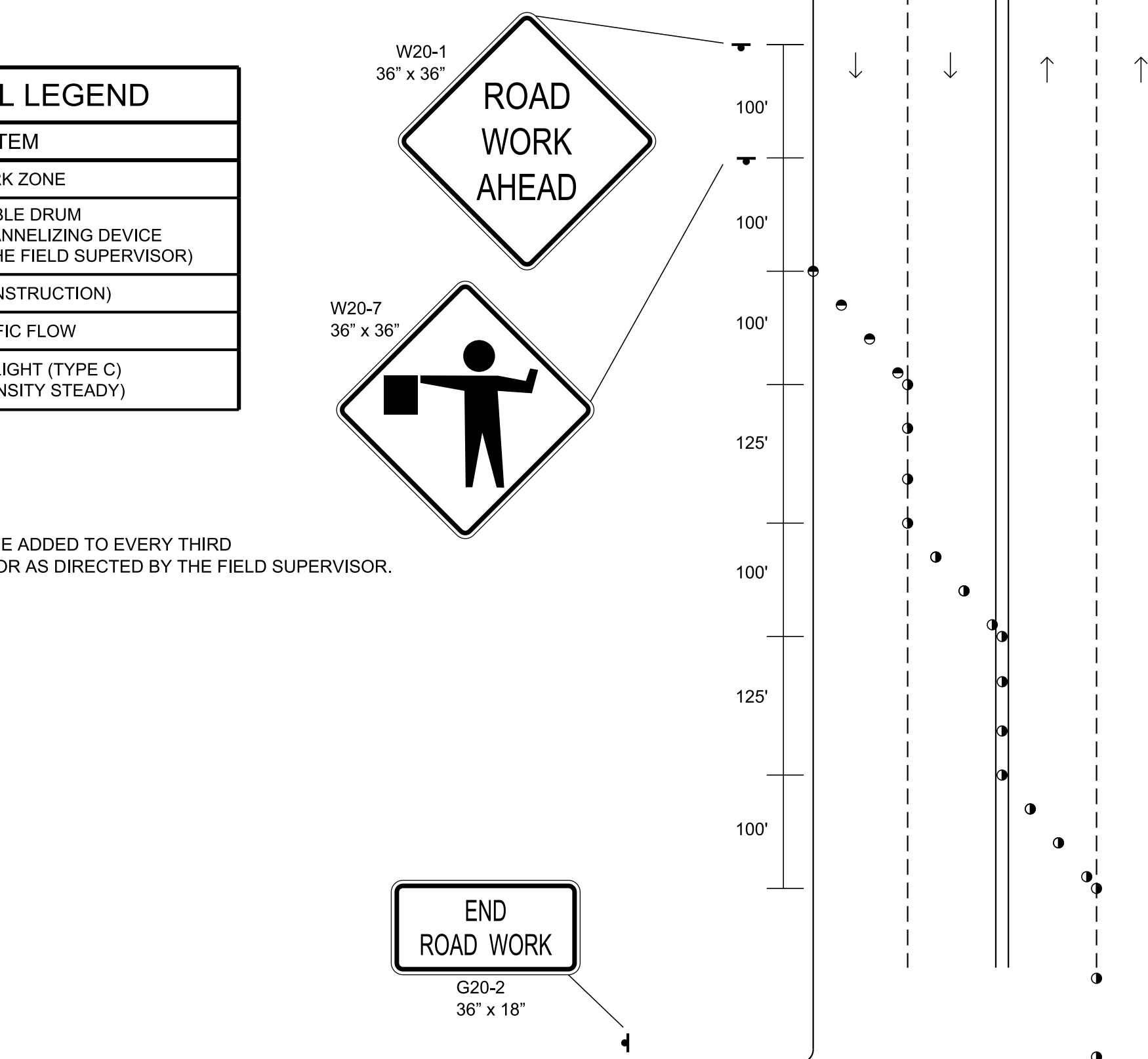
NOT TO SCALE

TYPE	YEAR	PROJECT NO.	SHEET NO.
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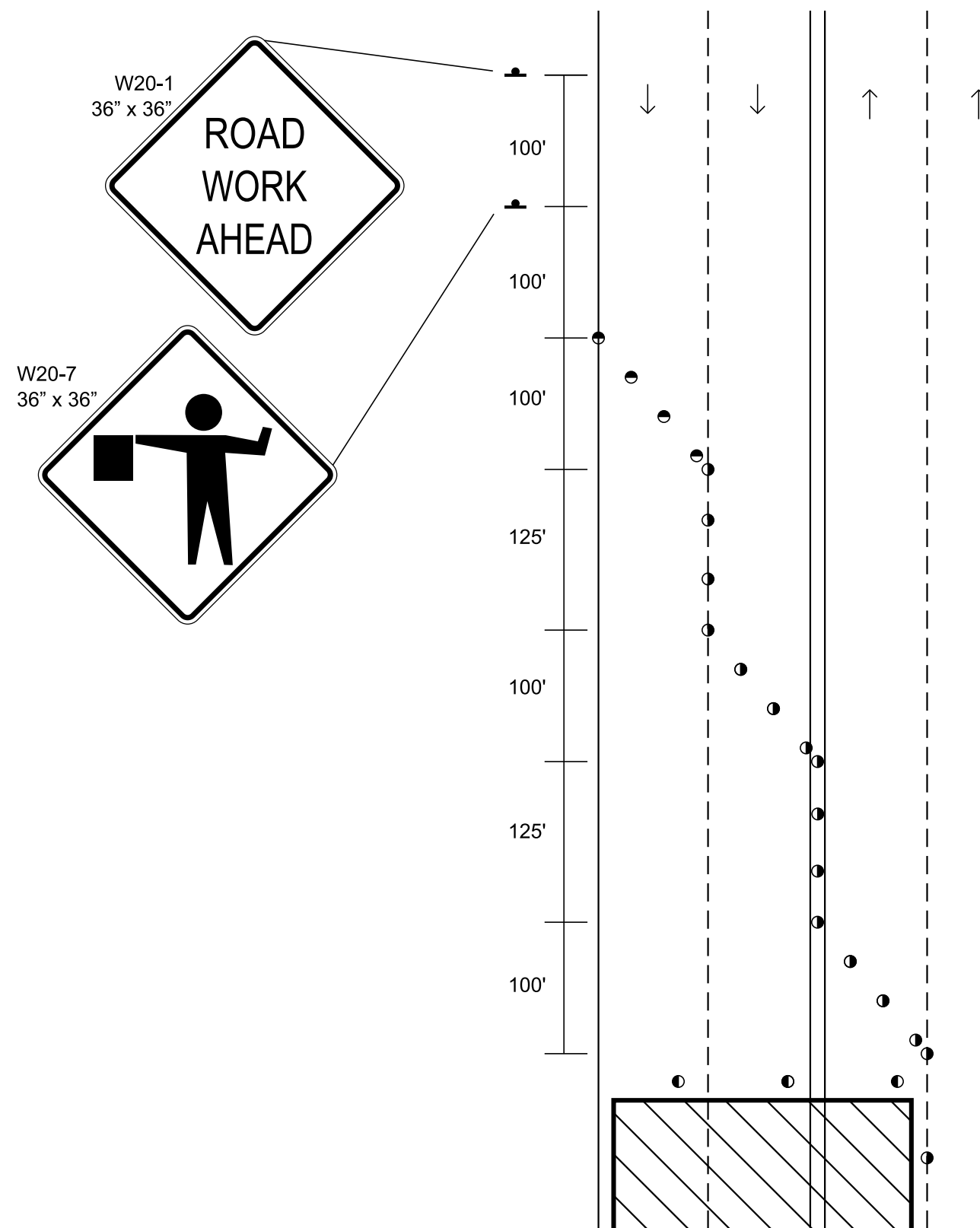
TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	* FLEXIBLE DRUM (OR OTHER CHANNELIZING DEVICE AS DIRECTED BY THE FIELD SUPERVISOR)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	* WARNING LIGHT (TYPE C) (LOW-INTENSITY STEADY)



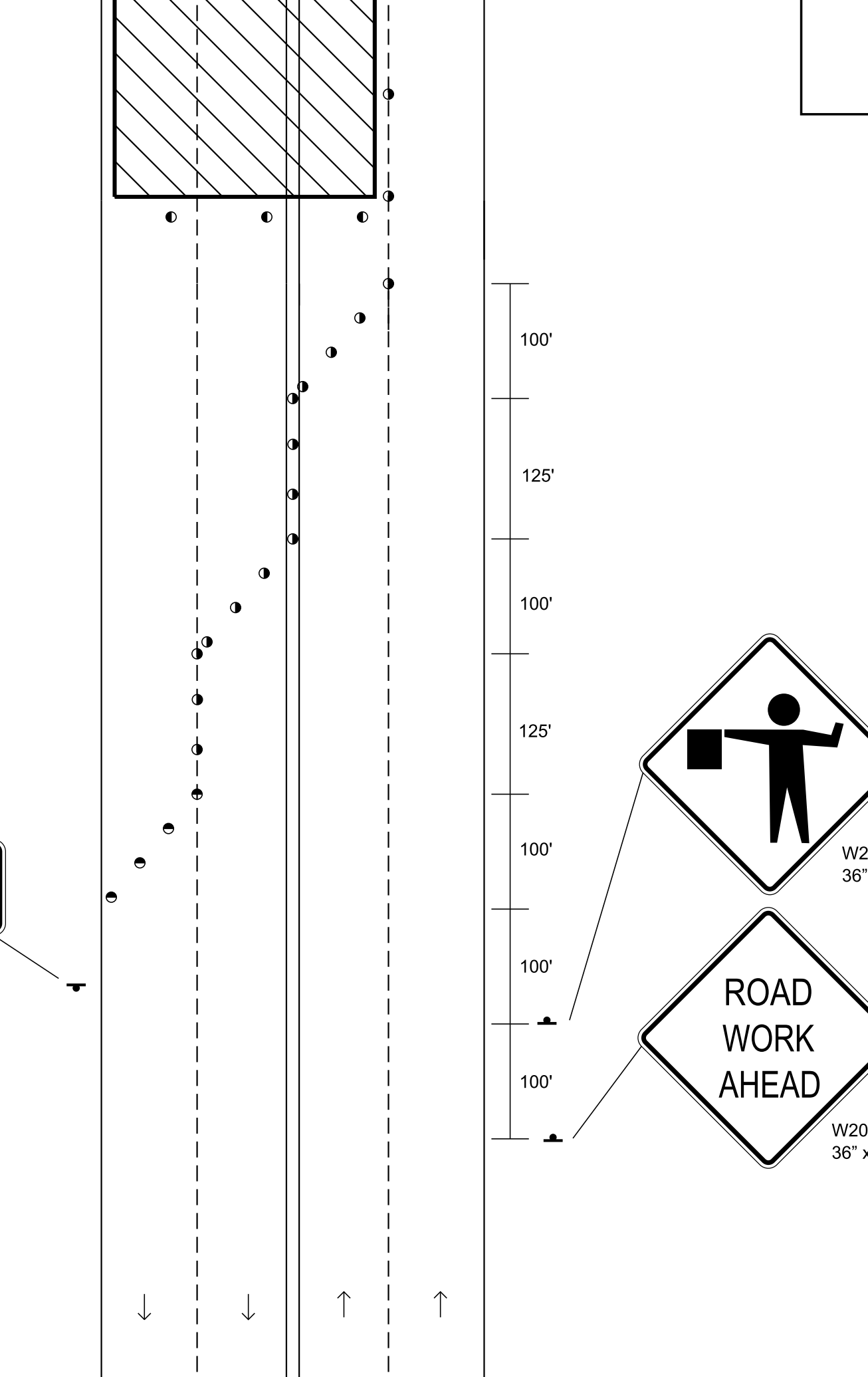
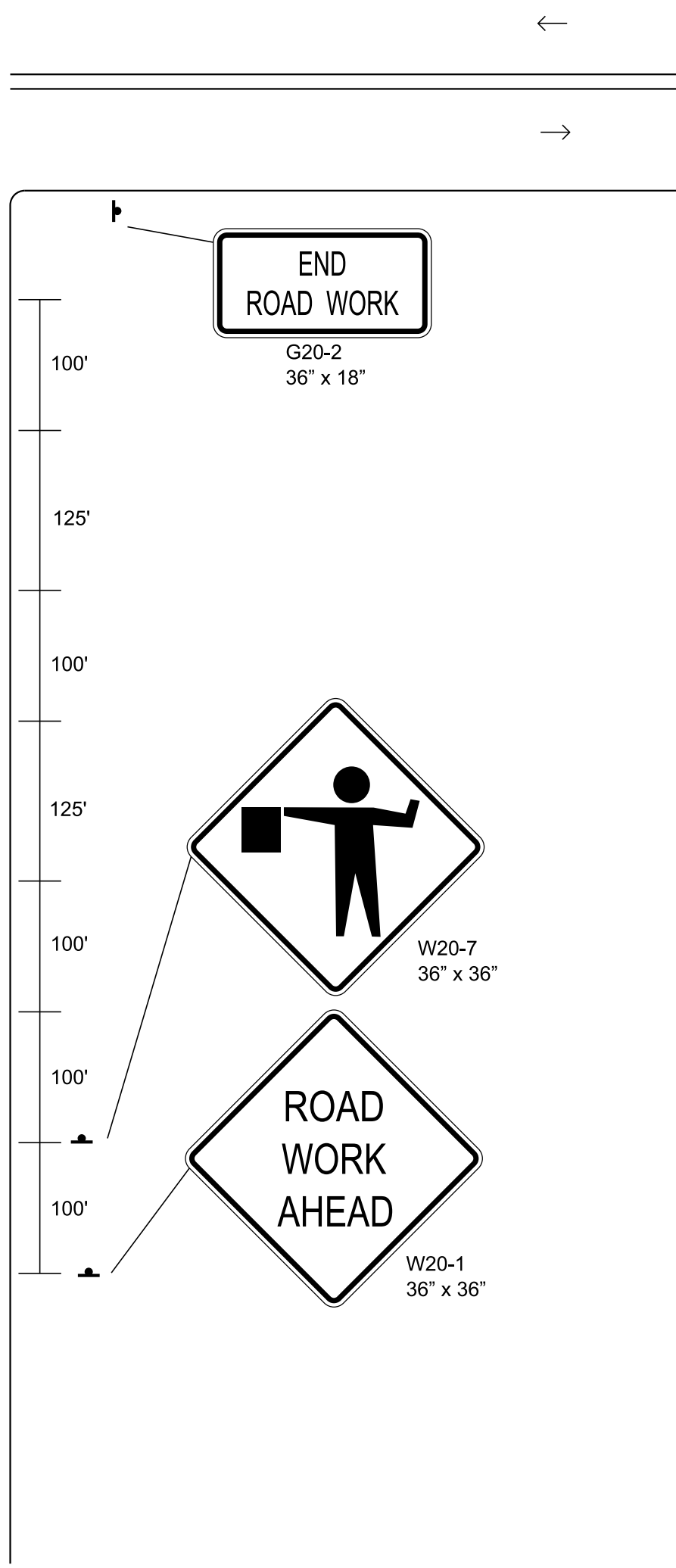
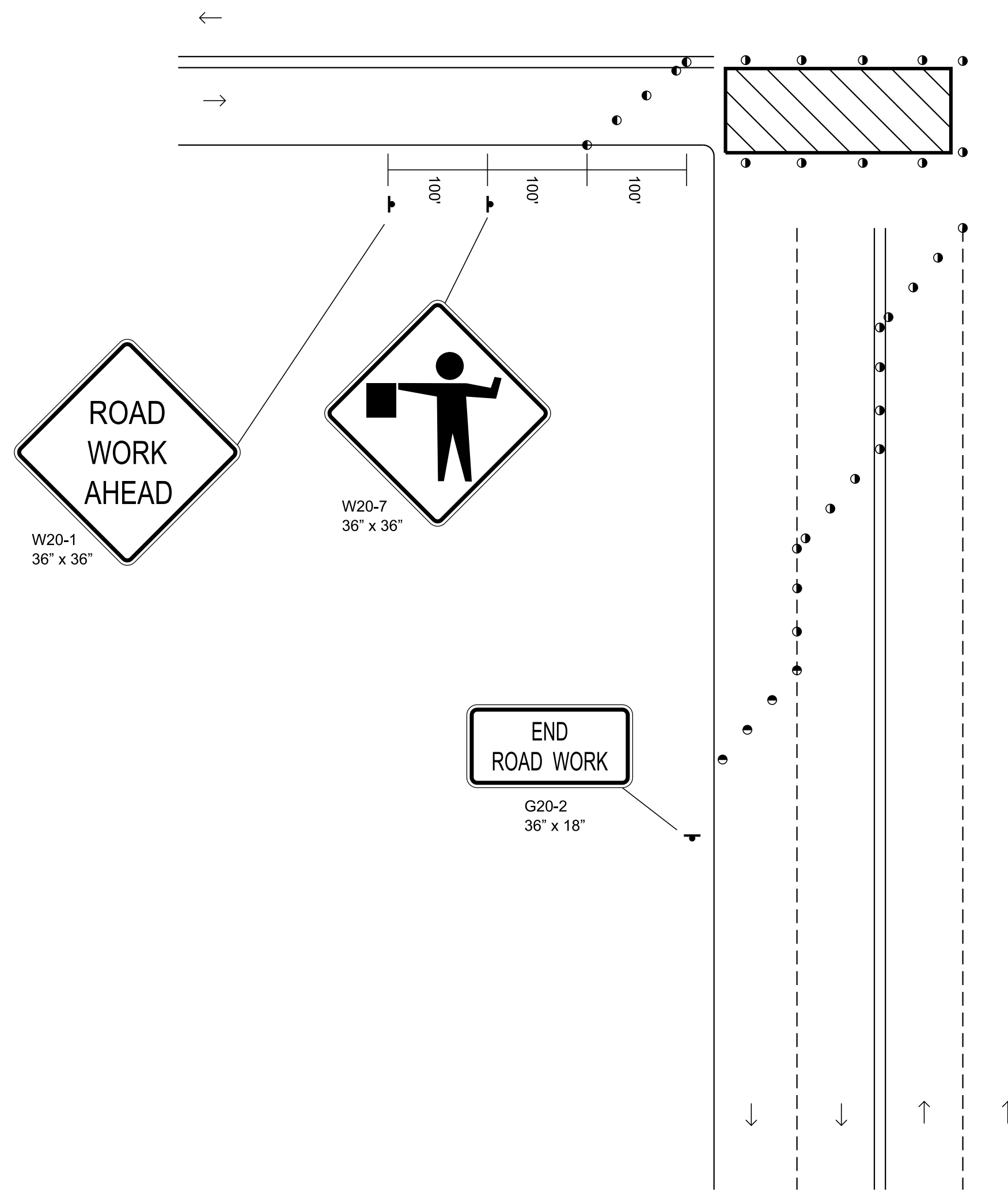
\* WARNING LIGHTS MAY BE ADDED TO EVERY THIRD CHANNELIZING DEVICE OR AS DIRECTED BY THE FIELD SUPERVISOR.



NOTE:  
DRUM SPAING IN TAPERS = 35'  
DRUM SPACING IN TANGENT = 75'



NOTE:  
DRUM SPAING IN TAPERS = 35'  
DRUM SPACING IN TANGENT = 75'



SEE SHEET 3  
FOR SIDEWALK TRAFFIC  
CONTROL DETAIL  
AND TRAFFIC  
CONTROL NOTES.

TYPICAL  
1 LANE OPEN WITH FLAGGERS  
DETAIL

TYPICAL  
1 LANE OPEN WITH FLAGGERS  
DETAIL

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GARNEY CONSTRUCTION

NASHVILLE - MAIN ST.

TEMPORARY  
TRAFFIC CONTROL  
PLAN

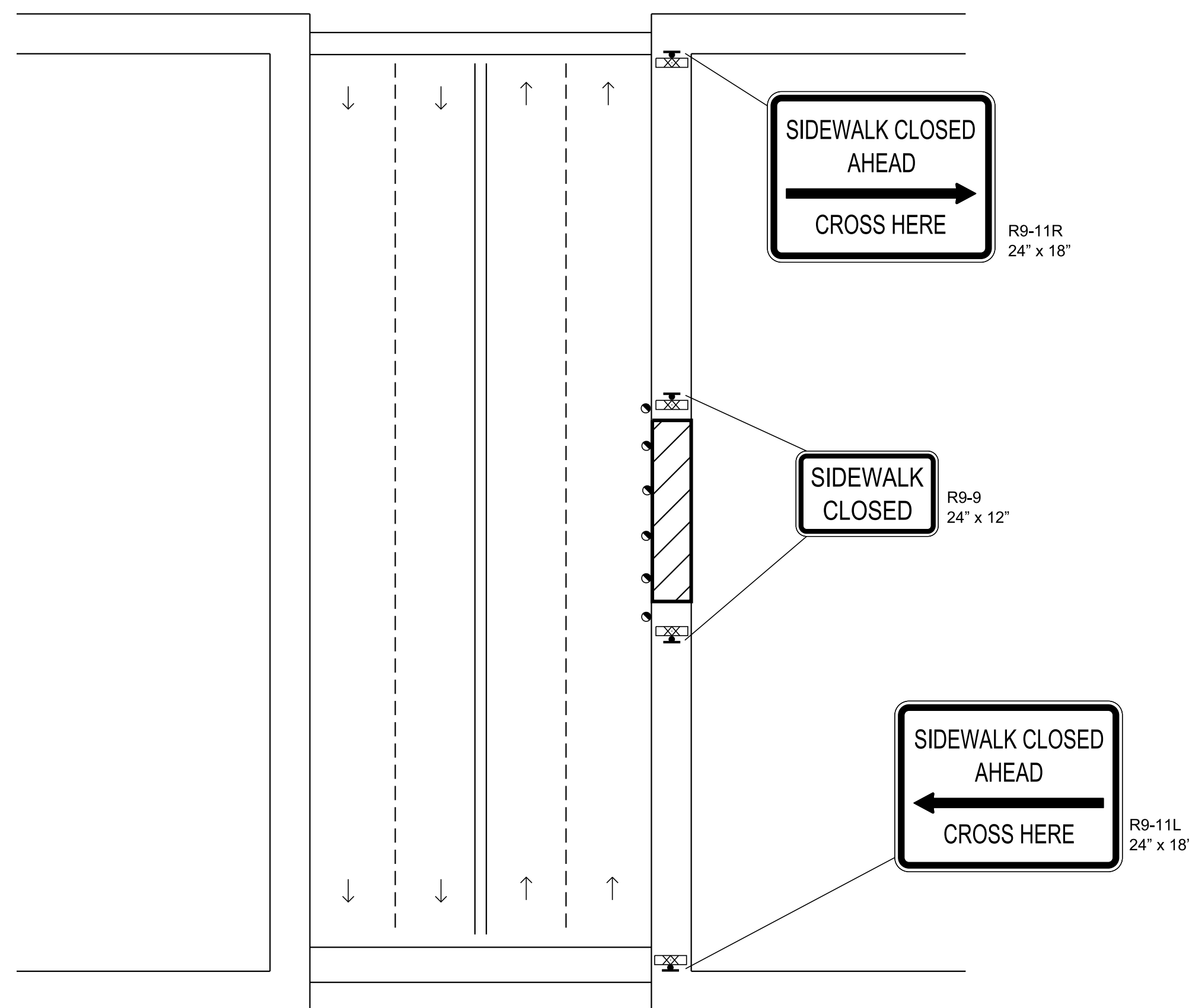
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TYPE	YEAR	PROJECT NO.	SHEET NO.
TCP	2024	PO 3423-850-19	3

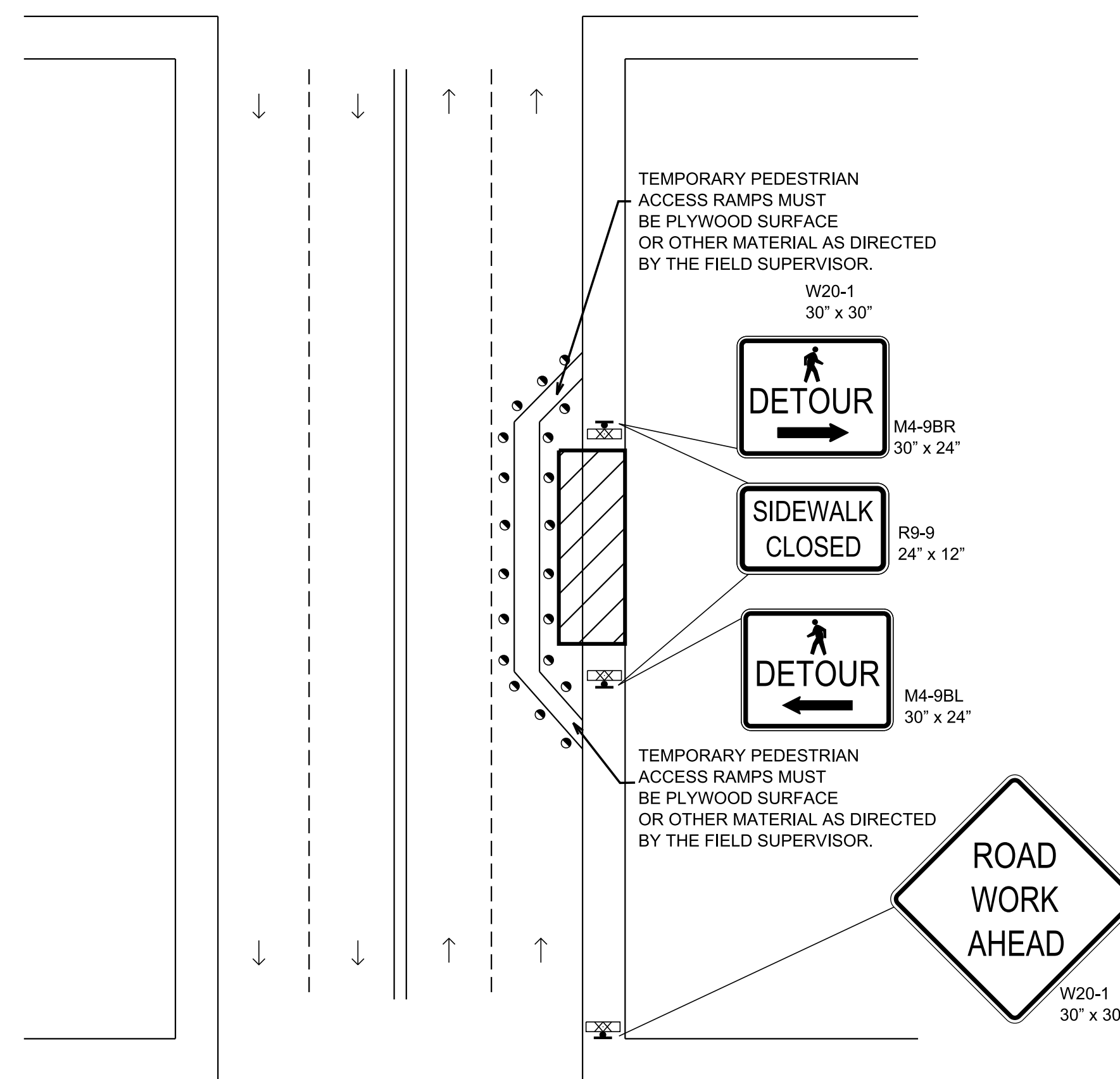
TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUM (OR OTHER CHANNELIZING DEVICE AS DIRECTED BY THE FIELD SUPERVISOR)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	TEMPORARY BARRICADE (TYPE II)

**TRAFFIC CONTROL NOTES:**

1. THIS TRAFFIC CONTROL PLAN IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE NEEDED DURING CONSTRUCTION.
2. THIS TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
3. TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING. DEVICES USED SPORADICALLY MAY REMAIN IN PLACE PROVIDED THE SIGN FACE IS COVERED.
4. PROVISIONS SHALL BE MADE AT ALL TIMES TO EXPEDITE THE MOVEMENT OF EMERGENCY VEHICLES THROUGH THE WORK ZONE.
5. ACCESS TO ALL PROPERTY OWNERS SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
6. BARRICADES SHALL BE PLACED ACROSS FULL WIDTH OF CLOSED SIDEWALK.
7. SIDEWALK DETOUR/DIVERSION SHALL BE CLEARLY IDENTIFIED AND COMPLIANT WITH THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG).
8. CHANNELIZING DEVICES USED TO CHANNELIZE PEDESTRIANS SHALL BE LOCATED SUCH THAT THERE ARE NO GAPS BETWEEN THE BASES OF THE DEVICES, IN ORDER TO CREATE A CONTINUOUS BOTTOM, AND THE HEIGHT OF EACH DEVICE SHALL BE NO LESS THAN 36IN TO BE DETECTABLE TO USERS OF LONG CANES.
9. CHANGEABLE MESSAGE BOARDS MAY BE USED NORTHBOUND FROM I-24 OR SOUTHBOUND FROM EAST NASHVILLE MAGNET SCHOOL OR AT LOCATIONS AS DIRECTED BY THE FIELD SUPERVISOR.



TYPICAL  
SIDEWALK CLOSURE  
W/ DETOUR  
DETAIL



TYPICAL  
SIDEWALK CLOSURE  
W/ DIVERSION  
DETAIL

SEALED BY



GARNEY CONSTRUCTION

NASHVILLE - MAIN ST.

TEMPORARY  
TRAFFIC CONTROL  
PLAN

NOT TO SCALE