

Metropolitan Planning Commission



Staff Reports

March 23, 2017



Metro Planning Commission Meeting of 03/23/2017

Mission Statement: The Planning Commission is to guide the future growth and development for Nashville and Davidson County to evolve into a more socially, economically and environmentally sustainable community with a commitment to preservation of important assets, efficient use of public infrastructure, distinctive and diverse neighborhood character, free and open civic life, and choices in housing and transportation.



SEE NEXT PAGE



NO SKETCH



Project No.	Text Amendment 2016Z-024TX-001
Project Name	Sidewalk Requirements
Council Bill No.	BL2016-493
Council District	Countywide
School District	Countywide

Deferrals This case was deferred at the December 8, 2016, January 12, 2017, January 26, 2017, and February 23, 2017, Planning Commission meetings. No public hearing was held.

Staff Reviewer	Logan
Staff Recommendation	<i>Approve the third substitute.</i>

TEXT AMENDMENT

An ordinance amending Chapters 17.04, 17.20 and 17.40 of the Metropolitan Code pertaining to sidewalks (Proposal No. 2016Z-024TX-001).

PROPOSED TITLE 17 (ZONING CODE) AMENDMENTS

Section 17.20.120 of the Zoning Code currently requires sidewalks for all multi-family and nonresidential development that meets the following criteria:

- The value of any one expansion is twenty-five percent or greater, or the value of multiple expansions during any five-year period is fifty percent or greater of the value of all improvements on the lot prior to expansion; or
- The total building square footage of any one expansion is twenty-five percent or greater, or the total building square footage of multiple expansions during any five-year period is fifty percent or greater of the total building square footage of all improvements on the lot prior to expansion; or
- The property is located within of the Urban Services District (USD) or within the General Services District (GSD) where the Sidewalk Priority Index (SPI) score is twenty or greater. The SPI is established in the Strategic Plan for Sidewalks and Bikeways, most recent edition adopted by the metropolitan planning commission, and incorporated herein by reference.

For multi-family and nonresidential development, this ordinance:

- Clarifies that it applies to new construction.
- Applies to renovations equal to or greater than the assessed value of all structures on the lot.
- Refines the geographic criteria to tie them to recent planning efforts, such as NashvilleNext and WalknBike by requiring sidewalks within the USD, on a street in the Major and Collector Street Plan (MCSP) and/or within a 1/4 mile of a center designated in the General Plan. The current measure, the Sidewalk Priority Index, was adopted in 2002.
- Removes the in-lieu contribution option if the property is within the Urban Zoning Overlay (UZO) or on a street in the MCSP.
- Adds a requirement for curbs or other equivalent means to be provided to prevent any vehicle using a parking area from encroaching on any on-site sidewalk. The curb is already



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required to prevent encroachment into the public right-of-way, required landscaping area or adjacent property.

Currently, there is no requirement for sidewalks for single-family and two-family development in the Zoning Code; however, sidewalks are required by the Subdivision Regulations on new streets and for some infill subdivisions. This ordinance would create a sidewalk requirement for new single-family or two-family units within the UZO, on a street within the MCSP in the USD and/or within a 1/4 mile of a center designated in the General Plan, but would allow a contribution in lieu of construction unless there is an existing sidewalk on the property or blockface, or a proposed sidewalk on an adjacent property. Additionally, the dedication of right-of-way, if necessary, will be required for any addition or any renovation equal to or greater than twenty-five percent of the assessed value of all structures on the lot.

This ordinance also adds the requirement for the Planning Commission to make recommendations to the Board of Zoning Appeals (BZA) on variances, increases the number of pedestrian benefit zones from eleven to sixteen and requires that improvements in the public right-of-way with an existing or planned sidewalk identified in the Strategic Plan for Sidewalks and Bikeways be graded in order to accommodate present or future sidewalk construction.

STAKEHOLDER AND COMMUNITY OUTREACH

The applicant has held a series of meetings since August 2016. These meetings have included the Policy SubCommittee of the WalknBike Strategic Plan, the full WalknBike Steering Committee, Homebuilders, Chamber South Leadership Team, Chamber East, Mayor's Bicycle & Pedestrian Advocacy Committee (BPAC), Alliance For Green Hills, Metro Stakeholders (Codes, Planning, Water Services/Stormwater, Public Works and Public Property Services), Chamber North, Active Transportation Working Group of the Healthy Nashville Leadership Council and Walk Bike Nashville.

ANALYSIS

NashvilleNext provides the following guidance:

Element: Land Use, Transportation & Infrastructure

Goal: LUTI 7

Nashville residents have safe, meaningful transportation choices within their neighborhoods for commuting to work, meeting daily needs, and getting to all of the places Nashvillians want to go throughout the county.

Action: Review Metro and State sidewalks standards and adjust design to correspond with the city's adopted Major and Collector Street Plan design guidelines which carry out the Mayor's Complete Streets Executive Order.

Element: Natural Resources and Hazard Adaptation

Goal: NR 1

Nashville invests in and increases its natural environment for beauty, biodiversity, recreation, food production, resiliency, and response to climate change through mitigation and adaptation strategies.



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Policy: NR 1.5

Invest in robust and diversified infrastructure including transportation choices which prioritize the maintenance of existing streets, expansion of mass transit service, and the creation of more walking and biking options in order to reduce sprawling development patterns, improve air and water quality, and preserve existing open spaces in Nashville.

Action: Reexamine existing policies related to infrastructure improvements with residential infill redevelopment and new development to determine appropriate policies, methods, and design standards to ensure the development of sidewalks, greenways, and bikeways as development occurs.

Planning initiatives led by Metro Transit Authority (MTA) and Public Works, nMotion and WalknBike respectively, detail and underscore the importance of safe pedestrian facilities and their connection to improved transit systems and growth in identified centers. This ordinance requires sidewalks when there are significant investments in appropriate geographic areas. It also coordinates with public investments and the Capital Improvements Budget process by making exceptions for required sidewalk installation where Public Works has funded and scheduled a sidewalk improvement project. It also requires that improvements such as driveways are graded appropriately for planned sidewalks identified in the Strategic Plan for Sidewalks and Bikeways. Further, the addition of five new pedestrian benefit zones will allow in lieu contributions to be spent closer to the project. Lastly, it allows the Planning Department to work with developers on alternative sidewalk designs through the BZA variance process.

ZONING ADMINISTRATOR RECOMMENDATION

Approve.

STAFF RECOMMENDATION

Staff recommends approval of the third substitute.

SECOND THIRD SUBSTITUTE ORDINANCE NO. BL2016-493 with proposed revisions shown in red

An ordinance amending Chapters 17.04, 17.20 and 17.40 of the Metropolitan Code pertaining to sidewalks (Proposal No. 2016Z-024TX-001).

NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:

Section 1. Section 17.20.120 (Provision of sidewalks) is hereby amended by deleting the section in its entirety and replacing with the following:



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Sidewalks are required to facilitate safe and convenient pedestrian movements for the residents, employees and/or patrons, and to reduce dependency on the automobile, thus reducing traffic congestion on the community's streets and protecting air quality. This article shall not decrease the allowable floor area ratio for development.

A. Applicability.

1. Multi-family or nonresidential redevelopment. ~~The~~All provisions of this section shall apply to the redevelopment of multi-family or nonresidential property when the property is located within the Urban Services District, ~~is on a street in the Major and Collector Street plan, and/or~~ or within a center designated in the general plan, or any of the property frontage is within a quarter mile of the boundary of a center designated in the general plan, or the property is on a street in the Major and Collector Street plan. Properties on the opposite side of navigable waterways or controlled access highways from a center designated in the general plan are not subject to this provision. Redevelopment of multi-family or nonresidential property shall include one or more of the following:
 - a. Construction of a new structure on a vacant lot, including lots on which all structures have been or are planned to be demolished; or
 - b. The cost value of any one renovation equal to or greater than fifty percent of the assessed value of all structures on the lot, or the value of multiple renovations during any five-year period equal to or greater than seventy five percent of the assessed value of all structures on the lot; or
 - c. The cost value of any one expansion equal to or greater than twenty-five percent of the assessed value of all structures on the lot, or the value of multiple expansions during any five-year period equal to or greater than fifty percent of the assessed value of all structures on the lot; or
 - d. The total building square footage of any one expansion is equal to or greater than twenty-five percent of the total square footage of all structures on the lot, or the total building square footage of multiple expansions during any five-year period is equal to or greater than fifty percent of the total square footage of all structures on the lot.

B.

2. Single-family or two-family construction. Single-family or two-family construction when the property is within the Urban Zoning Overlay, or within a center designated in the general plan, or any of the property frontage is within a quarter mile of the boundary of a center designated in the general plan, or the property is on a street in the Major and Collector Street plan in the Urban Services District. Properties on the opposite side of navigable waterways or controlled access highways from a center designated in the general plan are not subject to this provision.
 - a. ~~The~~All provisions of ~~this s~~Section 17.20.120 shall apply to the construction of a new one single-family or attached or detached two-family structure(s) when the property is within the Urban Zoning Overlay, is on a street in the Major and Collector Street plan in the Urban Services District and/or is within a quarter mile of a center designated in the general plan.



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- b. Dedication of Right-of-Way and Easements required by subsection E of this section shall apply to all single-family and two-family permits for an addition or any renovation **with a cost** equal to or greater than twenty-five percent of the assessed value of all structures on the lot.

~~C.B~~ On-Site Sidewalk Installation For Multi-Family and Nonresidential Development. A continuous, all-weather internal sidewalk network, constructed to a minimum width of five feet shall connect all pedestrian building entryways to parking areas and all public rights of way. Sidewalks shall be designed and constructed to be distinguishable from driving surfaces.

~~C.D~~. Public Sidewalk Installation. The provisions of this subsection apply to all property frontage, regardless of whether sidewalks are provided in public right-of-way or pedestrian easements. ~~are applicable only to the property frontage, with street right-of-way abutting the property for which a building permit is applied.~~

1. Construction of new sidewalks is required along the entire property frontage under any one or more of the following conditions, unless the property abuts a sidewalk segment that the Department of Public Works has funded and scheduled for construction:
 - a. **When there is e**Existing sidewalk **in need of** repair or replacement.
 - b. To extend the existing sidewalk or sidewalk proposed by an adjacent development.
 - c. Existing sidewalk present on the same block face.
 - d. ~~W~~Multi-family or nonresidential properties within the Urban Zoning Overlay.
 - e. Multi-family or nonresidential properties aAlong a street in the Major and Collector Street Plan.
2. Sidewalk Design Standards.
 - a. Sidewalks dimensions and required elements shall comply with the Major and Collector Street Plan or, for a street not in the Major and Collector Street Plan, the adopted standards of the Metropolitan Government. Design of sidewalks shall comply with approved Public Works' standards.
 - b. ~~However, upon recommendation from the Planning Commission, the director of the Public Works Department may approve an alternative material, design, or method of construction from that required by this section if the director of the Public Works Department determines that the alternative is safe, durable, and equivalent to the requirements set out in this section. Materials, designs, or methods of construction approved under this subsection must be used and installed in accordance with the terms of approval.~~
 - be. Obstructions are prohibited within the required pedestrian travelway, but may be located within a grass strip/green zone or frontage zone. Prior to the issuance of use and occupancy permits, existing obstructions shall be relocated outside of the required pedestrian travelway.



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DE. Contribution to the ~~pedestrian network~~ fund for the pedestrian benefit zone as an alternative to sidewalk installation.

1. When a public sidewalk is required by subsection A ~~or B~~, but ~~construction~~ installation is not required by subsection C ~~D~~ of this section, the building permit applicant may make a financial contribution to the ~~Metropolitan Government~~ fund for the pedestrian benefit zone in lieu of construction.
 - a. Multi-family or nonresidential development. The value of the contribution shall be the average linear foot sidewalk project cost, excluding repair projects, determined ~~on an annual basis~~ by July 1 of each year by the Department of Public Works' review of sidewalk projects contracted for or constructed by the Metropolitan Government, ~~or the average of three bids for the construction of the sidewalk completed by licensed contractors, whichever is less.~~
 - b. Single-family or two-family construction. The value of the contribution shall be the average linear foot sidewalk project cost, including repair projects, determined by July 1 of each year by the Department of Public Works' review of sidewalk projects contracted for or constructed by the Metropolitan Government.
2. Any such contributions received by the Metropolitan Government shall be assigned and designated for implementation of the Strategic Plan for Sidewalks and Bikeways, as approved by the Planning Commission. The applicant's payment shall be allocated within ~~twenty-four months~~ ten years of receipt of the payment within the same pedestrian benefit zone as the property to be developed; otherwise, the payment shall be refunded to the building permit applicant.
3. Contribution to the pedestrian network as an alternative to sidewalk installation required under this section shall be received by the Department of Public Works and written confirmation of the contribution sent to the Department of Codes Administration prior to the issuance of a building permit.

EF. Dedication of Right-of-Way and Easements Required. Dedication of right-of-way and/or public pedestrian easement is required ~~under the following conditions:~~

- ~~1. When construction of a public sidewalk is required by this section, right of way or public pedestrian easements needed to build the sidewalk to the current standards of the Metropolitan Government shall be dedicated.~~
2. When a building permit applicant makes a financial contribution to the ~~Metropolitan Government in lieu of sidewalk construction, right of way or public pedestrian easements needed to permit present or future installation of a public sidewalk built to the current standards of the Metropolitan Government shall be dedicated.~~ For pProperties abutting an existing sidewalk or planned sidewalk identified in the Priority Sidewalk Network in the Strategic Plan for Sidewalks and Bikeways, all driveways, walkways and other improvements within public right-of-way or pedestrian easement shall be designed and graded in accordance with Public Works' design standards necessary to accommodate future sidewalk construction.



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FG. Improvements required or elected on public rights-of-way and/or public pedestrian easements under subsection CD of this section shall be reviewed for compliance by the Department of Public Works. No building permit shall be issued by the Department of Codes Administration until the Department of Public Works has released the building permit. Prior to the Department of Codes Administration authorizing final use and occupancy, the Department of Public Works shall inspect and approve the sidewalk improvements in the public rights-of-way and/or public pedestrian easements.

Section 2. Section 17.20.125 (Right to appeal and seek variances) is hereby amended by deleting the section in its entirety and replacing with the following:

The provisions of Section 17.20.120 may be varied or interpretations appealed in conformance with Chapter 17.40, Administration and Procedures. The Board of Zoning Appeals may require a contribution to the pedestrian network, consistent with subsection DE of this section, an alternative sidewalk design, or other mitigation for the loss of the public improvement as a condition to a variance.

Section 3. Section 17.40.340 (Limits to jurisdiction) is hereby amended by deleting subsection B in its entirety and replacing with the following subsection B:

B. The board shall not grant variances within the following sections, tables, zoning districts, or overlay districts without first considering a recommendation from the Planning Commission.

Sections/Tables

Section 17.20.120 (Provision of sidewalks)

Section 17.28.103 (Underground utilities)

Zoning Districts

SP District

Overlay Districts

PUD

UDO

Institutional

Section 4. Section 17.04.060 (Definitions of General Terms) is hereby amended by adding the following definitions in alphabetical order:

“Pedestrian benefit zones” means the ~~eleven~~ sixteen zones in which contributions fees in lieu of sidewalk construction may be collected, and where such contributions fees shall be spent for the safety and convenience of pedestrians. Pedestrian benefit zones are as follows:

Zone 1: Bounded by I-40 on the south; I-65 on the southeast; Cumberland River on the north; State Route 155 on the west. (North Nashville, Metro Center, Nations areas)



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Zone 2: Bounded by Cumberland River and I-65 on the south; I-24 on the east; State Route 155 on the north and west. (Bordeaux area)

Zone 3: Bounded by Cumberland River on the south and east; State Route 155 on the north; I-24 on the east. (East Nashville, Parkwood areas)

Zone 4: Bounded by I-40 on the south; Stones River on the east; the Cumberland River on the north; I-24 on the west. (Spence Lane, Donelson areas)

Zone 5: Bounded by I-440, I-24, and State Route 155 on the south and east; I-40 on the north; I-65 on the west. (South Nashville, North Mill Creek areas)

Zone 6: Bounded by I-440 on the west and south; I-65 on the east; I-40 on the north. (Midtown area)

Zone 7: Bounded by the downtown interstate loop. (Downtown area)

Zone 8: Bounded by the county line on the south; I-65 on the east; I-440 and I-40 on the north; State Route 251, State Route 100, and Harpeth River on the west. (West Nashville, Green Hills areas)

Zone 9: Bounded by the county line on the west and south; Harpeth River, State Route 100, and State Route 251 on the east; I-40 on the north. (Bellevue, Pasquo, Harpeth River areas)

Zone 10: Bounded by I-40 on the south; State Route 155 on the east; Cumberland River on the north; county line on the west. (Newsom Station, Whites Bend, Cockrill Bend areas)

Zone 11: Bounded by Cumberland River and State Route 155 on the south; I-24 on the east and north; county line on the west. (Joelton, Beaman Park, Bells Bend areas)

Zone 12: Bounded by State Route 155 on the south; I-65 on the east; county line on the north; and I-24 on the west. (Paradise Ridge, Union Hill, Goodlettsville areas)

Zone 13: Bounded by State Route 155 and Cumberland River on the south and east; county line on the north; I-65 on the west. (Madison, Neelys Bend, Rivergate areas)

Zone 14: Bounded by I-40 on the south; county line on the east; Old Hickory Lake and Cumberland River on the north and west; Stones River on the southwest. (Old Hickory, Lakewood, Hermitage areas)

Zone 15: Bounded by I-24 on the southwest; county line on the south and east; I-40 on the north; State Route 155 on the northwest. (Antioch, Priest Lake areas)

Zone 16: Bounded by county line on the south; I-24 on the east; I-440 on the north; I-65 on the west. (Grassmere, Southeast Nashville, Cane Ridge areas)



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~~Zone 1-A: Bounded by I-40 and I-65 on the southeast; Cumberland River on north/northwest; county line on west. (West, edge)~~

~~Zone 1-B: Bounded by Cumberland River and I-65 on the south; I-24 on the east and north; county line on the west. (Northwest, edge)~~

~~Zone 1-C: Bounded by I-24 on the west; I-65 on the southeast and east; county line on the north. (North, edge)~~

~~Zone 2-A: Bounded by I-65 on the northwest; I-24 on the west; Cumberland River on the south and east; county line on the northeast. (Northeast, edge)~~

~~Zone 2-B: Bounded by Cumberland River on the north/northwest; I-40 on the south/southwest; county line on the east. (East, edge)~~

~~Zone 3: Bounded by the downtown loop. (Downtown)~~

~~Zone 4-A: Bounded by I-65 on the east/northeast; I-440 on the south and southwest; I-40 on the north and northeast. (Southwest, inner)~~

~~Zone 4-B: Bounded by I-65 on the east; I-440 on the north/northeast; I-40 on the northwest; county line on the south. (Southwest, edge)~~

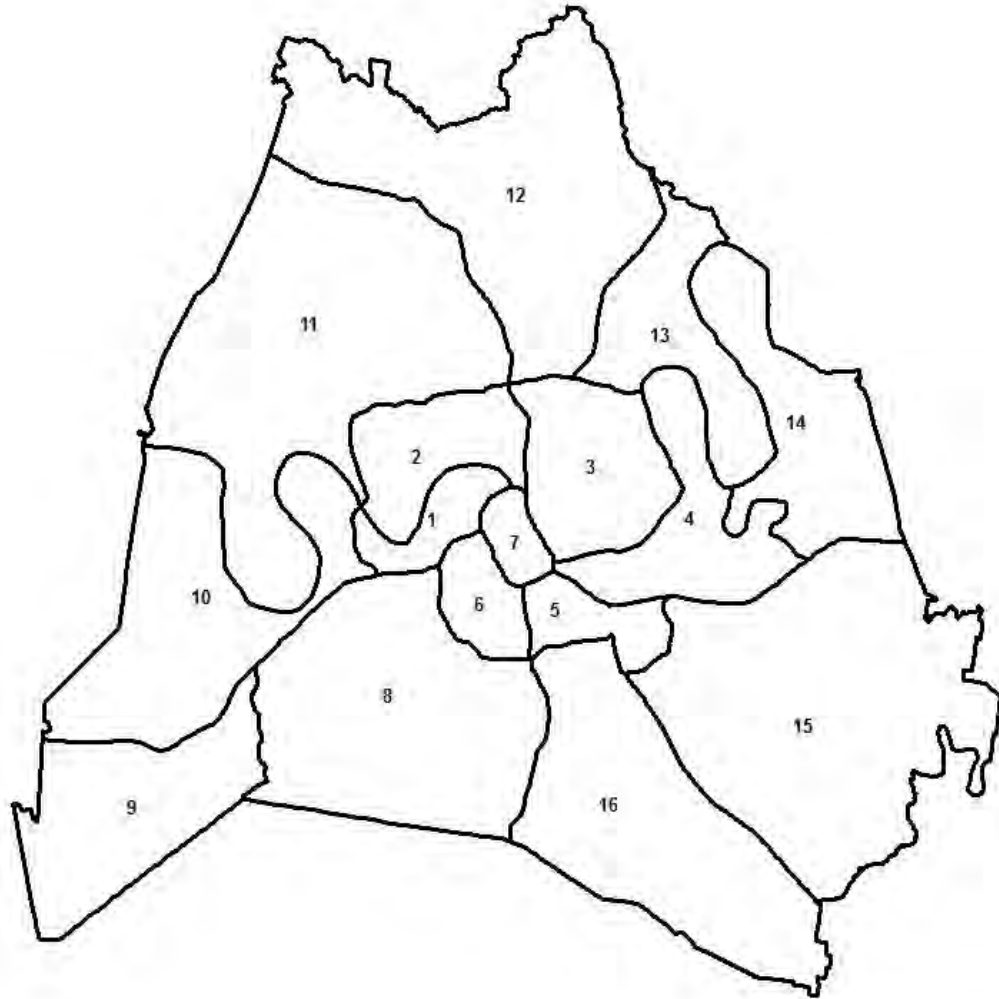
~~Zone 5-A: Bounded by I-440 on the south; I-24 on the northeast and east; I-40 on the north/northwest; and I-65 on the west. (South, inner)~~

~~Zone 5-B: Bounded by I-24 on the east/northeast; I-65 on the west; I-440 on the north; and county line on the south. (South, edge)~~

~~Zone 5-C: Bounded by I-40 on the north; I-24 on the west and southwest; county line on the east. (Southeast, edge)~~



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“Sidewalk” means all Streetside Elements included in the Major and Collector Street Plan and Streetside Elements for local streets required by other standards of the Metropolitan Government located within the public right-of-way or a pedestrian easement.

“On-site sidewalk” means pedestrian facilities located outside of the public right-of-way.

Section 5. Section 17.20.060 (Parking area design standards) is hereby amended by deleting subsection H in its entirety and replacing with the following:

H. Curbs. Curbs or other equivalent means shall be provided to prevent any vehicle using a parking area from encroaching on any public right-of-way, on-site sidewalk, required landscaping area or adjacent property.

Section 65. Be it further enacted, that this ordinance take effect immediately after its passage July 1, 2017, and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.

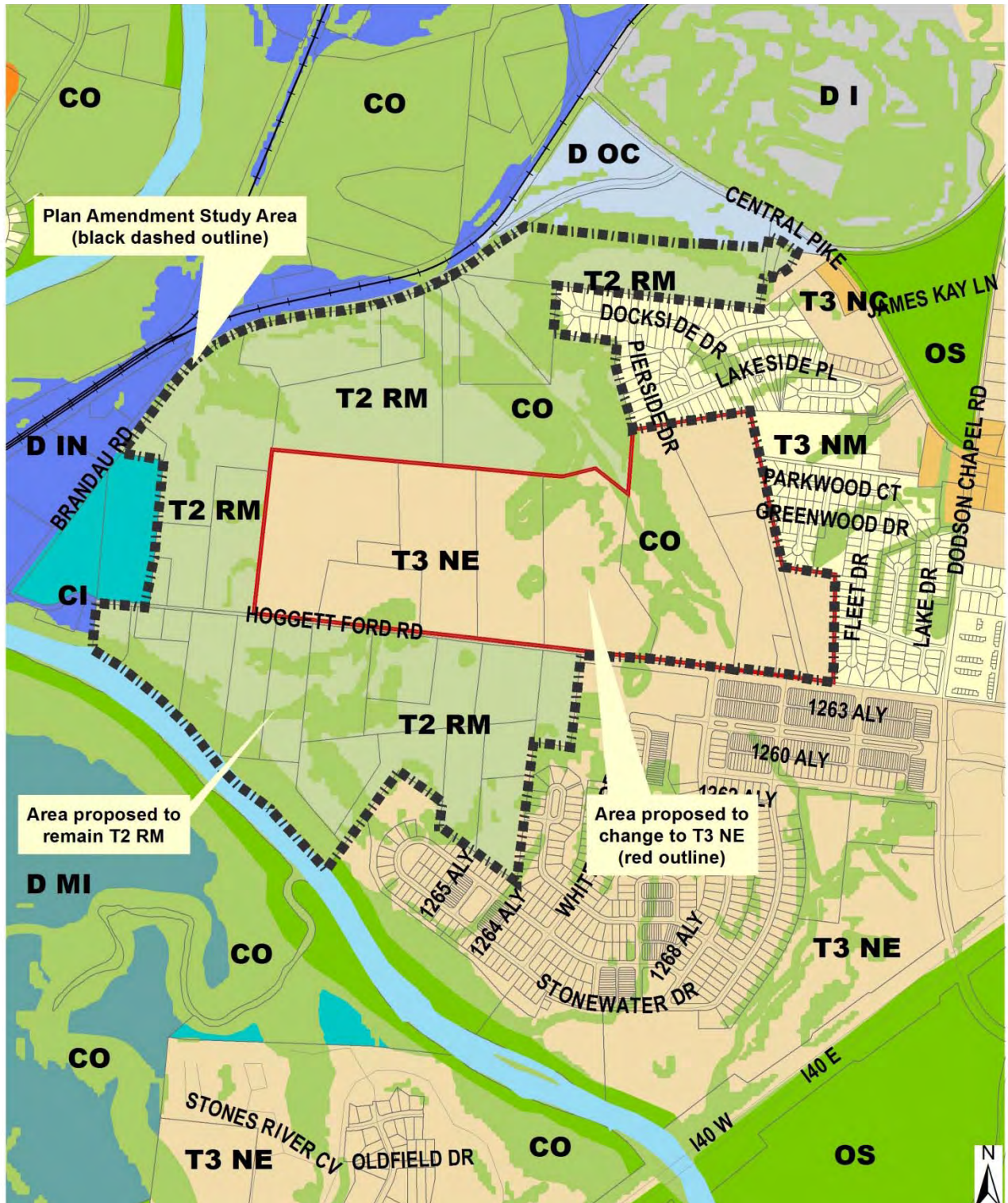


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Sponsored by: Angie Henderson, Freddie O'Connell, Russ Pulley, Jeremy Elrod, Burkley Allen, Bob Mendes, Nancy VanReece, Dave Rosenberg, Brett Withers, Karen Johnson, Jacobia Dowell, Mary Carolyn Roberts, Anthony Davis, Kevin Rhoten, Colby Sledge, Jeff Syracuse, Jim Shulman, Sharon Hurt, Kathleen Murphy, Tanaka Vercher



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2016CP-014-001
 DONELSON-HERMITAGE-OLD HICKORY COMMUNITY PLAN AMENDMENT
 Map 086, Parcel(s) 126,127, 751
 Hermitage – Hessey Farm along Hoggett Ford Road
 14 (Kevin Rhoten)



Project No.	Major Plan Amendment 2016CP-014-001
Project Name	Donelson-Hermitage-Old Hickory Community Plan Amendment
Associated Case	2016SP-021-001
Council District	14 – Rhoten
School District	4 – Shepherd
Requested by	Gresham, Smith and Partners, applicant; D.M. Hessey, owner.
 Deferrals	 This item was deferred at the March 9, 2017, Planning Commission meeting. The public hearing was held and closed.
 Staff Reviewer	 McCaig
Staff Recommendation	<i>Approve.</i>

APPLICANT REQUEST

Amend Donelson-Hermitage-Old Hickory Community Plan to change the policy.

Major Plan Amendment

A request to amend the Donelson-Hermitage-Old Hickory Community Plan by amending the Community Character policy from Conservation and T2 Rural Maintenance policy to Conservation and T3 Suburban Neighborhood Evolving policy for 3605 Hoggett Ford Road and Hoggett Ford Road (unnumbered) (approximately 120 acres).

DONELSON-HERMITAGE-OLD HICKORY COMMUNITY PLAN

Current Policies

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands, and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.

T2 Rural Maintenance (T2 RM) is intended to preserve rural character as a permanent choice for living within Davidson County and not as a holding or transitional zone for future urban development. T2 RM areas have established low-density residential, agricultural, and institutional development patterns. Although there may be areas with sewer service or that are zoned or developed for higher densities than is generally appropriate for rural areas, the intent is for sewer services or higher density zoning or development not to be expanded. Instead, new development in T2 RM areas should be through the use of a Conservation Subdivision at a maximum gross density of 1 dwelling unit per 2 acres with individual lots no smaller than the existing zoning and a significant amount of permanently preserved open space.



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Proposed Policy (Note: the CO policy is proposed to remain.)

T3 Suburban Neighborhood Evolving (T3 NE) is intended to create and enhance suburban residential neighborhoods with more housing choices, improved pedestrian, bicycle, and vehicular connectivity, and moderate density development patterns with moderate setbacks and spacing between buildings. T3 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network, block structure, and proximity to centers and corridors. T3 NE areas are developed with creative thinking in environmentally sensitive building and site development techniques to balance the increased growth and density with its impact on area streams and rivers.

BACKGROUND

The community plan amendment was requested in conjunction with zone change application 2016SP-021-001, which is a request to change the zoning from RS15 to SP-R for property along Hoggett Ford Road, also referred to as the Hessey Farm.

The property has historically been used as residential and a farm with some stands of trees. The applicant began consultation with staff on the property regarding a rezoning and plan amendment in 2015. The applicant applied for the plan amendment and associated rezoning in January 2016. However, both cases were deferred to continue working with the community and area Councilmember on design ideas and addressing concerns. The applicant’s initial consultations with staff predated the Commission’s adoption of amendments on April 28, 2016 to the Subdivision Regulations to include guidance for Rural neighborhood policy areas.

In the 2004 update to the Donelson-Hermitage-Old Hickory Community Plan, low density residential policy was applied to this area along Hoggett Ford Road. A portion of the area along Hoggett Ford Road and Brandau Road lacks access to sewer. In 2013, staff “translated” the older land use policies into the newer Community Character Manual policies. Land with low density residential uses that did not have sewer access at that time was placed in Rural Maintenance policy.

Also in 2004, after a lengthy debate, the zoning was changed to RM9 and MUN, along with an Urban Design Overlay, on the property in the adjacent area to the southeast, known as Browns Farm (approximately 220 acres), to allow for the suburban Villages of Riverwood. Many of the rural neighbors were not in support of that zone change as they stated it would have negative impacts on the adjacent rural area. Today, Villages of Riverwood contains over 700 lots for single-family homes and townhomes and area for multi-family units that have not been built at this time.

There are competing visions in this area. Donelson and Hermitage are predominantly suburban in character. On the one hand, there is a small pocket of properties that want to remain rural but are surrounded by suburban and industrial uses. The existing zoning for these properties is suburban, RS15, and has been in place since the 1998 update to the zoning code. Prior to 1998, the 1974 zoning code update shows R15 zoning. On the other hand, new suburban developments have been built in the adjacent Suburban policy area, directly abutting the Rural policy area. A few property owners within the Rural policy area have also expressed interest in developing their property with



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additional residential uses. One such owner is the applicant for the subject plan amendment and rezoning requests.

COMMUNITY PARTICIPATION

Combined community meeting and public hearing notices were mailed out in late August 2016 to property owners within 1,300 feet of the amendment area. In total, notices were mailed to approximately 900 surrounding property owners. Notice information was also placed on the department's webpage.

On September 13, 2016, Planning Department staff held a community meeting at the Hermitage Police Precinct to discuss the applicant's proposal. It was attended by approximately 45 people, including the area Councilmember, members of the development team, and Planning staff.

The attendees asked numerous questions about the details of the proposed development. Most of the questions were directed to the applicant regarding the details of the proposed project. In addition to questions regarding the application review process, the following questions and issues were discussed:

- Impact of the proposed approximately 375 houses on neighborhood streets. Traffic volume and congestion are already concerns in the neighborhood with Dodson Chapel Road, Central Pike, and Bell Road.
- Concern that Dodson Chapel Road cannot handle any more volume of vehicles due to existing congestion.
- Concern that the required street improvements to Hoggett Ford Road would connect Hoggett Ford to Brandau Road and create more traffic on both streets.
- Concern regarding further loss of rural property and the rural character of the area.
- Concern that the number of houses is too much to add to the neighborhood and that the total number of houses should be reduced.
- Concern about some of the housing types, especially townhomes, due to community issues with building design in the adjacent Villages of Riverwood development.
- Concern that such a large-scale development would harm area wildlife.
- Concern that improving Hoggett Ford Road would mean harm to family cemeteries that are located along what is currently a rural lane.
- Support for extending Hoggett Ford Road to connect with Brandau Road to provide more connectivity in a currently isolated area.
- Concern about increased burden on area infrastructure, such as water lines and water pressure.
- Request that the applicant provide off-site street lighting and traffic signals if the development proceeds.

Since the September 13, 2016, community meeting, the applicant has continued to work with Councilmember Rhoten and the community. The applicant held an additional community meeting on January 31, 2017, to discuss refinements to the project's design, including a decrease in the total number of homes, and to gather feedback from attendees.



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ANALYSIS

The proposed amendment area is a suitable location for the current T2 Rural Maintenance (T2 RM) policy since it currently lacks access to sewer. However, with infrastructure improvements and appropriate site design, the proposed amendment area becomes a suitable location for T3 Suburban Evolving (T3 NE) policy as the area is surrounded by suburban and industrial areas, and near major transportation corridors. The site is large enough to support a well-designed development while also preserving quality open space areas and sensitive natural features while taking advantage of its location near the Stones River greenway, J. Percy Priest Lake, and area services. The property is near Dodson Chapel Road, an Arterial-Boulevard (T3-R-AB3). The property is also adjacent to the T3 NE policy area for Villages of Riverwood, which is to the southeast.

The applicant has continued to work with staff on addressing design and infrastructure concerns, and their proposal now meets the design principles of the proposed T3 NE policy.

Staff studied the entire T2 RM policy area, approximately 331 acres. Staff's initial approach was that if a policy change is warranted, then the entire T2 RM area would be recommended for change. At the September 2016 community meeting, residents in the Rural policy area realized that their properties were zoned RS15 (suburban character) and not AR2a as they had thought. Some of the surrounding property owners have recently applied to change their zoning to AR2a to demonstrate their commitment to keeping the area rural. Their zone change request is currently under review. As such, staff's recommendation is to change the policy for the Hessey Farm, and four adjacent properties to the east, to T3 Suburban Neighborhood Evolving, while keeping T2 Rural Maintenance policy in the remainder of the study area.

STAFF RECOMMENDATION

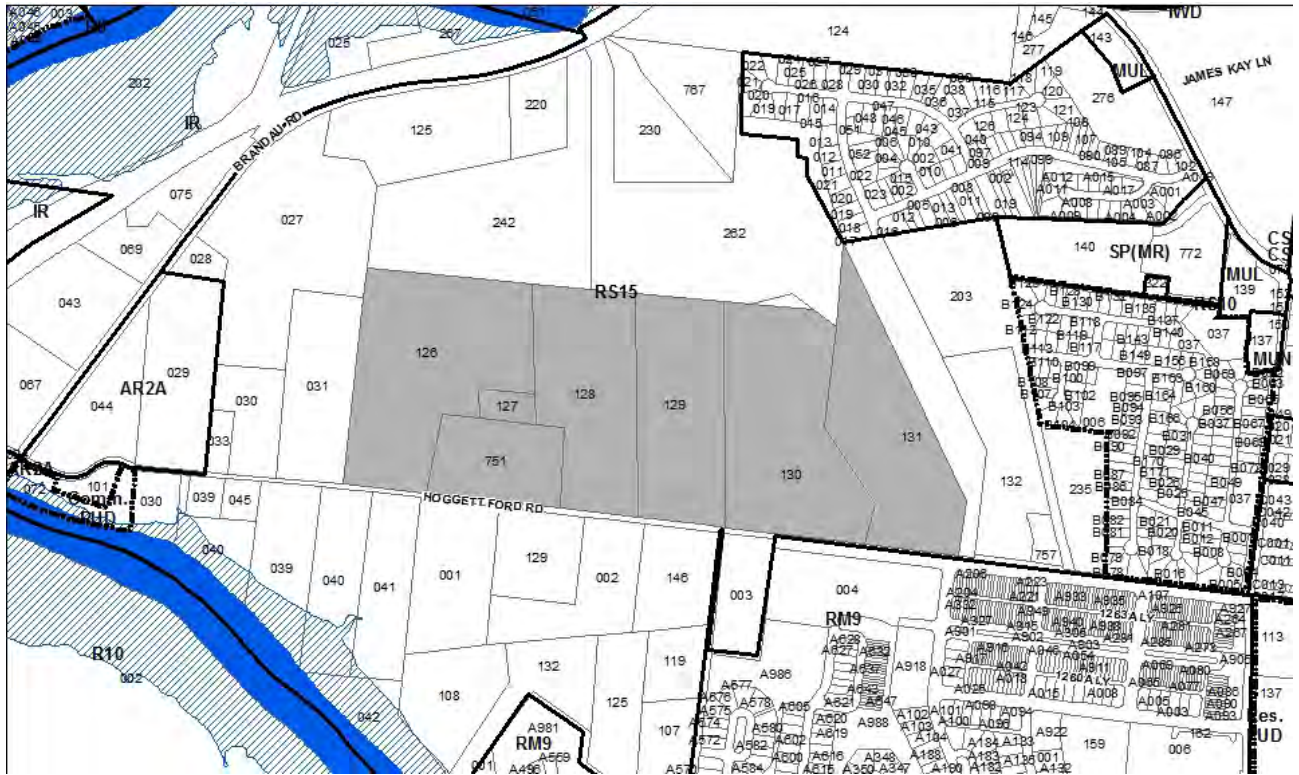
Staff recommends approval.



SEE NEXT PAGE



Metro Planning Commission Meeting of 03/23/2017



2016SP-021-001
HESSEY-HOGGETT FORD SP
Map 086, Parcel(s) 126-130, 751 and part of 131
14, Donelson-Hermitage-Old Hickory
14 (Kevin Rhoten)



Project No. Specific Plan 2016SP-021-001
Project Name Hessey-Hogget Ford SP
Associated Case No. 2016CP-014-001
Council District 14 - Rhoten
School District 04 – Shepherd
Requested by Gresham Smith and Partners, applicant; Daniel M. Hessey, owner.

Deferrals This item was deferred at the March 9, 2017, Planning Commission meeting. The public hearing was held and closed.

Staff Reviewer Birkeland
Staff Recommendation *Approve with conditions and disapprove without all conditions, subject to approval of the associated Community Plan amendment. If the associated Community Plan amendment is not approved, staff recommends disapproval.*

APPLICANT REQUEST

Preliminary SP to permit a residential development.

Preliminary SP

A request to rezone from Single-Family Residential (RS15) to Specific Plan-Residential (SP-R) zoning for properties located at 3605 and 3739 Hoggett Ford Road and Hoggett Ford Road (unnumbered), approximately 1,700 feet east of Brandau Road (87.91 acres), to permit up to 289 dwelling units.

Existing Zoning

Single-Family Residential (RS15) requires a minimum 15,000 square foot lot and is intended for single-family dwellings at a density of 2.47 dwelling units per acre. *RS15 would permit a maximum of 255 units based on the acreage included in the request; however, this property is subject to the Rural Subdivision Regulations. Therefore, the total number of units would vary depending on a proposed subdivision.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only three residential building types.

History

This request was heard at the March 9, 2017, Planning Commission meeting, and the public hearing was held and closed.



Metro Planning Commission Meeting of 03/23/2017



Proposed Plan



Metro Planning Commission Meeting of 03/23/2017

DONELSON –HERMITAGE-OLD HICKORY COMMUNITY PLAN

Existing Policy

T2 Rural Maintenance (T2 RM) is intended to preserve rural character as a permanent choice for living within Davidson County and not as a holding or transitional zone for future urban development. T2 RM areas have established low-density residential, agricultural, and institutional development patterns. Although there may be areas with sewer service or that are zoned or developed for higher densities than is generally appropriate for rural areas, the intent is for sewer services or higher density zoning or development not to be expanded. Instead, new development in T2 RM areas should be through the use of a Conservation Subdivision at a maximum gross density of 1 dwelling unit/2 acres with individual lots no smaller than the existing zoning and a significant amount of permanently preserved open space.

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.

Proposed Policy

T3 Suburban Neighborhood Evolving (T3 NE) is intended to create and enhance suburban residential neighborhoods with more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate density development patterns with moderate setbacks and spacing between buildings. T3 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network, block structure, and proximity to centers and corridors. T3 NE areas are developed with creative thinking in environmentally sensitive building and site development techniques to balance the increased growth and density with its impact on area streams and rivers.

Consistent with Policy?

The proposed SP is not consistent with the existing policy. Therefore, a community plan amendment (2016CP-014-001) has been requested to amend the Donelson-Hermitage-Old Hickory Community Plan by amending the current Community Character policy of Rural Maintenance (T2 RM) to Suburban Neighborhood Evolving (T3 NE) policy.

The T3 NE policy is a residential policy that is intended to create and enhance suburban residential neighborhoods with more housing choices and improved pedestrian, bicycle and vehicular connectivity. The SP includes three housing choices, which will contribute to the diversity of housing in the area. The SP includes a multi-use trail along the south side of Hoggett Ford Road and an interior sidewalk network connecting the residential units to the proposed multi-use trail. The proposed plan will extend Hoggett Ford Road from the eastern Hoggett Ford Road to the western side of Hoggett Ford Road. This connection will provide a vital connection for the existing neighborhoods and the community as a whole.



Metro Planning Commission Meeting of 03/23/2017

PLAN DETAILS

The site is located along Hoggett Ford Road, located east of Brandau Road and west of Dodson Chapel Road. The site is approximately 87.91 acres.

Site Plan

The plan proposes 289 residential units including front loaded single-family lots, alley loaded single-family lots, and villa lots. Each residential unit type includes specific standards for building type location, setbacks, height, architectural standards, character images of building types, etc.

Front loaded single-family lots

A total of 160 front loaded single-family lots are proposed within the development. The lots will be located in three areas of the proposed development: northeastern, middle and eastern end of the site. The maximum height of the single-family units is 3 stories.

Alley loaded single-family lots

A total of 61 alley loaded single-family lots are proposed within the development. Alley loaded lots are located along the eastern side of the site along the Hogget Ford Road extension, providing a transition from the existing residential units to the east. Alley loaded lots are also located along in the middle of the site along the new Hoggett-Ford Road.

Villa lots

A total of 34 villa lots are proposed within the development. Each villa lot contains two attached units for a total of 68 units. The proposed lots are located in the rear of the site and provide an additional housing type in the area.

Below are some of the architectural standards for all unit types:

- Building facades to be comprised of brick, stone, cultured stone, and/or cementitious siding. EFIS, vinyl siding, and untreated wood are not permitted (vinyl accents, vinyl trim, and vinyl soffit are permitted)
- Garages shall not protrude beyond the front facade of the residential structure except for Villa Elevation B
- All homes (single-family detached and villa) shall primarily be brick on all four sides
- Front loaded garages shall be upgraded carriage-style garage doors
- Corner units shall address both streets

Vehicular and Pedestrian Connections and Access

Hogget Ford Road is classified as a local street. A variable width of narrow Hoggett Ford Road right-of-way follows the southern portion of the proposed plan. A rural, one lane road is within most of the right-of-way. There is a gap, however, separating the east and west segments of Hoggett Ford Road. A heavily wooded valley and a spring-fed stream are located within the gap along Hogget Ford Road.

The SP proposes a new road to connect the east and west segments of Hoggett Ford Road. The existing western segment of Hogget Ford Road connects to Brandau Road. The existing eastern segment of Hogget Ford Road connects to Dodson Chapel Road. The new road will be built around the valley and spring-fed stream. By aligning the road around the valley and stream, the SP will avoid impacting the sensitive environmental features in this area.



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The proposed plan provides public streets throughout the site. A stub street connection is provided to the north and two stub connections are provided to the west. One connection to the west includes the extension of Hoggett Ford Road. A multi-use pedestrian/bike trail will be installed along the southern side of Hoggett Ford Road. Another multi-use trail will extend north from Hoggett Ford Road through the middle of the site. Sidewalks are being provided throughout the site. The proposed plan will provide for additional connectivity and pedestrian options.

The site has an extensive existing tree canopy. The proposed plan preserves trees throughout the site, especially along the northeastern side of the site, providing buffers along the property lines. Open space is provided throughout the site with a large open space / amenity area in the middle of the site. The amenity area includes a resident clubhouse, fitness center, and swimming pool.

Analysis

The proposed policy supports a range of housing choices. The surrounding neighborhood consists of one, two and multi-family residential units. The proposed SP will incorporate both single-family lots and villa (duplex) lots. Single-family unit types include front loaded and alley loaded units. Alley loaded lots have been arranged along the eastern portion of Hoggett Ford Road providing a transition into the proposed SP to front loaded single-family units. The proposed villa units are located in the rear of the SP.

The plan will provide an important connection of Hoggett Ford Road from east to west, connecting the existing gap in Hoggett Ford Road. The plan incorporates future connections to adjacent properties to the north and west. The proposal meets the local street standards of a five foot sidewalk and four foot planting strip.

FIRE DEPARTMENT RECOMMENDATION

Approve with conditions

- Fire Code issues will be addressed in the permit phase.

STORMWATER RECOMMENDATION

Approve

WATER SERVICES

Approved with conditions

- Approved as a Preliminary SP only. Public water and sewer construction plans must be submitted and approved prior to Final SP approval. These approved construction plans must match the Final Site Plan/SP plans. The required capacity fees must also be paid prior to Final Site Plan/SP approval.

PUBLIC WORKS RECOMMENDATION

Approve

TRAFFIC AND PARKING RECOMMENDATION

Approved with conditions

- Developer shall design signal plans for Metro traffic engineer approval and install signals at Dodson Chapel Rd and Hoggett Ford Rd intersection and at Dodson Chapel Rd and Bell Rd intersection when signal warrants are satisfied and signal plans are approved. Adequate left



Metro Planning Commission Meeting of 03/23/2017

turn lane storage at intersections shall be provided. Appropriate pedestrian infrastructure shall be included in signal design.

- Traffic signage and pavement marking plans shall be submitted with Final SP documents. Provide parking per metro code.

Maximum Uses in Existing Zoning District: **RS15**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single- Family Residential (210)	87.91	2.9 D	254 U	2452	188	249

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi- Family Residential (210)	87.91	-	289 U	2761	213	279

Traffic changes between maximum: **RS15 and SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 35 U	+309	+25	+30

METRO SCHOOL BOARD REPORT

Projected student generation existing RS15 district: 24 Elementary 20 Middle 18 High

Projected student generation proposed SP-R district: 31 Elementary 26 Middle 24 High

The proposed SP-R zoning district could generate 19 more students than what is typically generated under the existing RS15 zoning district. Students would attend Tulip Grove Elementary School, Dupont-Tyler Middle School, and McGavock High School. All students have been identified as having additional capacity. This information is based upon data from the school board last updated November 2016.

WORKFORCE AND AFFORDABLE HOUSING REPORT (information provided by applicant)

1. Will this project include any affordable or workforce housing units? No
2. If so, how many and what is the percentage of the entire development? N/A
3. How will you enforce the affordability requirements? N/A
4. Have any structures been demolished in the last 12 months? N/A

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions, subject to approval of the associated Community Plan amendment. If the associated Community Plan amendment is not approved, staff recommends disapproval.

CONDITIONS

1. Permitted land uses shall be limited to a maximum of 289 dwelling units, including no more than 34 villa lots (68 units).



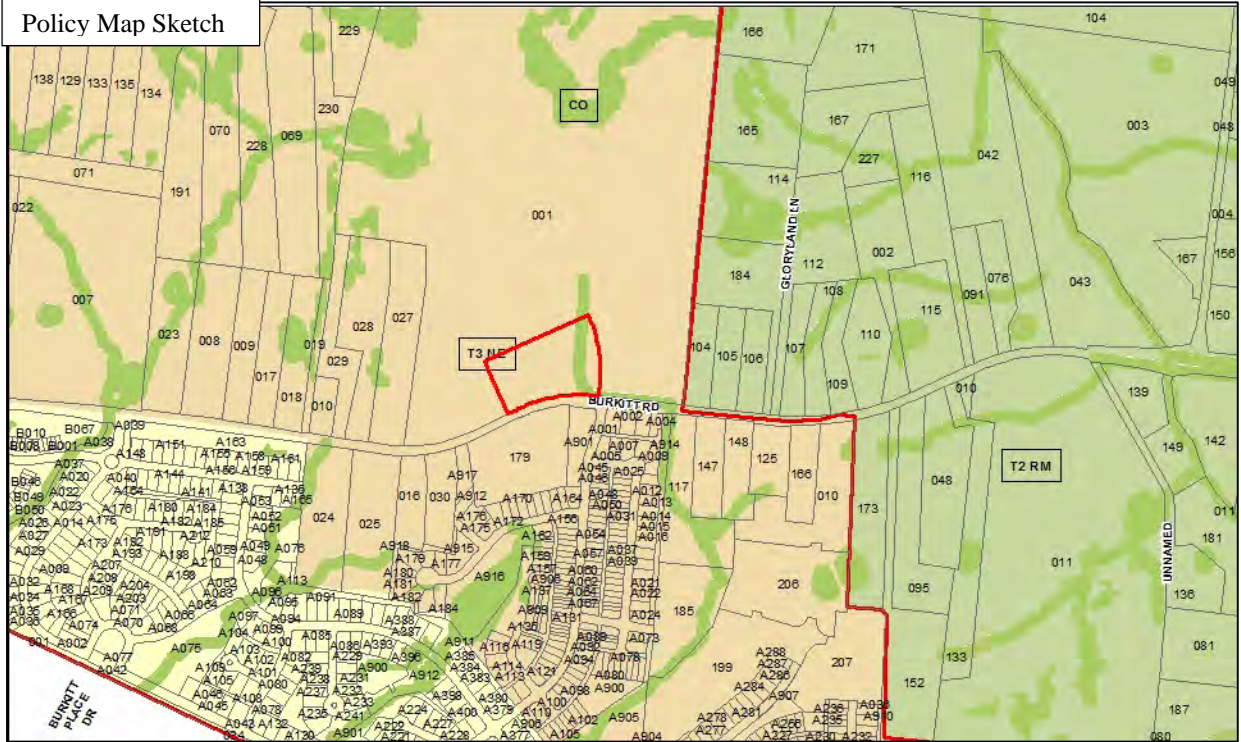
Metro Planning Commission Meeting of 03/23/2017

2. Villa type lots shall be limited to 25% of type "B" units.
3. Raised foundations of a minimum of 18" and a maximum of 36" are required for all residential buildings, except villa units.
4. The extension of Hogget Ford Road shall provide a complete connection to both existing sides of Hoggett Ford Road.
5. Existing residential driveways south along Hoggett Ford Road shall be connected to the proposed Hoggett Ford Road.
6. All sidewalks and trails shall be within right-of-way or a public pedestrian easement.
7. For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations and requirements of R6 zoning district.
8. A corrected copy of the preliminary SP plan incorporating the conditions of approval by the Metro Council shall be provided to the Planning Commission prior to or with the final site plan application.
9. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
10. The final site plan shall label all internal driveways as "Private Driveways". A note shall be added to the final site plan that the driveways shall be maintained by the Homeowner's Association.
11. The final site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.
12. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
13. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



Metro Planning Commission Meeting of 03/23/2017

Policy Map Sketch



2017CP-012-001
SOUTHEAST COMMUNITY PLAN AMENDMENT
Map 187, Part of Parcel(s) 001
12, Southeast
31 (Fabian Bedne)



Project No. Major Plan Amendment 2017CP-012-001
Project Name Southeast Community Plan Amendment
Associated Case 2017SP-023-001
Council District 31 – Bedne
School District 2 – Brannon
Requested by Smith Gee Studio, applicant; Stacy J. Carter, owner.

Deferrals This item was deferred at the March 9, 2017, Planning Commission meeting. No public hearing was held.

Staff Reviewer Sewell
Staff Recommendation *Defer to the April 13, 2017, Planning Commission Meeting*

APPLICANT REQUEST

Amend Southeast Community Plan to change the policy.

Major Plan Amendment

A request to amend the Southeast Community Plan by amending the Community Character Policy to change from T3 Suburban Neighborhood Evolving to T3 Neighborhood Center on property located at Burkitt Road (unnumbered), north of the terminus of Westcott Lane, zoned Agricultural/Residential (AR2A) (approximately 5.10 acres).

STAFF RECOMMENDATION

Staff recommends deferral to the April 13, 2017, Planning Commission meeting at the request of the applicant.



Metro Planning Commission Meeting of 03/23/2017



2017SP-023-001
BURKITT RIDGE
Map 187, Parcel(s) 001
12, Southeast
31 (Fabian Bedne)



Project No. Specific Plan 2017SP-023-001
Project Name Burkitt Ridge SP
Associated Case 2017CP-012-001
Council District 31 – Bedne
School District 2 - Brannon
Requested by Smith Gee Studio, applicant; Stacy Carter, owner.

Deferrals This item was deferred at the March 9, 2017, Planning Commission meeting. No public hearing was held.

Staff Reviewer Shepard
Staff Recommendation *Defer to the April 13, 2017, Planning Commission meeting.*

APPLICANT REQUEST

Permit a mixed use development.

Preliminary SP

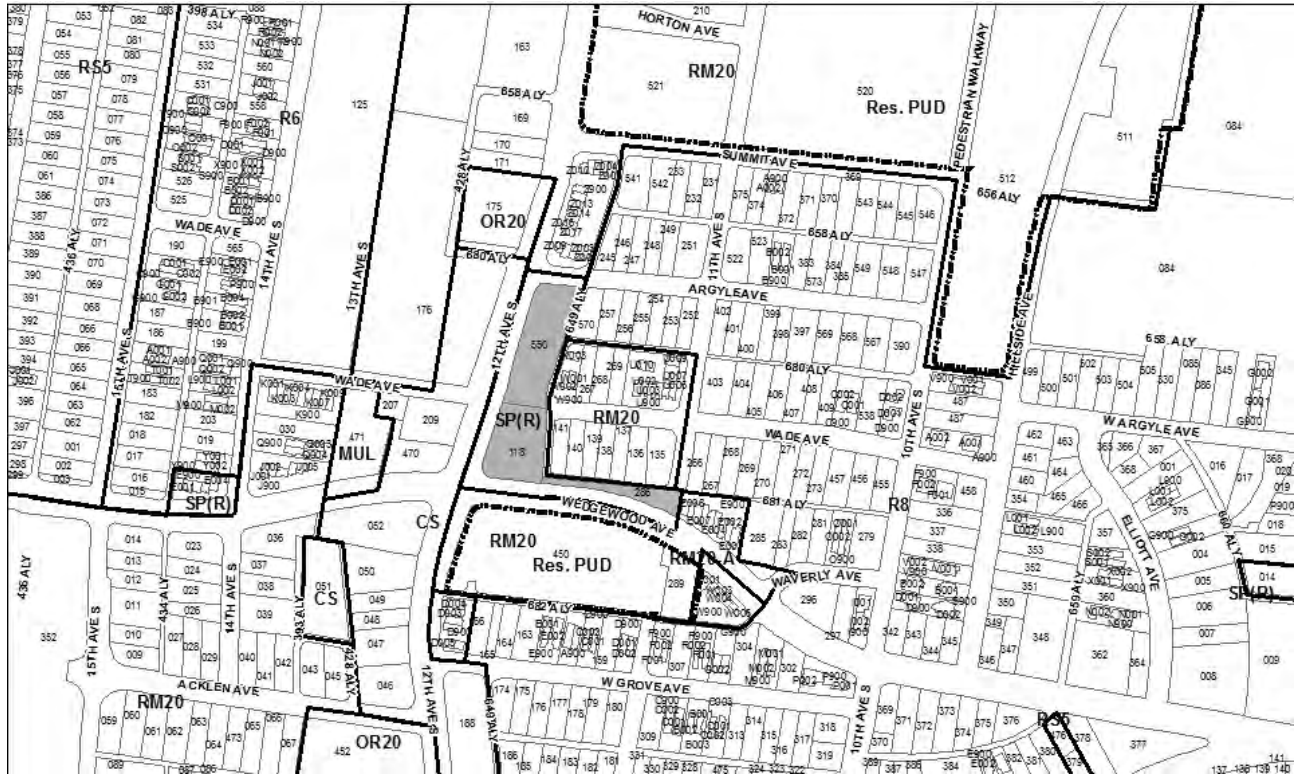
A request to rezone from Agricultural/Residential (AR2a) to Specific Plan – Mixed Use (SP-MU) zoning on property located at Burkitt Road (unnumbered), north of the terminus of Westcott Lane (143.92 acres), to permit a mixed-use development.

STAFF RECOMMENDATION

Staff recommends deferral to the April 13, 2017, Planning Commission meeting at the request of the applicant.



Metro Planning Commission Meeting of 03/23/2017



2016SP-045-003
12TH AND WEDGEWOOD SP
Map 105-05, Parcel(s) 550
Map 105-09, Parcel(s) 118, 286
10, Green Hills - Midtown
17 (Colby Sledge)



Project No. Specific Plan 2016SP-045-003
Project Name 12th & Wedgewood SP (Amendment)
Council District 17 - Sledge
School District 5 - Buggs
Requested by Metro Government, applicant and owner.

Deferrals This item was deferred at the March 23, 2017, Planning Commission meetings. No public hearing was held.

Staff Reviewer Birkeland
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

Amend an SP to permit up to 175 residential units.

SP Amendment

A request to amend a Specific Plan on properties located at 1440 and 1500 12th Avenue South and Wedgewood Avenue (unnumbered), at the northeast corner of Wedgewood Avenue and 12th Avenue South, zoned Specific Plan-Residential (SP-R) (1.83 acres), to increase units from a maximum of 150 to a maximum of 175 residential units.

Existing Zoning

Specific Plan-Residential (SP-R) provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type. The current SP allows a maximum of 150 multi-family units.

Proposed Zoning

Specific Plan-Residential (SP-R) provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type. The proposed SP allows a maximum of 175 multi-family units within the currently approved footprint.

CRITICAL PLANNING GOALS

N/A

GREEN HILLS-MIDTOWN COMMUNITY PLAN

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.



Metro Planning Commission Meeting of 03/23/2017

T4 Urban Neighborhood Evolving (T4 NE) is intended to create and enhance urban residential neighborhoods that provide more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate to high density development patterns with shallow setbacks and minimal spacing between buildings. T4 NE areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. T4 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network and block structure and proximity to centers and corridors.

Consistent with Policy?

Yes. The proposal to increase the permitted units from 150 to 175 within the same footprint is consistent with the policy. The policy encourages the creation of urban residential neighborhoods with a diverse mix of housing types at moderate to high intensities. The location of the site at the intersection of Wedgewood Avenue and 12th Avenue South makes it an appropriate location for multi-family residential development.

HISTORY

In June 2016, the Metro Planning Commission recommended approval of an SP to permit 150 residential dwelling units 1440 and 1500 12th Avenue South and Wedgewood Avenue (unnumbered). The applicant is currently proposing to amend the SP to add 25 more units, for a maximum of 175 residential units.

PLAN DETAILS

The site is located at 1440 and 1500 12th Avenue South and Wedgewood Avenue (unnumbered), at the northeast corner of Wedgewood Avenue and 12th Avenue South. The site is approximately 1.8 acres in size and is currently vacant.

The approved Specific Plan was a regulatory plan. No standards of the approved plan are changing except for the increase in the number of allowed units from 150 to 175. All other standards remain as previously approved. Parking will be addressed with the final site plan application.

ANALYSIS

The proposed development provides for an urban development on an infill site. The increase in units within the same footprint as the approved SP is appropriate given the policy and location along a corridor.

FIRE MARSHAL RECOMMENDATION

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Approve

WATER SERVICES RECOMMENDATION

Approve with conditions



Metro Planning Commission Meeting of 03/23/2017

- Approved as a Preliminary SP Amendment only, on the following three conditions: 1) Public water and sewer construction plans must be submitted and approved prior to Final SP approval. These approved construction plans must match the Final Site Plan/SP plans. 2) The latest availability study must be updated before the Final SP, to reflect the increased unit count. 3) The increased unit count will increase the amount of capacity fees owed. This increase in the required capacity fees must also be paid prior to Final Site Plan/SP approval.

PUBLIC WORKS RECOMMENDATION

N/A

TRAFFIC AND PARKING RECOMMENDATION

Approve with conditions

- Traffic study may be required at time of development.

Maximum Uses in Existing Zoning District: SP-R

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (220)	1.8		150 U	1033	78	101

Maximum Uses in Proposed Zoning District: SP-R

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (220)	1.8		175 U	1185	90	114

Traffic changes between maximum: SP-R and SP-R

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+25 U	+152	+12	+13

METRO SCHOOL BOARD REPORT

Projected student generation existing SP- district: 3 Elementary 2 Middle 1 High

Projected student generation proposed SP-R district: 3 Elementary 2 Middle 1 High

The proposed SP-R zoning is not anticipated to generate no more students than what is typically generated under the existing SP-R zoning district. Students would attend Eakin Elementary School, West End Middle School, and Hillsboro High School. All three schools have been identified as having additional capacity. This information is based upon data from the school board last updated November 2016.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.



Metro Planning Commission Meeting of 03/23/2017

CONDITIONS

1. Permitted land uses shall be limited to up to 175 multi-family residential units. All other uses, including Short Term Rental Property (STRP), shall be prohibited.
2. 40% of the residential units in the Project are both rent-restricted and limited to occupancy by tenants whose income is 60% or less of area median gross income.
3. 80% of the residential units in the Project are both rent-restricted and limited to occupancy by tenants whose income is 100% or less of area median gross income.
4. 100% of the residential units in the Project are both rent-restricted and limited to occupancy by tenants whose income is 120% or less of area median gross income.
5. All standards and conditions of BL2016-299 remain in effect.
6. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM60-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
7. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
8. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
9. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



SEE NEXT PAGE



Metro Planning Commission Meeting of 03/23/2017



2016SP-098-001
Map 082-03, Parcel(s) 085-086
05, East Nashville
05 (Scott Davis)



Project No. Specific Plan 2016SP-098-001
Council District 05 – S. Davis
School District 5 - Buggs
Requested by Councilmember Scott Davis, applicant; various property owners.

Deferrals This item was deferred at the December 8, 2016, January 23, 2017, and February 23, 2017, Planning Commission meetings. No public hearing was held.

Staff Reviewer Birkeland
Staff Recommendation *Disapprove.*

APPLICANT REQUEST
Zone change from SP to SP.

Zone Change

A request to rezone from Specific Plan-Residential (SP-R) to Specific Plan-Residential (SP-R) zoning on properties located at 910 and 912 North 2nd Street, at the northwest corner of Cleveland Street and North 2nd Street (0.32 acres), to permit uses limited to one single-family or one two-family unit per parcel.

Existing Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. *The zoning district allows for all uses permitted by the RS5 zoning district, as well as detached accessory dwelling units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. *The zoning district allows for one single-family or one two – family unit per parcel. Two-family units shall be fully connected and shall appear as one unit with only one primary entrance permitted per unit along North 2nd Street frontage.*

HISTORY

- In June 2016, the Planning Commission recommended disapproval of case number 2016Z-036PR-001 to rezone a large area in East Nashville from SP-R to R6-A.
- In September 2016, the applicant applied for a zone change (2016Z-119PR-001) to rezone SP-R to R6-A on properties located at 123, 125, and 127 Cleveland Street and 904, 906, 908, 910, and 912 north 2nd Street. These properties were included in the area covered by the 2016Z-036PR-001. The Planning Commission recommended disapproval of case 2016Z-119PR-001 on January 26, 2017. The Council Bill was withdrawn on February 7, 2017.



Metro Planning Commission Meeting of 03/23/2017

Begin Proposed Regulatory SP

Cleveland St./N. 2nd St. Specific Plan (SP)

Development Summary	
SP Name	Cleveland St/N. 2 nd St. Specific Plan
SP Number	2016SP-098-001
Council District	5
Map & Parcel	Map 082-03; Parcels 085-086

Site Data Table	
Site Data	0.32
Existing Zoning	SP
Proposed Zoning	SP
Allowable Land Uses	Residential

Proposed Specific Plan (SP) Standards

1. Uses within this SP shall be limited to one single-family or one two-family unit per parcel. Two-family units shall be fully connected and shall appear as one unit with only one primary entrance permitted per unit along North 2nd Street frontage.
2. Access for properties which redevelop shall be limited to alley access only.
3. A raised foundation of 18"- 36" is required for all residential structures.
4. Maximum height shall be limited to two stories in 35'.
5. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
6. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.
7. EIFS, vinyl siding and untreated wood shall be prohibited.
8. Porches shall provide a minimum of six feet of depth.
9. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the R6-A zoning district as of the date of the applicable request or application.
10. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.

End Proposed Regulatory SP



Metro Planning Commission Meeting of 03/23/2017

- The applicant converted case number 2016Z-119PR-001 to 2016SP-098-001. The proposed SP allowed one and two-family residential units on each of the 9 properties. This case was deferred and not heard by the Planning Commission.
- The applicant reduced the SP area from 9 parcels to 2 parcels, which is subject of the current request. The proposed zoning would permit two-family units on each lot.

CRITICAL PLANNING GOALS

N/A

EAST NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Maintenance (T4 NM) is intended to preserve the general character of existing urban residential neighborhoods. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T4 NM areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Consistent with Policy?

No. The area requested for rezoning is located within the T4 Urban Neighborhood Maintenance policy. The policy supports preserving the general character of the existing neighborhood. The proposed zone change would allow up to two units on each lot. While there are some two-family units in the surrounding area, the neighborhood is predominantly single-family dwelling units. The proposed zone change is midblock and may be more appropriate along a corridor or at a corner than within a Neighborhood Maintenance Policy where single family units are predominant.

ANALYSIS

The two properties are located mid-block along North 2nd Street, north of Cleveland Street. The existing zoning allows single-family residential units as well as detached accessory dwelling units, if the lot meets certain criteria. Both lots are eligible for a detached accessory dwelling unit. The neighborhood consists of primarily single-family units with some two-family residential units dispersed throughout.

The proposed zoning would allow a two-family dwelling unit on both properties which is inappropriate in this area considering the existing neighborhood pattern. The Neighborhood Maintenance policy is intended to preserve the general character of the area, which mainly consists of single-family dwelling units. Two-family units may be appropriate along a corridor or at a corner in Neighborhood Maintenance policy. However, the zoning proposal attempts to allow two-family units mid-block within a neighborhood. The existing zoning allows for detached accessory dwelling units, which provides for a mixture of residential units in the neighborhood.

TRAFFIC AND PARKING RECOMMENDATION

Approve with conditions

- A traffic study may be required at the time of development.

No traffic table was prepared as there is no anticipated increase in traffic.



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METRO SCHOOL BOARD REPORT

Projected student generation existing SP district: 0 Elementary 0 Middle 0 High

Projected student generation proposed SP district: 1 Elementary 0 Middle 0 High

The proposed SP zoning district could generate one more student than what is typically generated under the existing SP zoning district. Students would attend Glenn Elementary School, Jere Baxter Middle School and Maplewood High School. All schools have been identified as having additional capacity. This information is based upon data from the school board last updated November 2016.

STAFF RECOMMENDATION

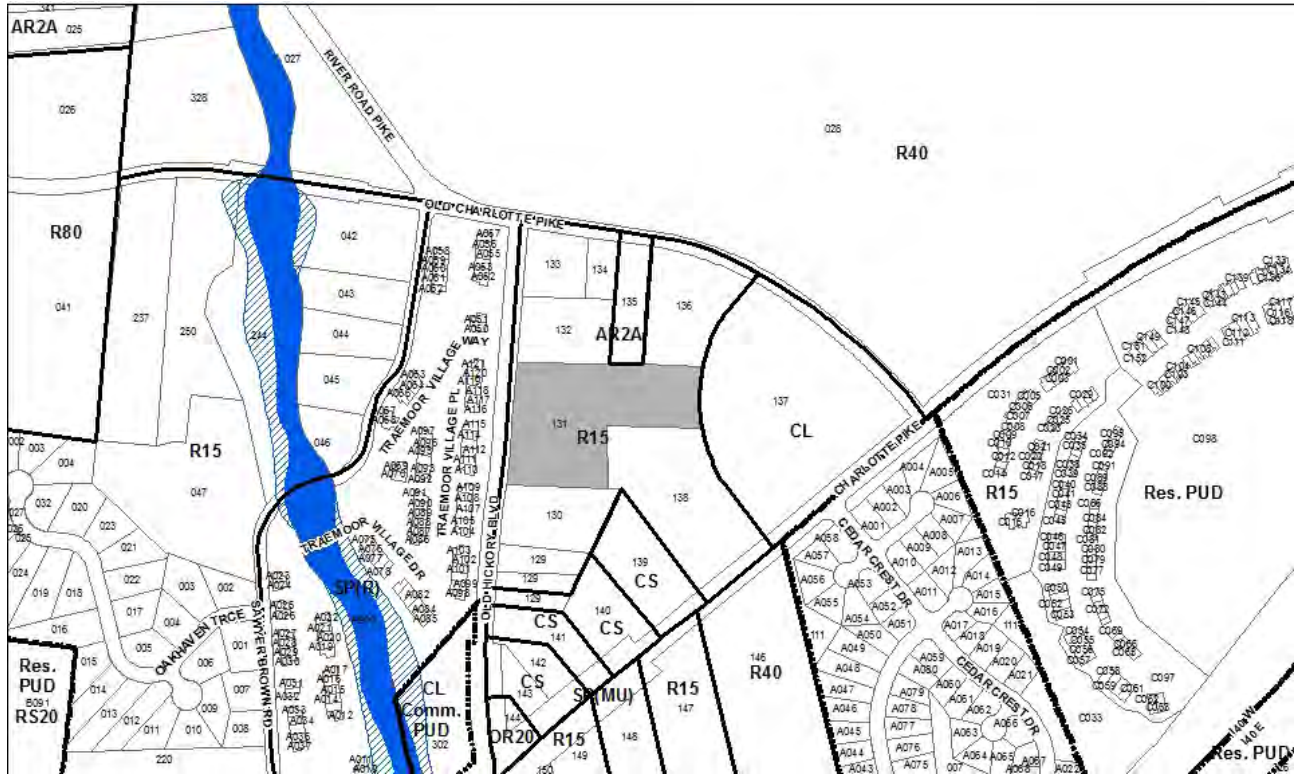
Staff recommends disapproval.



SEE NEXT PAGE



Metro Planning Commission Meeting of 03/23/2017



2017SP-012-001

730 OLD HICKORY BOULEVARD SP

Map 114, Parcel 131

06, Bellevue

22(Sheri Weiner)



Project No. Specific Plan 2017SP-012-001
Project Name 730 Old Hickory Boulevard SP
Council District 22 - Weiner
School District 9 - Frogge
Requested by Dale & Associates, Inc., applicant; Corinne Knight, owner.

Deferrals This item was deferred at the March 9, 2017, Planning Commission meeting. No public hearing was held.

Staff Reviewer Birkeland
Staff Recommendation *Defer to the April 13, 2017, Planning Commission meeting.*

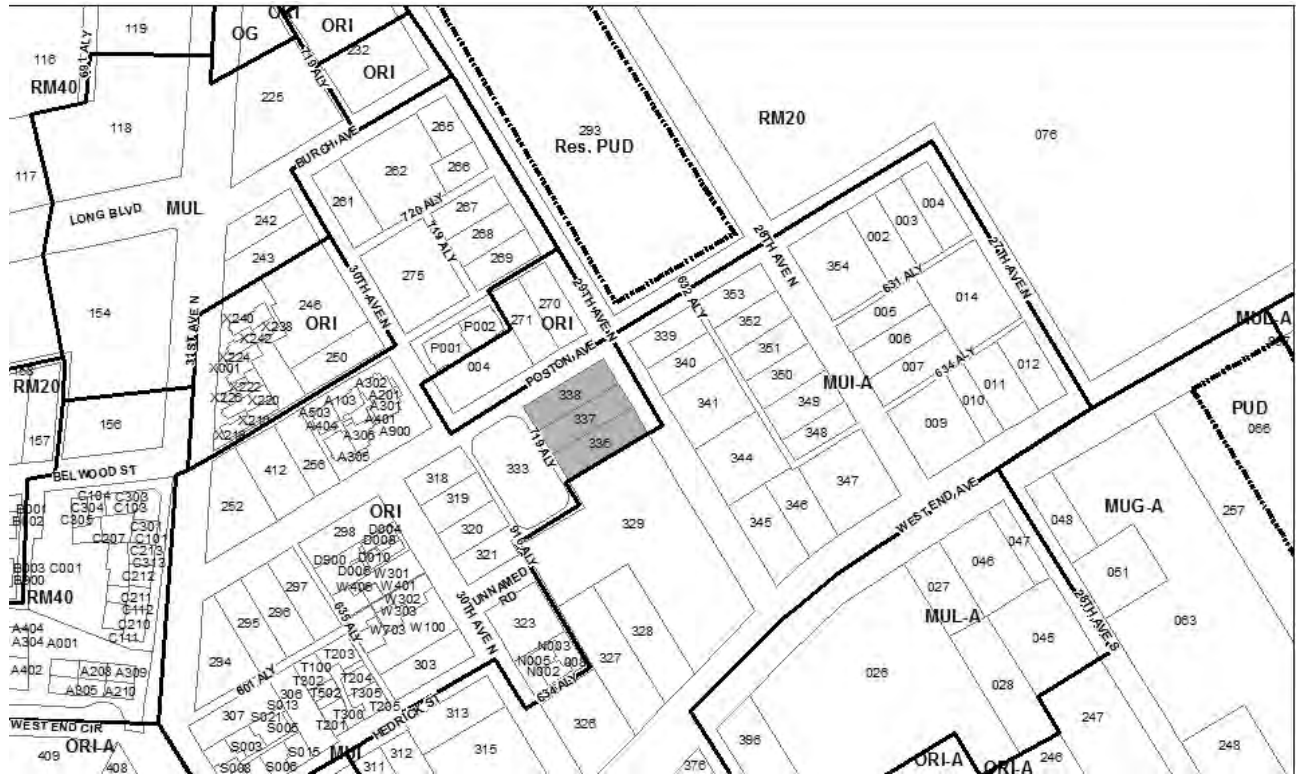
APPLICANT REQUEST
Rezone to SP to allow 53 residential units.

Preliminary SP
A request to rezone from One and Two-Family Residential (R15) to Specific Plan-Residential (SP-R) zoning on property located at 730 Old Hickory Boulevard, approximately 500 feet northwest of Charlotte Pike, (4.07 acres), to permit up to 53 residential units.

STAFF RECOMMENDATION
Staff recommends deferral to the April 13, 2017, Planning Commission meeting at the request of the applicant.



Metro Planning Commission Meeting of 03/23/2017



2017SP-020-001
FAIRFIELD INN AND SUITES
Map 104-02, Parcel(s) 336-338
10, Green Hills – Midtown
21 (Ed Kindall)



Project No. Specific Plan 2017SP-020-001
Project Name Fairfield Inn and Suites
Council District 21 – Kindall
School District 08 – Pierce
Requested by Humphreys and Associates, applicant; Midtown Hotel Partners, LLC, owner.

Deferrals This item was deferred at the March 9, 2017, Planning Commission meeting. No public hearing was held.

Staff Reviewer Shepard
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST
Permit a hotel development.

Preliminary SP

A request to rezone from Office Residential Intensive (ORI) to Specific Plan – Commercial (SP-C) on properties located at 109, 111 and 113 29th Avenue North, at the southwest corner of Poston Avenue and 29th Avenue North, located within the 31st Avenue and Long Boulevard Urban Design Overlay District (0.49 acres), to permit a hotel.

Existing Zoning

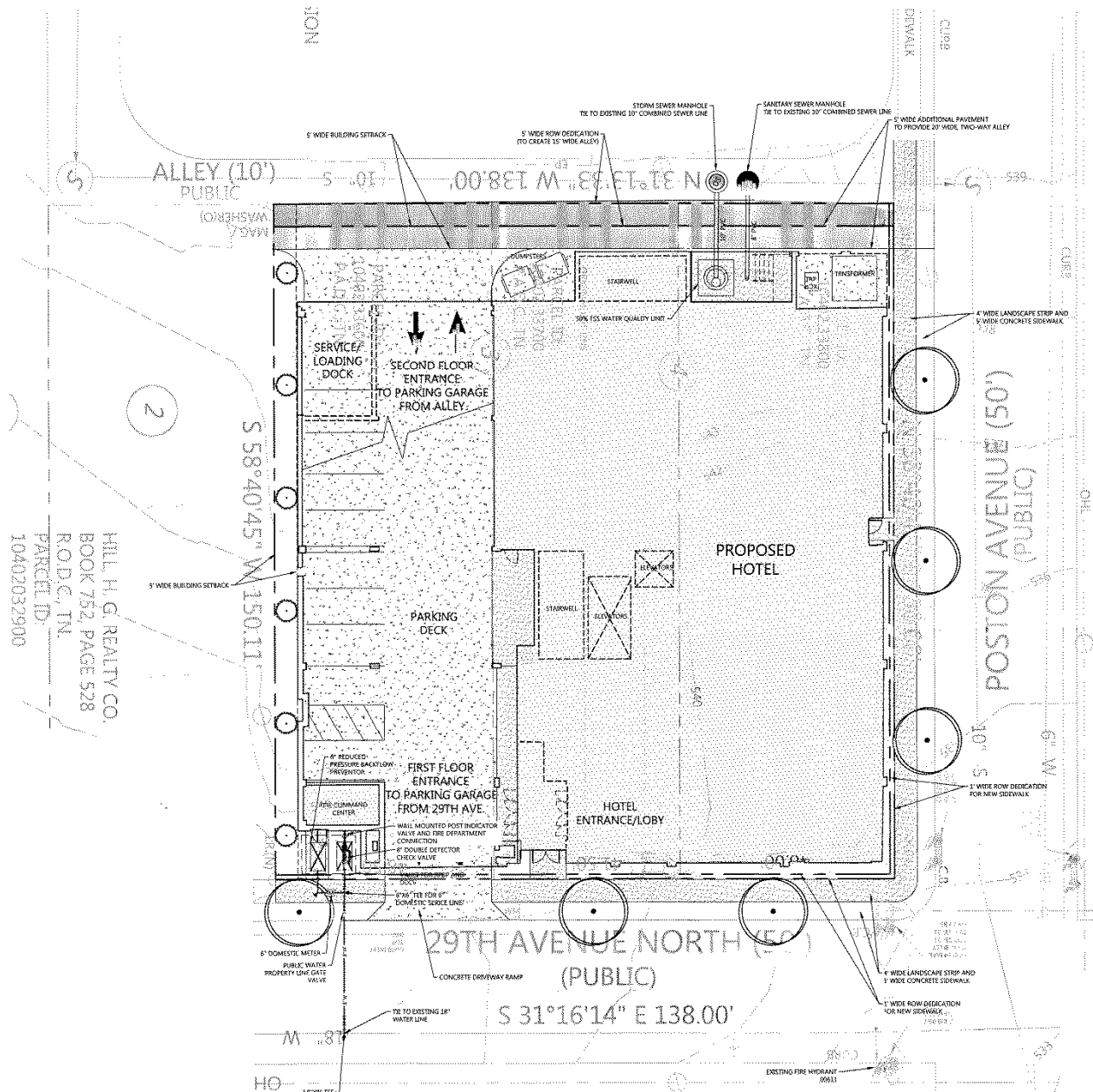
Office Residential Intensive (ORI) is intended for high intensity office and/or multifamily residential uses with limited retail opportunities.

31st Avenue and Long Boulevard Urban Design Overlay (31st and Long UDO): The purpose of the UDO is to allow for the application and implementation of special design standards with the intent of achieving a sense of place by fostering a scale and form of development that emphasizes sensitivity to the pedestrian environment, minimizes intrusion of the automobile into the urban setting, and provides for the sensitive placement of open spaces in relationship to building masses, street furniture and landscaping features in a manner otherwise not ensured by the application of the conventional bulk, landscaping and parking standards of the Metro Zoning Code. The 31st Avenue/Long Boulevard area has a variety of physical conditions and is divided into sub-districts. Where appropriate, specific design standards have been developed for each sub-district by building type.

This property is located in the CO-1 Core sub-district area. The sub-district is intended to remain a high intensity transition to the future urban edge of Centennial Park. Buildings may take many forms such as multi-story mixed-use, commercial, and stacked residential flats. The area should be developed in a dense, urban manner with a variety of building types as high as approximately six stories.



Metro Planning Commission Meeting of 03/23/2017



Proposed Site Plan



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Proposed Zoning

Specific Plan – Commercial (SP-C) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes commercial uses.

CRITICAL PLANNING GOALS

- Supports Infill Development
- Creates Walkable Neighborhoods
- Promotes Compact Building Design

Development in areas with adequate infrastructure is more appropriate than development in areas not served with adequate infrastructure such as roads, water and sewer, because it does not burden Metro with the cost of maintaining new infrastructure. The project proposes development on an infill site. Sidewalks are being provided along all street frontages to create a more walkable and pedestrian friendly environment. The design of the site and building efficiently utilizes urban land by incorporating structured parking and creating a compact building design.

GREEN HILLS – MIDTOWN COMMUNITY PLAN

T5 Center Mixed Use Neighborhood (T5 MU) is intended to preserve, enhance, and create high-intensity urban mixed use neighborhoods with a development pattern that contains a diverse mix of residential and non-residential land uses. T5 MU areas are intended to be among the most intense areas in Davidson County. T5 MU areas include some of Nashville's major employment centers such as Midtown that represent several sectors of the economy including health care, finance, retail, the music industry, and lodging. T5 MU areas also include locations that are planned to evolve to a similar form and function.

The property is located in the Midtown Study within the 10-MT-T5-MU-03 special policy. This special policy applies, in part, to properties surrounding West End Avenue between I-440 and 31st Avenue North. The special policy provides guidance regarding uses, building form, connectivity, height, and parking. The special policy calls for a maximum height of eight stories, but further states that punctuations of greater height may be appropriate at prominent locations, provided that the site and building design meet the policy.

Consistent with Policy?

Yes. T5 MU policy areas are intended to be among the most intense areas in Davidson County. These areas are intended to function as high-intensity mixed-use neighborhoods and to contain employment centers for many sectors of the economy, including lodging. The proposal to locate a hotel on this site is consistent with the T5 MU policy. The SP proposes a maximum height of 11 stories. The proposed height exceeds the guidance included in the special policy, which calls for a maximum of eight stories with an allowance for punctuations of greater height at prominent locations. The project site is located on a corner approximately 330 feet north of West End Avenue, which is a major corridor. Additionally, the site is adjacent to a special policy area which permits heights up to 20 stories. An existing 16 story building located at the northeast corner to of 29th Avenue North Poston Avenue is adjacent to the proposed site. The locational characteristics of the site make this an appropriate location for a punctuation of additional height, as the site and design are consistent with the policy.



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PLAN DETAILS

The site encompasses three parcels located at 109, 111, and 113 29th Avenue North, at the southwest corner of the intersection of 29th Avenue North and Poston Avenue. Current land uses include two-family residential, multi-family residential, and office.

Site Plan

The plan proposes a 94,000-square-foot, 150-room, hotel and 66,000 square feet of structured parking. The proposed square footage results in a Floor Area Ratio (FAR) of 4.42, with an overall permitted FAR of 5.0 for the SP. Per the standards of the Metro Zoning Ordinance, required structured parking spaces are excluded from the FAR calculation. The existing zoning permits an overall Floor Area Ratio (FAR) of 3.0, exclusive of required structured parking. The primary hotel entrance fronts 29th Avenue North. Vehicular access to the parking structure will be provided from 29th Avenue North and from the alley located to the west. A total of 151 parking spaces are required per Zoning Code; 154 spaces are provided. Five-foot sidewalks and four-foot planting strips, consistent with the requirements for local streets, are provided along both 29th Avenue North and Poston Avenue. The proposed height is limited to 11 stories in 130 feet.

UDO MODIFICATION

In January of 2017, the Design Review Committee (DRC) found that additional height is appropriate on this property due to its location at a corner and adjacent to a special policy which allows buildings 20 stories and above. The DRC included a condition of approval requiring the applicant to apply for SP zoning in order to achieve context sensitive development and building design.

ANALYSIS

The proposed SP includes a hotel and parking. The site is located within T5 MU policy, which supports lodging uses, and is consistent with the policy goals of creating and enhancing high intensity, urban, mixed-use neighborhoods. The SP requires compliance with all standards of the 31st and Long UDO, except for height, for which the applicant was previously granted a modification. The maximum height on the plan is consistent with the approved modification. The proposed FAR of 5.0 is necessary to achieve the height permitted by the approved modification and determined appropriate for this site based on the surrounding context.





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The preliminary SP includes conceptual elevations for the frontages along 29th Avenue and Poston Avenue which include stepbacks, glazing and other architectural elements designed to avoid blank walls and enhance the streetscape. Elevations demonstrating that all facades are in compliance with the UDO standards will be required with the Final SP. The site plan shows a transformer located along the alley. All mechanical equipment is required to be screened per the standards of the UDO. The transformer shall be screened to the extent permitted by NES standards and requirements. The rendered site plan indicates canopies that will extend over the sidewalk and into the right-of-way along 29th Avenue and Poston Avenue. Approval of a mandatory referral to permit the aerial encroachments will be required prior to building permits. Sidewalks are provided to enhance pedestrian connectivity in the area and bicycle parking is provided consistent with Metro Zoning Ordinance requirements. No landscape buffer yards are required, but street trees and other landscaping are provided to enhance the pedestrian experience.

FIRE MARSHAL RECOMMENDATION

Approve with conditions

- Fire Code issues will be addressed in the permit phase.

STORMWATER RECOMMENDATION

Approved

WATER SERVICES RECOMMENDATION

Approve with conditions

- Approved as a Preliminary SP only. The required capacity fees must be paid prior to Final Site Plan/SP approval. FYI - Before the Final SP is approved, applicant must address the combined sewer measures required by MWS.

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works, in effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable. Final design may vary based on field conditions.
- Prior to MPW approval of the building permit submit recorded ROW dedication to the back of sidewalk.
- Comply with MPW Traffic Engineering comments.

TRAFFIC AND PARKING RECOMMENDATION

Approved with conditions

In accordance with the findings of the TIS, the developer shall construct and provide the following roadway improvements.

- Developer shall apply to T&P staff for approval to restrict parking as recommended by TIS.
- If guests arrive or depart by bus, only temporary bus unloading/loading in the existing curbside parking area along the project frontage near the 29th Ave access shall be allowed.
- If Valet services are provided, they should be provided on site at the 29th Ave access drive. Developer shall apply to T&P staff for approval of additional valet or loading zone in the public ROW.



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- Developer shall post directions to check in/access drive on web site. If project is self- parked, Developer shall provide directions and route to other garage access off the alley.
- 29th Avenue North and Poston Avenue
 - The on-street parking on the west side of 29th Avenue North should be restricted for a minimum of 25 feet to the south of Poston Avenue.
 - The on-street parking on the south side of Poston Avenue should be restricted for a minimum of 25 feet to the west of 29th Avenue North.
- Poston Avenue and Alley #719
 - The on-street parking on the south side of Poston Avenue should be restricted for a minimum of 25 feet to the east and west of Alley #719.
 - Alley #719 should be widened to include sufficient width for one entering travel lane and one exiting travel lane.
- 29th Avenue North and Site Access
 - The site access driveway on 29th Avenue North should be designed to include one entering travel lane and one exiting travel lane with adequate clearance to structure walls.
 - Approximately 5% of the parking spaces are to be provided within the first level, which will serve as the loading area for check-in/check-out vehicles.in an area of the access drive off 29th Ave.
 - Developer shall also provide a loading area for rideshare/taxi operations on site.
 - The on-street parking on the west side of 29th Avenue North should be restricted for a minimum of 25 feet to the north and south of the proposed site access.
- Alley #719 and Site Access
 - The site access driveway on Alley #719 should be designed to include one entering travel lane and one exiting travel lane with adequate clearance to structure walls.
 - If parking/access control equipment is to be provided, a minimum of 25 feet should be provided between the gate/arm and the edge of pavement of Alley #719. This will ensure adequate space for one vehicle to queue at the entry gate without impacting the traffic flow on the alley.
- Alley #719
 - To accommodate two-way travel, Alley #719 should be widened to include a minimum pavement width of 20 feet between the site access and Poston Avenue.

Maximum Uses in Existing Zoning District: ORI

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Office (710)	0.49	3.0	64,033 SF	947	132	151

Maximum Uses in Proposed Zoning District: SP-C

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Hotel (310)	0.49	5.0	94,000 SF (150 Rooms)	1338	88	87



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Traffic changes between maximum: **ORI and SP-C**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	391	-44	-64

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Permitted land uses shall be limited to a maximum of 94,000 square foot hotel and 66,000 square feet of parking garage.
2. On the corrected set, update the Maximum FAR to 4.5.
3. Prior to use and occupancy permit, a mandatory referral approving aerial encroachments for the proposed canopies shall be required.
4. Elevations for all facades of the building consistent with the elevations in the Preliminary SP and all standards of the 31st and Long UDO shall be provided with the submittal of the Final Site Plan.
5. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the 31st Avenue and Long Boulevard UDO and the ORI zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
6. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
7. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
8. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
9. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



Metro Planning Commission Meeting of 03/23/2017



2017SP-021-001
GLENROSE SP
Map 119-02, Parcel(s) 131,135
11, South Nashville
16 (Mike Freeman)



Project No. Specific Plan 2017SP-021-001
Project Name Glenrose SP
Council District 16- Freeman
School District 7 – Pinkston
Requested by Dale and Associates, applicant; SC2 Properties, LLC, owner.

Deferrals This item was deferred at the March 9, 2017, Planning Commission meeting. No public hearing was held.

Staff Reviewer Burse
Staff Recommendation *Defer to the April 13, 2017, Planning Commission meeting.*

APPLICANT REQUEST

Zone change to permit six residential units.

Preliminary SP

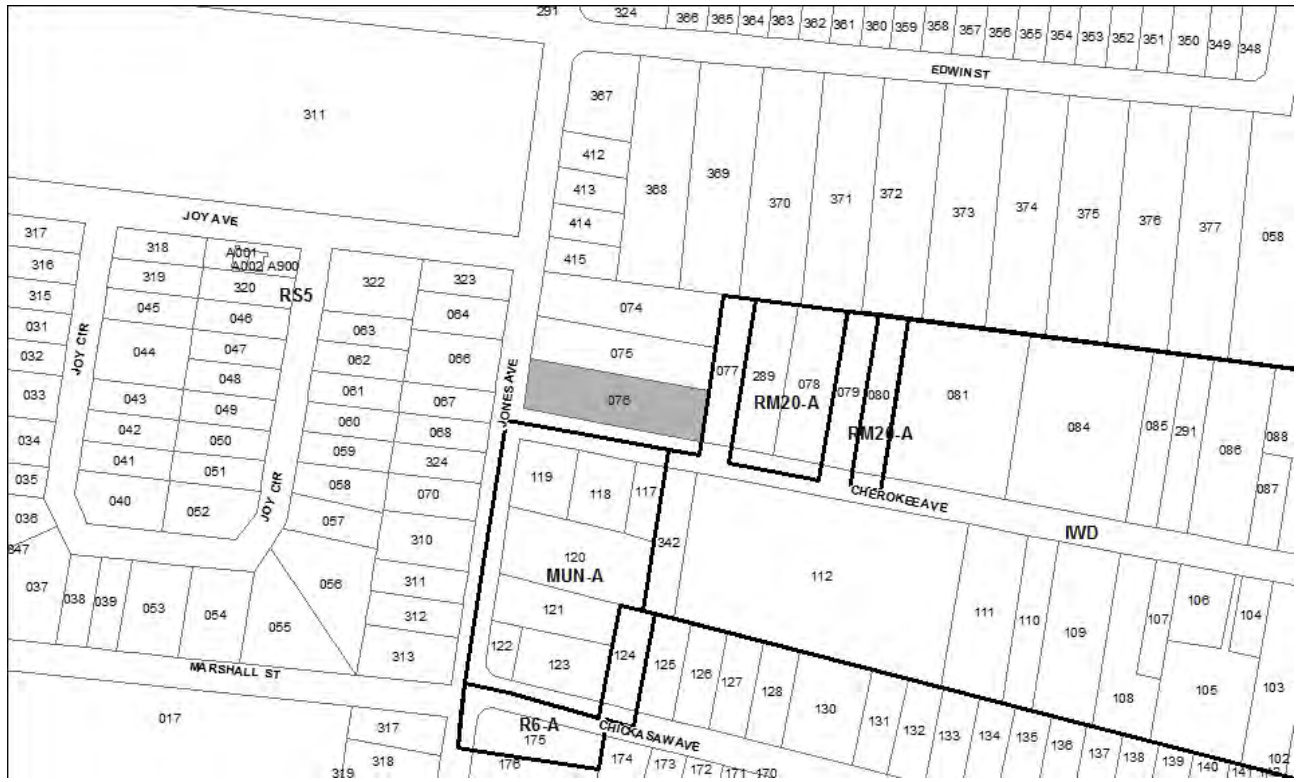
A request to rezone from Single-Family Residential (RS10) to Specific Plan-Residential (SP-R) zoning on properties located at 98 Rose Street and 99 Evelyn Drive, (0.82 acres), to permit six residential units.

STAFF RECOMMENDATION

Staff recommends deferral to the April 13, 2017, Planning Commission meeting at the request of the applicant.



Metro Planning Commission Meeting of 03/23/2017



2016S-253-001
1601 JONES AVE RESUB
Map 071-12, Parcel(s) 076
05, East Nashville
05, Scott Davis



Project No. Final Plat 2016S-253-001
Project Name 1601 Jones Avenue Resub
Council District 08 - VanReece
School District 3 - Speering
Requested by Rocky Montoya, RLS, applicant; David and Rachel Peiffer, owners.

Deferrals This item was deferred at the February 9, 2017, February 23, 2017, and March 9, 2017 Planning Commission meeting. No public hearing was held.

Staff Reviewer Napier
Staff Recommendation *Defer to the April 13, 2017, Planning Commission meeting.*

APPLICANT REQUEST
Final Plat to create two lots.

Final Plat
A request for final plat approval to create two lots and for a variance from the subdivision regulations for sidewalk requirements for property located at 1601 Jones Avenue, at the northeast corner of Jones Avenue and Cherokee Avenue, zoned RS5 (0.53 acres).

STAFF RECOMMENDATION
Staff recommends deferral to the April 13, 2017, Planning Commission meeting at the request of the applicant.



Metro Planning Commission Meeting of 03/23/2017



2017S-012-001

BINKLEY PROPERTY SUBDIVISION REPLAT OF LOT 1

Map 041-08, Parcel(s) 068

02, Parkwood-Union Hill

03, (Brenda Haywood)



Project No. **Final Plat 2017S-012-001**
Project Name **Binkley Property Subdivision Replat of Lot 1**
Council District 03 -Haywood
School District 1 - Gentry
Requested by Clint T. Elliott Surveying, applicant; Connerth Construction, LLC, owner.

Deferrals This item was deferred at the January 26, 2017, February 9, 2017, and February 23, 2017, Planning Commission meeting. No public hearing was held.

Staff Reviewer Birkeland
Staff Recommendation *Defer to the April 13, 2017, Planning Commission meeting unless a recommendation of approval is received from all reviewing agencies. If a recommendation of approval is received from all agencies, staff recommends approval with conditions.*

APPLICANT REQUEST

Create three lots.

Final Plat

A request for final plat approval to create three lots on property located at 1227 Old Hickory Boulevard, approximately 360 feet northwest of Marydale Drive, zoned Single-Family Residential (RS20) (2.65 acres).

Existing Zoning

Single-Family Residential (RS20) requires a minimum 20,000 square foot lot and is intended for single-family dwellings at a density of 1.85 dwelling units per acre. *RS20 would permit a maximum of 5 units. This property is subject to the infill subdivision regulations, which could result in fewer lots, depending on the compatibility criteria.*

CRITICAL PLANNING GOALS

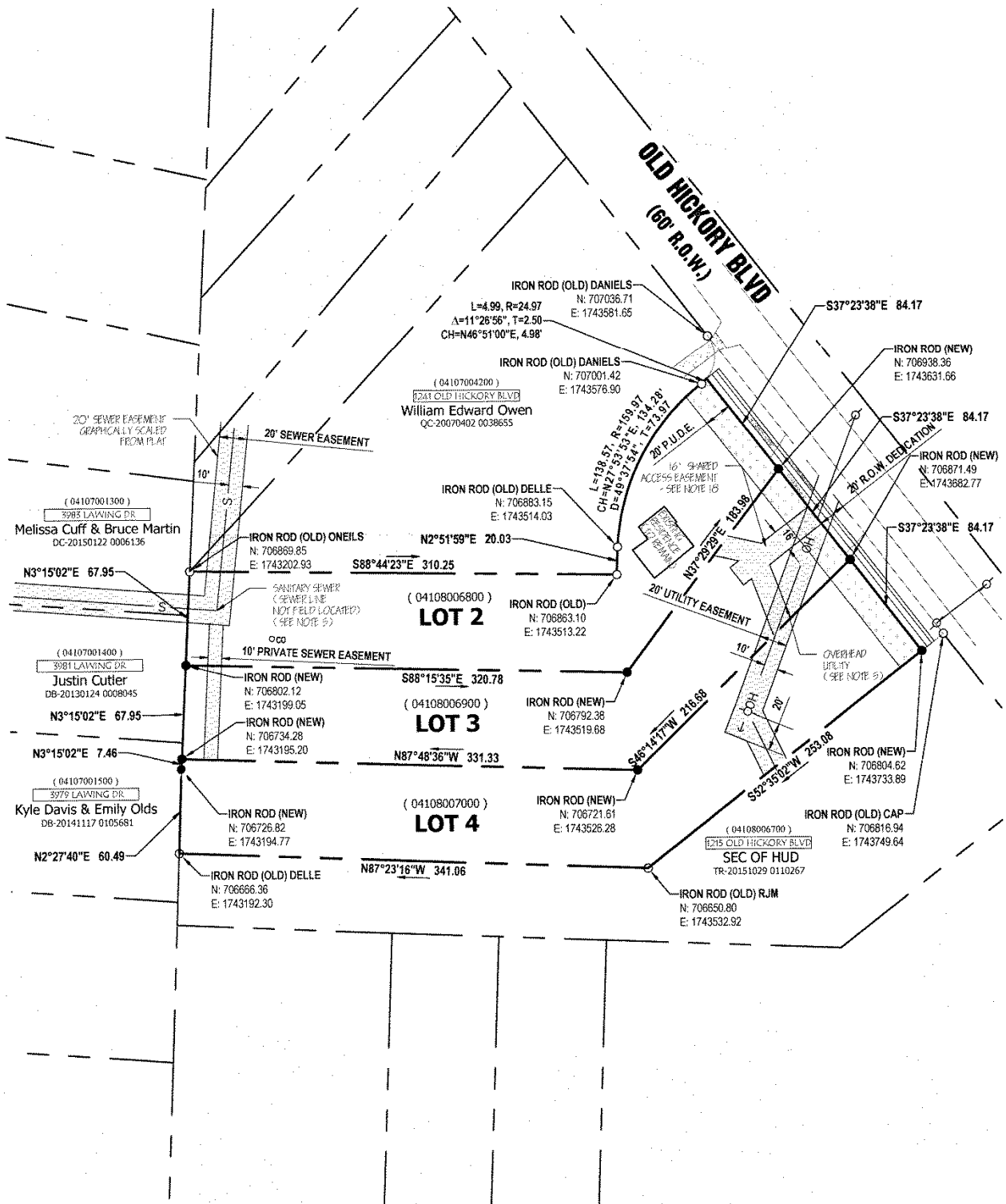
N/A

PARKWOOD-UNION HILL COMMUNITY PLAN

T3 Suburban Neighborhood Maintenance (T3 NM) policy is intended to preserve the general character of developed suburban residential neighborhoods. T3 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T3 NM areas have an established development pattern consisting of low to moderate density residential development and institutional land uses. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.



Metro Planning Commission Meeting of 03/23/2017



Proposed Subdivision



Metro Planning Commission Meeting of 03/23/2017

PLAN DETAILS

This request is for final plat approval to create three lots on property located at 1227 Old Hickory Boulevard, where one lot currently exists. Vehicular access would be limited to a shared access easement on Lot 3. There are no sidewalks present along Old Hickory Boulevard.

The existing lot is 115,419 square feet (2.65 acres) and is proposed to be subdivided into three lots with the following square footage/ acreage:

- Lot 2: 34,225 SF (0.766 acres)
- Lot 3: 36,074 SF (0.835 acres)
- Lot 4: 39,947 SF (0.930 acres)

ANALYSIS

Section 3-5.2 of the Subdivision Regulations establishes criteria for reviewing infill subdivisions and for determining their compatibility in Neighborhood Maintenance policies.

Zoning Code

The proposed lots meet the minimum standards of the RS20 zoning district.

Street Frontage

Lots would front on Old Hickory Boulevard which is a public street.

Density

The T3 Urban Neighborhood Maintenance policy no longer includes density limitations.

Community Character

1. Lot frontage analysis; the proposed lots must have frontage either equal to or greater than 70% of the average frontage of surrounding parcels or equal to or greater than the surrounding lot with the least amount of frontage, whichever is greater. Calculations for the lot frontage analysis were performed on Old Hickory Boulevard.

In this instance, the lots created must be equal to or greater than 80.5 feet. The proposed lots meet lot frontage requirements.

Lot 2 – 4 Frontage	
Proposed Frontage	84.17 ft.
Minimum Frontage	60 ft.
70% Average	80.5ft.

2. Lot size analysis; the proposed lots must have lot area that is either equal to or greater than 70% of the lot size of the average size of surrounding parcels or equal to or larger than smallest surrounding lot, whichever is greater.

In this instance, the proposed lots must equal to or greater than 33,025 SF. The proposed lots meet lot size requirement.



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Lot 2 Size	
Proposed Size	34,225 SF
Minimum Size	16,572 SF
70% Average	33,025 SF

Lot 3 Size	
Proposed Size	36,074 SF
Minimum Size	16,572 SF
70% Average	33,025SF

Lot 4 Size	
Proposed Size	39,947 SF
Minimum Size	16,572 SF
70% Average	33,025 SF

3. Street setbacks; future structures must comply with setbacks per the Metro Zoning Code.
4. Lot orientation; proposed lots would orient Hartford Drive

Agency Review

All agencies have not recommended approval.

Harmony of Development

Staff finds that this proposal would provide for harmonious development along Old Hickory Boulevard. The height of future development will be limited to two stories in 35 feet, which is consistent with the adjacent properties. to the proposal includes a 16-foot shared access drive of all three lots (this would eliminate parking pads in the street setbacks) and enhance the public realm.

FIRE MARSHAL RECOMMENDATION

N/A

STORMWATER RECOMMENDATION

Approved

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- If sidewalks are required by Planning and the applicant chooses to construct rather than pay the in-lieu fee, then they should be shown and labeled on the plan. Due to the speed limit of this section of OHB, TDOT requires a 4 foot shoulder prior to curb and gutter. From the back of the curb and gutter an 8 foot planting strip and 6 foot sidewalk are required by the MCSP. Engineering construction plans for the sidewalk will need to be submitted due to the site characteristics. The relocation of utilities, grading, and drainage will need to be shown on the engineering construction plans.

TRAFFIC & PARKING RECOMMENDATION

Approved

WATER SVCES RECOMMENDATION

Returned for corrections

- For the latest re-plat (stamped-received 1/9/17), our original comments still apply: Awaiting submittal and approval of public sewer construction plans (see MWS letter dated 8/2/16).



Metro Planning Commission Meeting of 03/23/2017

Once these plans have been approved, the plat can be accurately reviewed. The required capacity fees have been paid.

STAFF RECOMMENDATION

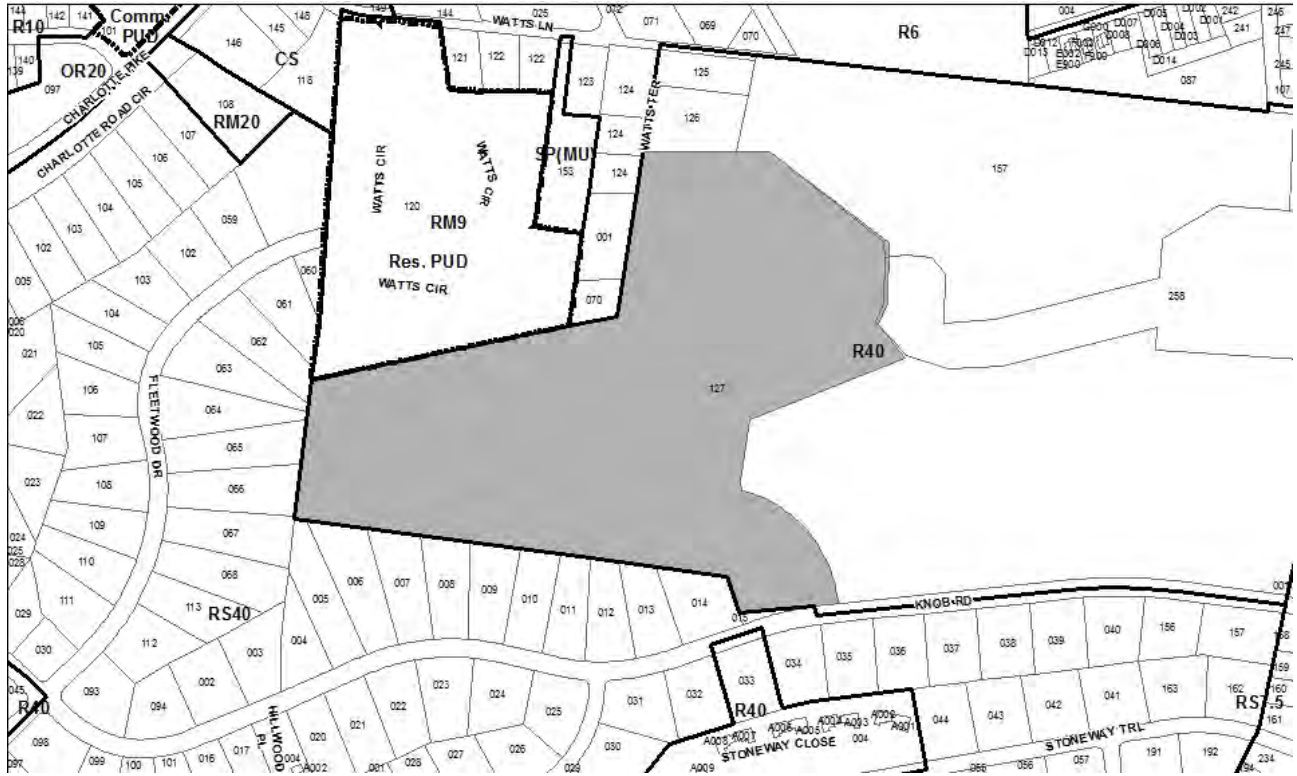
Staff recommends deferring to the April 13, 2017, Planning Commission meeting unless a recommendation of approval is received from all reviewing agencies. If a recommendation of approval is received from all agencies, staff recommends approval with conditions.

CONDITIONS (if approved)

1. Sidewalks are required along Old Hickory Boulevard. Therefore, prior to final plat recordation, one of the options must be chosen related to sidewalks:
 - a. Submit a bond application and post a bond with the Planning Department,
 - b. Construct sidewalk and have it accepted by Public Works,
 - c. Submit contribution in-lieu of construction to the Planning Department. The rate of \$96 per linear foot of total frontage area will require a \$24,240.96 (252.51 feet x \$96) contribution to Pedestrian Benefit Zone 1-A.
 - d. Construct an equal length of sidewalk within the same Pedestrian Benefit Zone, in a location to be determined in consultation with the Public Works Department.
2. Add "See Notes 13-18" on Lots 2-4.



Metro Planning Commission Meeting of 03/23/2017



2017S-033-001
HIGHLAND VIEW AT THE KNOB
Map 103-01, Part of Parcel(s) 127
07, West Nashville
20 (Mary Carolyn Roberts)



Project No. Concept Plan 2017S-033-001
Project Name Highland View at the Knob
Council District 20 –Roberts
School District 9 - Frogge
Requested by Dale & Associates, Inc., applicant; Highland Park Church, Inc., owner.

Deferral This item was deferred at the March 9, 2017, Planning Commission meeting. No public hearing was held.

Staff Reviewer Rickoff
Staff Recommendation *Approve with conditions including a variance for the length of a dead end street.*

APPLICANT REQUEST

Create thirty residential cluster lots

Concept Plan

A request for concept plan approval to create 30 clustered lots on a portion of property located at 5710 Knob Road, north of Stoneway Trail, zoned One and Two-Family Residential (R40) (36.15 acres)

Existing Zoning

One and Two-Family Residential (R40) requires a minimum 40,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 1.16 dwelling units per acre including 25 percent duplex lots. *R40 would permit a maximum of 33 lots with 8 duplex lots for a total of 41 units based on acreage only. However, application of the Subdivision Regulations may result in fewer units on this property.*

CRITICAL PLANNING GOALS

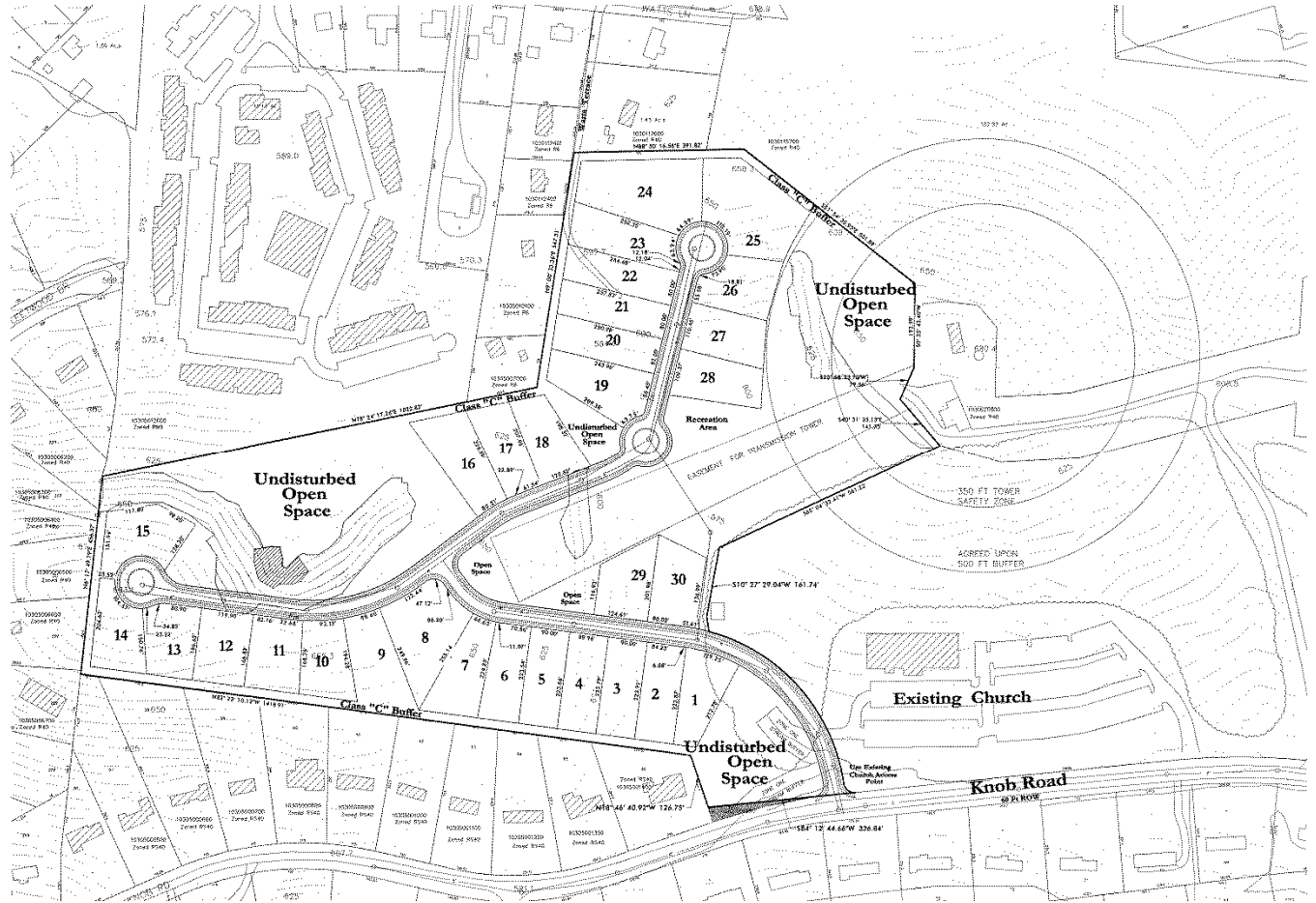
N/A

WEST NASHVILLE COMMUNITY PLAN

T3 Suburban Neighborhood Maintenance (T3 NM) is intended to preserve the general character of developed suburban residential neighborhoods. T3 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T3 NM areas have an established development pattern consisting of low to moderate density residential development and institutional land uses. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.



Metro Planning Commission Meeting of 03/23/2017



Proposed Subdivision



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Conservation policy (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed

Infill Area 07-T3-NM-02-IA02 (Knob Hill) is in the northern portion of Special Policy Area 07-T3-NM-02 where infill development may be appropriate if the land is not permanently protected as open space. Infill development is appropriate in this location because of the existing access and potential for additional access and connectivity, and because of existing zoning entitlements. More flexibility may be allowed on new streets created internal to the development, behind the parcels created along Knob Road. The character of the area is suburban, primarily single-family dwellings on parcels that are generally slightly less than one acre in size. Parcels created along Knob Road could be slightly smaller, subject to the creation of permanently protected open space within the development that prioritizes the protection of steep slopes, mature vegetation, and view sheds. There is a stream that flows along Knob Road, and any development along Knob Road should be arranged to minimize the disturbance of the stream. In the configuration of parcels and any new right-of-way, priority should be given to the preservation of the environmentally sensitive features over consistence with surrounding parcels and right-of-way patterns.

PLAN DETAILS

This development is located on a portion of property (approximately 36.15 of 66.63 acres) off of Knob Road, east of Charlotte Pike and west of White Bridge Pike. The existing 66.63 acre property is heavily wooded and contains a church. The 36.15 acres slated for redevelopment are located on the eastern half of the existing property. The church is located on the western portion of the property and is not included in the subdivision.

Additional surrounding land uses include single-family residential development (north, south, and west), multi-family development (northwest), and the WSMV-TV studios and transmission tower which are located directly northeast of the subject parcel. A 500 foot buffer has been provided around the tower and will remain undisturbed. A transmission tower easement extends diagonally from the WSMV tower through the 500 foot buffer towards the center of the development area.

Site Plan

The plan proposes up to 30 residential cluster. Lots are clustered down to the R20 district size and range in size from approximately 20,000 square feet to approximately 40,000 square feet. All of the lots are at least 50 feet in width. None of the lots are located on natural slopes of 20 percent or greater, and no problem soils have been identified on the site plan.

All of the lots are oriented along a new internal dead-end road accessed from Knob Road. The access point will be shared with the existing church, which is located outside of the development area. The new road extends towards the center of the site approximately 1,100 feet, and then splits off to form two cul-de-sacs near the northern and western property lines. A roundabout is located equidistant between the road split and the northern cul-de-sac. Five-foot wide sidewalks with a four-foot planting strip are provided throughout the interior of the development.



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In cluster lot subdivisions, a minimum of 15 percent of each phase of the development shall be open space. Of the 16.78 acres (46.5% of the site) designated as open space:

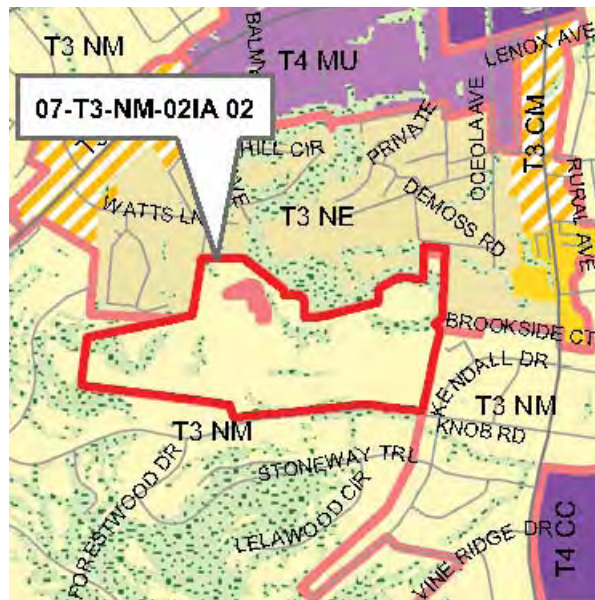
- 1.75 acres (10.4%) is designated as stormwater facilities
- 6.12 acres (36.4%) is located in the WSMV tower easement and/or 500 foot buffer. The tower easement will not be available to the public.
- The remaining 8.91 acres (53%) is designated as active or passive space:
 - Approximately 0.75 acres is designated as active and contains a playground.
 - Approximately 8.16 acres is designated as passive and will remain undisturbed

A standard “C” buffer is provided along all perimeter lots in accordance with the cluster lot provisions.

ANALYSIS

The plan is consistent with the cluster lot provisions and standards specified in the Metro Code, which state that lots so created shall be clustered on those portions of the site that have natural slopes of less than twenty percent. This proposal was evaluated against the site’s land use policies, which include Suburban Neighborhood Maintenance, Conservation, and Infill Area 07-T3-NM-02-IA 02 – Knob Hill (Infill Area) of Special Policy Area 07-T3-NM-02. The Knob Hill Infill Area comprises nearly 80 total acres on two parcels: the subject parcel slated for redevelopment, and the adjoining parcel which houses the WSMV studios and communication tower. The Infill Area policy provides specific guidance for the development of these two parcels. The proposed subdivision comprises a portion (36.15 acres) of the nearly 80 acres included in the Knob Hill Infill Area.

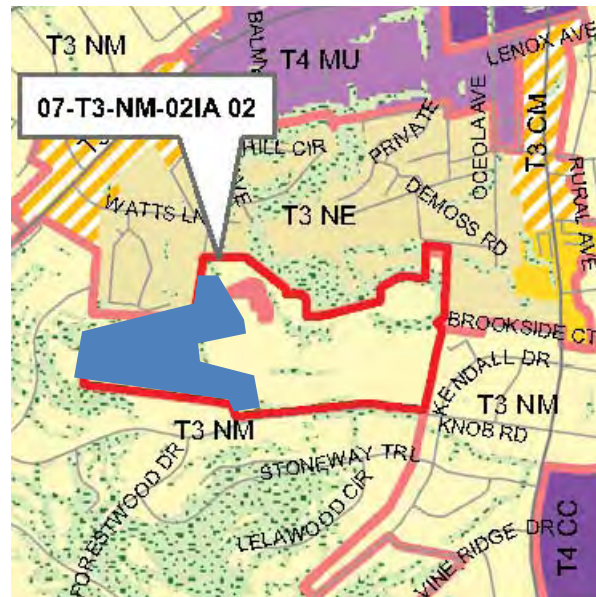
Infill Area 07-T3-NM-02-IA 02





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Approximate location of proposed subdivision is shaded below



Infill Area 07-T3-NM-02-IA 02 Analysis

The site plan proposes one access point off of Knob Road. Regarding access and connectivity, the Infill Area design principles state:

Infill development is appropriate in this location because of the existing access and potential for additional access and connectivity, and because of existing zoning entitlements.

In this case, potential for additional access and connectivity is limited since only the western portion of the Infill Area is slated for development. More opportunities may be available if a larger portion of the Infill Area was included in the development simply because there may be additional connection points further north and east. Connections to the south and west are not feasible because of already developed properties, and the eastern portion of the site is bounded by the tower easement and buffer.

The site plan includes areas with steep slopes and a stream buffer nearest the Knob Road access point, which are recognized through the designation of Conservation policy in these areas. Regarding the preservation of sensitive features, the Infill Area principles state:

In the configuration of parcels and any new right-of-way, priority should be given to the preservation of the environmentally sensitive features over consistency with surrounding parcel and right-of-way patterns.

The initial site plan proposed 33 lots, including 4 critical lots that were located near the western property line. All critical lots have since been removed and this area will now be preserved as permanent open space. Stream buffers have been identified on the site plan nearest the Knob Road access point. Prior to any land disturbance, an archeological survey will be required in order to identify any streams or other protected water features in accordance with stormwater regulations.



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Variance Request

Section 3-9.2(i).2 requires that the maximum length of dead end streets with turnarounds shall be 750 feet. The applicant does not meet this requirement and is requesting a variance from this section of the Subdivision Regulations. As proposed, the length of the access road from Knob Road to center of the site (the "Tee") is approximately 1,100 feet. The distance from the Tee to the western cul-de-sac is approximately 700 feet. Approximately 500 feet is provided between the Tee and the mid-block turnaround, and another 500 feet is provided between the mid-block turnaround and the eastern cul-de-sac.

Variance Standards

If the Planning Commission finds that extraordinary hardships or practical difficulties may result from strict compliance with these regulations, a variance from these regulations may be granted, provided that such variance shall not have the effect of nullifying the intent and purpose of these regulations. The Planning Commission shall make findings based upon the evidence presented to it in each specific case that:

- a) The granting of the variance shall not be detrimental to the public safety, health, or welfare or injurious to other property or improvements in the neighborhood in which the property is located.
- b) The conditions upon which the request for a variance is based are unique to the property for which the variance is sought and are not applicable generally to other property.
- c) Because of the particular physical surroundings, shape, or topographical conditions of the specific property involved, a particular hardship to the owner would result, as distinguished from a mere inconvenience, if the strict letter of these regulations were carried out.
- d) The variance shall not in any manner vary from the provisions of the adopted General Plan, including its constituent elements, the Major Street Plan, or the Zoning Code for Metropolitan Nashville and Davidson County (Zoning Code).

The access road exceeds the maximum length of 750 feet. However, a road connection to the north may be impractical due to steep slopes and the grading required to construct a road that meets Metro standards. Given the environmental constraints that would prevent the applicant from creating additional street networks on this site, staff recommends approval of a variance to the maximum length of a dead-end street standard. A second means of access is not required by the Fire Code since this development does not exceed 30 dwelling units.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Provide flow data.

STORMWATER RECOMMENDATION

Approved

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- Slopes along roadways shall not exceed 3:1.
- Retaining walls along roadways shall comply with TDOT standards.



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TRAFFIC AND PARKING RECOMMENDATION

Approved with conditions

- Comply with note 5 on concept plan with final site plan:
Access road shall have 1 entering lane and 2 exiting lanes to church parking lot drive aisle as currently exists. Provide adequate sight distance at access road. Submit pavement marking plan to modify DYSL on Knob Rd at new road.

WATER SERVICES RECOMMENDATION

Approved with conditions

- Approved as a Concept Plan only. Public sewer construction plans must be submitted and approved prior to Final Site/Development Plan approval. These approved construction plans must match the Final Site/Development Plans. The required capacity fees must also be paid prior to Final Site/Development Plan approval.

STAFF RECOMMENDATION

Staff recommends approval with conditions including a variance for the length of a dead end street.

CONDITIONS

1. Sidewalks are required along all internal streets.
2. The final site plan/ building permit site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.
3. Add the following note to all open spaces except for designated recreational areas: All existing trees in designated open spaces shall be preserved. These areas shall remain undisturbed. A tree survey will be required with the final site plan.
4. A tree survey indicating all trees with an 8 inch caliper or greater shall be submitted with the final site plan.
5. Prior to any land disturbance, an archeological survey shall be required to determine the location of any historic structures, including rock or stone walls. If any walls are determined to have historical significance, additional conditions may be applied to the final site plan.
6. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
7. Pursuant to 2-3.5.e of the Metro Subdivision Regulations, because this application has received conditional approval from the Planning Commission, that approval shall expire unless revised plans showing the conditions on the face of the plans are submitted prior to or with any application for a final site plan or final plat.



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2017S-035-001
HALL ESTATES SECTION 3 RESUB OF LOT 235
Map 146-12 Parcel(s) 131
12, Southeast
26 (Jeremy Elrod)



Project No. **Final Plat 2017S-035-001**
Project Name **Hall Estates Section 3 Resub of Lot 35**
Council District 26 - Elrod
School District 2 - Brannon
Requested by Delle Land Surveying, applicant; Jack Barrett and Cynthia Barrett, owners.

Defer This item was deferred at the February 23, 2017, and March 9, 2017, Planning Commission meetings. No public hearing was held.

Staff Reviewer Napier
Staff Recommendation *Defer to the April 13, 2017, Planning Commission meeting.*

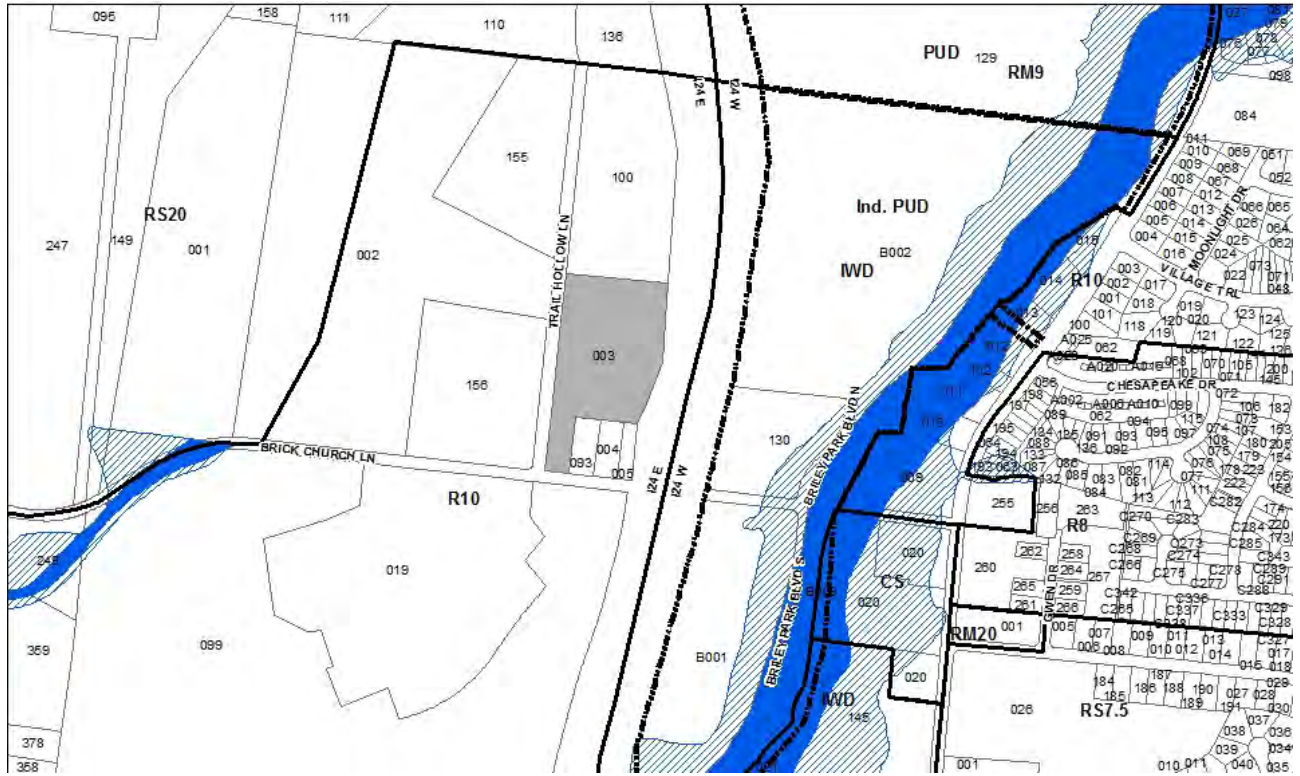
APPLICANT REQUEST
Final Plat to create two lots.

Final Plat
A request for final plat approval to create two lots and for a variance from the subdivision regulations for sidewalk requirements for property located at 5024 Trousdale Drive, approximately 224 feet north of Barrywood Drive, zoned Single-Family Residential (RS20) (1.10 acres).

STAFF RECOMMENDATION
Staff recommends deferral to the April 13, 2017, Planning Commission meeting at the request of the applicant.



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2017S-043-001
TRAIL HOLLOW SUBDIVISION
Map 050, Parcel(s) 003
03, Bordeaux-Whites Creek
03 (Brenda Haywood)



Project No. Final Plat 2017S-043-001
Project Name Trail Hollow Subdivision
Council District 03 – Haywood
School District 1 - Gentry
Requested by Crenshaw Land Surveying, applicant; Gulf Front Investment, LLC, owner.

Deferrals This item was deferred at the February 23, 2017, Planning Commission meeting. No public hearing was held.

Staff Reviewer Rickoff
Staff Recommendation Disapprove.

APPLICANT REQUEST

Create two lots.

Final Plat

A request for final plat approval to create two lots, including a variance to the setback, depth, area, and frontage requirements of the Subdivision Regulations, on property located at 3501 Trail Hollow Lane, at the northeast corner of Trail Hollow Lane and Brick Church Lane, zoned One and Two-Family Residential (R10) (approximately 6.75 acres)

Existing Zoning

One and Two-Family Residential (R10) requires a minimum 10,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 4.63 dwelling units per acre including 25 percent duplex lots. *Although R10 may allow two-family lots, that determination is made by the Metro Codes Department. Two-family lots have not been identified on the plat.*

CRITICAL PLANNING GOALS

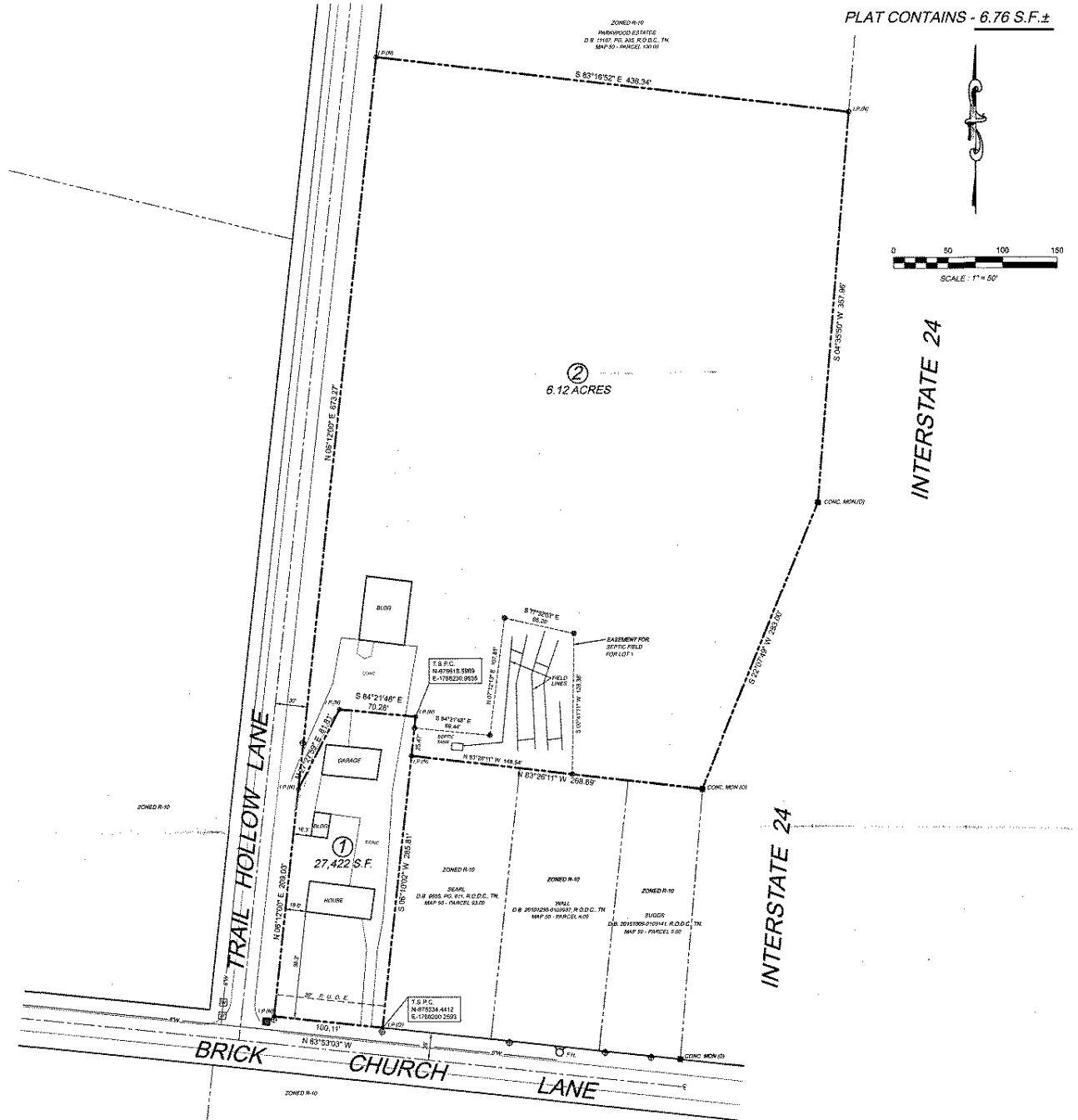
N/A

BORDEAUX-WHITES CREEK COMMUNITY PLAN

T2 Rural Maintenance (T2 RM) is intended to preserve rural character as a permanent choice for living within Davidson County and not as a holding or transitional zone for future urban development. T2 RM areas have established low-density residential, agricultural, and institutional development patterns. Although there may be areas with sewer service or that are zoned or developed for higher densities than is generally appropriate for rural areas, the intent is for sewer services or higher density zoning or development not to be expanded. Instead, new development in T2 RM areas should be through the use of a Conservation Subdivision at a maximum gross density of 1 dwelling unit/2 acres with individual lots no smaller than the existing zoning and a significant amount of permanently preserved open space.



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Proposed Subdivision



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Conservation (CO) is intended to preserve and enhance environmentally sensitive land within all Transect Categories except T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils.

PLAN DETAILS

This request is for final plat approval to create two lots on property located at 3501 Trail Hollow Lane, where one parcel currently exists. The existing parcel has frontage on Brick Church Lane and Trail Hollow Lane, and contains a single-family home and multiple accessory structures. The proposed subdivision will result in Lot 1 fronting on Brick Church Lane and Lot 2 fronting on Trail Hollow Lane. Sidewalks are not required with this subdivision because this property is located in the General Services District and has a Sidewalk Priority Score of less than 20, as established in the Strategic Plan for Sidewalk and Bikeways.

The property is located directly west of Interstate 24, separating higher density development patterns east of the interstate from low-density development patterns to the west. Property directly south of Brick Church Lane is in Neighborhood Evolving policy, and further east along Brick Church Lane is a mixture of Conservation, Industrial, Suburban Community Center, Neighborhood Evolving, and Neighborhood Maintenance policies. Though the majority of the site is in Rural Neighborhood policy, portions of the site are designated as Conservation policy due to potential steep slopes.

The plat includes an on-site septic system. All private septic systems are reviewed by the Metro Public Health Department. Metro Health has not received the necessary documentation and fees needed to review and certify the existing system.

ANALYSIS

Chapter 4 of the Subdivision Regulations, Rural Character Subdivisions, regulates the subdivision of land in T2 Rural Neighborhood policies, with the intent of ensuring that new development is in harmony with the established rural character of these areas.

Section 4-2.5 permits two Character Options for the development of land in Rural Neighborhood policies: Countryside Character Option and Agricultural Character Option. In this case, the Agricultural Character Option is not appropriate because the primary function of the subdivision is not for agricultural use. Therefore, the Countryside Character Option is the most appropriate option. Under the Countryside Character Option, two alternatives exist: Open Alternative and Screened Alternative. Since the site does not utilize existing vegetative screening required by Screened Alternative, staff reviewed the plat against the following Open Alternative criterion:

Building Setback along existing public streets

Section 4.2.5 includes conditions and standards for determining the minimum building setback, including consideration of the average setback of abutting parcels and whether those parcels are vacant or developed.



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The required building setback shall be varied between lots. When the minimum required street setback is less than the average of the street setback of the two parcels abutting either side of the lot(s) proposed to be subdivided, a minimum building setback line shall be included on the proposed lots at the average setback of the abutting parcels. When one of the abutting parcels is vacant, the next developed parcel shall be used. For a corner lot, both block faces shall be used. Where the majority of the abutting parcels are not developed, the minimum building setback shall be two times the amount of lot frontage. However, in no instance shall the minimum building setback be greater than 1,000 square feet.

For the purposes of this analysis, “surrounding parcels” is defined by the Subdivision Regulations as the five R, RS, AR2A, or AG parcels oriented to the same block face on either side of the parcel proposed for subdivision, or to the end of the same blockface, whichever is less. All surrounding parcels are utilized for area and frontage calculations. Abutting parcels, defined as the two closest parcels on either side of the proposed subdivision, are utilized for setback calculations. Trail Hollow and Brick Church Lanes are local and minor streets, respectively.

For Lot 1, a minimum building setback line of 58 feet (the average street setback) is required along Brick Church Lane. Lot 1 meets the required setback since the existing front setback exceeds 58 feet. However, the accessory structure on Lot 1 nearest the rear property line would not comply with the side setback requirement.

	Min. Setback	Approx. Average Setback of Adjacent Parcels	Approx. Required Setback	Meets setback requirement?
Lot 1	20 feet (based on R10)	58 feet	58 feet	Yes

For Lot 2, staff evaluated the adjacent parcels to determine the required setback. However, the northern parcel adjacent to Lot 2 contains a communications tower and two small equipment boxes, which are not used for occupancy and were not considered. The next developed parcel fronts a private drive, which is not considered part of the same blockface; therefore, this parcel could not be included in the building setback determination.

The rural subdivision regulations state:

Where the majority of the parcels are not developed, the minimum building setback shall be two times the amount of lot frontage. However, in no instance shall the minimum building setback be greater than 1,000 feet.

In this case, the proposed frontage for Lot 2 is 673.21 feet. Two times the amount of lot frontage is 1,346.42 feet, greater than 1,000. Therefore, the minimum 20-foot required street setback is less than the required setback of 1,000 feet for Lot 2. Lot 2 does not meet the minimum building setback requirement.



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	Min. Setback	Proposed frontage	Two times amount of frontage	Frontage requirement greater than 1,000?	Required setback	Meets setback requirement?
Lot 2	20 feet (based on R10)	673.21 feet	1,346.42 feet	Yes. 1,346 > 1,000	1,000 feet	No

Lot 1 meets the minimum building setback requirement of approximately 58 feet. Lot 2 does not meet the minimum building setback requirement of 1,000 feet.

Lot Depth along existing public streets

Lot depth means the horizontal distance between the rear lot line (or some other lot line in cases where there is no rear lot line) and the midpoint of the front lot line, measured back from such midpoint in the mean direction of the side lot line. The minimum depth for lots along existing public streets shall be the building setback required by Section 4-2.5(a) plus 300 feet. Since the required building setbacks are approximately 58 feet (Lot 1) and 1,000 feet (Lot 2), the minimum depth requirements are as follows:

- Lot 1: Approximately 358 feet (58 feet plus 300 feet)
- Lot 2: Approximately 1,300 feet (1,000 feet plus 300 feet)

The depths of Lot 1 and Lot 2 are proposed as follows:

- Lot 1: Approximately 285.81 feet
- Lot 2: Approximately 445 feet

Lots 1 and 2 do not meet the minimum depth requirement of 358 feet (Lot 1) or 1,300 feet (Lot 2).

Lot size along existing public streets

Individual lot sizes shall vary in size to reflect the rural character. The minimum lot size is either equal to or greater than 70% of the lot size of the average size of the surrounding parcels or equal to or larger than the smallest of the surrounding parcels, whichever is greater. If there are no surrounding parcels, the screened alternative shall be used. Flag lots shall not be included in the analysis.

Lot 1 fronts Brick Church Lane and meets lot size requirements. Along Brick Church Lane, lots created must have at least approximately 15,681.6 square feet. Lot 2 has one surrounding parcel oriented to the same blockface with which to compare lot size along Trail Hollow Lane. Along Trail Hollow Lane, lots created must have at least approximately 368,082 square feet. Since Lot 2 is not equal to or larger than the smallest surrounding parcel, Lot 2 does not meet the area requirements:

Lot 1 Size	
Proposed Size	27,422 SF
Minimum Size	15,681.6 SF
70% Average	15,246 SF

Lot 2 Size	
Proposed Size	266,587 SF
Minimum Size	368,082 SF
70% Average	N/A



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Lot frontage abutting existing public streets

Lot frontage is either equal to or greater than 70% of the average frontage of the surrounding parcels or equal to or greater than the smallest of the surrounding parcels, whichever is greater. Lot 1 fronts Brick Church Lane and meets frontage requirements. Along Brick Church Lane, lots created must have at least 75 feet of frontage. Lot 2 has one surrounding parcel oriented to the same blockface with which to compare lot frontage along Trail Hollow Lane. Along Trail Hollow Lane, lots created must have at least approximately 910 feet of frontage. Lot 2 fronts Trail Hollow Lane and does not meet frontage requirements:

Lot 1 Frontage	
Proposed Frontage	100.11 ft.
Minimum Frontage	75 ft.
70% Average	64.16 ft.

Lot 2 Frontage	
Proposed Frontage	673.27 ft.
Minimum Frontage	910 ft.
70% Average	N/A

Street Lights

Not applicable to this application

Cluster lot option

Not applicable to this application

Variance Request

The applicant requests a variance from Section 4.2.5(a) of the Subdivision Regulations. Variances are needed for building setbacks, lot depth, lot area, and lot frontage.

Section 1-11, Variances, permits the Planning Commission to grant variances to the Subdivision Regulations when it finds that extraordinary hardships or practical difficulties may result from strict compliance with the regulations. While the regulations grant the Commission the authority to grant variances, the regulations state that “such variance shall not have the effect of nullifying the intent and purpose of the Subdivision Regulations.” In order to grant a variance the Commission must find that:

1. The granting of the variance shall not be detrimental to the public safety, health, or welfare or injurious to other property or improvements in the neighborhood in which the property is located.
2. The conditions upon which the request for a variance is based are unique to the property for which the variance is sought and are not applicable generally to other property.
3. Because of the particular physical surroundings, shape, or topographical conditions of the specific property involved, a particular hardship to the owner would result, as distinguished from a mere inconvenience, if the strict letter of these regulations were carried out.
4. The variance shall not in any manner vary from the provisions of the adopted General Plan, including its constituent elements, the Major Street Plan, or the Zoning Code for Metropolitan Nashville and Davidson County (Zoning Code).



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Variance Analysis

The applicant is requesting a variance under Section 1-11, which permits the Planning Commission to grant variances to the Subdivision Regulations when it finds that extraordinary hardships or practical difficulties may result from strict compliance with the regulations. Although both of the proposed lots do not appear to be inconsistent with surrounding parcels, Lot 2 does not meet the minimum building setback, depth, area or frontage requirements, and Lot 1 does not meet the minimum depth requirement. Staff does not find that granting a variance from Section 4-2.5(a) would be consistent with the community's vision to guide development in T2 Rural Neighborhood policies. The applicant has provided information for the property, but staff has not identified a unique hardship specific to the property.

FIRE MARSHAL RECOMMENDATION

N/A

STORMWATER RECOMMENDATION

Approved with conditions

- Cite the new flood panel information to the plans (231 H and 4/5/2017)

PUBLIC WORKS RECOMMENDATION

Approved

TRAFFIC AND PARKING RECOMMENDATION

Approved

WATER SERVICES RECOMMENDATION

Approved with conditions

- MWS recommends approval for public water issues only. Metro Health Department must approve all on-site septic systems.

METRO HEALTH RECOMMENDATION

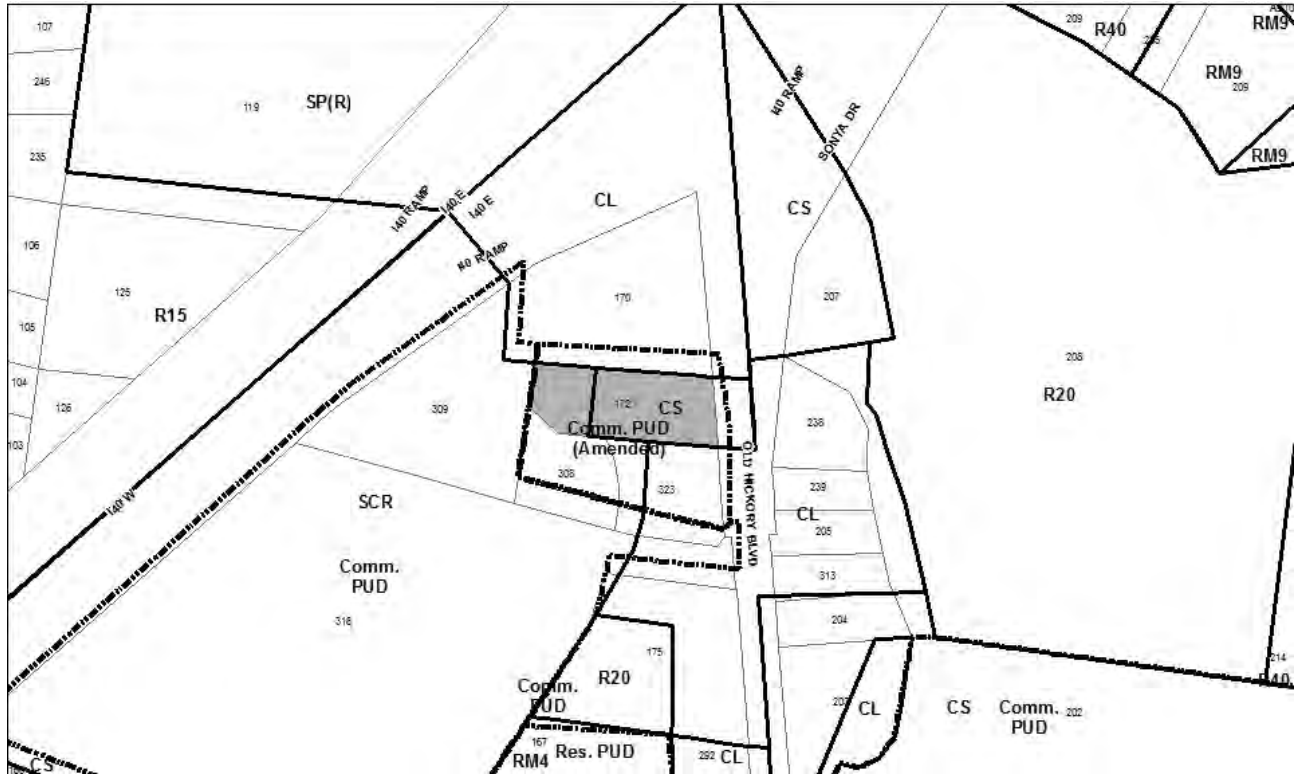
Disapproved

STAFF RECOMMENDATION

Staff recommends disapproval.



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88P-040-001
COMFORT INN AND SUITES
Map 114, Parcel(s) 172
06, Bellvue
22 (Sheri Weiner)



Project No. **Planned Unit Development 88P-040-001**
Project Name **Comfort Inn and Suites (Revision)**
Council District 22 – Weiner
School District 9 – Frogge
Requested by Civil and Environmental Consultants, applicant; ORO Hotels, LLC, owner.

Deferrals This item was deferred at the March 9, 2017, Planning Commission meeting. No public hearing was held.

Staff Reviewer Napier
Staff Recommendation *Approve with conditions.*

APPLICANT REQUEST
Revision to a PUD.

Revise Preliminary PUD

A request to revise the preliminary plan for a Planned Unit Development Overlay District to permit a hotel on property located at 627 Old Hickory Boulevard, approximately 260 feet southwest of Sonya Drive, zoned Commercial Service (CS) and Shopping Center Regional (SCR) (1.34 acres).

Existing Zoning

Commercial Service (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.

Shopping Center Regional (SCR) is intended for high intensity retail, office, and consumer service use for a regional market area.

Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provision of an adequate roadway system or essential utilities and services. In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provision of essential utilities and streets.

CRITICAL PLANNING GOALS

N/A

NOTICES

After a preliminary review, planning staff made the determination that the proposal would be processed as an amendment to the PUD. PUD amendments require public notification and council approval. Upon further review, planning staff determined the initial square footage calculations of the PUD were inaccurate, and therefore the request is within the parameters of a revision as outlined in the Zoning Code. A revision does not require mailed public notices or council approval.



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PLAN DETAILS

The site is located along Old Hickory Boulevard southeast of Interstate 40 and is surrounded by primarily commercial uses with some multi-family residential uses located to the south of the site. The site is currently vacant and zoned CL, which permits hotel uses.

History

Metro Council approved the PUD in 1988 for 279,275 square feet. This site was added to the PUD through an amendment in 2001. The amendment approved a 10,900 square foot one-story automobile service use which increased the total square footage to 290,175 for the PUD. At present 138,660 square feet of floor area has been constructed within the PUD. Therefore, the PUD has 151,515 square feet of floor area remaining. This request will use 46,676 square feet of the approved floor area within the PUD.

Site Plan

The plan revises the PUD to allow a 5-story hotel with a total of 80 rooms. An existing centrally located access drive will provide access to the site. This private drive currently provides access to the majority of the lots within the PUD. A second point of access shown on the site plan will provide access for emergency vehicles only. The proposed structure is located within the center of the site, outside of the areas of steep slopes.

The majority of the parking is located at the rear of the site behind the proposed structure, with some parking stalls located in front of the building. The proposed parking meets the Metro Zoning Code parking standards. A 12 foot multi use path and an 8 foot grass strip will be installed along Old Hickory Boulevard. Old Hickory Boulevard is designated as a Scenic Arterial Boulevard, which requires a 10 foot landscape easement along the frontage of the site.

ANALYSIS

Section 17.40.120.F permits the Planning Commission to approve modifications to a master development plan under certain conditions.

F. Changes to a Planned Unit Development District.

1. Modification of Master Development Plan. Applications to modify a master development plan in whole or in part shall be filed with and considered by the planning commission according to the provisions of subsection A of this section. If approved by the commission, the following types of changes shall require concurrence by the metropolitan council in the manner described:
 - a. Land area being added or removed from the planned unit development district shall be approved by the council according to the provisions of Article III of this chapter (Amendments);
 - b. Modification of special performance criteria, design standards, or other requirements specified by the enacting ordinance shall be authorized by council ordinance;
 - c. A change in land use or development type beyond that permitted by the specific underlying zoning district shall be authorized only by council ordinance; or
 - d. An increase in the total number of residential dwelling units above the number last authorized by council ordinance or, for a PUD district enacted by council ordinance after September 1, 2006, an increase in the total number of residential dwelling units above the



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- number last authorized by council ordinance or above the number last authorized by the most recent modification or revision by the planning commission; or
- e. When a change in the underlying zoning district is associated with a change in the master development plan, council shall concur with the modified master development plan by ordinance.
 - f. Any modification to a master development plan for a planned unit development or portion thereof that meets the criteria for inactivity of Section 17.40.120.H.4.a.

The revised plan and proposed development is consistent with the concept of the approved Planned Unit Development and does not include any unapproved uses or increase in floor area. No changes are proposed that conflict with the Council approved plan.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues will be addressed in the permit phase.

STORMWATER RECOMMENDATION

Approved

HARPETH VALLEY UTILITY DISTRICT

Approved with conditions

- The developer must submit utility plans or any water or sewer improvements required to serve this development to the District for review and approval. The developer must also submit hydraulic calculations, which indicate that the existing water system infrastructure and any required improvements will support domestic demands, and meet fire flow requirements as set by the local building authority and maintain a minimum pressure of 50 psi within the public distribution system including all water service meters. The static hydraulic grade line use for these calculations must be 710 feet or less.
- All extensions water lines and sewer lines will be at the expense of the developer. The design of all water lines shall be in accordance with the Harpeth Valley Utility District standard specifications and TDEC design criteria. All plans are subject to review and approval of Harpeth Valley and the State of Tennessee.

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works, in effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable. Final design may vary based on field conditions.
- If sidewalks are required, indicate sidewalk construction along OHB in compliance with the MCSP and MPW standards and specifications.

TRAFFIC AND PARKING RECOMMENDATION

Approved with conditions

- Developer shall install a stop sign at SB approach of Sonic /Comfort Inn access with Sam's Club access road.
- Developer shall gate and sign emergency access at Old Hickory Blvd.



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STAFF RECOMMENDATION

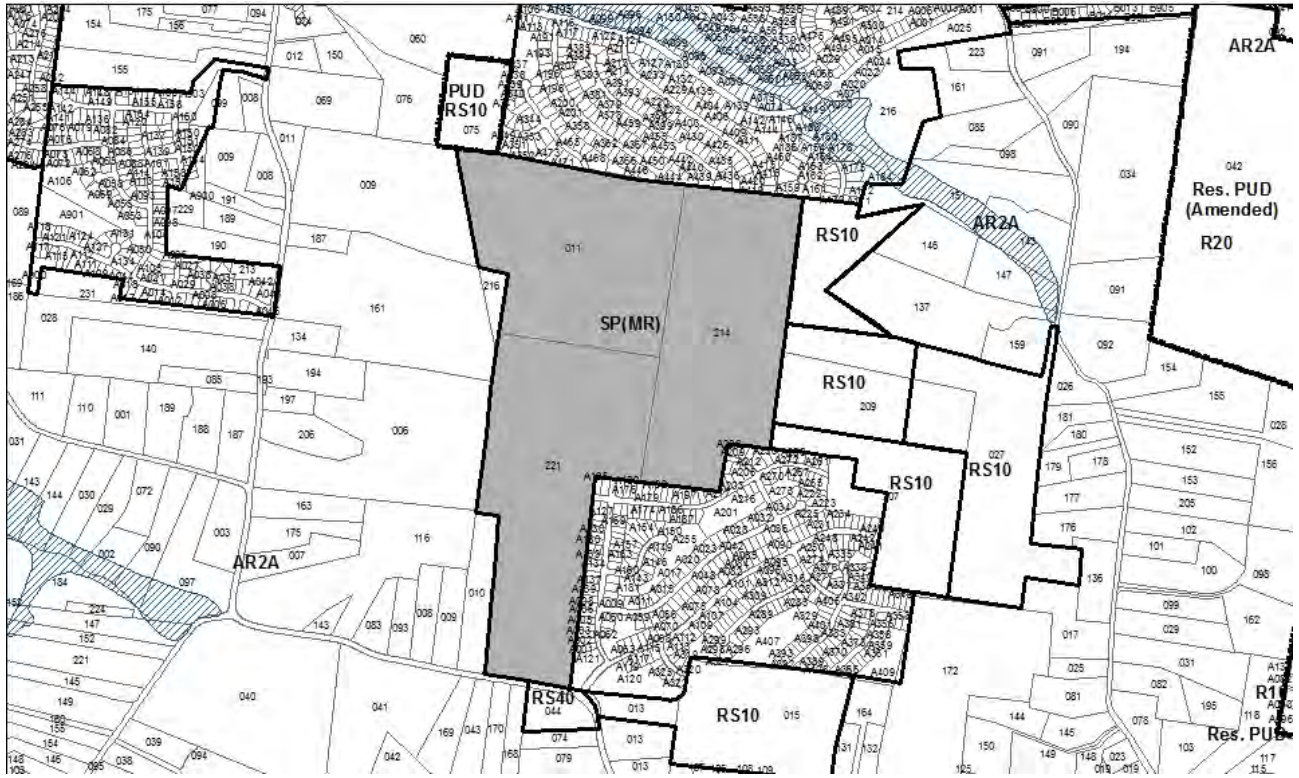
Staff recommends approval with conditions.

CONDITIONS

1. Permitted land uses shall be limited to a hotel with a maximum of 80 rooms.
2. Height is limited to 5 stories within 69 feet.
3. There shall be no pole signs allowed, and all free standing signs shall be monument type not to exceed five feet in height. Changeable LED, video signs or similar signs allowing automatic changeable messages shall be prohibited. All other signs shall meet the base zoning requirements, and must be approved by the Metro Department of Codes Administration.
4. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
5. If the PUD final site plan or final plat indicates that there is less acreage than what is shown on the approved preliminary plan, the final site plan shall be appropriately adjusted to show the actual total acreage, which may require that the total number of dwelling units or total floor area be reduced.



Metro Planning Commission Meeting of 03/23/2017



2006SP-181-001
EVERGREEN HILLS SP(AMENDMENT)
Map 182, Parcel(s) 011, 214, 221
12, Southeast
31 (Fabian Bedne)



Project No.	Specific Plan 2006SP-181-001
Project Name	Evergreen Hills SP (Amendment)
Council District	31 – Bedne
School District	6 – Hunter
Requested by	Anderson Delk Epps and Associates, applicant; Evergreen Hills, G.P., owner.
Staff Reviewer	Napier
Staff Recommendation	<i>Approve with conditions and disapprove without all conditions.</i>

APPLICANT REQUEST

Amend SP to permit up to 634 residential units.

Preliminary SP

A request to amend the Evergreen Hills SP on properties located at 13880 Old Hickory Boulevard and Old Hickory Boulevard (unnumbered), approximately 2,250 feet east of Pettus Road, zoned Specific Plan-Mixed Residential (SP-MR) (188.7 acres), to allow up to 634 residential units.

Existing Zoning

Specific Plan-Mixed Residential (SP-MR) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes a mixture of housing types. *The current SP is approved for 900 residential units.*

Proposed Zoning

Specific Plan-Mixed Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes a mixture of housing types. *The proposed SP allows for 634 residential units.*

SOUTHEAST COMMUNITY PLAN

Conservation (CO) is intended to preserve and enhance environmentally sensitive land in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they area in and whether or not they have already been disturbed.

T3 Suburban Neighborhood Evolving (T3 NE) policy is intended to create and enhance suburban residential neighborhoods with more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate density development patterns with moderate setbacks and spacing between buildings. T3 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a



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Proposed Site Plan



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different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network, block structure, and proximity to centers and corridors. T3 NE areas are developed with creative thinking in environmentally sensitive building and site development techniques to balance the increased growth and density with its impact on area streams and rivers.

Consistent with Policy?

The existing SP Plan was approved in November of 2006. It included a number of positive features that supported the policy, including a mixture of housing types and 49 acres of open space. The proposed SP is limited to single-family homes and includes a similar open space plan which includes a total of 51.6 acres of open space. While staff strongly supports a diversity of housing types in neighborhoods, depending on the context and policy, both single-family and more moderate density patterns can be appropriate for the T3 Suburban Neighborhood Evolving policy. Staff recommends several conditions to improve pedestrian access to the open spaces in the current proposal. Further, the previously approved SP Plan included lots located on steep slopes and sensitive areas. Staff recommends conditions that remove lots in these areas to protect the sensitive environmental areas within the site and meet the goals of the Conservation policy. On balance, with the conditions that staff is proposing to improve open spaces and protect environmentally sensitive areas, the amended SP will improve the existing plan, even if limited to single-family homes.

PLAN DETAILS

The approximately 188.7 acre site is north of Old Hickory Boulevard. The property is vacant, and densely forested. Adjacent subdivisions include Cane Ridge Farms to the north and Old Hickory Hill to the southeast.

Site Plan

The plan calls for a maximum of 634 residential units with a minimum lot size of 5,000 square feet. The SP includes 17 phases of development and provides four different lot sizes:

- 311 – 50 foot wide lots (average lot size 5,625 square feet)
- 176 – 55 foot wide lots (average lot size 6,187 square feet)
- 121 – 60 foot wide lots (average lot size 6,750 square feet)
- 26 – 65 foot wide lots (average lot size 7,312 square feet)

Lots range in depth from 100 feet to 115 feet. The average lot size of all lots is 6,648 square feet. The plan provides architectural standards pertaining to window orientation, primary entrances, glazing, and building materials. The SP limits the height for single-family homes to three stories in 35 feet.

All streets will be public and future potential connections are provided to the east and west. In areas with adjacent existing streets, connections are shown on the plan. The street connections shown on the SP are generally consistent with the street connections shown on the previously approved SP plan. Sidewalks will be constructed along all streets consistent with the local standard, four foot grass strip and five foot sidewalk. The Major and Collector Street Plan indicates three future collector streets through the property, two of which are incorporated into the plan. This property is



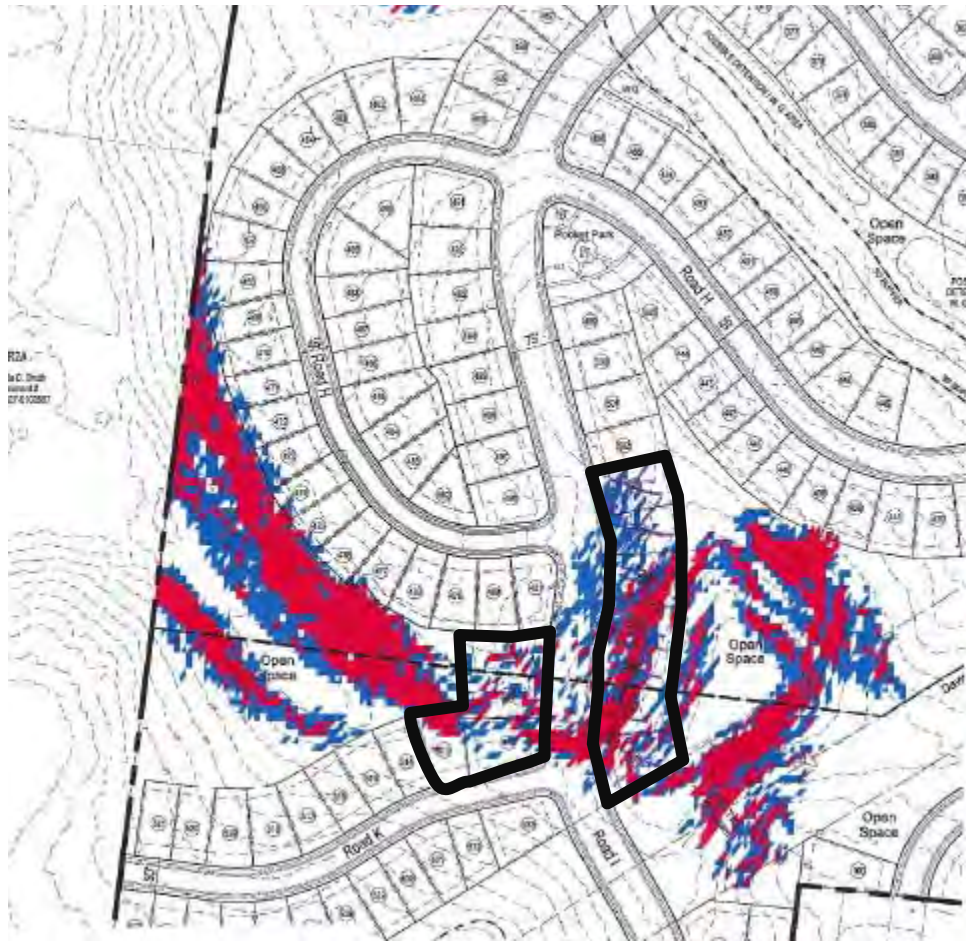
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bisected by a 200 foot TVA easement which currently contains high voltage transmission lines and is shown on the site plan as a future corridor for the Southeast Collector street.

ANALYSIS

The proposed lot size is consistent with the existing lots in the adjacent neighborhood to the north and southeast of this site. Each of these neighborhoods contains single-family homes with front loaded, non-recessed garages. This SP will provide various lot layouts which provide non-recessed garages, recessed garages, and side loaded garages. This will provide a varying character of single-family housing throughout the neighborhood.

The site includes some sensitive environmental features. The proposed site plan provides larger stream buffers than the previously approved plan. The previously approved plan contained 25-foot stream buffers while the proposed plan contains 30 foot stream buffers. The site also contains some areas with steep slopes in excess of 25 percent, partially shown in red below. Staff recommends removing ten lots located in steep slopes areas, outlined in black below:



The proposed site plan contains fewer, but larger open space areas than were shown on the previously approved SP plan. The previously approved plan contained 49 acres of open space, much of which was configured into small courtyards and public squares placed along the collector



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streets. The proposed site plan provides 51.6 acres of total open space, which includes 20.6 acres of usable open space. Approximately 13.1 acres of the total open space, outside of the usable area, is reserved for the Southeast Collector street which bisects the property from the southwest to the northeast. As proposed, several of the parks are accessed through a pathway. These may feel like the backyards of the surrounding homes, not useable space for the neighborhood. Therefore, staff recommends removing lots to open these spaces up for neighborhood use. Staff recommends removing the lots, shown below highlighted in red:



The previously approved site plan for this SP was consistent with the goals of the T3 NE policy. The proposed plan has removed the mixture of housing type in favor of single-family lots. However, with the conditions proposed by staff, the proposed development layout will protect environmentally sensitive areas, provide a moderate amount of open space, and provide high levels of pedestrian and vehicular connectivity. Staff recommends approval as the SP plan is consistent with policy.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues will be addressed in the permit phase.

STORMWATER RECOMMENDATION

Approved with conditions

- Any areas determined to be waters of the state / or required to have buffers (per Metro) shall be located in Open Space unless an approved variance is received.



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WATER SERVICES RECOMMENDATION

Approved with conditions

- Approved as a Preliminary SP Amendment only. Public water and sewer construction plans must be submitted and approved prior to Final SP approval. These approved construction plans must match the Final Site Plan/SP plans. The required capacity fees must also be paid prior to Final Site Plan/SP approval.

PUBLIC WORKS RECOMMENDATION

Approved

TRAFFIC & PARKING RECOMMENDATION

Approved with conditions

In accordance with the findings of the TIS, developer shall construct the following roadway improvements.

- Developer shall construct the proposed collector access road off Old Hickory Blvd with first phase of subdivision development.

Intersection of Cane Ridge Road and Blairfield Drive:

- Developer shall construct a right turn lane for the southbound approach of Cane Ridge Road to Blairfield Drive. The turn lane should include a minimum of 100 feet of storage.
- Additional traffic analysis may be required to verify adequate turn lane storage at all improved intersections specifically if an additional school site is located in Evergreen Hills subdivision.

Intersection of Burkitt Road and Old Hickory Boulevard:

- Developer shall install a traffic signal at the intersection of Burkitt Road and Old Hickory Boulevard. Developer shall design signal plan and submit to MPW traffic engineer for approval by MPW traffic engineer prior to 100 Use and Occupancy permits for subdivision.
- Developer shall construct a left turn lane for the northbound approach of Burkitt Road to Old Hickory Boulevard. The turn lane should include a minimum of 100 feet of storage.
- Developer shall construct a right turn lane for the southbound approach of Old Hickory Boulevard to Old Hickory Boulevard/Burkitt Road. The turn lane should include a minimum of 200 feet of storage.
- Upon installation of a traffic signal at the intersection, the existing Stop Ahead sign facing eastbound traffic on Old Hickory Boulevard, which is located on the south side of Old Hickory Boulevard approximately 540 feet west of Old Hickory Boulevard/Burkitt Road, should be removed and replaced with a Signal Ahead sign (W3-3) and an "Ahead" plaque (W16-9P).

Traffic Management Plan

- A traffic management plan for construction of the Evergreen Hills development should be developed and implemented.
- This plan should include recommended measures for controlling the speed of construction vehicles through residential areas and making repairs of any damages incurred by construction.



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Intersection of Old Hickory Boulevard and Pettus Road (North):

- Developer shall install a traffic signal or construct a single lane roundabout at the intersection of Old Hickory Boulevard and Pettus Road (north) if there is available ROW. The traffic signal shall be installed or roundabout constructed prior to the platting of 301 lots.
- If a traffic signal is installed at the intersection of Old Hickory Boulevard and Pettus Road (north), developer shall include a right-turn overlap phase for the eastbound approach of Old Hickory Boulevard, and protected/permissive left turn signal phasing for the northbound approach of Old Hickory Boulevard. Developer shall design and submit signal plan to MPW traffic engineer and install signal when approved.
- Developer shall construct a left turn lane for the northbound approach of Old Hickory Boulevard to Old Hickory Boulevard/Pettus Road. The turn lane should include a minimum of 100 feet of storage.
- Developer shall construct a right turn lane for the eastbound approach of Old Hickory Boulevard to Old Hickory Boulevard/Pettus Road. The turn lane should include a minimum of 200 feet of storage.
- The signal plan shall indicate that the existing Turn sign and “20 MPH” advisory speed limit plaque facing eastbound traffic on Old Hickory Boulevard, which is located on the south side of Old Hickory Boulevard approximately 735 feet west of Pettus Road/Old Hickory Boulevard, should be removed and replaced with a Signal Ahead sign (W3-3) and an “Ahead” plaque (W16-9P).
- The existing Stop Ahead sign facing southbound traffic on Pettus Road which is located on the west side of Pettus Road approximately 315 feet north of Old Hickory Boulevard, should be removed and replaced with a Signal Ahead sign (W3-3) and an “Ahead” plaque (W16-9P).
- A Signal Ahead sign (W3-3) and an “Ahead” plaque (W16-9P) should be provided on the east side of Old Hickory Boulevard facing northbound traffic located in advance of the existing utility pole that is located approximately 550 feet south of Old Hickory Boulevard/Pettus Road.

Intersection of Old Hickory Boulevard and New Site Access

- Developer shall construct the southbound approach of the new site access with one left turn lane and one right turn lane.
- Developer shall construct a left turn lane should be provided for the eastbound approach of Old Hickory to the new site access. The turn lane should include a minimum of 75 feet of storage.
- In order to provide sufficient intersection sight distance and visibility for vehicles turning left from the new site access onto Old Hickory Boulevard, the access road should be located approximately 85 feet east of the location shown on the current concept plan, which results in a location that is approximately 635 feet west of Legacy Drive. Internal drives should align with the access location on Old Hickory Boulevard.
- The final design for the new site access on Old Hickory Boulevard and approaches to the new site access should be completed such that departure sight triangles, as specified in AASHTO, will be clear of all potential sight obstructions, including vertical and horizontal curvature, landscaping, existing trees and vegetation, decorative monument signs/walls, fences, building faces, etc. Sight Distance exhibit may be required.



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Maximum Uses in Existing Zoning District: **SP(MR)**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential(210)	173.9		900 U	7850	640	775

Maximum Uses in Proposed Zoning District: **SP**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential*(210)	188.7		634 U	5687	454	566

*Based on two-family lots

Traffic changes between maximum: **SP(MR) and SP**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	-2,163	-186	-209

METRO SCHOOL BOARD REPORT

Projected student generation existing SP-MR district: 135 Elementary 81 Middle 81 High

Projected student generation proposed SP-R district: 139 Elementary 85 Middle 94 High

The proposed SP-MR zoning district would generate 21 more students than what is typically generated under the existing SP-MR zoning district. Students would attend A.Z. Kelly Elementary, Marshall Middle School and Cane Ridge High School. There is capacity for additional students in all three schools. This information is based upon data from the school board last updated November 2016. The unit count and unit type for this SP has been altered from the previously approved SP, removing multi-family housing in favor of single-family housing. Therefore, the school calculator has shown more students will be generated as a result. A school site dedication is required when the number of potential students exceeds 100.

School Site Dedication

Due to the potential impact of this development on the public school system, the applicant is required by Planning Commission policy to offer for dedication a school site in compliance with the standards of Section 17.16.040 for middle schools with a capacity of 800 students.

This land dedication requirement is proportional to the development's student generation potential. Such site shall be in accordance with the site condition and location criteria of the Metropolitan Board of Education and shall be within the Cane Ridge High School cluster. The Board of Education may decline such dedication if it finds that a site is not needed or desired. No final site plan for development of any residential uses on the site shall be approved until a school site has been dedicated to the Metro Board of Education or the Board has acted to relieve the applicant of this requirement. However, failure of the Board of Education to act prior to final site plan consideration and approval by the Metropolitan Planning Commission in accordance with its schedule and requirements shall constitute a waiver of this requirement by the Board of Education.



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AFFORDABLE AND WORKFORCE HOUSING REPORT (information provided by applicant)

1. Will this project include any affordable or workforce housing units? No
2. If so, how many and what is the percentage of the entire development? N/A
3. How will you enforce the affordability requirements? N/A
4. Have any structures been demolished in the last 12 months? No

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Uses in the SP shall be limited to a maximum of 634 single-family lots.
2. Lots 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, and 513 shall be removed with the submittal of the final site to eliminate lots located on steep slopes.
3. Lots 400, 401, and 402 or lots 391, 392, and 393 shall be removed with the submittal of the final site plan to provide open space access.
4. Lots 323, 324, 325, 326, and 327 shall be removed with submittal of the final site plan to provide open space access.
5. Lots 370 and 369 shall be removed with submittal of the final site plan to provide open space access.
6. Due to the potential impact of this development on the public school system, the applicant is required by Planning Commission policy, to offer for dedication a school site in compliance with the standards of Section 17.16.040 for middle schools with a capacity of 800 students. This land dedication requirement is proportional to the development's student generation potential. Such site shall be in accordance with the site condition and location criteria of the Metropolitan Board of Education and shall be within the Cane Ridge High School cluster. The Board of Education may decline such dedication if it finds that a site is not needed or desired. No final site plan for development of any residential uses on the site shall be approved until a school site has been dedicated to the Metro Board of Education or the Board has acted to relieve the applicant of this requirement. However, failure of the Board of Education to act prior to final site plan consideration and approval by the Metropolitan Planning Commission in accordance with its schedule and requirements shall constitute a waiver of this requirement by the Board of Education.
7. Comply with all conditions of Public Works in regards to the TIS.
8. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RS5 zoning district for single-family lots as of the date of the applicable request or application.
9. The final site plan/ building permit site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.
10. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
11. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.



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12. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
13. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



SEE NEXT PAGE



Metro Planning Commission Meeting of 03/23/2017



2016SP-062-001

Earhart Subdivision

Map 098, Parcel(s) 093

14, Donelson - Hermitage

12 (Steve Glover)



Project No.	Specific Plan 2016SP-062-001
Project Name	Earhart Subdivision SP
Council District	12 - Glover
School District	4 - Shepherd
Requested by	T-Square Engineering, Inc., applicant; KDS Investments GP, owner.
Staff Reviewer	Napier
Staff Recommendation	<i>Defer to the April 13, 2017, Planning Commission Meeting.</i>

APPLICANT REQUEST

Zone change to permit up to 181 residential units.

Preliminary SP

A request to rezone from Single-Family Residential (RS15) to Specific Plan-Residential (SP-R) zoning for property located at Earhart Road (unnumbered), approximately 1,000 feet south of John Hagar Road, (69.76 acres).

STAFF RECOMMENDATION

Staff recommends deferral to the April 13, 2017, Planning Commission meeting at the request of the applicant.



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2017SP-008-001

400 HUME STREET SP

Map 082-05, Parcel(s) 067-069

Map 082-05-0-B, Parcel(s) 001-003

Map 082-05-0-F, Parcel(s) 001-003

08, North Nashville

19 (Freddie O'Connell)



Project No. Specific Plan 2017SP-008-001
Project Name 400 Hume Street SP
Council District 19 – O’Connell
School District 1 – Gentry
Requested by 4Site, Inc., applicant; Julius B. Olds, Carolyn F. and Harry Lee Johnson, O.I.C. 1603 4th Avenue North Townhomes and O.I.C. Hume Street Patio Homes Condos, owners.

Staff Reviewer Birkeland
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST
Zone change to permit a residential development.

Preliminary SP
 A request to rezone from One and Two-Family Residential (R6) to Specific Plan-Residential (SP-R) zoning on properties located at 400, 402, 404, 406, 408 and 408 B Hume street and 1603 A, 1603 B and 1603 C 4th Avenue North, at the northwest corner of Hume Street and 4th Avenue North, (0.8 acres), to permit 18 residential units.

Existing Zoning
One and Two-Family Residential (R6) requires a minimum 6,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 7.71 dwelling units per acre including 25 percent duplex lots. *R6 would permit a maximum of 5 lots with 1 duplex lot for a total of 6 units.*

Proposed Zoning
Specific Plan-R (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes two residential building types.

CRITICAL PLANNING GOALS
 N/A

NORTH NASHVILLE COMMUNITY PLAN
T4 Urban Neighborhood Evolving (T4 NE) is intended to create and enhance urban residential neighborhoods that provide more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate to high density development patterns with shallow setbacks and minimal spacing between buildings. T4 NE areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. T4 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network and block structure and proximity to centers and corridors.



Metro Planning Commission Meeting of 03/23/2017



Proposed Site Plan



Metro Planning Commission Meeting of 03/23/2017

Consistent with Policy?

Yes. The proposed SP is consistent with the T4 Suburban Neighborhood Evolving Policy. The T4 NE policy is a residential policy intended to enhance urban neighborhoods with opportunities for improved pedestrian, bicycle, and vehicular connectivity. The policy supports a range of housing choices. The proposed development is redeveloping urban lots and provides for additional housing choice within the neighborhood.

PLAN DETAILS

The properties included in this SP are located along Hume Street and 4th Avenue North and are currently zoned R6. The existing zoning district allows for one and two-family residential uses. The site is located across the street from Morgan Recreation center.

Site Plan

The plan includes 18 residential dwelling units. Five units are oriented toward Hume Street and six units are oriented toward 4th Avenue North. Three stacked flat units are oriented at the corner of Hume Street and 4th Avenue North. Four units are located interior to the site and separated by a drive aisle. The height is limited to 4 stories and 45 feet. The building will consist of 3 stories of residential units with a rooftop patio/common space above the three stacked flats in the middle of the building at the corner of Hume Street and 4th Avenue North.

Vehicular access is limited to the alley by a private drive. The alley will be widened and improved per Metro Code standards. All units will have a two car garage. The plan proposes seven parking stalls for guest parking. Sidewalks along Hume Street and 4th Avenue North will be upgraded to meet the local street standards of a five foot sidewalk and a four foot wide grass strip. The site is surrounded by one and two-family residential units as well as multi-family buildings ranging from one to three stories in height. The neighborhood has experienced redevelopment of residential units along Hume Street in the form of Specific Plans and under existing MUG zoning. The proposed SP will provide an additional housing type that is consistent with the development in the area

Landscaping is provided along both street frontages and interior to the site.

Analysis

The plan is consistent with the T4 Suburban Neighborhood Evolving policy as it provides additional housing options in the area. Sidewalks will be improved along the frontage to provide an improved pedestrian environment to the existing sidewalks along Hume Street and 4th Avenue North.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues will be addressed in the permit phase.

STORMWATER RECOMMENDATION

Approve with conditions

- Provide the FEMA Note / Information to plans (use 2017 panel number and date).
- Add Preliminary Note to plans:

Drawing is for illustration purposes to indicate the basic premise of the development, as it pertains to Stormwater approval / comments only. The final lot count and details of the plan shall be governed by the appropriate stormwater regulations at the time of final application.



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WATER SERVICES RECOMMENDATION

Approved with conditions

- Approved as a Preliminary SP only. Public water and sewer construction plans must be submitted and approved prior to Final SP approval. FYI - no new combined sewers are allowed. Sanitary and storm sewer must be separated leaving the site; sanitary sewer may not empty into a open-grate catch basin. These approved construction plans must match the Final Site Plan/SP plans. The required capacity fees must also be paid prior to Final Site Plan/SP approval.

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works, in effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable. Final design may vary based on field conditions.
- Indicate on the plans the location of the recycling container.
- ROW dedication is to be recorded prior to MPW sign off on the building permit.

TRAFFIC AND PARKING RECOMMENDATION

Approve

Maximum Uses in Existing Zoning District: **R6**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two- Family Residential* (210)	0.8	7.26 D	7 U	67	6	8

*Based on two-family lots

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (230)	0.8	-	18 U	145	14	15

Traffic changes between maximum: **R6** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-		+78	+8	+7

METRO SCHOOL BOARD REPORT

Projected student generation existing R6 district: 1 Elementary 1 Middle 1 High

Projected student generation proposed SP-R district: 1 Elementary 0 Middle 0 High



Metro Planning Commission Meeting of 03/23/2017

The proposed SP zoning is expected to generate 2 less students than the existing R6 zoning. Students would attend Buena Vista Elementary School, John Early Middle School and Pearl-Cohn High School. All schools have capacity for additional students. This information is based upon data from the school board last updated November 2016.

AFFORDABLE AND WORKFORCE HOUSING REPORT (information provided by applicant)

1. Will this project include any affordable or workforce housing units? No.
2. If so, how many and what is the percentage of the entire development? N/A
3. How will you enforce the affordability requirements? N/A
4. Have any structures been demolished in the last 12 months? No.

STAFF RECOMMENDATION

The proposed SP is consistent with the T4 Neighborhood Evolving policy; therefore staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Uses shall be limited to a maximum of 18 residential units
2. Height shall be limited to a maximum of 4 stories in 45 feet, measured to the roofline.
3. Townhome units fronting Hume Street and 4th Avenue North shall have a porch or a stoop.
4. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM40-A zoning district as of the date of the applicable request or application.
5. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
6. The final site plan shall label all internal driveways as "Private Driveways". A note shall be added to the final site plan that the driveways shall be maintained by the Homeowner's Association.
7. The final site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.
8. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
9. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
10. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



Metro Planning Commission Meeting of 03/23/2017



2017SP-025-001
1424 STAINBACK AVENUE SP
Map 071-11, Parcel(s) 249
05, EAST NASHVILLE
05 (Scott Davis)



Project No.	Specific Plan 2017SP-025-001
Project Name	1424 STAINBACK AVENUE SP
Council District	05 – Davis
School District	5 – Buggs
Requested by	Councilmember Scott Davis, applicant; Matthew & Ariana Evans, owners.
Staff Reviewer	Napier
Staff Recommendation	<i>Approve with conditions and disapprove without all conditions.</i>

APPLICANT REQUEST

Rezone from RS5 to SP-R to permit a detached accessory dwelling unit (DADU).

Preliminary SP

A request to rezone from Single-Family Residential (RS5) to Specific Plan-Residential (SP-R) zoning for property located at 1424 Stainback Avenue, approximately 225 feet south of Gatewood Avenue, (0.30 acres) to permit a detached accessory dwelling unit (DADU).

Existing Zoning

Single-Family Residential (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre.

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan.

EAST NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Maintenance (T4 NM) is intended to preserve the general character of existing urban residential neighborhoods. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T4 NM areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Consistent with Policy?

Yes. The use of an existing structure as a Detached Accessory Dwelling Unit (DADU) is consistent with the T4 NM policy. When change occurs within the Neighborhood Maintenance area, the change should be sensitive to the existing neighborhood character. This SP request will require an existing structure to be re-used, therefore maintaining the existing character of the neighborhood. The DADU provides for an additional housing option while maintaining the existing character of the area.



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Proposed Regulatory SP Start

1424 Stainback Avenue Specific Plan (SP)

Development Summary	
SP Name	1424 Stainback Avenue
2016SP-086-001	2017SP-025-001
Council District	5
Map & Parcel	Map 071-11, Parcel 249

Site Data Table	
Site Data	0.30 acres
Existing Zoning	RS5
Proposed Zoning	SP-R
Allowable Land Uses	All uses permitted by the RS5 zoning district and the use of the existing accessory structure as a dwelling unit.

Proposed Specific Plan (SP) Standards

1. All uses permitted by the RS5 zoning district and the use of the existing accessory structure as a dwelling unit.
2. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RS5 zoning district as of the date of the applicable request or application.
3. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.

Proposed Regulatory SP End



Metro Planning Commission Meeting of 03/23/2017

ANALYSIS

Detached accessory dwelling units are not allowed within RS zoning districts. They are allowed within R zoning districts, if certain conditions are met. Because of the current base zoning, a detached accessory dwelling unit would not be permitted on the lot which is why the applicant has requested a Specific Plan.

The existing structure to be utilized as the detached accessory dwelling unit meets that standards that are required for a detached accessory dwelling unit allowed within an R zoning district. The living space does not exceed 700 square feet and the height does not exceed the height of the principle structure. The existing structure is also proportional to the principle structure in regards to mass.

The property located at 1424 Stainback Avenue contains three underlying lots which could be re-established by instrument and a single family dwelling constructed on each lot. Therefore, applying an SP to the site which allows for the use of an existing structure as a dwelling unit is less than existing development entitlements for the property. Staff recommends approval as the request to use an existing structure as an accessory dwelling unit provides an additional housing choice within in a maintenance land use policy, while maintaining the existing development pattern.

FIRE DEPARTMENT RECOMMENDATION

- N/A

STORMWATER RECOMMENDATION

- Approved

WATER SERVICES

Approved with conditions

- Approved as a Preliminary SP only. If DADU is to have a separate water meter, than capacity fees must be paid, prior to Final SP approval.

PUBLIC WORKS RECOMMENDATION

- N/A

TRAFFIC AND PARKING RECOMMENDATION

- N/A

Maximum Uses in Existing Zoning District: **RS5**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.3	8.7 D	1 U	10	1	2

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.3	8.7 D	2 U	10	1	2

Traffic changes between maximum: **RS5 and SP-R**



Metro Planning Commission Meeting of 03/23/2017

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	-	-	-

METRO SCHOOL BOARD REPORT

Projected student generation existing RS5 district: 0 Elementary 0 Middle 0 High

Projected student generation proposed SP-R district: 0 Elementary 0 Middle 0 High

The proposed SP-R zoning is not anticipated to generate no more students than what is typically generated under the existing SP-R zoning district. Students would attend Shwab Elementary School, Jere Baxter Middle School, and Maplewood High School. All three schools have been identified as having additional capacity. This information is based upon data from the school board last updated November 2016.

AFFORDABLE AND WORKFORCE HOUSING REPORT (information provided by applicant)

1. Will this project include any affordable or workforce housing units? no
2. If so, how many and what is the percentage of the entire development? n/a
3. How will you enforce the affordability requirements? n/a
4. Have any structures been demolished in the last 12 months? no

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Uses shall be limited to all uses permitted by the RS5 zoning district and the use of the existing accessory structure as an accessory dwelling unit.
2. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RS5 zoning district as of the date of the applicable request or application.



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Metro Planning Commission Meeting of 03/23/2017



2016S-001HM-001
3106 CURTIS STREET
Map 070-09, Parcel(s) 067
03, Bourdeaux – Whites Creek
02 (Decosta Hastings)



Project No.	House Move 2016S-001HM-001
Project Name	3106 CURTIS STREET
Council District	02 – Hastings
School District	1 – Gentry
Requested by	Toothman Structure Movers, applicant; Kyle Brown, owner.
Staff Reviewer	Napier
Staff Recommendation	<i>Approve with conditions.</i>

APPLICANT REQUEST

Move a house from 914 32nd Avenue North to 3106 Curtis Street.

House Move

A request to move a house from 914 32nd Avenue North to 3106 Curtis Street, approximately 210 feet southeast of Alpine Avenue, zoned One and Two-Family Residential (R8) (0.19 acres).

Existing Zoning

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes.

STATE LAW

Tennessee State Code (Title 13, Chapter 3, Part 5) regulates the relocation of a residence from one location to another location (house move).

13-3-502. Requirements for moving single family residence from one foundation to another.

(a) No single family residence shall be moved from an existing foundation to another foundation located within a developed area of single family residences unless:

(1) The residence to be moved is consistent with the age, value, size and appearance of existing residences within the developed area of single family residences to which the single family residence is to be moved; provided, that the value of the house may be greater than that of the existing residences and the size of the house may be larger than that of the existing residences; and

(2) Approval for the movement of the single family residence to a foundation within a developed area of single family residences has been given by:

- (A) The home owners' association of the development where the residence is to be moved, if a home owners' association is in existence;
- (B) A neighborhood association where the residence is to be moved that has been in existence for more than one (1) year prior to the date the residence is to be moved, if a neighborhood association is in existence in the area;
- (C) The regional planning commission, if a regional planning commission is in existence in the area where the residence is to be moved, and subdivision (a)(2)(A) or (B) does not apply;



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(D) The municipal planning commission, if a municipal planning commission is in existence in the municipality where the residence is to be moved and subdivision (a)(2)(A), (B) or (C) does not apply; or

(E) The municipal or county legislative body in the jurisdiction where the residence is to be moved, and subdivision (a)(2)(A), (B), (C) or (D) does not apply.

(b) As used in this section, single family residence does not include manufactured or modular homes as manufactured or modular homes are defined in § 47-9-102, § 55-1-105, or title 68, chapter 1, parts 1-4.

The residence is consistent with:

(1) The age of existing residences within the developed area of single family residences, if the residence to be moved is within ten (10) years of the average age of the existing structures within the developed area;

(2) (A) The value of existing residences within the developed area of single family residences, if the valuation of the residence being moved appraised, prior to being moved, at a value that is at least equal to the average appraisal of the existing structures within the developed area; provided, that nothing in this subdivision (2) shall be construed to prevent the residence from exceeding the value of the existing structures. In establishing the value of existing structures, the value of modular homes located in the developed area shall not be used in arriving at the average appraisal of the existing structures;

(B) If the value of the residence, prior to being moved, appraised at a value that is at least equal to the average appraisal of the existing structures within the developed area, then it shall be presumed that the residence shall appraise at least at the same or greater value once it is moved;

(C) In obtaining approval from a governing body identified in § 13-3-502, as proof that the value of the residence or appearance of the residence is consistent with the value or appearance of the existing residences, evidence may be presented that includes photographs of the inside and outside of the residence to be moved as well as the appraised value of the residence as determined by the assessor of property, or the fair market value of the residence as determined by an independent appraiser. The proof shall be a rebuttable presumption that the value and appearance of the residence is at least equal to the value and appearance of the existing structures within the developed area. Additional documents showing intended improvements may also be presented;

(3) The size of existing residences within the developed area of single family residences, if the size of the residence being moved is at least within one hundred square feet (100 sq. ft.) of the average size of the existing structures within the developed area; provided, that nothing in this subdivision (3) shall be construed to prevent the residence from exceeding the average square footage. In establishing the average size of existing structures, the square footage of modular homes shall not be used in making the calculations; and

(4) The appearance of existing residences within the developed area of single family residences as determined by the body giving its approval for the single family residence to be moved to the developed area.



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13-3-504. Structural improvements -- Timeframe requirements -- Penalties.

All structural improvements that will affect the value and appearance of a residence moved to a developed area of single family residences shall be made to the residence within one (1) year of being moved into the area. If within one (1) year of that date, improvements have not been completed on the residence at least equal to the average standards in the developed area, the residence shall be deemed to be in violation of the local government building codes, and penalties associated with the violations may begin to accrue in accordance with all relevant local building codes and ordinances adopted and in effect in the local jurisdiction where the residence is located.

ANALYSIS

The proposed location for the house move is 3106 Curtis Street. The subdivision in which the proposed move is located does not contain a Home Owner's Association (HOA) or a Neighborhood Association (NA). Since there is neither a HOA nor a NA, the law requires that the house move be approved by a governmental body, in this case the municipal planning commission.

The law requires that the residence being moved be consistent with the age, value, size and appearance of surrounding residences within the "developed area". The lot which is proposed as the new location for the house is PT LOT 2 H E SIMPKINS Subdivision. This subdivision constitutes the "developed area" as specified by the law.

The state law governing house moves allows an applicant to satisfy the requirements therein within a year of moving a structure to a new location. In this instance the structure does not meet the requirement for appraised value or size. The applicant has proposed an addition to the structure which would satisfy the requirements for appraised value and size. The addition proposed by the applicant would increase the total habitable square footage of the structure to 1,293 square feet, which would bring the house within 100 square feet of all the structures within the subdivision as required by the state law. The average appraised price per square foot for the existing structures in the subdivision is calculated at 71 dollars per square foot. Therefore, if the proposed improvements for the structure are completed the house will likely satisfy the remaining requirements of the state law.

- Age – The house's age must be within 10 years of the average age of existing single-family residential structures in the subdivision.
 - The structure which is proposed to be moved is 68 years old, the average age of the existing residential structures within the subdivision is 71 years old.
- Value – The house's appraised value must be at least the average appraised value of all existing single-family residential structures within the subdivision. Per state law, do not include manufactured or modular structures in the calculation of the average appraised value of existing single-family residences.
 - When the planned improvements are completed the structure will likely satisfy the requirement for appraised value.
- Size – The house's size must be within 100 square feet of the average square footage of existing single-family residential structures within the subdivision.
 - When the planned improvements are completed the structure will satisfy the requirement for square footage.



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- Appearance - The house to be moved must be consistent in appearance with existing residences in subdivision.
- The house which is proposed to be moved is consistent with massing and appearance with the existing structures along Curtis Street.

The house is consistent with homes on Curtis Street and the surrounding neighborhood. The majority of the existing homes within the neighborhood were constructed at the same time as the structure which is to be moved. The value of the home to be moved is consistent with the appearance and character of the surrounding homes. The applicant plans an addition that will satisfy the requirements for appraised value and size.

FIRE MARSHAL RECOMMENDATION

N/A

STORMWATER RECOMMENDATION

N/A

WATER SERVICES RECOMMENDATION

Condition if approved

- Permit through Codes Administration.

PUBLIC WORKS RECOMMENDATION

Condition if approved

- Permit through Codes Administration.

TRAFFIC AND PARKING RECOMMENDATION

Condition if approved

- Permit through Codes Administration.

STAFF RECOMMENDATION

Staff recommends approval with conditions

CONDITIONS

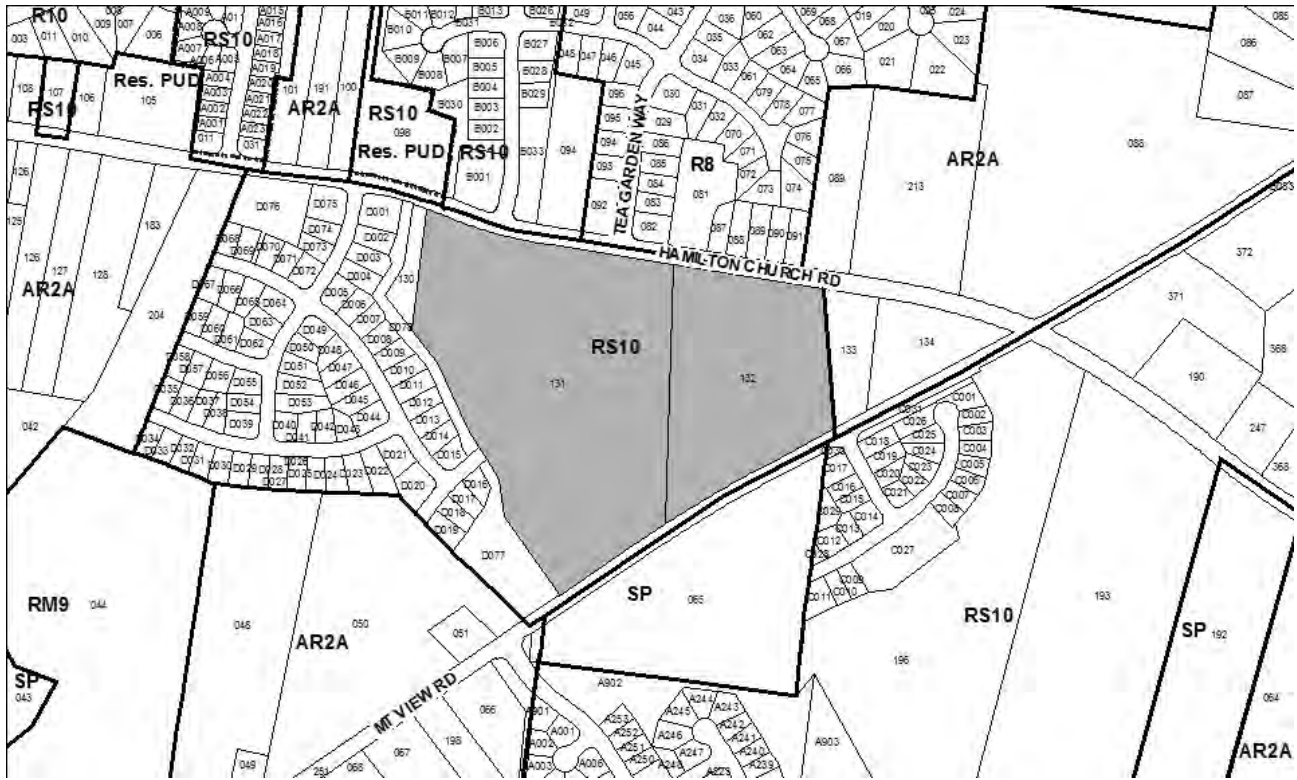
1. Within one year of the completed house move the applicant must complete the proposed addition to the structure.
2. Once the construction of the addition to the structure has been completed the applicant must submit documentation that for the value of the home to demonstrate compliance with section 13-3-502.b.2.a. of the State law governing house moves.



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Metro Planning Commission Meeting of 03/23/2017



2016S-102-001
KEENELAND DOWNS
Map 150, Parcel(s) 131-132
13, Antioch-Priest Lake
33 (Sam Coleman)



Project No.	Concept Plan 2016S-102-001
Project Name	Keeneland Downs
Council District	33- Coleman
School District	6 – Hunter
Requested by	Civil-Site Design Group, PLLC, applicant; KDNS Ventures LLC, owners.
Staff Reviewer	Burse
Staff Recommendation	<i>Defer to the April 13, 2017, Planning Commission meeting.</i>

APPLICANT REQUEST

A revision to an approved concept plan.

Concept Plan

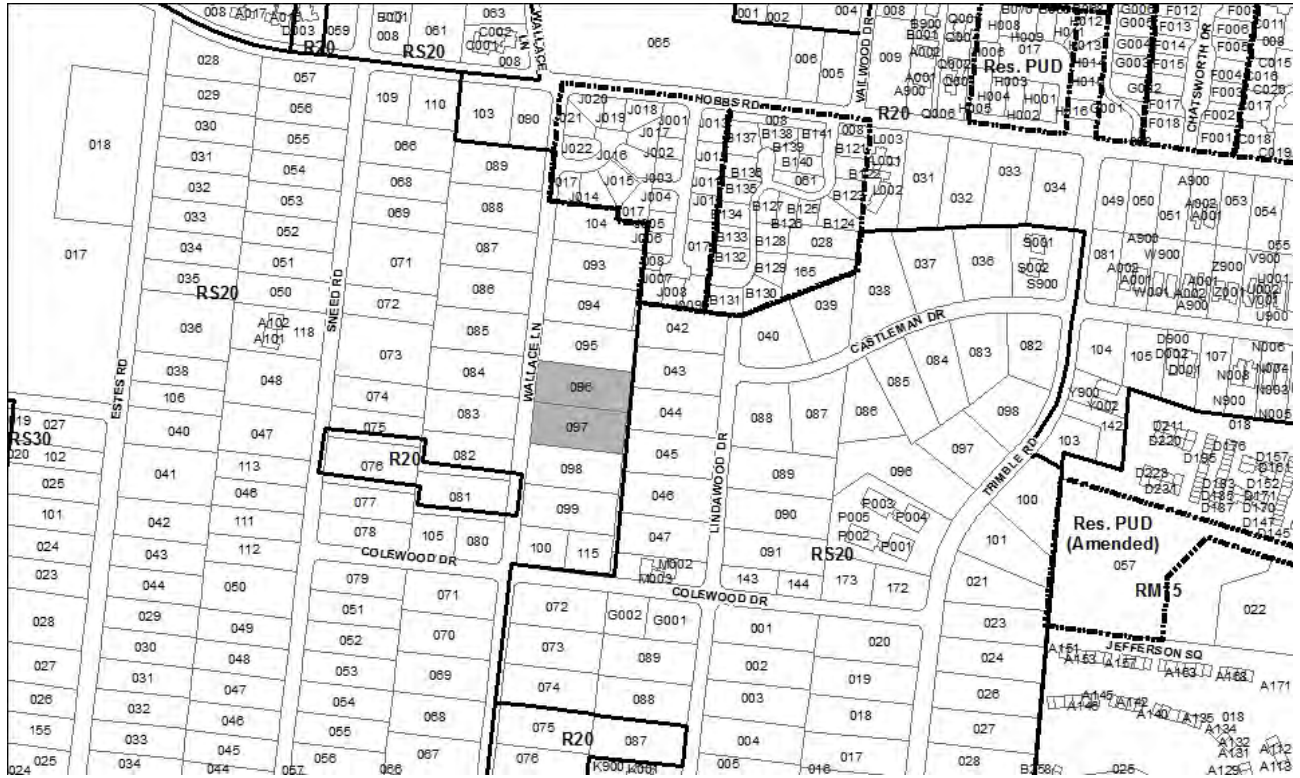
A request for a revision to an approved concept plan for approval to create up to 72 cluster lots on properties located at 6033 Mt. View Road and Mt. View Road (unnumbered), 160 feet northeast of Cottage View Lane (24.8 acres), zoned Single-Family Residential (RS10).

STAFF RECOMMENDATION

Staff recommends deferral to the April 13, 2017, Planning Commission meeting at the request of the applicant.



Metro Planning Commission Meeting of 03/23/2017



2017S-015-001
SNEED ESTATES SUBDIVISION
Map 130-04, Parcel(s) 096-097
10, Green Hills - Midtown
34 (Angie Henderson)



Project No.	Final Plat 2017S-015-001
Project Name	Sneed Estates Subdivision
Council District	34 - Henderson
School District	8 - Pierce
Requested by	Dbbs & Associates Engineering, applicant; Vintage 508, LLC, owner.
Staff Reviewer	Rickoff
Staff Recommendation	<i>Approve with conditions.</i>

APPLICANT REQUEST

Create three lots.

Final Plat

A request for final plat approval to create 3 lots on properties located at 4224 and 4228 Wallace Lane, approximately 870 feet south of Hobbs Road, zoned Single-Family Residential (RS20) (1.85 acres)

Existing Zoning

Single-Family Residential (RS20) requires a minimum 20,000 square foot lot and is intended for single-family dwellings at a density of 1.85 dwelling units per acre. *RS20 would permit a maximum of 3 units.*

CRITICAL PLANNING GOALS

N/A

GREEN HILLS-MIDTOWN COMMUNITY PLAN

T3 Suburban Neighborhood Maintenance (T3 NM) is intended to preserve the general character of developed suburban residential neighborhoods. T3 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T3 NM areas have an established development pattern consisting of low to moderate density residential development and institutional land uses. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

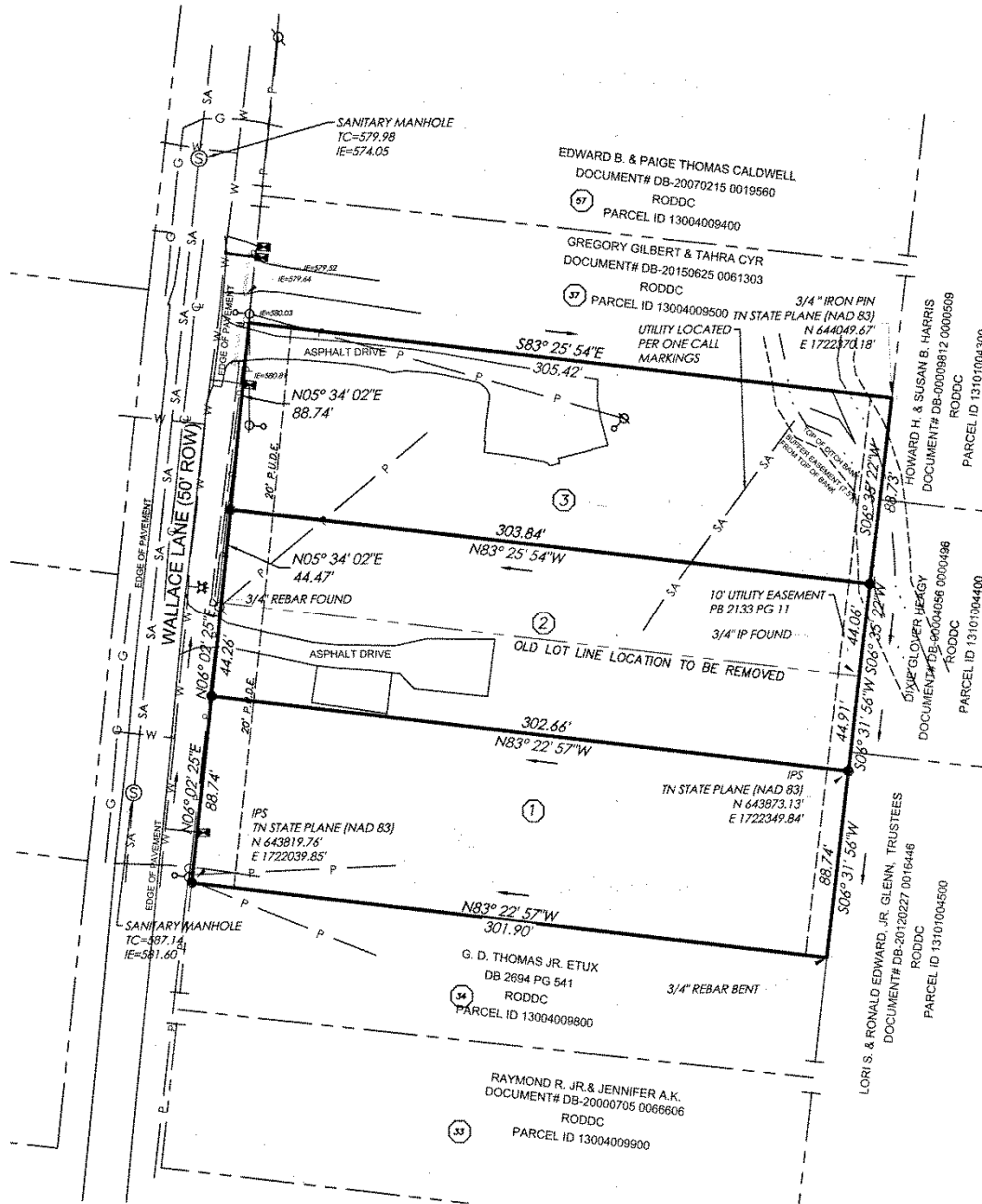
PLAN DETAILS

This request is for final plat approval to create three lots where two lots currently exist at 4224 and 4228 Wallace Lane, south of Hobbs Road. 4224 Wallace Lane previously contained a legally non-conforming duplex and 4228 Wallace Lane contained a single-family dwelling, both of which were recently demolished.

Section 3-5.2 of the Subdivision Regulations requires that newly created lots in areas that are previously subdivided and predominately developed must be comparable to the surrounding lots in regards to frontage and area. All three of the proposed lots meet the compatibility requirement for area. However, none of the three proposed lots meet the compatibility requirement for frontage.



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Proposed Subdivision



Metro Planning Commission Meeting of 03/23/2017

The applicant requests approval under Section 3-5.2(f) of the Subdivision Regulations, under which the Planning Commission may grant approval of a subdivision that does not meet the compatibility criteria if the subdivision can provide for harmonious development within the community.

The two existing lots are each approximately 40,400 square feet with 133 feet of frontage.

The three proposed lots are as follows:

- Lot 1: 26,824 sq. ft. and 88.74 feet of frontage
- Lot 2: 26,931 sq. ft. and 88.96 feet of frontage
- Lot 3: 27,029 sq. ft. and 88.74 feet of frontage

There are no sidewalks along Wallace Lane. If approved, sidewalks will be required with this development.

ANALYSIS

Lot Compatibility

Section 3-5.2 of the Subdivision Regulations outline the criteria for reviewing infill subdivisions located within a Neighborhood Maintenance policy area. The intent of these regulations is to consider the established development pattern when reviewing infill subdivisions. Staff reviewed the final plat against the following criteria as required by the Subdivision Regulations:

Zoning Code

Lots 1, 2, and 3 meet the minimum standards of the RS20 zoning district.

Street Frontage

Both lots would have frontage on a public street.

Density

The T3 Suburban Neighborhood Maintenance policy no longer includes density limitations.

Community Character

1. Lot frontage: The proposed lots must have frontage either equal to or greater than 70% of the average frontage of surrounding parcels or equal to or greater than the surrounding lot with the least amount of frontage, whichever is greater. Along Wallace Lane, lots created must have frontage at least equal to 111 feet. Lots 1, 2, and 3 have approximately 88 feet of frontage and do not meet compatibility requirements for frontage.

Frontage (Lots 1-3)	
Proposed Approximate Frontage	88 ft.
Minimum Frontage	111 ft.
70% Average	90.1 ft.

2. Lot size: The proposed lots must have lot area that is either equal to or greater than 70% of the lot size of the average size of surrounding parcels or equal to or larger than the smallest surrounding lot, whichever is greater. Along Wallace Lane, lots created must have at least 25,090 square feet. Lots 1, 2, and 3 meet compatibility for lot size requirements.



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	Lot 1 Size	Lot 2 Size	Lot 3 Size
Proposed Size	26,824 SF	26,931 SF	27,027 SF
Minimum Size	20,473 SF	20,473 SF	20,473 SF
70% Average	25,090 SF	25,090 SF	25,090 SF

3. Street setback: Where the minimum required street setback is less than the average of the street setback of the two parcels abutting either side of the lot proposed to be subdivided, a minimum building setback line shall be included on the proposed lots at the average setback. The minimum street setback required by the Zoning Code will be consistent with the setback of surrounding structures.
4. Lot orientation: Orientation of the proposed lots shall be consistent with the surrounding parcels. Lots 1, 3, and 3 front Wallace Lane and are consistent with the surrounding parcels.

Analysis

In this case, zoning entitlements would permit the reconstruction of a two-family structure at 4224 Wallace Lane since the former duplex was legally established and maintained in continuous operation for several decades. As such, staff – in consultation with the councilmember and community - evaluated two different development options in order to determine development implications on the surrounding neighborhood pattern, which is largely single-family.

- The first option would be for the properties to develop under existing entitlements. The likely result would be a duplex or two-family residence on one lot and a single family home on the second lot. The neighborhood expressed concerns about the aesthetic appearance of a duplex or two-family structure next to a single-family home. Therefore, staff evaluated a second option.
- The second option was a subdivision that would result in three equally spaced single-family lots, which would result in the same number of units.

The two options were presented at a neighborhood meeting on February 13, 2017. Feedback from adjacent property owners suggests an overall desire for three equally spaced single family lots.

Staff considered the existing entitlements, surrounding development pattern, and desires of the community and concluded that, in this case, a 3-lot, single-family subdivision would provide for more harmonious development along Wallace Lane and the surrounding area than reconstruction of a duplex on one larger lot. However, based on the Subdivision Regulation’s criteria for determining compatibility, the proposed lots do not meet frontage requirements. The Planning Commission may grant approval if it determines that the subdivision provides for harmonious development of the community. The applicant has proposed additional conditions to attempt to meet the harmonious development provision: limiting height to a maximum of 2 stories in 35 feet, limiting all lots to single-family dwellings only, and limiting garage access to the side or rear. Additionally, staff has included a condition prohibiting parking in front of principle structures. Although surrounding properties do include parking in the front, the widths of the proposed lots are approximately 40 feet smaller than the average of the surrounding properties.



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FIRE MARSHAL RECOMMENDATION

N/A

STORMWATER RECOMMENDATION

Approved with conditions

- Approved subject to MWS Stormwater's Infill Regulations.

PUBLIC WORKS RECOMMENDATION

Approved

TRAFFIC AND PARKING RECOMMENDATION

Approved

WATER SERVICES RECOMMENDATION

Approved

STAFF RECOMMENDATION

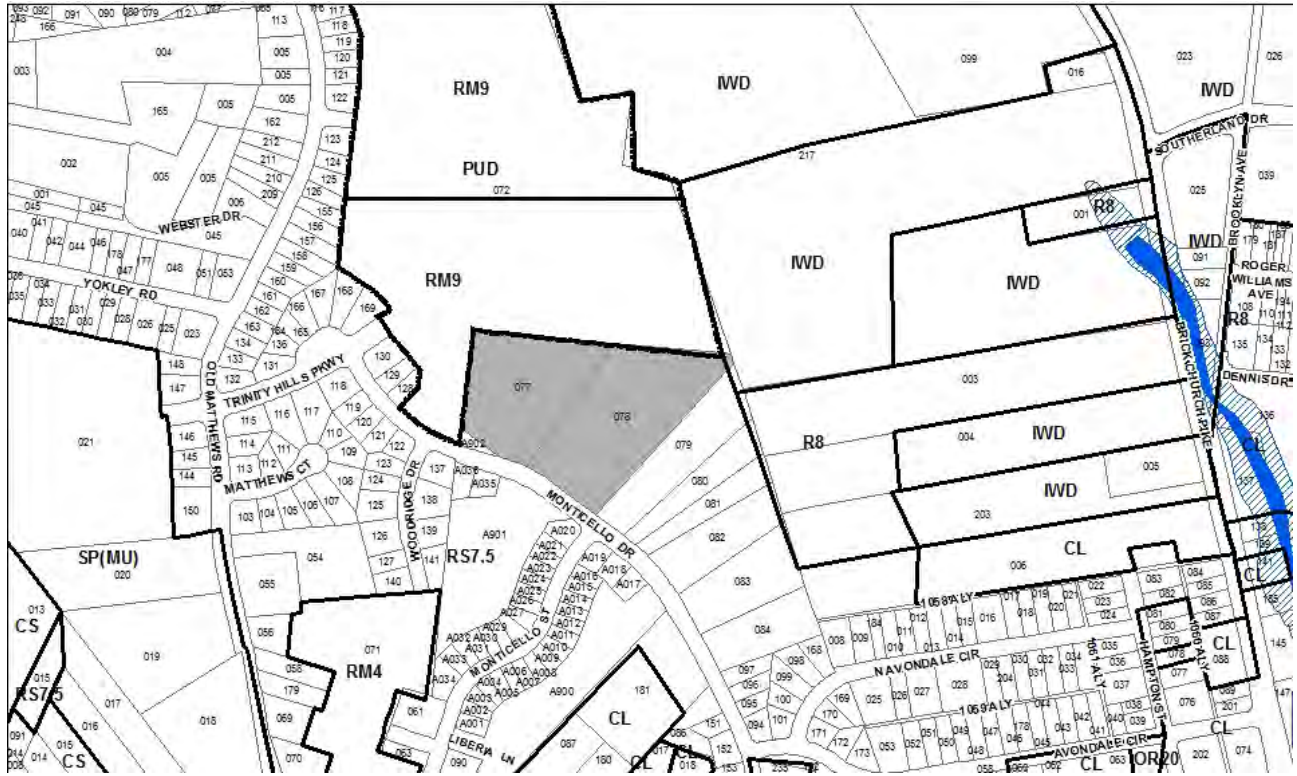
Staff recommends approval with conditions.

CONDITIONS

1. Lots 1, 2, and 3 shall be limited to one single-family home per lot.
2. No parking is permitted between the primary structure and street. Hard surfaces for vehicular access shall be a driveway a maximum of 16 feet wide located between the primary structure and the street.
3. Setbacks shall be as per the Metro Zoning Code.
4. Height is limited to two stories in 35 feet.
5. Garage access is limited from the rear or side.
6. A raised foundation of 18 inches to 36 inches is required for all residential structures.
7. Sidewalks are required along Wallace Lane. Therefore, prior to final plat recordation, one of the options must be chosen related to sidewalks:
 - a) Submit a bond application and post a bond with the Planning Department,
 - b) Construct sidewalk and have it accepted by Public Works,
 - c) Submit contribution in-lieu of construction to the Planning Department. The rate of \$96 per linear foot of total frontage area (266 l.f) will require a \$25,536 contribution to Pedestrian Benefit Zone 4-B, or
 - d) Construct an equal length of sidewalk within the same Pedestrian Benefit Zone, in a location to be determined in consultation with the Public Works Department.
8. Add the following note to the plat: The building permit site plan shall depict a minimum 5 foot clear path of travel for pedestrian ways, including public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed utility poles. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.



Metro Planning Commission Meeting of 03/23/2017



2017S-040-001
MONTICELLO VALLEY SUBDIVISION
Map 071-01, Parcel(s) 077-078
03, Bordeaux-Whites Creek
02 (Decosta Hastings)



Project No.	Concept Plan 2017S-040-001
Project Name	Monticello Valley Subdivision
Council District	02 – Hastings
School District	1 – Gentry
Requested by	DBS Engineering and Associates, applicant; Monticello Valley Partners, LLC, owner.
Staff Reviewer	Rickoff
Staff Recommendation	<i>Approve with conditions.</i>

APPLICANT REQUEST

Create up to 29 residential cluster lots.

Concept Plan

A request for concept plan approval to create up to 29 cluster lots on properties located at Monticello Drive (unnumbered) approximately 215 feet east of Woodridge Drive, zoned Single-Family Residential (RS7.5) (7.93 acres)

Existing Zoning

Single-Family Residential (RS7.5) requires a minimum of 7,500 square foot lot and is intended for single-family dwellings at a density of 4.94 units per acre. *RS7.5 would permit a maximum of 39 lots based on the acreage only. However, application of the Subdivision Regulations may result in fewer units on this property.*

CRITICAL PLANNING GOALS

N/A

BORDEAUX–WHITES CREEK COMMUNITY PLAN

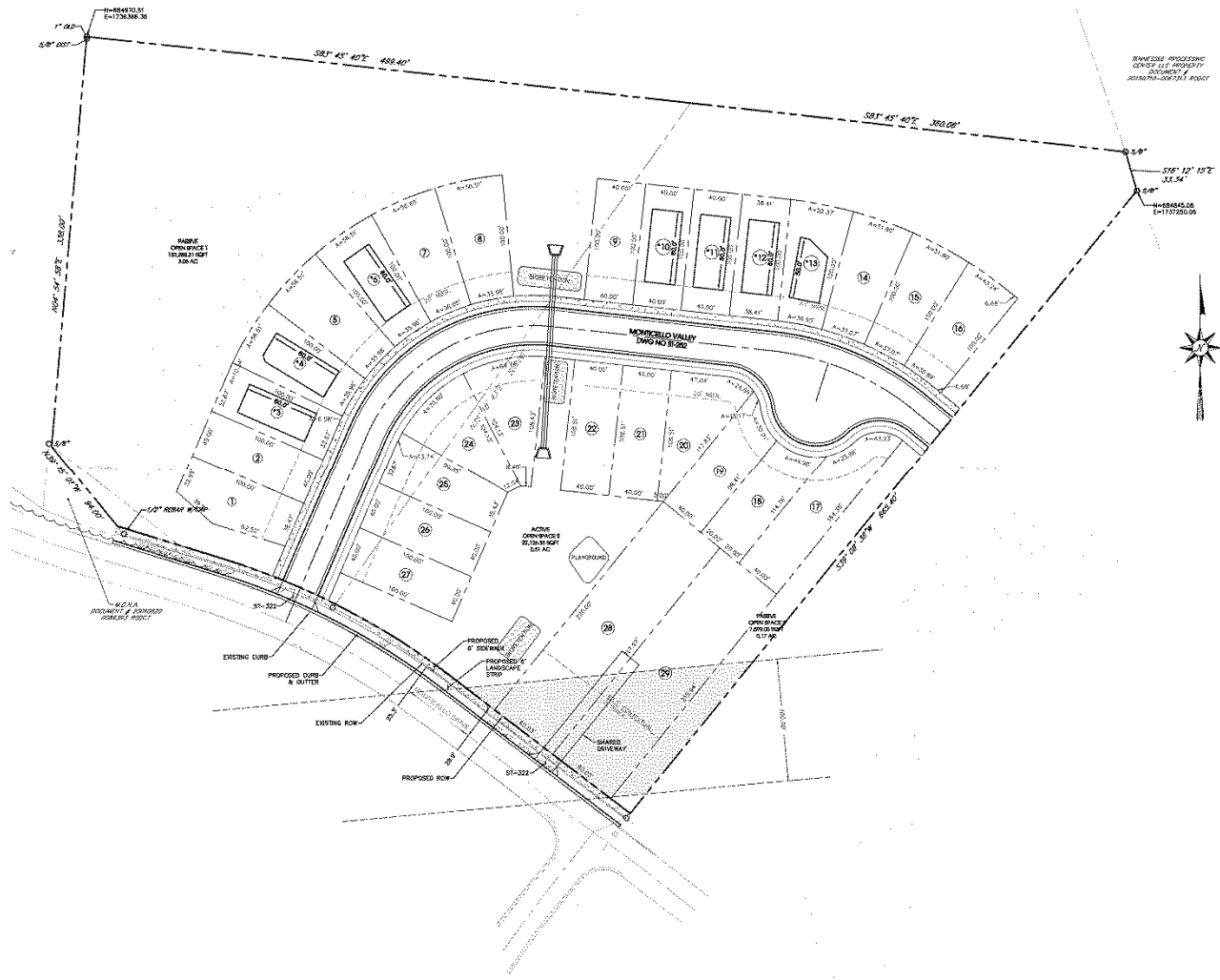
T3 Suburban Neighborhood Evolving (T3 NE) is intended to create suburban neighborhoods that are compatible with the general character of classic suburban neighborhoods as characterized by their building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern will have higher densities than classic suburban neighborhoods and/or smaller lot sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing - challenges that were not faced when the original classic, suburban neighborhoods were built.

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.



Metro Planning Commission Meeting of 03/23/2017

RODIGO GROUP INC. PROPERTY
DOCUMENT # 2013123-012017A P0001



Proposed Subdivision



Metro Planning Commission Meeting of 03/23/2017

PLAN DETAILS

The properties are approximately 7.93 acres, located off of Monticello Drive, north of W. Trinity Lane and west of Interstate 65. The site is vacant and contains heavy vegetation. Portions of the site contain natural slopes of 20 percent or greater. Though the majority of the site is Neighborhood Evolving policy, Conservation policy acknowledges a wet weather conveyance that runs north-south through the site.

Site Plan

The site plan proposes 29 single-family cluster lots with an overall density of 3.65 dwelling units per acre. Two lots front onto Monticello Drive, and the remaining 27 lots face onto a new internal road accessed from Monticello Drive. The 2 perimeter lots are approximately 13,000 square feet, significantly larger than the interior lots that range from 4,000-5,000 square feet. A 100 foot TVA easement bisects the southern and eastern property boundaries, including the 2 perimeter lots.

Lots are clustered down to the RS3.75 district size and range in size from approximately 4,000 square feet to approximately 13,000 square feet. Three of the lots are at least 50 feet in width (lots 20, 23, and 24). The remaining lots are less than 50 feet in width and will require a shared access easement between the primary structure and the street. Seven interior lots are designated as critical lots due to steep slopes in excess of 20 percent. With the exception of Lot 13, all of the critical lots include minimal areas of slopes above 20 percent. Lot 13 includes larger areas of slopes above 20 percent; therefore, staff recommends removing this lot and converting it to open space. None of the critical lots include slopes of 25 percent or greater, and no problem soils have been identified. Building footprints, located outside of the natural areas with steep slopes, have been included on these 7 lots. The applicant has provided a preliminary grading plan indicating the general limits of disturbance, including off-site grading which will occur to the east of the subject properties.

Monticello Valley, a new internal road, will extend from Monticello Drive through the site to the eastern property line providing for a future connection. A partial cul-de-sac is also provided near this future connection. Monticello Valley will include of a 50 foot right-of-way with a 5 foot sidewalk and 4 foot planting strip, consistent with the local street standards. Monticello Drive will include a 6 foot sidewalk and 6 foot planting strip, consistent with the Major and Collector Street Plan (MCSP) Collector-Avenue standards. Any existing vegetation that is located near the intersection of Monticello Drive and Monticello Valley will need to be cleared to provide appropriate site distance before the issuance of a grading permit.

In cluster lot subdivisions, a minimum of 15 percent of each phase of the development shall be open space. This concept plan provides approximately 3.8 acres of open space (47% of the site), including stormwater management areas which are generally located in middle of the site. Most of the remaining open space is passive, with the exception of a playground area located near the front of the site along Monticello Drive.



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ANALYSIS

The plan is consistent with the cluster lot standards specified in the Zoning Code. The original site plan proposed 37 lots, including 19 critical lots. The layout was modified by shifting the road and lots out of the steepest slopes, resulting in a reduction in the number of lots from 37 to 29 (staff is recommending 28 lots). The plan also provides for more street connectivity by creating a future roadway connection to the east. As this property is located within a Suburban Neighborhood Evolving policy, lot compatibility is not applicable.

FIRE MARSHAL RECOMMENDATION

Approved w/conditions

- Provide flow data for subdivision pre-approval.
- Fire Code issues will be addressed in the permit phase.

STORMWATER RECOMMENDATION

Approved with conditions

- Cite the flood panel information to plans (233H and 4/5/2017)

PUBLIC WORKS RECOMMENDATION

Approved

TRAFFIC AND PARKING RECOMMENDATION

Approved with conditions

- Clear vegetation to provide intersection sight distance per AASHTO prior to issuance of the Grading Permit.

WATER SERVICES RECOMMENDATION

Approved with conditions

- Approved as a Concept Plan only. Public water and sewer construction plans must be submitted and approved prior to Final Site Plan approval. These approved construction plans must match the Final Site Plan. The required capacity fees must also be paid prior to Final Site Plan approval. FYI - please revise availability study before Final Site Plan stage, so an accurate capacity fee total is assigned (our latest study proposes 39 units, and this SP shows 37).

STAFF RECOMMENDATION

Staff recommends approval with conditions.

CONDITIONS

1. Lot 13 shall be removed and converted to open space. This revision shall be reflected on the final site plan and final plat.
2. Sidewalks are required along all streets, including Monticello Drive. Therefore, prior to final plat recordation, one of the options must be chosen related to sidewalks along Monticello Drive:
 - a. Submit a bond application and post a bond with the Planning Department,
 - b. Construct sidewalk and have it accepted by Public Works,



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3. No parking is permitted between the primary structure and street. Hard surfaces for vehicular access shall be a driveway a maximum of 16 feet wide for lots with a street frontage at least 50'; or a shared access easement for lots with street frontage less than 50' located between the primary structure and the street.
4. A raised foundation of 18"- 36" is required for all residential structures.
5. Add the following note to all open spaces except for designated recreational areas: All existing trees in designated open spaces shall be preserved as permanent open space. These areas shall remain undisturbed.
6. A tree protection plan may be required with final site plan approval.
7. The final site plan/ building permit site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.
8. If critical lots cannot be developed in compliance with the critical lot requirements, the lot layout shall be modified accordingly.
9. Pursuant to 2-3.5.e of the Metro Subdivision Regulations, because this application has received conditional approval from the Planning Commission, that approval shall expire unless revised plans showing the conditions on the face of the plans are submitted prior to or with any application for a final site plan or final plat.



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2005UD-009-007
BELCOURT VILLAGE
Map 104-08, Parcel(s) 117-118; Map 104-12, Parcel(s) 003
10, Green Hills-Midtown
18 (Burkley Allen)



Project No. UDO Major Modification and Final Site Plan
2005UD-009-007

Project Name Belcourt Village

Council District 18 – Allen

School District 8 – Pierce

Requested by Southeast Venture, applicant; ECG Belcourt, LLC, owner.

Staff Reviewer Wallace

Staff Recommendation *Defer to the April 13, 2017, Planning Commission meeting unless recommendations of approval are received from all reviewing agencies. If recommendations of approval are received from all agencies, staff recommends approval with conditions.*

APPLICANT REQUEST

Modification to the maximum height and the maximum number of stories standards of the Hillsboro Village Urban Design Overlay.

UDO Major Modification and Final Site Plan

A request for major modification of the maximum height and number of stories standards for the Hillsboro Village Urban Design Overlay and final site plan approval for properties located at 2111 and 2113 Belcourt Avenue and 1803 21st Avenue South, at the southwest corner of Belcourt Avenue and 21st Avenue South (0.90 acres), zoned Mixed Use Limited (MUL).

Existing Zoning

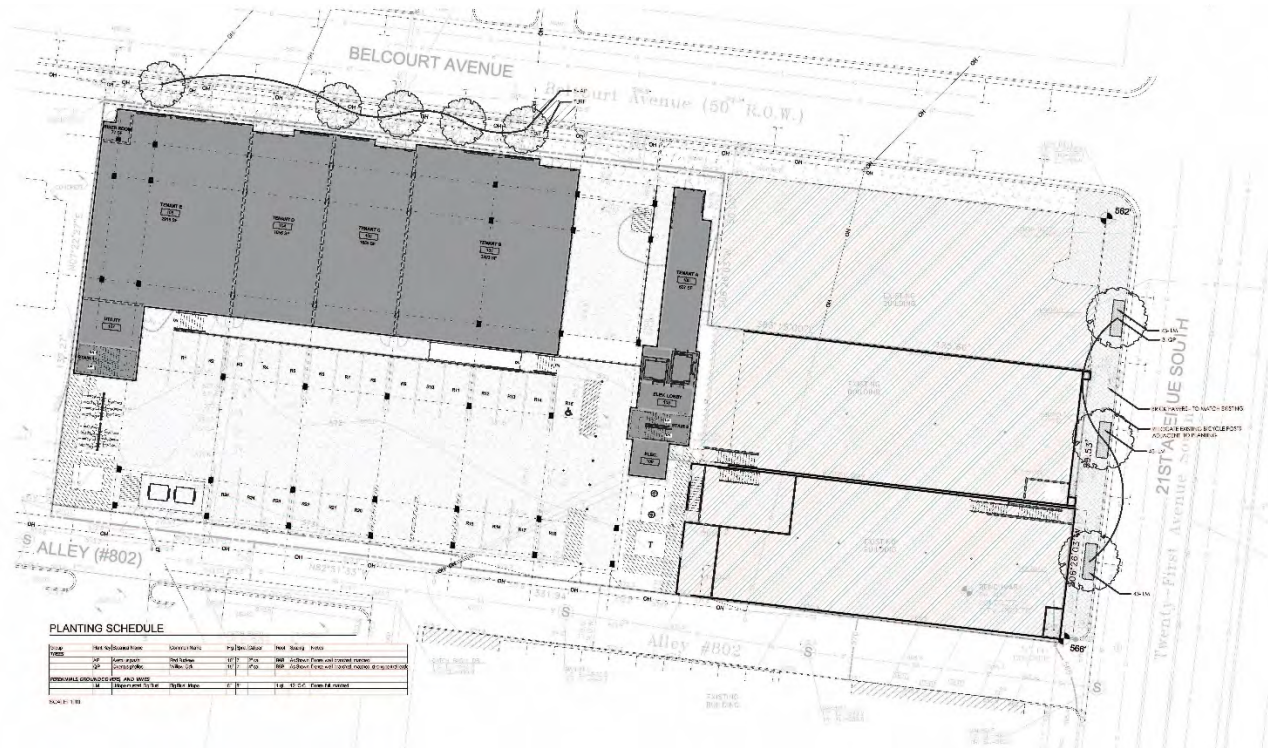
Mixed Use Limited (MUL) is intended for a moderate intensity mixture of residential, retail, restaurant, and office uses.

Hillsboro Village Urban Design Overlay (UDO) is intended to preserve and enhance the special character of Hillsboro Village by encouraging rehabilitation and new construction that is sensitive to the existing urban form. The guidelines recognize that no single architectural style predominates in Hillsboro Village, and the guidelines allow for creativity in the design of individual buildings. However, there are certain established urban design principles shared by most properties within the district which give it a cohesive character and strong sense of place. Due to the diversity in Hillsboro Village, the area has been divided into seven subdistricts. Subdistricts are based upon original and surviving building types and development character rather than present uses. Where appropriate, specific design standards have been developed for each.

The property is located within Subdistrict 1B (Commercial Core: South). Subdistrict 1B is located in the heart of the Village and the existing character is to be preserved. The one and two story buildings in this area serve as a transition between subdistrict 1A and the residential area to the south.



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Proposed Site Plan



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In 2015, The Planning Commission and Metro Council approved a text amendment to the Hillsboro Village Urban Design Overlay. The amendment grants ten feet of additional bonus height for development that preserves character defining buildings (those designated as worthy of conservation) along 21st Avenue.

GREEN HILLS - MIDTOWN COMMUNITY PLAN

T4 Urban Mixed Use Neighborhood Policy (T4 MU) is intended to preserve, enhance, and create urban, mixed use neighborhoods characterized by a development pattern that contains a diverse mix of residential and non-residential land uses, and that are envisioned to remain or develop in a mixed use pattern. T4 MU areas are intended to be mixed use in nature with the presence of commercial and even light industrial uses, but also a significant amount of moderate to high density residential development.

Consistent with Policy?

Yes. The project proposes a mix of residential with non-residential uses including retail, restaurant, and office and preserves buildings designated worthy of conservation while creating new development offering to enhance Hillsboro Village as a vibrant mixed use neighborhood.

PLAN DETAILS

The property is approximately 0.902 acres located at 211 Belcourt Avenue and forms an L-shape around an existing building at the southwest corner of 21st Avenue South and Belcourt Avenue. The proposed project is mixed use with retail, residential and office use.

Use of Building Preservation for Bonus Height

Two existing buildings (formerly Sam's Sports Bar and Grill and Boscors) along 21st Avenue South are designated as worthy of conservation status. The applicant is utilizing the Building Preservation Bonus Height provision outlined in the UDO, which grants one additional story in ten feet of height for preserving character defining buildings with Hillsboro Village.

ANALYSIS

The request for additional height and number of stories is in keeping with the intent of the Hillsboro Village Urban Design Overlay to preserve character defining buildings along 21st Avenue and supports infill development with provisions for mixed use that benefit the Village as a whole. The additional height and number of stories does not diminish the quality and form of the building and its relationship to the public realm as described below.



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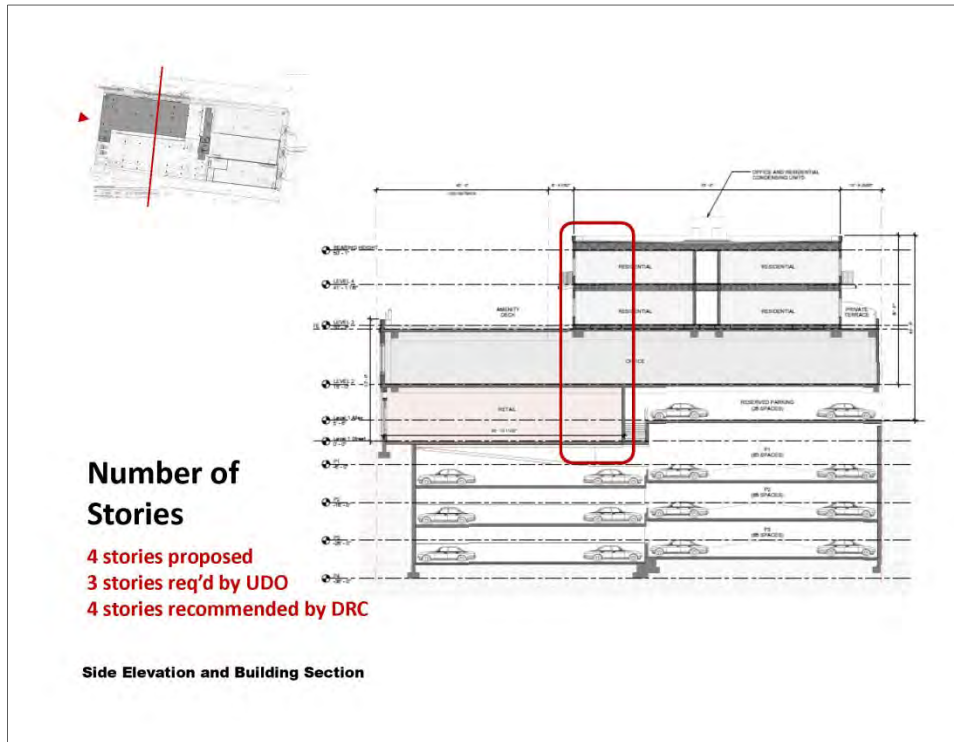


Figure 1: Cross-section of building

Regarding the number of stories, the profile and massing of the overall building remain unaffected by the increase of one story. Because the UDO excludes parking structures from the calculation of number of stories, a standard that provides an incentive to accommodate off-street parking on site, the appearance of the building from Belcourt Avenue remains the same. Specifically, the request calls for a 60 foot depth of space at the ground floor level to accommodate standard market retail dimensions. Approximately 15 feet of the total 60 foot depth of the ground floor level causes the building to have four stories where three is permitted at a point internal to the development.

The design compensates for the overlapping portion at the ground level by stepping back the upper portion of the building at the front and rear of the building. The proposed upper levels are set back 52 feet from the face of the building along Belcourt Avenue where only 45 feet is required. The proposed upper levels are setback over 120 feet from the face of the building along 21st Avenue where only 60 feet is required. The proposed balconies on the upper residential levels along the alley are recessed to provide increased spacing from the adjacent building located directly across the alley.



Figure 2: Elevations of building

Regarding overall building height, the proposed floor to floor heights are appropriate for retail, office and residential uses whereas the UDO only accounts for residential uses permitting a height of 10 feet. The proposed floor to floor heights are 15 feet for the ground floor level retail, 15 feet and 6 inches for the second floor office level, and 10 feet for the residential floors above. A 3 foot tall parapet adds to the overall building height at approximately 53 feet. Note that the average overall building height is approximately 50 feet when topography is considered since the site changes 5 feet in grade from Belcourt Avenue to the alley.

Overall, the proposed massing is appropriate for its context. The massing provides a transition in scale to the existing structures on adjacent property and steps back after the second level at the front, side, and rear of the building providing adequate spacing from the public right of way. At the street level, the façade varies to create a rhythm and scale consistent with the existing structures of Hillsboro Village.

Hillsboro Village Design Review Committee Recommendation

The Hillsboro Village Design Review Committee convened on November 28, 2016, and unanimously voted to recommend approval of the modification requests for height and number of stories.

FIRE DEPARTMENT RECOMMENDATION

Approve with conditions

- Fire Code issues will be addressed in the permit phase.



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STORMWATER RECOMMENDATION

Under Review

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works, in effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable.
- Final design may vary based on field conditions.
- Submit recorded ROW dedication prior to the building permit approval by MPW.
- Indicate on the plans the loading space per the Metro Zoning Code. ~ Coordinate with Metro Zoning Admin. Typically, loading zone is to be onsite. Any modification to the curb side lane usage requires approval by Metro Traffic and Parking Commission.
- Tenant E, and all tenant spaces, pedestrian access is to be recessed. Door swing should not be into the pedestrian way.
- Comply with the conditions of the MPW Traffic Engineer.

TRAFFIC AND PARKING RECOMMENDATION

Approve with conditions

In accordance with TIS findings, developer shall construct the following roadway improvements.

21st Avenue South at Belcourt Avenue

- Developer shall continue to work with Metro Public Works to identify an appropriate pedestrian crossing on 21st Ave near Belcourt Ave prior to building permit approval of Elmington Belcourt Village.
- Developer shall continue to work with Metro Public Works prior to building permit approval to identify appropriate manner to manage or reduce WB traffic queue on Belcourt Ave .

21st Avenue South at Alley #802

- Developer shall install a curb ramp with truncated dome surface on the northwest corner of this intersection that satisfies both TDOT and Metro Nashville specifications.

Belcourt Avenue

- Developer shall apply to T&P staff to modify any existing on street metered parking
- Developer shall provide adequate sight distance at project access drive on Belcourt. Ave. A sight distance exhibit shall be submitted with construction plans.

Alley #802

- East of the existing north-south alley between Acklen Avenue and Alley #802, maintain the existing 20.5 feet of right-of-way along Alley #802 – but provide an additional four (4) feet of pavement on the north side (on private property as opposed to the public right-of-way), resulting in the pavement width being widened from 20.5 feet to 24.5 feet.
- West of the existing north-south alley, widen Alley #802 (both pavement width and right-of-way) from 12 feet to 16 feet.



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Site development

- POST Wayfinding directions on any applicable websites, particularly for the non-residential land uses.
- Provide bicycle parking within the parking structure that is accessible to the public.
- Electronic gates should be installed a minimum of 25ft from the public right-of-way, specifically referring to any electronic gate that controls entry into the parking structure along Proposed Driveway 1 (from Belcourt Avenue) or along Proposed Driveway 2 (from Alley #802). A denial lane may be required.

Belcourt Avenue at Proposed Driveway 1

- Construct Proposed Driveway 1 approximately 150 feet west of 21st Avenue South.
- Construct Proposed Driveway 1 with two (2) lanes for vehicular movement: one (1) lane for vehicle ingress and one (1) lane for vehicle egress.
- Provide STOP control along the northbound approach of Proposed Driveway 1.
- Provide adequate intersection sight distance in accordance with the criteria provided in A Policy on Geometric Design of Highways and Streets.
- Install signs and pavement markings in accordance with the standards provided in the Manual on Uniform Traffic Control Devices (MUTCD).

Alley #802 at Proposed Driveway 2

- Construct Proposed Driveway 2 approximately 225 feet west of 21st Avenue South, directly across from the existing north-south alley between Acklen Avenue and Alley #802.
- Construct Proposed Driveway 2 with two (2) lanes for vehicular movement: one (1) lane for vehicle ingress and one (1) lane for vehicle egress.
- Provide STOP control along the southbound approach of Proposed Driveway 2.
- Provide adequate intersection sight distance in accordance with the criteria provided in A Policy on Geometric Design of Highways and Streets.
- Install signs and pavement markings in accordance with the standards provided in the Manual on Uniform Traffic Control Devices (MUTCD).

WATER SERVICES

Returned for corrections

- This Utility Plan proposes a public sanitary sewer extension. Therefore, public sewer construction plans must be approved, and match this Final Site Plan, prior to approval of this Final Site Plan. The required capacity fees must also be paid, prior to Final Site Plan approval.

STAFF RECOMMENDATION

Staff recommends to deferral to the April 13, 2017, Planning Commission meeting unless a recommendation of approval is received from all reviewing agencies. If a recommendation of approval is received from all agencies, staff recommends approval with conditions.



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CONDITIONS (if approved)

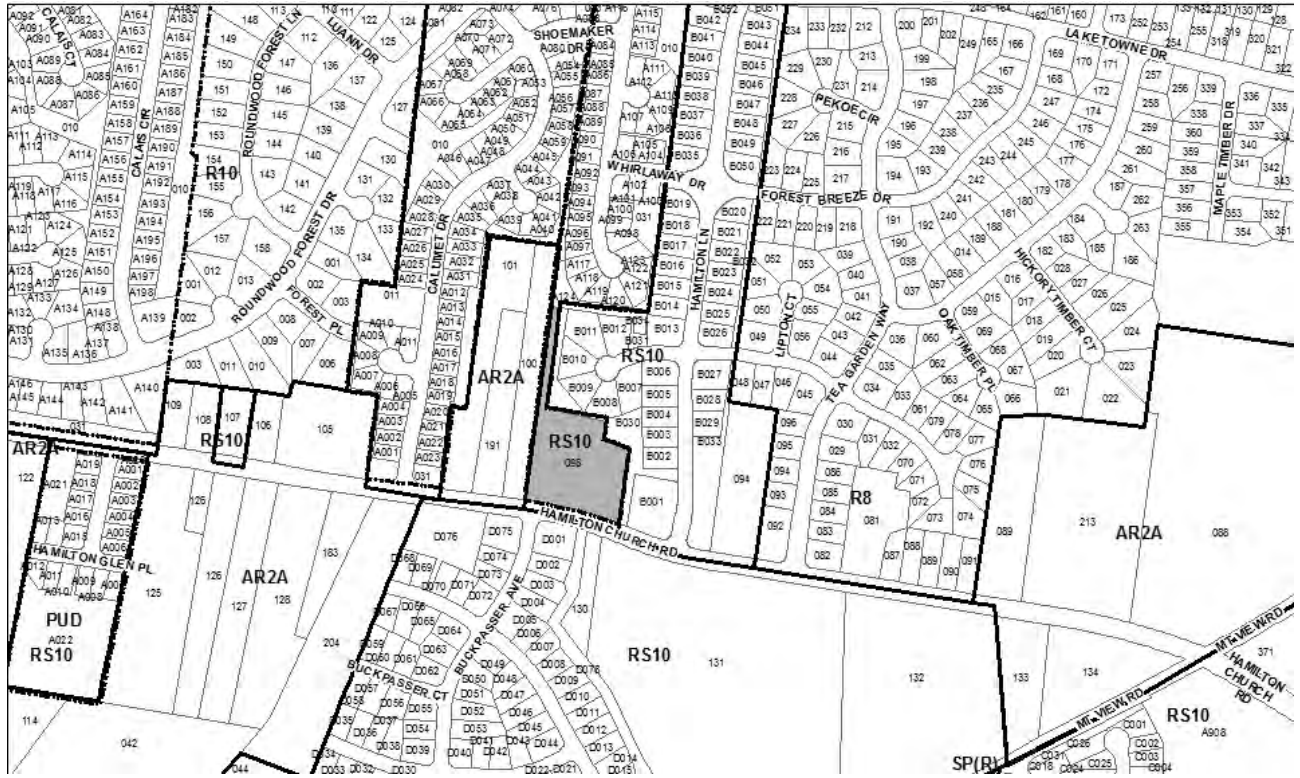
1. Prior to issuance of a final use and occupancy permit, an instrument shall be recorded in the register of deeds that depicts the following:
 - a. the preservation of the building to the extent determined by the required step-back standard in perpetuity requiring any exterior alterations including demolition in whole or in-part be reviewed and approved by the Hillsboro Village Design Review Committee;
 - b. the project's height bonus; and,
 - c. the forfeiture or any future claim for additional building intensity of development, including any type of variance of the preserved building.
2. Building permits shall be consistent with any approved modifications granted and meet all unmodified UDO standards. Any deviations from the approved final site plan are subject to review by the Hillsboro Village Design Review Committee.
3. Signage approval is not included as part of this approval.



SEE NEXT PAGE



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79-87P-001
CALUMET (PHASE 9)
Map 150, Parcel(s) 098
13, Antioch – Priest Lake
33 (Sam Coleman)



Project No.	Planned Unit Development 79-87P-001
Project Name	Calumet (Phase 9)
Council District	33 - Coleman
School District	6 – Hunter
Requested by	Wamble and Associates, applicant; James McLean Sr., owner.
Staff Reviewer	Birkeland
Staff Recommendation	<i>Approve with conditions.</i>

APPLICANT REQUEST

Revise a portion of a Planned Unit Development to permit 12 residential lots

Revise PUD

A request to revise the preliminary plan for a Planned Unit Development Overlay District on property located at Hamilton Church Road (unnumbered), approximately 155 feet west of Hamilton Lane, zoned RS10 (2.57 acres), to permit 12 residential lots.

Existing Zoning

Single-Family Residential (RS10) requires a minimum of 10,000 square foot lot and is intended for single-family dwellings at a density of 3.7 dwelling units per acre. *RS10 would permit a maximum of 11 units. The PUD controls the number of units. This phase of the PUD allows up to 16 residential units.*

Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provisions of an adequate roadway system or essential utilities and services. In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provisions of essential utilities and streets.

PLAN DETAILS

The site is located north of the intersection of Hamilton Church Road and Buckpasser Avenue on an approximately 2.57 acre parcel. Council approved the original PUD in 1987, and it has received numerous revisions. The PUD permits single-family residential uses and 8 out of the 9 phases are built. The last phase, phase 9, would allow up to 16 single-family residential units. The proposed revision proposes 12 single-family residential lots for phase 9.



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Site Plan

The plan proposes 12 single-family residential lots. The lots will be accessed by a new road on the eastern side of Hamilton Church Road, across the street from Buckpasser Avenue. A six foot sidewalk and six foot planting strip is proposed along Hamilton Church Road. A five foot sidewalk and four foot planting strip is proposed along the new road. All proposed sidewalks meet the Major and Collector Street and local street standards. A proposed 20 foot type "B" landscape bufferyard is proposed along Hamilton Church Road and the northern property line.

Originally the PUD included a new public road along the east side of the site to serve the 12 new units. At that time Buckpasser Avenue was not built. The proposed revision moves the new public road, from the east side of the site to the west side of the site, to line up with Buckpasser Avenue across Hamilton Church Road. The previous plan for Phase 9 was approved for 12 zero lot line units whereas the new plan provides detached homes.

ANALYSIS

Section 17.40.120.G permits the Planning Commission to approve "minor modifications" under certain conditions. Staff finds that the request is consistent with and meets all of the criteria of Section 17.40.120.G, and is provided below for review.

- G. Status of Earlier Planned Unit Developments (PUDs). The following provisions shall apply to a planned unit development (PUD) approved under the authority of a previous Zoning Code and remaining a part of the official zoning map upon the enactment of this title.
1. The planned unit development (PUD) shall be recognized by this title according to the master development plan and its associated conditions specified in the PUD ordinance last approved by the metropolitan council prior to the effective date of the ordinance codified in this title.
 2. The planning commission may consider and approve minor modifications to a previously approved planned unit development subject to the following limitations. All other modifications shall be considered by the planning commission as an amendment to the previously approved planned unit development and shall be referred back to the council for approval according to the procedures of Section 17.40.120(A)(5). That portion of a planned unit development master plan being amended by the council shall adhere to all provisions of this code:
 - a. In the judgment of the commission, the change does not alter the basic development concept of the PUD;
 - b. The boundary of the planned unit development overlay district is not expanded;
 - c. There is no change in general PUD classification (e.g. residential to any classification of commercial or industrial PUD; any change in general classification of a commercial PUD; or any change in general classification of an industrial PUD);
 - d. There is no deviation from special performance criteria, design standards, or other specific requirements made part of the enacting ordinance by the council;
 - e. There is no introduction of a new vehicular access point to an existing street, road or thoroughfare not previously designated for access;
 - f. There is no increase in the total number of residential dwelling units originally authorized by the enacting ordinance;



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- g. There is no change from a PUD approved exclusively for single-family units to another residential structure type;
- h. The total floor area of a commercial or industrial classification of PUD shall not be increased more than ten percent beyond the total floor area last approved by the council;
- i. If originally limited to office activities, the range of permitted uses in a commercial PUD shall not be expanded to broader classifications of retail, commercial or industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- j. If originally limited to office, retail and other general commercial activities, the range of permitted uses in a commercial PUD shall not be expanded to include industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- k. If originally limited to commercial activities, the range of permitted uses in a commercial PUD shall not be expanded to broader classifications of retail, commercial or industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- l. In the determination of the commission, the nature of the change will have no greater adverse impact on those environmentally sensitive features identified in Chapter 17.28 of this code than would have occurred had the development proceeded in conformance with the previous approval.
- m. In the judgment of the commission, the planned unit development or portion thereof to be modified does not meet the criteria for inactivity of Section 17.40.120.H.4.a.

FIRE MARSHAL RECOMMENDATION

Approve with conditions

- Fire Code issues will be addressed in the permit phase.

STORMWATER RECOMMENDATION

Approve with conditions

- Add Preliminary Note to the plans: Drawing is for illustration purposes to indicate the basic premise of the development, as it pertains to Stormwater approval / comments only. The final lot count and details of the plan shall be governed by the appropriate stormwater regulations at the time of final application.

PUBLIC WORKS RECOMMENDATION

Approved



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TRAFFIC & PARKING RECOMMENDATION

Approved

WATER SERVICES RECOMMENDATION

Approve with conditions

- Approved as a Preliminary PUD Amendment only. Public water and sewer construction plans must be submitted and approved prior to Final SP approval. These approved construction plans must match the Final Site Plan/SP plans. The required capacity fees must also be paid prior to Final Site Plan/SP approval.

STAFF RECOMMENDATION

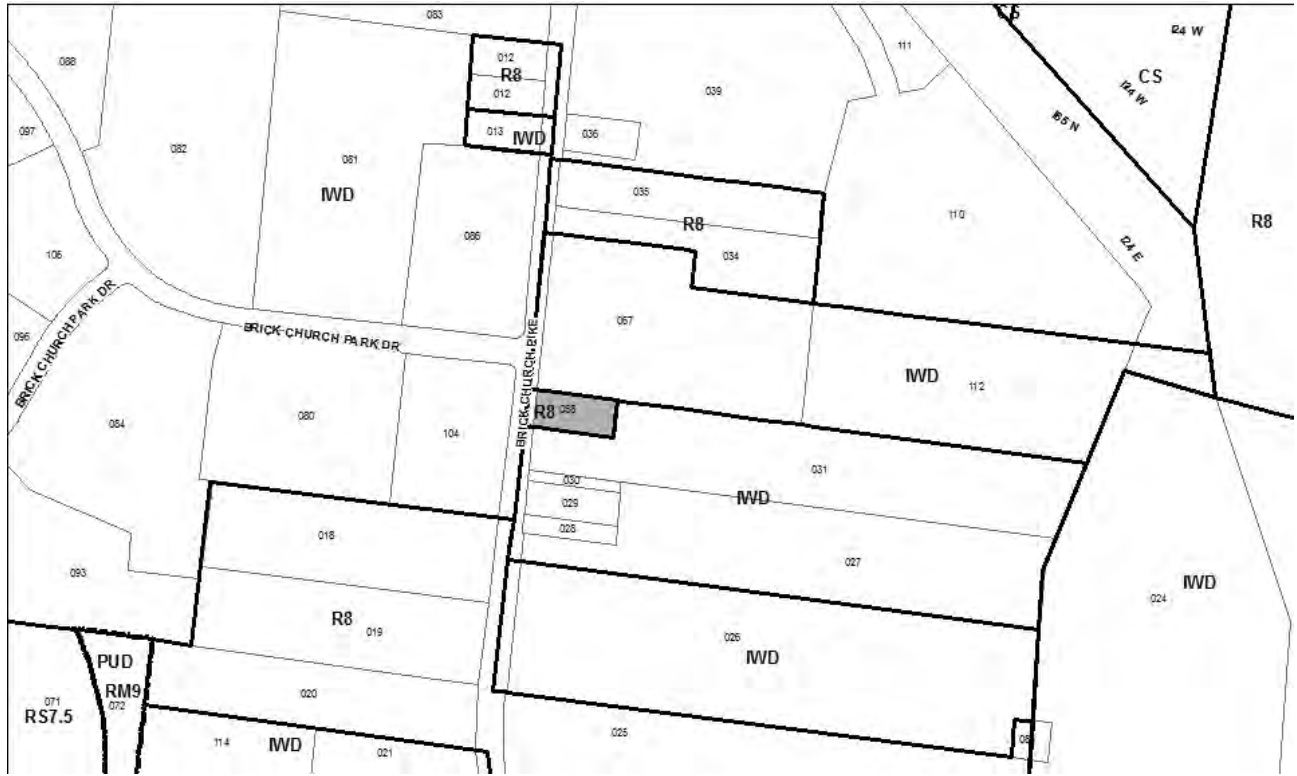
Staff recommends approval with conditions.

CONDITIONS

1. A six foot wide sidewalk and six foot wide planting strip shall be installed along Hamilton Church Road prior issuance of a Use and Occupancy permit.
2. The final site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.
3. This approval does not include any signs. Signs in planned unit developments must be approved by the Metropolitan Department of Codes Administration except in specific instances when the Metro Council directs the Metro Planning Commission to review such signs.
4. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
5. Prior to or with any additional development applications for this property, the applicant shall provide the Planning Department with a corrected copy of the preliminary PUD plan.



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2017Z-031PR-001
Map 060 Parcel 058
03, Bordeaux-Whites Creek
02 (DeCosta Hastings)



Project No.	Zone Change 2017Z-031PR-001
Council District	02 - Hastings
School District	1 - Gentry
Requested by	Rippetoe Properties, applicant and owner.
Staff Reviewer	Napier
Staff Recommendation	<i>Approve.</i>

APPLICANT REQUEST
Zone change from R8 to IWD.

Zone Change

A request to rezone from One and Two-Family Residential (R8) to Industrial Warehousing/ Distribution (IWD) zoning on property located at 2923 Brick Church Pike, approximately 50 feet southeast of the terminus of Brick Church Park Drive (0.47 acres).

Existing Zoning

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes. *R8 would permit a maximum of 2 lots including 1 duplex lot, for a total of 3 units.*

Proposed Zoning

Industrial Warehousing/Distribution (IWD) is intended for a wide range of warehousing, wholesaling, and bulk distribution uses.

CRITICAL PLANNING GOALS

N/A

BORDEAUX-WHITES CREEK COMMUNITY PLAN

District Industrial (D IN) is intended to preserve, enhance, and create Industrial Districts in appropriate locations. The policy creates and enhances areas that are dominated by one or more industrial activities, so that they are strategically located and thoughtfully designed to serve the overall community or region, but not at the expense of the immediate neighbors. Types of uses in D IN areas include non-hazardous manufacturing, distribution centers and mixed business parks containing compatible industrial and non-industrial uses. Uses that support the main activity and contribute to the vitality of the D IN are also found.

Consistent with Policy?

Yes. The rezoning is consistent with the D IN policy. The uses most suitable within the proposed IWD zoning district are those which operate within completely enclosed buildings with limited outdoor storage. Potential impacts on abutting properties are minimized by the light industrial nature of the uses permitted in the district and high operational standards.



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ANALYSIS

The request is to rezone .47 acres located southeast of the terminus of Brick Church Park Drive. The property contains a single family residential structure. The requested rezoning to IWD is consistent with the policy for the area and is appropriate given the surrounding industrial land uses. There are various parcels to the north and south which are currently zoned R8, however the surrounding area is largely zoned IWD.

Prior to development, the applicant may be required to perform a traffic impact study to address the increased vehicle trips which may result from the potential increase in density generated by the future development of this site.

FIRE MARSHAL RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

N/A

WATER SERVICES RECOMMENDATION

N/A

STORMWATER RECOMMENDATION

N/A

TRAFFIC AND PARKING RECOMMENDATION

Approve with conditions

- A traffic study may be required at the time of development

Maximum Uses in Existing Zoning District: **R8**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential* (210)	0.47	5.4 D	4 U	39	3	5

*Based on two-family lots

Maximum Uses in Proposed Zoning District: **IWD**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Warehousing (150)	0.47	0.8	16,378 SF	59	31	19

Traffic changes between maximum: **R8 and IWD**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+20	+28	+14



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STAFF RECOMMENDATION

Staff recommends approval as the requested zone change is consistent with the District Industrial land use policy and with the zoning of surrounding parcels.



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2017Z-032PR-001

Map 092-03, Parcel(s) 116

08, North Nashville

21 (Ed Kindall)



Project No.	Zone Change 2017Z-032PR-001
Council District	21 – Kindall
School District	5 – Buggs
Requested by	Rhythm Homes and Development, LLC, applicant and owner.
Staff Reviewer	Shepard
Staff Recommendation	<i>Approve</i>

APPLICANT REQUEST

Zone change from RS5 to R6-A.

Zone Change

A request to rezone from Single-Family Residential (RS5) to One and Two-Family Residential – Alternative (R6-A) zoning on property located at 2401 Alameda Street, at the southwest corner of Alameda Street and 24th Avenue North (0.18 acres).

Existing Zoning

Single-Family Residential (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre. *RS5 would permit a maximum of one unit.*

Proposed Zoning

One and Two-Family Residential – Alternative (R6-A) requires a minimum 6,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 7.71 dwelling units per acre including 25 percent duplex lots and is designed to create walkable neighborhoods through the use of appropriate building placement and bulk standards. *R6-A would permit a maximum of one duplex lot for a total of two units.*

NORTH NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Evolving (T4 NE) is intended to create and enhance urban residential neighborhoods that provide more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate to high density development patterns with shallow setbacks and minimal spacing between buildings. T4 NE areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. T4 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network and block structure and proximity to centers and corridors.



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Consistent with Policy?

The proposed zone change is consistent with policy. The property is located in an area with an existing mix of one and two-family residential dwellings. The lot is located on a corner and has access via an existing alley. Determinations of duplex eligibility are made by the Metro Codes Department, but if the lot is determined to be duplex eligible, the proposed zone change would permit two-family residential unit which contributes to increased housing choice in the neighborhood. Alternatively, a detached accessory dwelling unit may be allowed. The availability of alley access minimizes the need for curb cuts along the front of the property and enhances the pedestrian environment.

ANALYSIS

The site is located at the southwest corner of the intersection of Alameda Street and 24th Avenue North. The property is currently vacant. The existing land use pattern in the neighborhood includes a mix of one and two-family residential. Determinations of duplex eligibility are made by the Metro Codes Department. Should the site be determined eligible for a duplex, its location on a corner and the availability of alley access make the site an appropriate location for two-family residential development, which would help increase the variety of housing choices available in the neighborhood. The R6-A zoning district includes standards for the location of access, driveways, and parking designed to enhance the pedestrian environment. The proposed rezoning is consistent with the goals of the T4 Urban Neighborhood Evolving land use policy.

PUBLIC WORKS RECOMMENDATION

N/A

TRAFFIC AND PARKING RECOMMENDATION

Approved with conditions

- A traffic study may be required at the time of development.

Maximum Uses in Existing Zoning District: **RS5**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.18	8.71 D	1 U	10	1	2

Maximum Uses in Proposed Zoning District: **R6-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential* (210)	0.18	7.26 D	2 U	20	2	3

*Based on two-family lots

Traffic changes between maximum: **RS5 and R6-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+1 U	+10	+1	+1



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METRO SCHOOL BOARD REPORT

Projected student generation existing RS5 district: 0 Elementary 0 Middle 0 High

Projected student generation proposed R6-A district: 0 Elementary 0 Middle 0 High

The proposed zoning is not expected to generate any additional students beyond the existing zoning. Students would attend Park Avenue Elementary School, McKissack Middle School, and Pearl Cohn High School. All three schools have been identified as having additional capacity. This information is based upon data from the school board last updated November 2016.

AFFORDABLE AND WORKFORCE HOUSING REPORT (information provided by applicant)

Will this project include any affordable or workforce housing units? Not yet determined

1. If so, how many and what is the percentage of the entire development? N/A
2. How will you enforce the affordability requirements? N/A
3. Have any structures been demolished in the last 12 months? No

STAFF RECOMMENDATION

Staff recommends approval as the request is consistent with the T4 Urban Neighborhood Evolving policy.