

Metropolitan Planning Commission



Staff Reports

March 26, 2015



Metro Planning Commission Meeting of 03/26/2015

Mission Statement: The Planning Commission is to guide the future growth and development for Nashville and Davidson County to evolve into a more socially, economically and environmentally sustainable community with a commitment to preservation of important assets, efficient use of public infrastructure, distinctive and diverse neighborhood character, free and open civic life, and choices in housing and transportation.

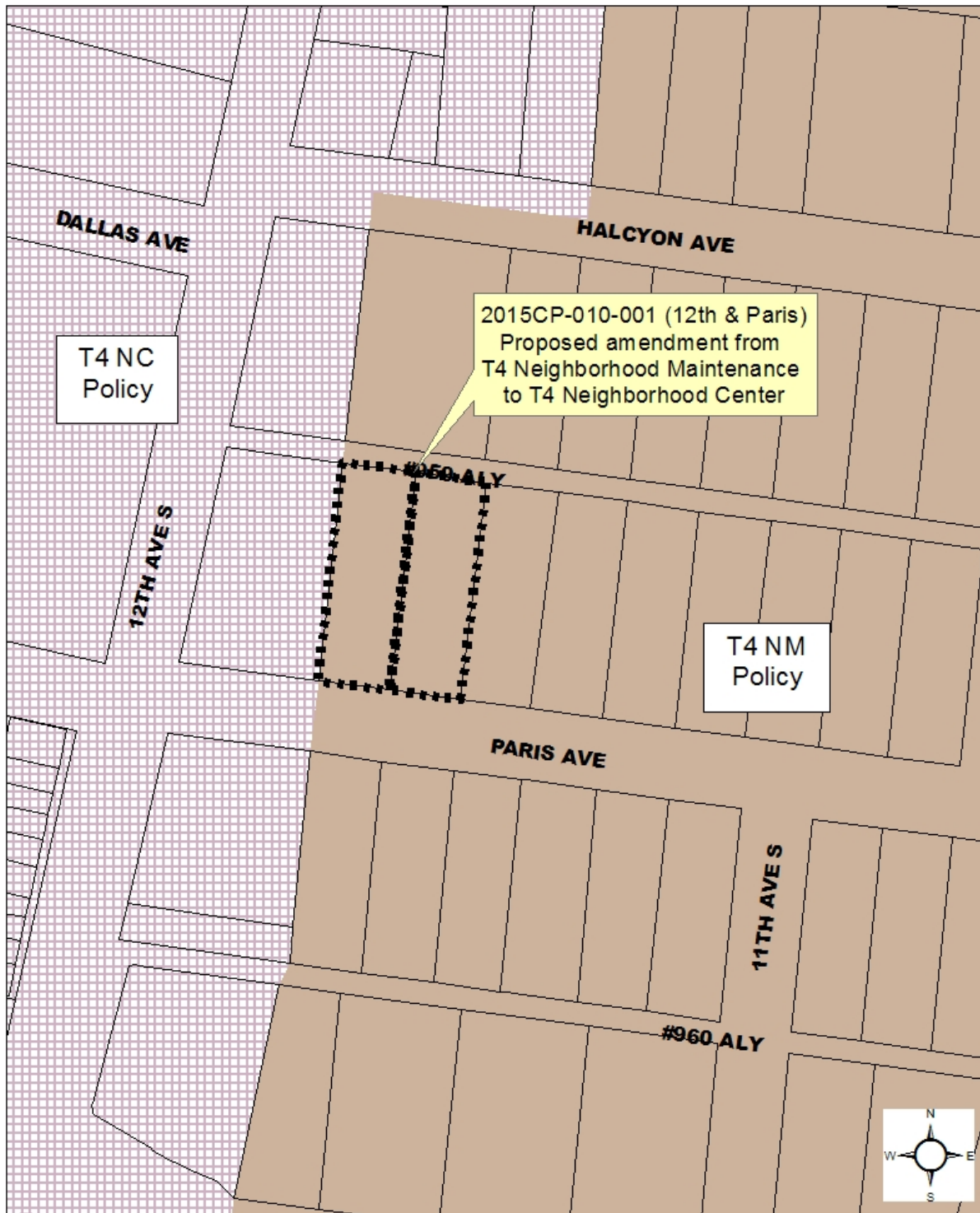


PREVIOUSLY DEFERRED ITEMS

- **Community Plan Amendments**
- **Specific Plans**
- **Subdivisions (Final Plats)**



Metro Planning Commission Meeting of 03/26/2015



2015CP-010-001 GREEN HILLS-MIDTOWN COMMUNITY PLAN AMENDMENT

Map 118-01, Parcel 163 and Parcel 164
Green Hills-Midtown
17 – Sandra Moore



| | |
|-----------------------------|---|
| Project No. | Major Plan Amendment 2015CP-010-001 |
| Project Name | Green Hills-Midtown Community Plan: 2005 Update – Paris Avenue |
| Associated Case | 2014SP-089-001 |
| Council District | 17 – Moore |
| School District | 8 – Pierce |
| Requested by | Civil Site Design Group, applicant; 1221 Partners, LLC, owner. |
| Deferral | This case was deferred from the February 12, 2015, Planning Commission meeting. |
| Staff Reviewer | McCaig |
| Staff Recommendation | <i>Approve.</i> |

APPLICANT REQUEST

Change the policy from Urban Neighborhood Maintenance (residential) to Urban Neighborhood Center (mixture of uses at a neighborhood-scale).

Major Plan Amendment

A request to amend the Green Hills-Midtown Community Plan by changing the Community Character policy from Urban Neighborhood Maintenance policy to Urban Neighborhood Center policy for two properties located on Paris Avenue (unnumbered), approximately 140 feet east of 12th Avenue South (0.34 acres).

CRITICAL PLANNING GOALS

- Creates Walkable Neighborhoods

The application of Urban Neighborhood Center policy encourages an integrated, diverse blend of compatible land uses that allows for living, working, shopping, and services. By focusing mixed use development along 12th Avenue South, the policy will permit a mix of uses and support a strong pedestrian environment.

GREEN HILLS-MIDTOWN COMMUNITY PLAN

Current Policy

Urban Neighborhood Maintenance (T4 NM) policy is intended to preserve the general character of existing urban neighborhoods as characterized by their development pattern, building form, primarily residential land use and associated public realm. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and the public realm. Enhancements may be made to improve pedestrian, bicycle, and vehicular connectivity.

DRAFT Preferred Future Policy

No changes proposed.



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Proposed Policy

Urban Neighborhood Center (T4 NC) policy is intended to preserve, enhance, and create urban neighborhood centers that fit in with the general character of urban neighborhoods. Infrastructure and transportation networks may be enhanced to improve pedestrian, bicycle, and vehicular connectivity. T4 NC areas are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, residential, and civic and public benefit uses.

BACKGROUND

The properties on Paris Avenue have never been developed with houses and have served for years as an informal parking area with gravel and green space. In 1994, the applicant purchased the property at 2814 12th Avenue South as three lots.

COMMUNITY PARTICIPATION

Community Meeting Notices were mailed out to property owners within 1,300 feet on November 21, 2014.

A community meeting was held on December 4, 2014 to discuss the three plan amendment requests and associated rezoning requests currently active in the area. Approximately 55 people attended the meeting, along with the applicants, and the area councilmember. Several attendees voiced concerns and left written comments regarding this proposal. Attendees were mainly concerned that the proposed amendment and rezoning, if approved, would:

- allow commercial to intrude into the adjacent residentially-zoned single-family neighborhood;
- set a precedent for other businesses to expand into residential areas in other locations along the 12th Avenue South corridor;
- continue growth of the center, increase the volume of businesses into the residential area, and push the center's scale beyond that of just serving the immediate neighborhood;
- allow a parking structure that is out of character with the adjacent and surrounding single-family homes;
- allow the introduction of parking garages in the neighborhood center area that may foster additional business intensification and expansion and push the center's scale beyond serving the immediate neighborhood by building taller parking structures; and,
- result in the loss of residential zoning in a desirable area to live.

Several attendees discussed how the current limited amount of available surface parking along 12th Avenue South in turn helps limit the intensity and scale of uses along the corridor. One attendee expressed support for the project. Some attendees thought that the proposed development might be acceptable if the parking structure was removed and only a limited amount of surface parking was created. However, they still remained concerned about the precedent it would set for future commercial encroachment into the adjacent residential area and the implications for increasing the center's scale beyond that of serving the neighborhood.

In January, Public Hearing Notices were mailed out to property owners within 1,300 feet prior to the MPC Public Hearing. Local neighborhood associations were also notified of both the community meeting and the public hearing. Copies of the notices were also placed on the Planning Department website.



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The applicant has continued to work with the community, and Planning staff has received several letters of support for the project from residents in the larger area.

ANALYSIS

Currently, the two lots contain a driveway with an informal parking area of gravel and open space. The two lots are part of a stormwater drainage area with water draining towards the northern end of the second lot. Due to the stormwater drainage, houses have never been built on these two lots. These two properties are unique in that there is stormwater drainage affecting the lots that makes it difficult to build residential structures that would be in character to adjacent residential structures. As such, no houses have ever been constructed on the two lots, and the two lots have been used as parking for many years.

The Green-Hills Midtown Community Plan was last updated in 2005. However, a more specific planning study created the 12th Avenue South Corridor Detailed Neighborhood Design Plan in 2008. The overarching goal of the design plan is to maintain the corridor as a livable and walkable community by providing a well-balanced mix of housing, neighborhood-scaled businesses, real transportation options, easily accessible open spaces, employment and social services, and civic and cultural opportunities. Off the 12th Avenue corridor, the design plan highlights the importance of preserving the existing housing stock and single-family residential character of the immediately adjacent residential area. The design plan accommodates additional housing types by allowing them along the corridor itself. This provides additional housing choices but also helps to preserve the existing single family housing in the area.

At this location, the currently adopted Urban Neighborhood Center policy is only one lot deep. Allowing the Neighborhood Center policy to expand to these two lots does not create a greater depth of center policy than what is to the north along the corridor at Halcyon Avenue.

The applicant understands the neighbors' concerns about commercial encroachment into the residential area. As such, the applicant is providing a landscape buffer on their easternmost property to provide a transition to the adjacent single family house to the east and to clearly mark the limit of the Urban Neighborhood Center.

Furthermore, granting the requested expansion of Urban Neighborhood Center policy should not be taken to mean that center policy should expand in other locations along the corridor. The houses on the west side of the 12th Avenue South corridor are protected by a historic district. However, the houses on the east side of the corridor do not have the same level of protection. To the east, north and south is an established single-family residential area. Additional housing is in great demand in the 12South area, and the neighborhood is extremely concerned about commercial continuing to intrude into the residential area and the potential loss of the existing single-family housing stock and character over time.

The 12th Avenue South corridor is envisioned as a neighborhood-scaled center. Currently, it is experiencing growth pressures similar to what is found along major corridors, such as 8th Avenue South. Negative implications for other properties in the area are considered with any plan amendment. Without a clearly defined boundary to limit any commercial expansion, the potential exists for fragmented business expansion into the adjacent residential neighborhoods. Each case



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should be considered and analyzed carefully to meet the policy goals for the area and avoid any negative consequences for the larger neighborhood.

STAFF RECOMMENDATION

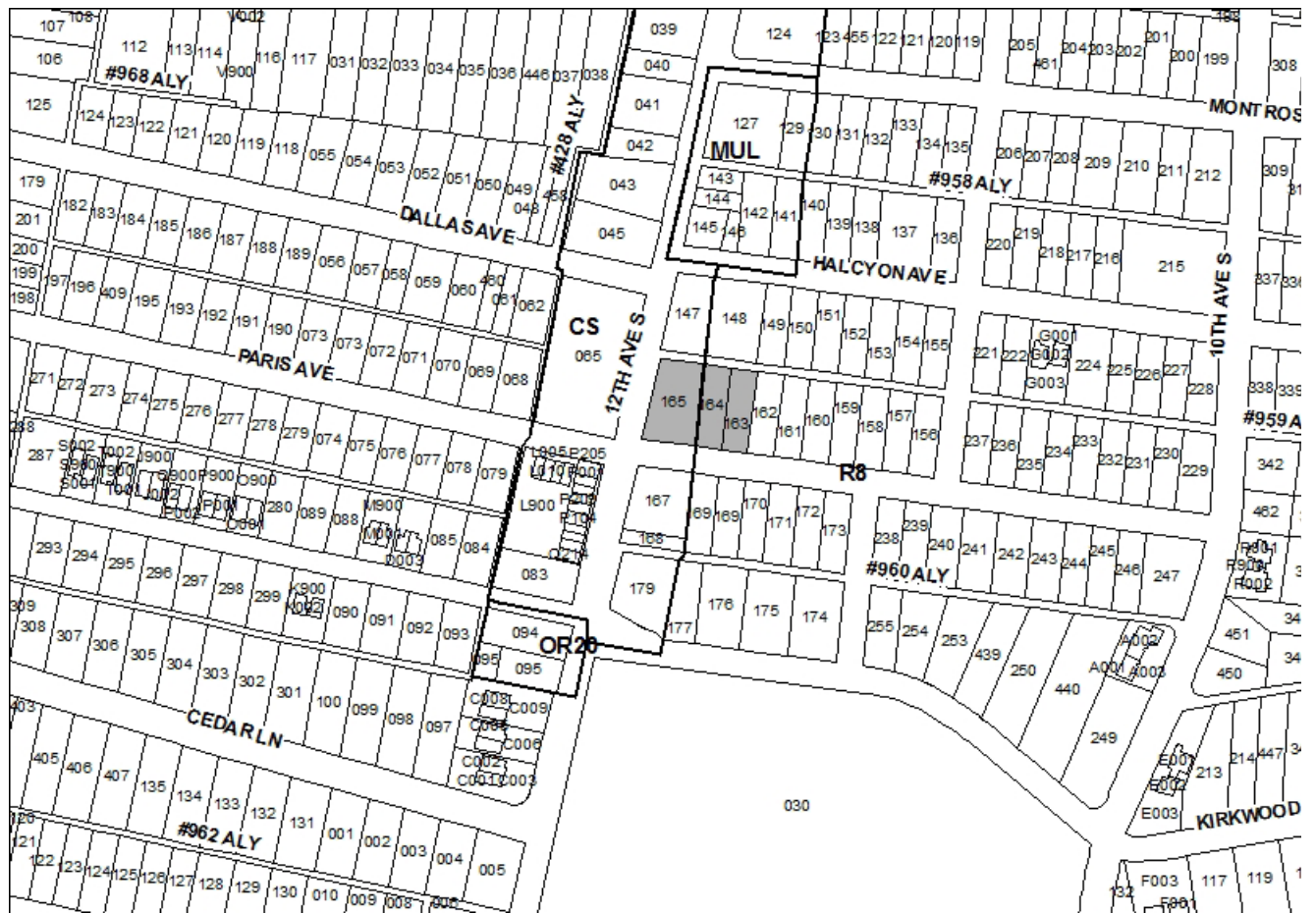
Staff recommends approval of the amendment application due to the unique circumstances regarding the stormwater drainage on these properties and the minimal depth that Urban Neighborhood Center policy would increase into the adjacent residential area at this location.



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2014SP-089-001
12TH & PARIS
Map 118-01, Parcel(s) 163-165
10, Green Hills - Midtown
17 (Sandra Moore)



Project No. 2014SP-089-001
Project Name 12th & Paris
Associated Case 2015CP-010-001
Council District 17 - Moore
School District 8 - Pierce
Requested by Civil Site Design Group, applicant; 1221 Partners, LLC, owner.

Deferrals This case was deferred from the February 12, 2015, Planning Commission Meeting

Staff Reviewer Milligan
Staff Recommendation *Approve with conditions and disapprove without all conditions, subject to approval of the associated policy change.*

APPLICANT REQUEST

Preliminary SP to permit mixed-use development.

Preliminary SP

A request to rezone from Commercial Services (CS) and One and Two-Family Residential (R8) to Specific Plan-Mixed Use (SP-MU) for properties located at 2814 12th Avenue South and Paris Avenue (unnumbered) (0.64 acres) to permit a mixed-use development with up to 6 residential units.

Existing Zoning

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 5.79 dwelling units per acre including 25 percent duplex lots. *R8 would permit a maximum of 1 lot with 1 duplex lot for a total of 2 units.*

Commercial Service (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.

Proposed Zoning

Specific Plan-Mixed Use (SP-MU) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes residential uses, retail uses, and office uses.

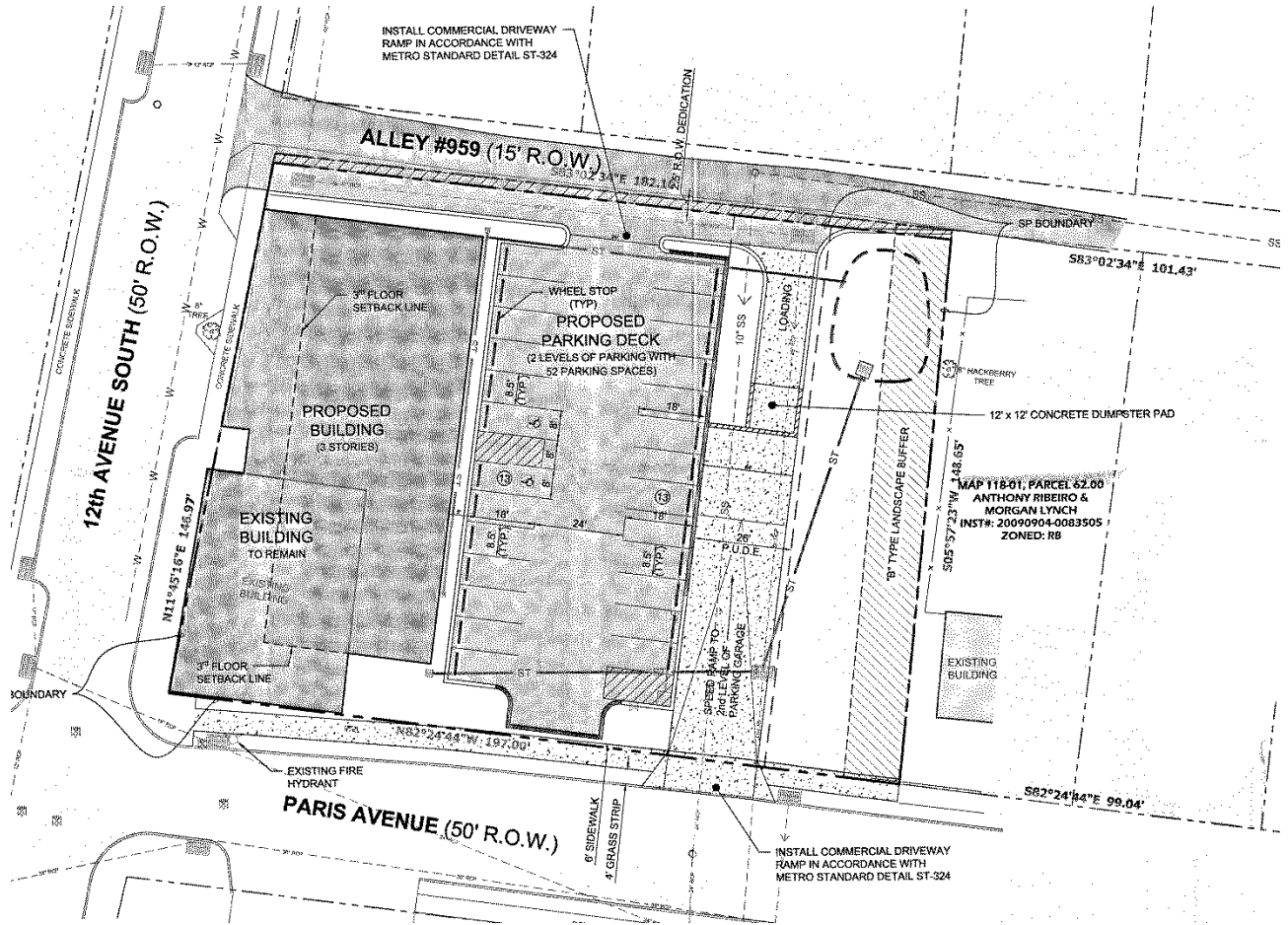
Critical Planning Goals

- Create Walkable Neighborhoods
- Supports a Variety of Transportation Choices
- Supports Infill Development

The proposed development supports several critical planning goals. By adding a mixture of uses and improving sidewalks, the development is creating a more walkable neighborhood. The



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Proposed Site Plan



Paris Avenue Elevation



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proposed retail uses will be a destination for existing nearby residences and the ground floor uses will be active providing for an interesting streetscape that promotes walking. The development is along an existing bus route and creates more opportunity for transit ridership by providing for services along the route. The plan supports infill development on an underutilized urban site placing more intensity in a location where infrastructure is available placing less burden upon Metro.

GREEN HILLS MIDTOWN COMMUNITY PLAN

Current Policy

T4 Urban Neighborhood Maintenance (T4 NM) policy is intended to preserve the general character of urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use and the public realm. Where not present, enhancements are made to improve pedestrian, bicycle and vehicular connectivity.

T4 Urban Neighborhood Center (T4 NC) policy is intended to preserve, enhance, and create urban neighborhood centers that are compatible with the general character of urban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T4 Urban Neighborhood Centers are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, civic and public benefit land uses, with residential only present in mixed use buildings. T4 Urban Neighborhood Centers serve urban neighborhoods within a 5 minute walk.

DRAFT Preferred Future Policy

No changes proposed.

Proposed Policy

T4 Urban Neighborhood Center (T4 NC) is intended to preserve, enhance, and create urban neighborhood centers that are compatible with the general character of urban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T4 Urban Neighborhood Centers are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, civic and public benefit land uses, with residential only present in mixed use buildings. T4 Urban Neighborhood Centers serve urban neighborhoods within a 5 minute walk.

Consistent with Policy?

Yes. The portion of the development fronting on 12th Avenue South is currently within the T4 Neighborhood Center Policy. Staff is recommending approval of the policy change for the parcel fronting on Paris Avenue to become T4 Neighborhood Center. The proposed development is consistent with the existing and proposed policy in that it is providing an intensified use within an existing neighborhood center. Residential uses are being incorporated into the development which enhances the neighborhood by providing for an additional housing option.



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PLAN DETAILS

The site is located at 2814 12th Avenue South and Paris Avenue (unnumbered) at the northeast corner of 12th Avenue South and Paris Avenue. The site is approximately 0.64 acres in size. There is currently an existing commercial building fronting on 12th Avenue South and the remainder of the property is used for parking or is vacant.

Site Plan

The plan proposes to keep the existing building that is located at the corner of 12th Avenue South and Paris Avenue and to add a building around and over the existing building. The existing building is a 1-story commercial building. The addition is proposed to include retail on the 1st floor, office on the 2nd floor and six multi-family residential units on the 3rd floor. The maximum height of the building is 3 stories. There is a stepback for the 3rd floor from both 12th Avenue South and Paris Avenue, ranging from 15 feet to just under 20 feet. The stepback along Paris Avenue is 15 feet.

A two level parking structure is proposed behind the three story building. The structure will have one level of parking at street level and one level below street level. Buffering and screening is being provided where the parking structure abuts existing residential properties. A knee wall and landscaping is being proposed adjacent to Paris Avenue to screen the parking area. The applicant moved the parking structure closer to 12th Avenue South with the resubmitted plan.

Access to the parking structure is proposed from Paris Avenue and from the existing alley to the north of the property. A 4' planting strip and 6' sidewalk is being proposed along the Paris Avenue frontage. The Major and Collector Street Plan requires a 12' sidewalk along 12th Avenue South, which is not being provided. Staff recommends that, if approved, a condition be added to comply with the Major and Collector Street Plan along the portion of the frontage where the new building is being added.

ANALYSIS

The site plan is consistent with T4 Urban Community Center adjacent to T4 Neighborhood Maintenance. The plan is providing for additional buffering and improves upon the current use of the property.

FIRE DEPARTMENT RECOMMENDATION

Approve with conditions

1. Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Approved

WATER SERVICES

Approved

PUBLIC WORKS RECOMMENDATION

No exception taken

1. The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.



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TRAFFIC AND PARKING RECOMMENDATION

No exception taken

Maximum Uses in Existing Zoning District: **CS**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Retail (814) | 0.29 | 0.6 F | 7,579 SF | 362 | 14 | 40 |

Maximum Uses in Existing Zoning District: **R8**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Two-Family Residential (210) | 0.34 | 5.44 D | 2 U* | 20 | 2 | 3 |

*Based on one two-family lot.

Maximum Uses in Proposed Zoning District: **SP-MU**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Retail (814) | 0.63 | - | 7,000 SF | 338 | 13 | 39 |

Maximum Uses in Proposed Zoning District: **SP-MU**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Office (710) | 0.63 | - | 14,000 SF | 294 | 39 | 95 |

Traffic changes between maximum: **CS** and **R8** and **SP-MU**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | - | +250 | +36 | +91 |

METRO SCHOOL BOARD REPORT

Projected student generation existing R8 district: 0 Elementary 0 Middle 0 High

Projected student generation proposed SP-MU district: 0 Elementary 0 Middle 0 High

The proposed SP-MU zoning district will not generate any additional students than the existing zoning.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions subject to the approval of the associated community plan amendment. If the community plan amendment is not approved, staff recommends disapproval.



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CONDITIONS

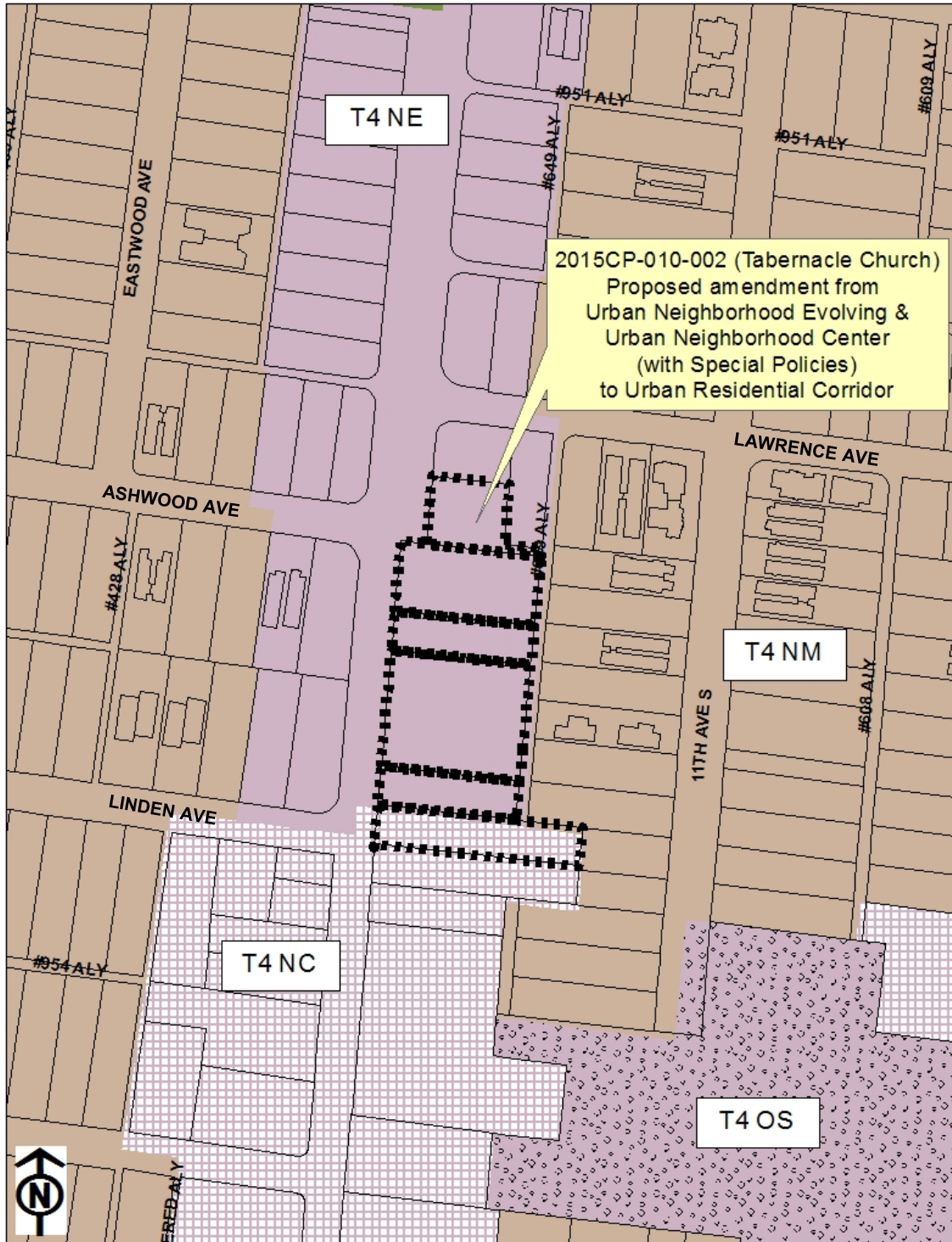
1. Uses shall be limited to up to 6 multi-family residential dwelling units, office, and retail uses.
2. The 12th Avenue South frontage shall include a sidewalk 10 feet in width by increasing the building setback or removing on street parking.
3. On the corrected set of plans, the proposed parking must meet or exceed the required parking.
4. On the corrected set of plans, provide bike parking as per the adopted Bicycle Parking Ordinance.
5. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the MUN-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
6. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
7. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
8. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2015CP-010-002
GREEN HILLS-MIDTOWN COMMUNITY PLAN AMENDMENT
Map 105-13, Parcels 198, 200-203, and 420
Green Hills-Midtown
17 – Sandra Moore



**Project No.
Project Name**

**Minor Plan Amendment 2015CP-010-002
Green Hills-Midtown Community Plan: 2005
Update – Bristol 12 South**

**Associated Case
Council District
School District
Requested by**

2015SP-010-001
17 – Moore
8 – Pierce
Littlejohn Engineering Associates, applicant; Tabernacle Baptist Church, owner.

Deferral

This case was deferred from the February 12, 2015, and the February 26, 2015, Planning Commission meetings.

**Staff Reviewer
Staff Recommendation**

McCaig
Approve, including retaining and amending the special policy.

APPLICANT REQUEST

Change the policy from Urban Neighborhood Evolving and Urban Neighborhood Center to Urban Residential Corridor from the 12th Avenue South Corridor Detailed Neighborhood Design Plan.

Minor Plan Amendment

A request to amend the 12th Avenue South Corridor Detailed Neighborhood Design Plan element of the Green Hills-Midtown Community Plan: 2005 Update by changing the Community Character policy from Urban Neighborhood Evolving policy and Urban Neighborhood Center policy with Special Policies to Urban Residential Corridor policy for property located at 2206-2220 12th Avenue South, approximately 140 feet east of 12th Avenue South, (1.89 acres).

CRITICAL PLANNING GOALS

Provides a Range of Housing Choices

The Urban Residential Corridor policy supports various types of residential uses, including townhouses and flats, which add housing options to the surrounding 12South neighborhood, consisting of primarily single family housing.

Creates Walkable Neighborhoods

The Urban Residential Corridor policy allows for a variety of housing types that will help create a more walkable environment than exists today. The zoning districts used to implement the new policy place an emphasis on building and site design that support walkability, such as making strong connections between main entrances and sidewalks, orienting buildings toward the sidewalk, and minimizing the prominence of parking facilities.

Supports Infill Development

The Urban Residential Corridor policy will foster new development and redevelopment in an urban infill location where much of the needed infrastructure is already in place and additional infrastructure, such as sidewalks, can be made as zone changes occur to implement the new policy.



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GREEN HILLS-MIDTOWN COMMUNITY PLAN

Current Policy

Urban Neighborhood Maintenance (T4 NM) policy is intended to preserve the general character of existing urban neighborhoods as characterized by their development pattern, building form, primarily residential land use and associated public realm. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and the public realm. Enhancements may be made to improve pedestrian, bicycle, and vehicular connectivity. This policy applies to the majority of the site.

Urban Neighborhood Center (T4 NC) policy is intended to preserve, enhance, and create urban neighborhood centers that fit in with the general character of urban neighborhoods. Infrastructure and transportation networks may be enhanced to improve pedestrian, bicycle, and vehicular connectivity. T4 NC areas are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, residential, and civic and public benefit uses. This policy applies to the southernmost property.

Special Policies: The *12th Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

- rezone properties to Specific Plan with the density of RM15;
- maintain and continue throughout the Subdistrict the current pattern of built setbacks and spacing that is found in the portion of this Subdistrict located on the west side of 12th Avenue South between #951 Alley (opposite South Douglas Avenue) and #666 Alley (alley north of Ashwood Avenue);
- limit heights in this Subdistrict to a maximum of three stories at 35 feet with additional height allowed for raised foundation when necessary for providing privacy;
- place buildings so that the primary pedestrian entrance is oriented to the street;
- access buildings from alleys; and,
- provide parking behind, beneath, or beside buildings.

DRAFT Preferred Future Policy

Urban Residential Corridor (T4 RC) policy is intended to preserve, enhance, and create urban residential corridors that support predominantly residential land uses; fit in with the general character of urban neighborhoods; and move vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

Urban Neighborhood Center (T4 NC) policy is intended to preserve, enhance, and create urban neighborhood centers that fit in with the general character of urban neighborhoods. Infrastructure and transportation networks may be enhanced to improve pedestrian, bicycle, and vehicular connectivity. T4 NC areas are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, residential, and civic and public benefit uses. This policy applies to the southernmost property.

Special Policies: The *12th Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing



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with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

- rezone properties to Specific Plan with the density of RM15;
- maintain and continue throughout the Subdistrict the current pattern of built setbacks and spacing that is found in the portion of this Subdistrict located on the west side of 12th Avenue South between #951 Alley (opposite South Douglas Avenue) and #666 Alley (alley north of Ashwood Avenue);
- limit heights in this Subdistrict to a maximum of three stories at 35 feet with additional height allowed for raised foundation when necessary for providing privacy;
- place buildings so that the primary pedestrian entrance is oriented to the street;
- access buildings from alleys; and,
- provide parking behind, beneath, or beside buildings.

Proposed Policy

Urban Residential Corridor (T4 RC) policy is intended to preserve, enhance, and create urban residential corridors that support predominantly residential land uses; fit in with the general character of urban neighborhoods; and move vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

BACKGROUND

The properties located at 2206-2220 12th Avenue South have been used as a religious institution and single family homes for years. Property records show that the church purchased its main property in 1966. The church acquired additional adjacent properties in 1982, 1985, 1998, and 2005, both for parking and for single family homes.

The Green-Hills Midtown Community Plan was last updated in 2005. However, a more specific planning study created the *12th Avenue South Corridor Detailed Neighborhood Design Plan* in 2008. The overarching goal of the design plan is to maintain the corridor as a livable and walkable community by providing a well-balanced mix of housing, neighborhood-scaled businesses, real transportation options, easily accessible open spaces, employment and social services, and civic and cultural opportunities. As one moves off the 12th Avenue corridor, the design plan highlights the importance of preserving the existing housing stock and single family residential character of the immediately adjacent residential area. The design plan accommodates additional housing types by allowing them along the corridor itself. This provides additional housing choices but also helps to preserve the existing single family housing.

COMMUNITY PARTICIPATION

This is a minor plan amendment, and, as such, no community meeting is required. However, with two other major plan amendments currently active in the area, a community meeting was held on December 4, 2014, to discuss the three plan amendment requests and associated rezoning requests. Approximately 55 people attended the meeting, along with the applicants, and the area councilmember. For many attendees, the community meeting was the first time they were hearing about the plan amendment for the church property. Several attendees voiced concerns and left written comments regarding this proposal. Attendees were mainly concerned that the proposed amendment and rezoning, if approved, would:

- create a large-scale building that is out of character with the corridor due to its intensity;
- create too much density on a small piece of land;



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- create negative traffic impacts due to the property's location – at the point where 12th Avenue South narrows and enters the business area which is already congested; and
- create the need for a traffic light that in turn would create more cut-through traffic on neighborhood streets.

In January, Public Hearing Notices were mailed out to property owners within 600 feet prior to the MPC Public Hearing. Local neighborhood associations were also notified of both the community meeting and the public hearing. Copies of the notices were also placed on the Planning Department website.

ANALYSIS

Currently, the six properties involved in the plan amendment contain a church, parking, and four single family homes. The properties are located along 12th Avenue South and have been designated by policy for additional housing opportunities. However, additional design guidance is provided through the detailed neighborhood design plan that discusses building height, access, density, and setbacks to provide a harmonious development with what exists along the corridor (see guidance under the Current Policy section above).

The applicant has requested Urban Residential Corridor policy which is applied to prominent urban corridors with adequate transportation capacity for higher intensity residential uses. Urban Residential Corridor policy allows buildings up to three stories in height. In deciding the character and form of what is appropriate in a specific location, locational factors – including the width of the street, depth of the lots, topography, existing character of the surrounding properties along the corridor, and how the development transitions to what is behind it – are considered. Appropriate density is secondary to the building's form and design. At this location along the 12th Avenue South corridor, staff recommends that the special policy be amended to remove the density limitation of RM15, but retain the other design features, including the limited height of 35 feet. The proposed special policy language is:

Special Policies: The *12th Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

- maintain and continue throughout the Subdistrict the current pattern of built setbacks and spacing that is found in the portion of this Subdistrict located on the west side of 12th Avenue South between #951 Alley (opposite South Douglas Avenue) and #666 Alley (alley north of Ashwood Avenue);
- limit heights in this Subdistrict to a maximum of three stories at 35 feet with additional height allowed for raised foundation when necessary for providing privacy;
- place buildings so that the primary pedestrian entrance is oriented to the street;
- access buildings from alleys; and,
- provide parking behind, beneath, or beside buildings.

The 12th Avenue South corridor is envisioned as a neighborhood-scaled center that includes opportunities for additional housing. Having this property redevelop as higher density residential



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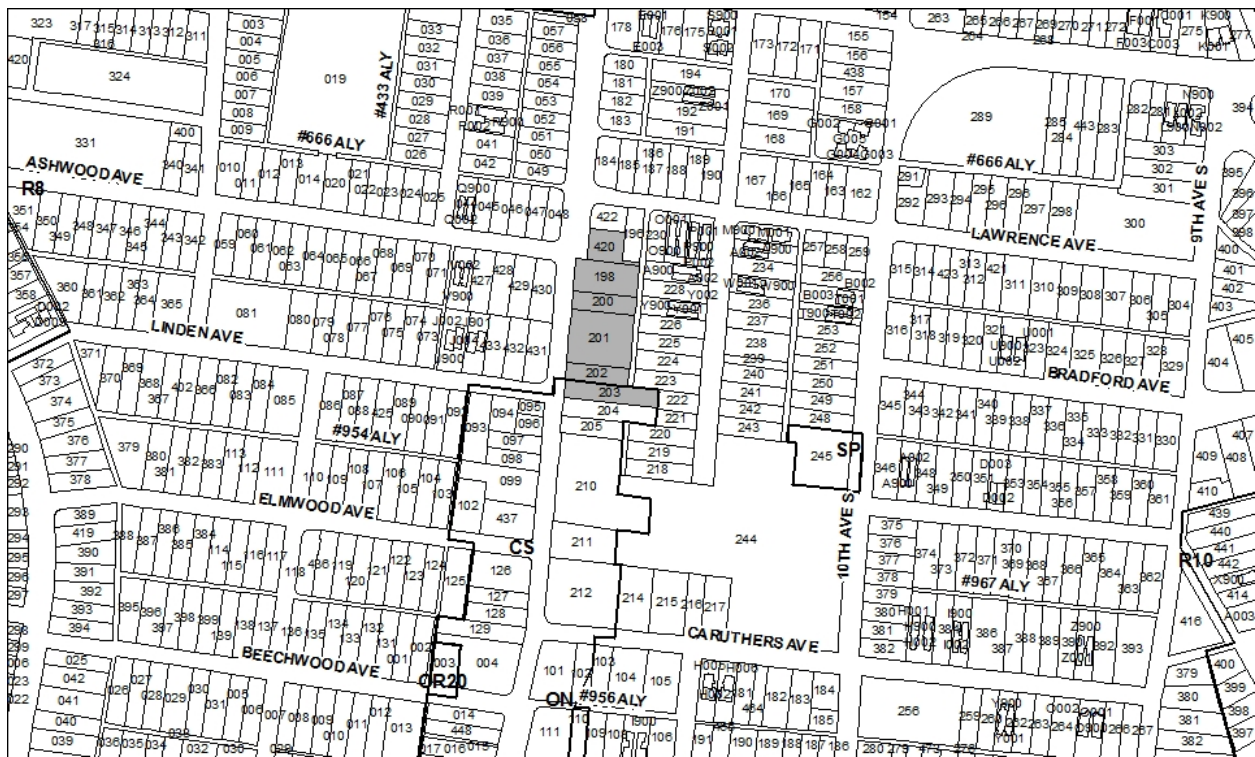
uses is appropriate. The proposed development meets the policy intent of the proposed Urban Residential Corridor policy.

STAFF RECOMMENDATION

Staff recommends approval of the plan amendment, including retaining and amending the special policy, as it reflects the area's recommended policy change as part of NashvilleNext .



Metro Planning Commission Meeting of 03/26/2015



2015SP-010-001

BRISTOL 12 SOUTH

Map 105-13, Parcel(s) 198, 200-203, 420

10, Green Hills - Midtown

17 (Sandra Moore)



Project No. 2015SP-010-001
Project Name Bristol 12 South
Associated Case 2015CP-010-002
Council District 17 - Moore
School District 8 - Pierce
Requested by Littlejohn Engineering Associates, applicant; Tabernacle Baptist Church, owner.

Deferrals This case was deferred from the February 12, 2015, and the February 26, 2015, Planning Commission meeting.

Staff Reviewer Milligan
Staff Recommendation *Approve with conditions and disapprove without all conditions, subject to approval of the associated policy change.*

APPLICANT REQUEST

Preliminary SP to permit a multi-family residential development.

Preliminary SP

A request to rezone from Commercial Services (CS) and One and Two-Family Residential (R8) to Specific Plan-Residential (SP-R) for properties located at 2206, 2208, 2212, 2214, 2218, and 2220 12th Avenue South (1.89), to permit a multi-family residential development with up to 158 units.

Existing Zoning

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 5.79 dwelling units per acre including 25 percent duplex lots. *R8 would permit a maximum of 8 lots with 2 duplex lots for a total of 10 units.*

Commercial Service (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.

Proposed Zoning

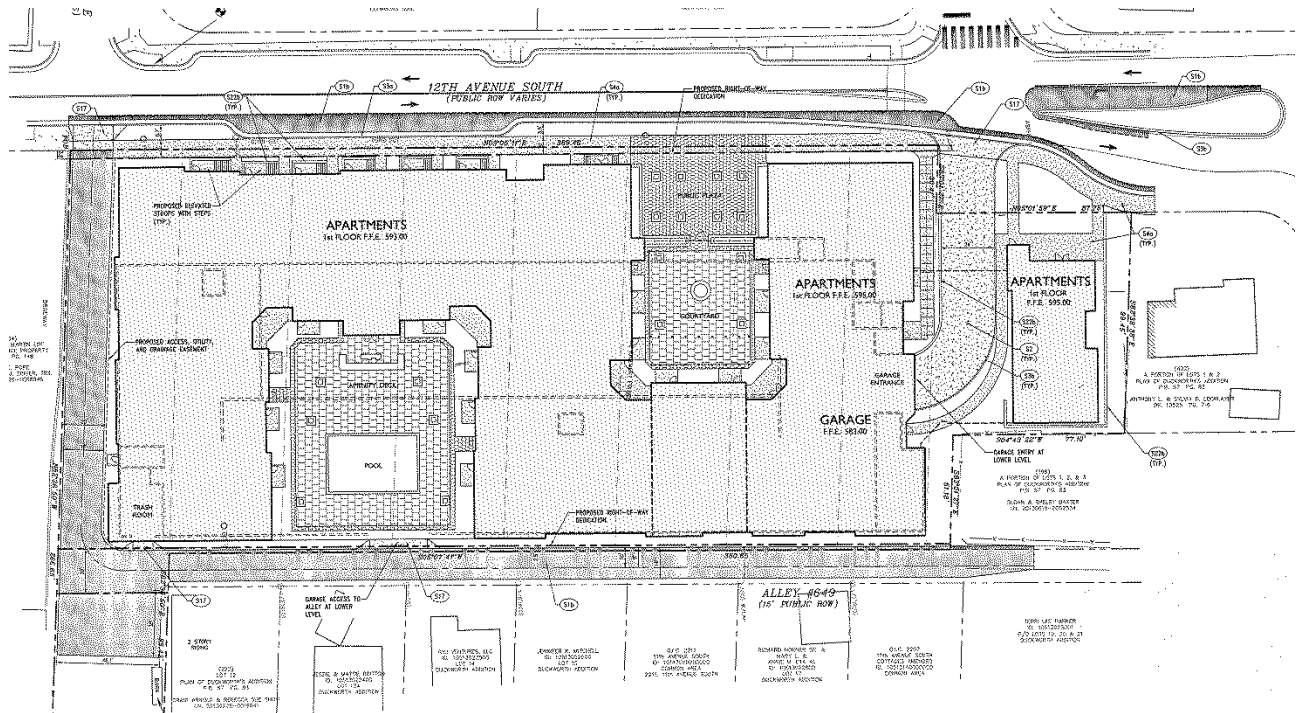
Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

CRITICAL PLANNING GOALS

- Supports a Variety of Transportation Choices
- Creates Walkable Neighborhoods
- Provides a Range of Housing Choices
- Supports Infill Development



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Proposed Site Plan



Proposed 12th Avenue S Elevation



Metro Planning Commission Meeting of 03/26/2015

The proposed development supports several critical planning goals. The development is intensifying an underutilized urban site and providing for an additional housing option within an urban neighborhood. Providing for infill development on a site with existing infrastructure decreases the burden on Metro to provide for new infrastructure. The development provides sidewalks, increasing the walkability in an urban neighborhood. The additional residents will also be able to walk to nearby retail uses and restaurants, decreasing the dependency on automobile travel. The project is located on an existing bus line, providing for additional transportation choices for future residents.

GREEN HILLS MIDTOWN COMMUNITY PLAN

Current Policy

T4 Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

T4 Urban Neighborhood Center (T4 NC) policy is intended to preserve, enhance, and create urban neighborhood centers that are compatible with the general character of urban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T4 Urban Neighborhood Centers are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, civic and public benefit land uses, with residential only present in mixed use buildings. T4 Urban Neighborhood Centers serve urban neighborhoods within a 5 minute walk.

Special Policies: The *12th Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

- rezone properties to Specific Plan with the density of RM15;
- maintain and continue throughout the Subdistrict the current pattern of built setbacks and spacing that is found in the portion of this Subdistrict located on the west side of 12th Avenue South between #951 Alley (opposite South Douglas Avenue) and #666 Alley (alley north of Ashwood Avenue);
- limit heights in this Subdistrict to a maximum of three stories at 35 feet with additional height allowed for raised foundation when necessary for providing privacy;
- place buildings so that the primary pedestrian entrance is oriented to the street;
- access buildings from alleys; and
- provide parking behind, beneath, or beside buildings.

DRAFT Preferred Future Policy

The T4 Urban Neighborhood Center and Special Policies are not proposed to change. The T4 Urban Neighborhood Evolving area is proposed to change to T4 Urban Residential Corridor.



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T4 Urban Residential Corridor (T4 RC) policy is intended to preserve, enhance and create urban residential corridors that support predominately residential land uses; are compatible with the general character of urban neighborhoods as characterized by development pattern, building form, land use, and associated public realm; and that move vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

Special Policies: The *12th Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

- rezone properties to Specific Plan with the density of RM15;
- maintain and continue throughout the Subdistrict the current pattern of built setbacks and spacing that is found in the portion of this Subdistrict located on the west side of 12th Avenue South between #951 Alley (opposite South Douglas Avenue) and #666 Alley (alley north of Ashwood Avenue);
- limit heights in this Subdistrict to a maximum of three stories at 35 feet with additional height allowed for raised foundation when necessary for providing privacy;
- place buildings so that the primary pedestrian entrance is oriented to the street;
- access buildings from alleys; and
- provide parking behind, beneath, or beside buildings.

Proposed Policy

T4 Urban Residential Corridor (T4 RC) policy is intended to preserve, enhance and create urban residential corridors that support predominately residential land uses; are compatible with the general character of urban neighborhoods as characterized by development pattern, building form, land use, and associated public realm; and that move vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

Consistent with Policy?

The proposed development is consistent with the T4 Residential Corridor policy in regards to the height of the project. The development also meets the height restriction of the Special Policy for the area, providing for 3 stories within 35 feet, with some allowance for raised foundations. The development meets the intent of the T4 Residential Corridor policy and the Special Policies by providing for a development that is compatible with the general character of the existing urban neighborhood in both scale and design. Sidewalks and an improved pedestrian environment are being provided to enhance the existing neighborhood.

PLAN DETAILS

The site is located at 2206, 2208, 2212, 2214, 2218, and 2220 12th Avenue South. The site is approximately 1.89 acres in size. The property is currently in use as a church, parking lot, and several residential dwelling units.

Site Plan

The plan proposes a multi-family residential development with up to 158 residential dwelling units. The proposed building is 3 stories in 35 feet along 12th Avenue South, not including a raised foundation of no more than 36". Along the alley, the building has one garage level with three stories of residential above. The building will read as 4 stories on the alley.



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Vehicular access is being proposed at two points along 12th Avenue South, one at the northern property line and one at the southern property line. The access on the southern property line will connect to the existing alley. The parking garage can be accessed from the alley. Along 12th Avenue South, eight foot sidewalks are being proposed. The plan would meet the requirements of the Bike Parking Ordinance.

The plan proposes raised foundations along 12th Avenue South with the inclusion of stoops/porches for some of the ground floor units. The façade plane of the building will be interrupted every 50' to avoid a continuous, uninterrupted blank façade. Interruptions shall include a change in material, horizontal undulation, or a porch/stoop/balcony. Street facades shall also include a minimum of 15% glazing. A public plaza has been provided along 12th Avenue South. A landscape buffer is being provided along the northern boundary, adjacent to the existing residential properties.

ANALYSIS

The proposed development meets the intent of the T4 Residential Corridor policy and the Special Policies for the area by complementing the existing form and intensity within the 12th Avenue South corridor. The height of the building along 12th Avenue South meets the general and special policies. The design of the building and the inclusion of stoops, porches, and the courtyard area along 12th Avenue South provides for a pedestrian friendly streetscape that is broken up and lessens the intensity along the street. Given the location of the building along the existing urban corridor and based on the design, staff recommends approval.

FIRE DEPARTMENT RECOMMENDATION

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Approved with conditions

- Minimum storm pipe in ROW shall be 15" minimum

WATER SERVICES

Approved

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Record ROW dedication to the back of the proposed sidewalks, prior to building permit approval.

TRAFFIC AND PARKING RECOMMENDATION

Approved with conditions

In accordance with TIS findings, developer shall construct the following roadway improvements.



Metro Planning Commission Meeting of 03/26/2015

12th Avenue South and Ashwood Avenue/Site Access

- The proposed site access on 12th Avenue South shall be aligned with Ashwood Avenue and designed to include a minimum of one entering lane and one exiting lane.
- Developer shall construct a southbound left turn lane on 12th Avenue South at garage access. The left turn lane can be constructed by reconstructing the existing median. A minimum of 50 feet of storage and 50 feet of taper should be provided. The reconstruction of the median may require modification or relocation of the existing “12 South Neighborhood” sign within the median. As part of this requirement, storage shall be provided to accommodate the NBLT movement onto Ashwood in order to provide appropriate sight distance for vehicle and pedestrian movements.
- In order to improve intersection sight distance for traffic exiting the site, the driveway should be extended into the public right-of-way so that the sight lines are in front of the building face. On-street parking should not be provided on the east side of 12th Avenue South within 200 feet south of the site access in order to provide sufficient intersection sight distance for vehicles exiting the site access.
- A bulb-out should be constructed within the excess right-of-way north of the site access. A minimum of 16 feet of pavement should be provided between the median and the curb extension on the north side of the site access.
- Developer shall install a crosswalk for the west leg of Ashwood Avenue.

Maximum Uses in Existing Zoning District: CS

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Retail (814) | 0.33 | 0.6 F | 8,624 SF | 407 | 15 | 43 |

Maximum Uses in Existing Zoning District: R8

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Two-Family Residential (210) | 1.56 | 5.4 D | 10 U * | 96 | 8 | 11 |

*Based on two two-family lots.

Maximum Uses in Proposed Zoning District: SP-R

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|--------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Multi-Family Residential (220) | 1.89 | - | 158 U | 1082 | 82 | 105 |

Traffic changes between maximum: CS and R8 and SP-R

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | - | +579 | +59 | +51 |



Metro Planning Commission Meeting of 03/26/2015

METRO SCHOOL BOARD REPORT

Projected student generation existing R8 district: 1 Elementary 0 Middle 1 High

Projected student generation proposed SP-R district: 1 Elementary 0 Middle 1 High

The proposed SP-R zoning district could generate the same number of students than what is typically generated under the existing R8 zoning district utilizing the urban infill factor. Students would attend Julia Green Elementary, J.T. Moore Middle School and Hillsboro High School. Julia Green and J.T Moore Middle have been identified as over capacity and there is no capacity for elementary or middle school students within the cluster. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

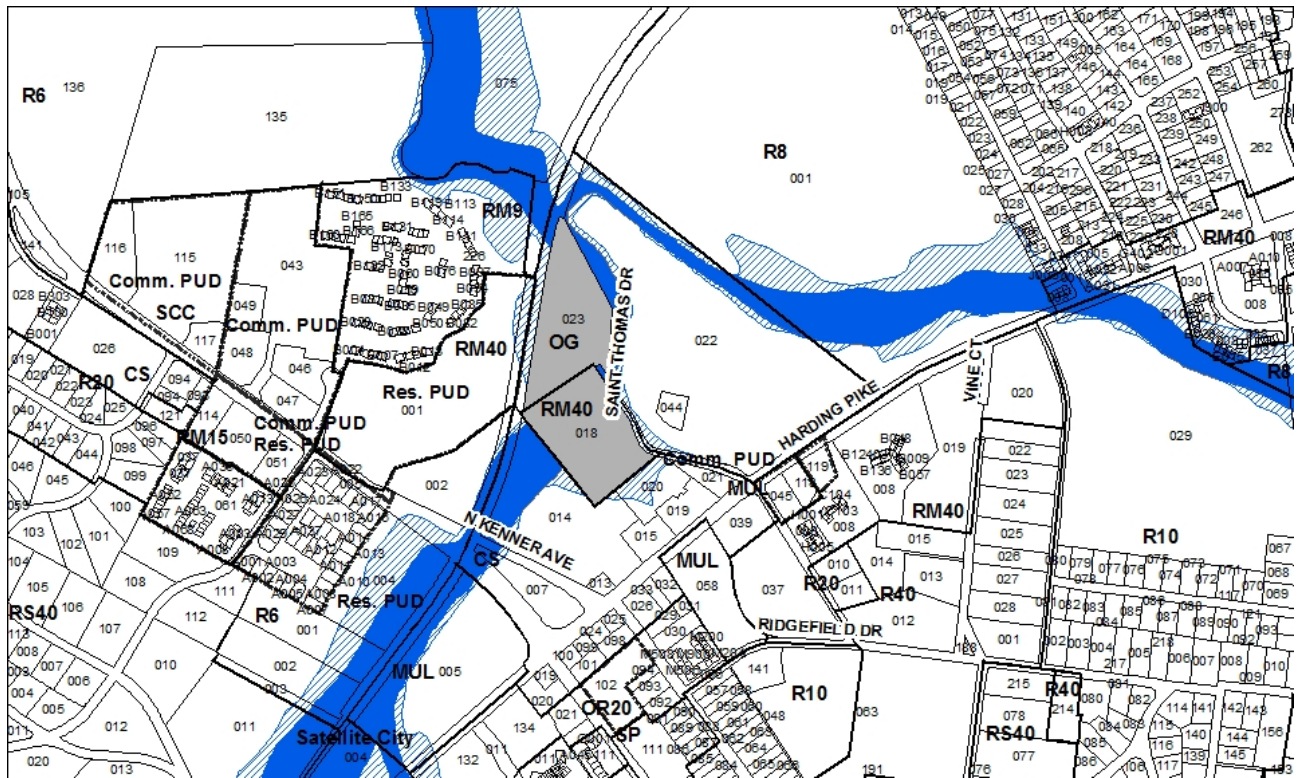
Staff recommends approval with conditions and disapproval without all conditions subject to the approval of the associated policy change. If the associated policy change is not approved, staff recommends disapproval.

CONDITIONS

1. Uses shall be limited to up to 158 multi-family residential dwelling units.
2. On the corrected set, add a note that the minimum raised foundation along 12th Avenue South shall be 18” and the maximum raised foundation along 12th Avenue South shall be 36”.
3. The final site plan shall show and adjusted building location and public plaza in order to provide a minimum of 4 foot grass strip between the curb and the sidewalk and an 8 foot sidewalk up to the crosswalk on the south side of the entrance drive opposite Ashwood Avenue. The width of the frontage planting area shall remain as shown on the current plan.
4. With the Final Plat, dedicate right-of-way to the Major and Collector Street specifications. Any additional improvements, including sidewalks, shall be incorporated into a pedestrian easement on the final plat.
5. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM80-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
6. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
7. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
8. The requirements of the Metro Fire Marshal’s Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2013SP-018-001
SAINT THOMAS HOSPITAL DISTRICT
Map 103-15, Parcel(s) 018 and 023
10, Green Hills - Midtown
24 (Jason Holleman)



Project No. **Zone Change 2013SP-018-001**
Project Name **St. Thomas Hospital District SP**
Council District 24 – Holleman
School District 9 – Frogge
Requested by Civil Site Design Group, applicant, Saint Thomas Hospital, owner.

Deferrals This case was deferred from the March 12, 2015, Planning Commission meeting.

Staff Reviewer Swaggart
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

Preliminary SP to permit a mixed use development.

Preliminary SP

A request to rezone from Office General (OG) and Multi-Family Residential (RM40) to Specific Plan-Mixed Use (SP – MU) zoning for property located at 109 and 123 Bosley Springs Road, approximately 530 feet west of Harding Pike and located within the Harding Town Center Urban Design Overlay District and the Floodplain Overlay District (13.12 acres), to permit a mixed use development.

Existing Zoning

Office General (OG) is intended for moderately high intensity office uses. *The existing OG district would permit a maximum of 399,880 square feet of floor space.*

Multi-Family Residential (RM40) is intended for single-family, duplex, and multi-family dwellings at a density of 40 dwelling units per acre. *RM40 would permit a maximum of 230 units.*

Harding Town Center Urban Design Overlay (UDO) is a zoning tool that allows for a specifically designated area to have unique physical design standards in order to either protect the design character already established, or to create a design character that would otherwise not be ensured by the standard provisions of the zoning regulations.

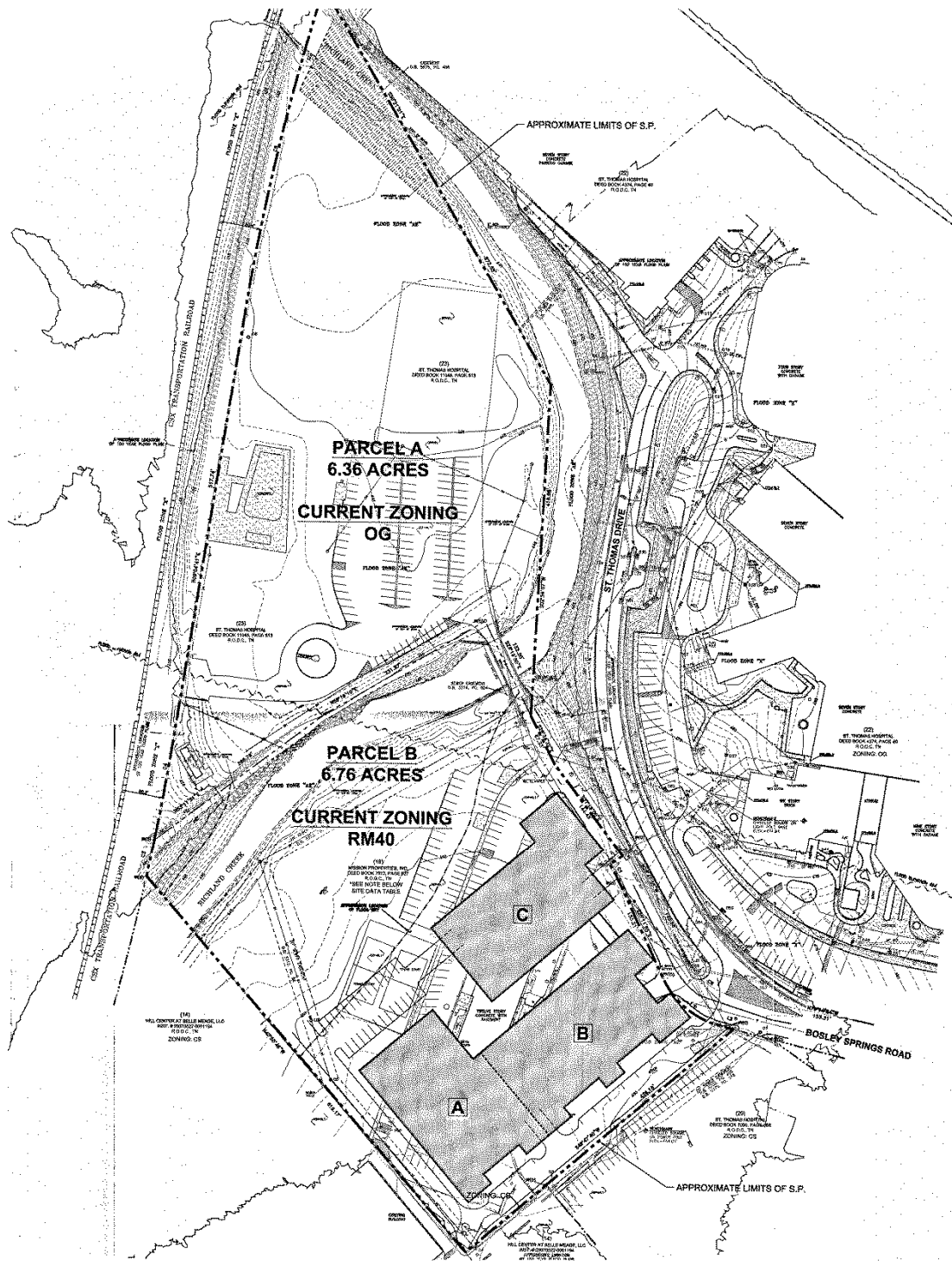
Flood Plain Overlay District (FPO) is an overlay district which is in addition to the floodplain and floodway protection provisions of the Zoning Code. The purpose of the overlay district is to prevent the obstruction of water courses and the protection of lives and property from the hazards of flooding. Regulation of flood-prone properties further allows for the reasonable protection of the natural ecosystems and wetlands areas and qualifies Nashville for flood insurance under Public Law.

Proposed Zoning

Specific Plan-Mixed Use (SP – MU) is a zoning District category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to



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Proposed Site Plan



Metro Planning Commission Meeting of 03/26/2015

implement the specific details of the General Plan. *This Specific Plan includes residential, office and commercial uses.*

CRITICAL PLANNING GOALS

- Preserves/Creates Open Space
- Supports Infill Development
- Provides a Range of Housing Choices
- Creates Walkable Neighborhoods
- Supports a Variety of Transportation Choices

Richland Creek bisects the site. A majority of the site is encumbered by floodway and all of it is within the 100 year floodplain and within the Flood Plain Overlay District. The entire site has been previously disturbed and a portion of the site contains the Imperial House Apartments which is currently vacant and is recognized as a historic property and is National Register Eligible (NRE). Because the site has been disturbed, the Zoning Code exempts it from the stormwater buffer requirements along Richland Creek. Because of this a majority of the site, regardless of flood issues, could be redeveloped. The plan calls for close to half of the site (6.36 acres) to be reserved for greenways, park and outdoor recreation trails only and does not permit any other development. This creates additional open space which should help improve the Richland Creek water shed as well as provide outdoor recreational opportunities. Staff is also recommending that remediation work be done on this portion of the site. Remediation work may include but is not limited to removing hard surfaces and replanting, bank restoration, and tree plantings.

The area is also served by adequate infrastructure. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure, such as substandard roads, water and sewer, because it does not burden Metro with the cost of upgrading or building new infrastructure. The request provides an additional housing option in the area. Additional housing options are important to serve a wide range of people with different housing needs. Also, the subject site is located in an area that employs a significant amount of people and will provide additional employment opportunities. Additional housing at this location provides opportunities for people to live near where they work. This helps create vibrant, walkable mixed-use neighborhoods. This also helps support public transit. People living in more dense mixed-use areas are more likely to use public transit because every day services are located closer by and it can be more efficient than driving oneself.

GREENHILLS-MIDTOWN COMMUNITY PLAN

Conservation (CO) policy is intended to preserve and enhance environmentally sensitive land within all Transect Categories except T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils.

DRAFT Preferred Future Policy

Center Regional Center (T5 RC) policy is intended to enhance regional centers, encouraging their redevelopment as intense mixed use areas that serve multiple communities as well as the entire County with supporting land uses that create opportunities to live, work, and recreate. T5 Regional Centers are pedestrian friendly areas, generally located at the intersection of two arterial streets, and contain commercial, mixed use, residential, civic and public benefit land uses. T5 Regional Centers



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serve communities within a reasonable driving distance or a 5 to 10 minute walk. Intensity is generally placed within boundaries not exceeding ½ mile in diameter and transitional uses placed within boundaries not exceeding 1 mile in diameter measured from the prominent intersection.

Consistent with Policy?

The request is not consistent with the existing CO policy; however, it is consistent with the draft preferred future policy. As proposed the SP would permit a variety of residential, office and commercial uses that are urban in form and in keeping with the existing Harding Town Center UDO. Since this site is already developed and already zoned for additional development, including the Harding Town Center UDO, rezoning this site to SP is not inconsistent with what is already planned for this area and provides a balance in terms floodplain/floodway protection and development.

PLAN DETAILS

The approximately 13 acres site is located on the northwest of Harding Pike and is accessed by Bosley Springs Road that runs along the north east boundary of the property. St. Thomas Hospital is on the opposite side of Bosley Springs Road. The Imperial House Apartments, which is currently vacant and is eligible for the National Register of Historic Places, is located on the site. Richland Creek bisects the property and the entire site is encumbered by the 100 year floodplain and a large portion of the site is encumbered by floodway. The site is within the Hospital Sub District of the Harding Town Center UDO.

Site Plan

The plan is mostly regulatory in nature. While a site plan does identify building foot prints, the locations, size and shape are only conceptual. The bulk of the SP includes specific standards that would regulate any development within the site. The SP contains two building types, nonresidential/mixed use and residential only. The nonresidential uses are limited to a maximum of 450,000 square feet of floor area. Residential is limited to a maximum of 230 units.

The plan provides a specific list of permitted uses. The regulatory standards of the SP incorporates all the UDO requirements including but not limited to bulk, parking, landscaping, buffering, screening and architectural treatments.

ANALYSIS

The SP would permit a variety of uses that are not currently permitted under the RM40 and OG zoning districts. Most notable uses are restaurant, hotels and retail. With the exception of hotel, these uses are permitted in the OG district; however, they are only permitted as an accessory use. The proposed uses are consistent with the draft preferred future T5 RC policy. While a mixed use zoning district could provide the additional uses and intensity proposed with this SP, the SP permits the ability to limit the uses from what would be otherwise permitted under a mixed use district. The SP incorporates all the objectives of the UDO, including building and lots, parks and open space and transportation. The plan incorporates all the specific design standards of the UDO including bulk, parking, landscaping, buffering, screening and architectural treatments. This is important because it maintains the intent of the UDO and will require development in the SP to be in concert with future redevelopment inside the UDO. The SP permits more intensity for nonresidential uses than what is currently permitted by the current OG zoning. The current OG zoning would permit a



Metro Planning Commission Meeting of 03/26/2015

maximum of approximately 400,000 square feet of floor area where the SP proposes a maximum of 450,000 square feet.

As noted above, the entire site is encumbered with floodway and floodplain associated with Richland Creek. A majority of the site can currently be developed. The SP would remove development rights from approximately half of the site (6.36 acres). This area would be limited to public benefit uses including parks, greenways, and trails. Staff is also recommending that some remediation be made to the site designated as open space. This should include but not limited to, the removal of any non-pervious surfaces. Staff finds that the additional open space to be a significant part of the proposed SP as it improves the current situation. Staff also finds that this strikes an adequate balance between development and open space.

Staff is recommending that the SP be approved with conditions because staff finds that it is consistent with the draft preferred future policy and meets several critical planning goals. It is also important to note while the Imperial House Apartments is designated as NRE, the Historic Zoning Commission staff has recommended approval of this proposal.

FIRE MARSHAL'S OFFICE

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review.

METRO HISTORIC ZONING COMMISSION

Approved

- Property is NR; however, because the lot is located in a floodplain there are construction requirements the existing building will not be able to meet if rehabbed.

PUBLIC WORKS RECOMMENDATION

Conditions if approved

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Plan submitted is regulatory in nature; with the submittal of a Final SP a detailed review will be completed. Final SP Plans must conform to MPW and ADA standards and specifications.
- Prior to Final SP indicate the location of the solid waste and recycling containers.

Traffic

The traffic expected to be generated by the proposed SP appear to be equal to or less than those that were evaluated in the Harding Town Center Transportation Plan. A traffic study(s) should be required prior to approval of any Final SP to address the implementation of proposed transportation improvements.

STORMWATER RECOMMENDATION

Approved

WATER SERVICES

Approved



Metro Planning Commission Meeting of 03/26/2015

METRO SCHOOL BOARD REPORT

**Projected student generation existing RM40 district: 34 Elementary 24 Middle 20 High
Projected student generation proposed SP-MU district: 28 Elementary 20 Middle 17 High**

The proposed SP-MR zoning district would generate 13 fewer students than what is typically generated under the existing RM40 zoning district. Students would attend Sylvan Park Elementary, West End Middle School, and Hillsboro High School. There is no capacity for additional elementary and middle school students, but, there is additional capacity for high school students. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends that the proposed SP be approved with conditions and disapproved without all conditions as it is consistent with the proposed draft preferred future T5 RC policy and it meets several critical planning goals.

CONDITIONS

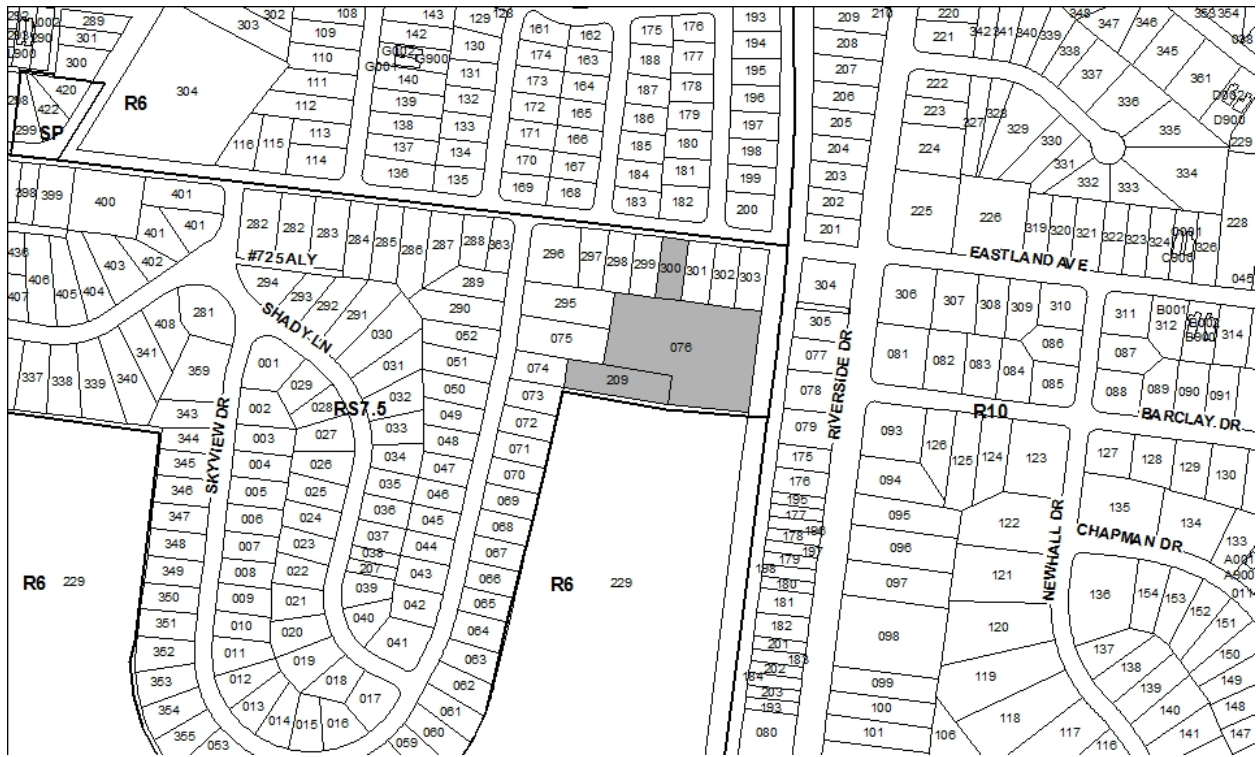
1. Uses shall be limited to those uses outlined in the SP plan.
2. Prior to the issuance of any use and occupancy permit on Parcel B, impervious areas located on Parcel A shall be removed and revegetated. Other remediation work shall also be conducted as approved by Metro Stormwater and may include but is not limited to bank restoration, and tree plantings, and shall be completed prior to the issuance of any use and occupancy permit on Parcel B.
3. Development in the SP may precede, follow or develop concurrently with the projected transportation improvements contained in the UDO, so long as the configuration of proposed development does not preclude the implementation of the transportation recommendations outlined in the UDO.
4. A Traffic Impact Study (TIS) may be required prior to the approval of any final site plan.
5. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the Harding Town Center Urban Design Overlay district as of the date of the applicable request or application.
6. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
7. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
8. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



SEE NEXT PAGE



Metro Planning Commission Meeting of 03/26/2015



2014SP-085-001

2208 EASTLAND AVENUE

Map 083-07, Parcel(s) 300

Map 083-11, Parcel(s) 076, 209

05, East Nashville

06 (Peter Westerholm)



Project No. 2014SP-085-001
Project Name 2208 Eastland Avenue
Council District 6 – Westerholm
School District 5 – Kim
Requested by Dale and Associates, applicant; Upside, LLC, owner.

Deferrals This case was deferred from the December 11, 2014, and the March 12, 2015, Planning Commission meetings.

Staff Reviewer Sajid
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

Preliminary SP to permit up to 24 residential units.

Preliminary SP

A request to rezone from Single-Family Residential (RS7.5) to Specific Plan-Residential (SP-R) zoning for properties located at 2208 Eastland Avenue and Skyview Drive (unnumbered), approximately 775 feet west of Riverside Drive, (3.27 acres), to permit up to 24 detached dwelling units.

Existing Zoning

Single-Family Residential (RS7.5) requires a minimum 7,500 square foot lot and is intended for single-family dwellings at a density of 4.94 dwelling units per acre. *RS7.5 would permit a maximum of 16 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

CRITICAL PLANNING GOALS

- Supports Infill Development
- Supports a Variety of Transportation Choices

The proposed SP supports development that is consistent with the character of surrounding development and creates an opportunity for infill housing. In addition, the site is served by an existing bus and bicycle routes that run along Eastland Avenue which will be supported by the additional density proposed by the SP.

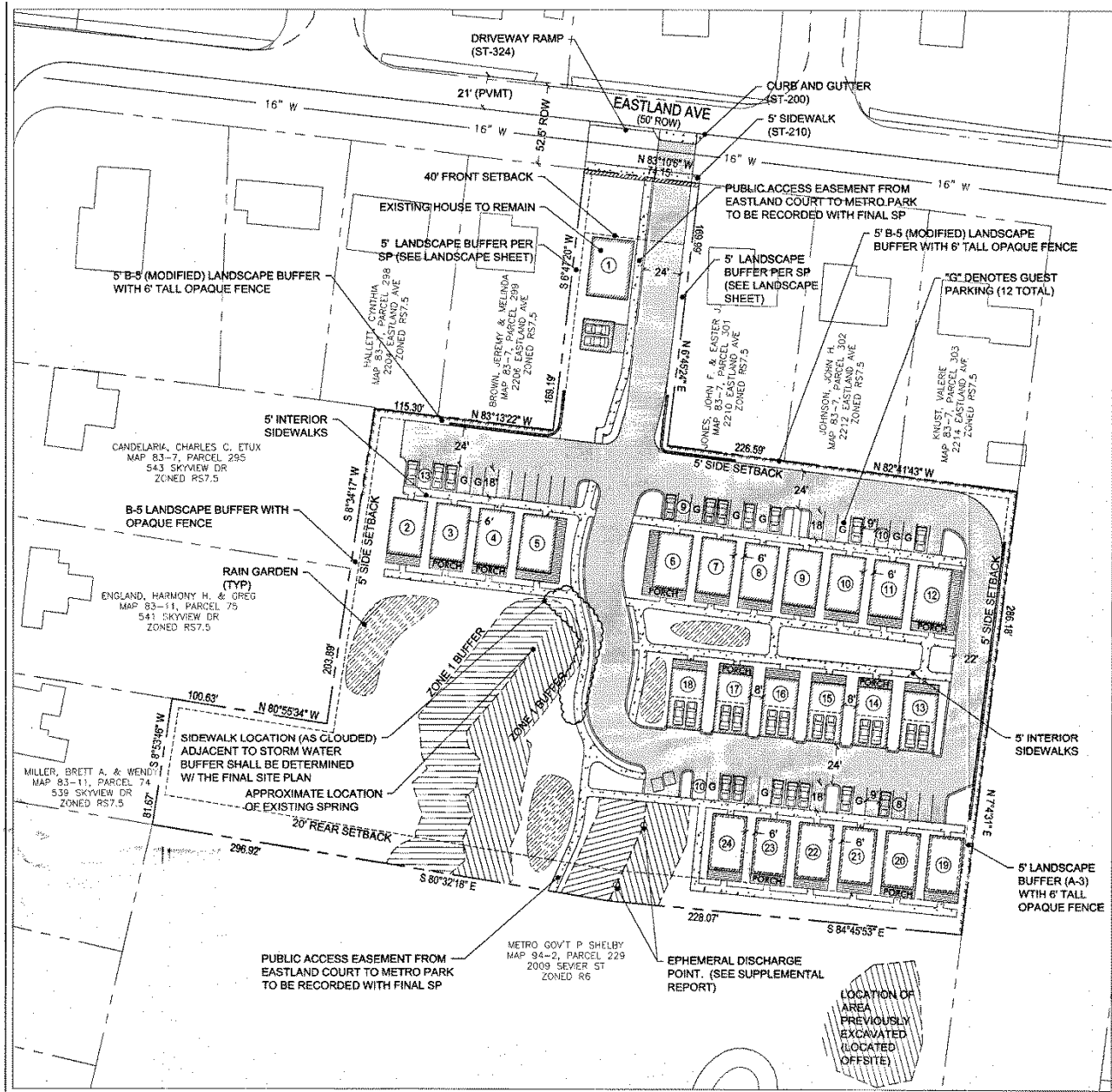
EAST NASHVILLE COMMUNITY PLAN

Existing Policy

Urban Neighborhood Maintenance (T4 NM) policy is intended to preserve the general character of urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing



Metro Planning Commission Meeting of 03/26/2015



Proposed Site Plan



Metro Planning Commission Meeting of 03/26/2015

character of the neighborhood, in terms of its development pattern, building form, land use and the public realm. Where not present, enhancements are made to improve pedestrian, bicycle and vehicular connectivity.

DRAFT Preferred Future Policy

No change is proposed.

Consistent with Policy?

Yes. The proposed SP is consistent with the Urban Neighborhood Maintenance policy which is intended to preserve the character of the existing neighborhood. The SP proposes detached dwelling units, which reflects the predominant development pattern in the area. Also, the rezoning request is a site plan based district that encourages flexibility in design so that the result is well suited to the subject property and the neighborhood.

PLAN DETAILS

The site is located at 2208 Eastland Avenue, east of Skyview Drive and north of the Shelby Golf Course. Surrounding zoning includes RS7.5, R6 and R10, and the area is predominantly characterized by single-family residential development. Access to the site is from one driveway proposed on Eastland Avenue.

Site Plan

The plan proposes 24 detached residential units. The maximum height for all units except for Unit 1 is 3 stories in 42'. The plan proposes to retain the existing dwelling that fronts Eastland Avenue. That unit falls within the East Nashville Contextual Overlay, so if it were redeveloped in the future, any new construction must comply with the provisions of the contextual overlay. Type B-5 landscape buffers are proposed along all property boundaries that are adjacent to existing residential uses with the exception of the Type B-1 buffer proposed along both sides of the driveway at Eastland Avenue.

The overall site layout includes 24 units with Unit 1 oriented toward Eastland Avenue and the remaining units oriented toward either a courtyard or open space. The site layout has changed from the previous plan to incorporate findings from the environmental site assessment requested by the Planning Commission. That evaluation concluded that caves are not present at the site, but did find evidence of two small karst cavities which discharge water and a spring feature. The locations of these findings are shown on the map.

One of the spring locations is located within the required stream buffer. The other two discharge points are located to the south of the site. The number of units along the southern property line has been reduced by two in order to accommodate an additional buffer to preserve those discharge points. One unit has been relocated to the row just north of the stream buffer, and the plan has retained the existing house fronting Eastland Avenue to maintain a total of 24 units.

Architectural images have not been included with the preliminary SP. The SP, however, includes notes that address design considerations for the SP. The design conditions address doorway placement, glazing, window orientation and porches. Also, EIFS and vinyl siding will not be permitted as building materials.



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Parking is provided via a mixture of garages and surface parking and includes ample guest parking. The SP is in close proximity to an existing transit line that runs along Eastland Avenue. Sidewalks are currently located on the north side of Eastland Avenue, but not on the south side. The SP proposes to construct sidewalks along the Eastland Avenue frontage of the site, and interior sidewalks are provided throughout the site to connect the units to the proposed sidewalk network as well as to the golf course to the south. Staff recommends that the plan incorporate a 6' sidewalk with a 6' planting strip along the Eastland Avenue frontage per the Major and Collector Street Plan.



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ANALYSIS

This is a unique property located in a predominately developed area. The subject property is a large, undeveloped property that is constrained as it has only about 75' of street frontage available. In addition, the site includes environmentally sensitive features and is located adjacent to the Shelby Golf Course. The proposed SP manages to work in concert with these challenges to create a plan where the character is consistent with the Urban Neighborhood Maintenance policy, while also protecting the environmentally sensitive portions of the site and providing a transition to the surrounding open space. The proposed detached dwelling units reflect the predominant development pattern in the area and maintain the existing context on Eastland Avenue. As the proposed SP is consistent with the Urban Neighborhood Maintenance land use policy, and the plan meets two critical planning goals. Therefore, staff recommends approval with conditions and disapproval without all conditions.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review. Radius on turns to meet the requirements of Appendix D of the 2006 IFC

STORMWATER RECOMMENDATION

Approved

TRAFFIC & PARKING RECOMMENDATION

No exception taken

WATER SERVICES RECOMMENDATION

Approve

- Approved as Prelim SP. Since unit count has been reduced since the latest availability study, applicant should submit a revised study to re-assess the capacity fee balance. Applicant will need to revise their study, pay required Capacity Fees, and have Approved Construction Plans before approval will be issued at the Final SP stage.

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Coordinate with MPW prior to Final SP for appropriate roadside cross section on Eastland Ave, i.e. curb placement, grass strip, sidewalk, drainage, infrastructure, utilities, etc.
- ROW dedication must be recorded prior to MPW sign off on the building permit.

Maximum Uses in Existing Zoning District: **RS7.5**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Single-Family Residential (210) | 3.27 | 5.80 D | 18 U | 173 | 14 | 19 |



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Maximum Uses in Proposed Zoning District: **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|--------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Multi-Family Residential (230) | 3.27 | - | 25 U | 193 | 18 | 20 |

Traffic changes between maximum: **RS7.5** and **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | + 7 U | +20 | +4 | +1 |

SCHOOL BOARD REPORT

Projected student generation existing RS7.5 district: 1 Elementary 1 Middle 1 High

Projected student generation proposed SP-R district: 2 Elementary 1 Middle 1 High

The proposed SP-R zoning district would generate one more student than what is typically generated under the existing RS7.5 district. Students would attend Warner Elementary School, Bailey Middle School, and Stratford High School. All three schools have been identified as having additional capacity. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Interior sidewalk from southeast side of Unit 4 to sidewalk shown north of Unit 16 to be determined with final site plan.
2. The final site plan shall include a 6' planting strip with 6' sidewalks along the Eastland Avenue frontage.
3. Prior to the issuance of building permits, the property shall be resubdivided into two lots. Lot 1 shall contain Unit 1 and have 40 feet of frontage along Eastland Avenue. Lot 2 shall contain the driveway to the site and Units 2-24.
4. If a contextual overlay is adopted by Metro Council, any construction or development on Lot 1 shall meet all provisions of the contextual overlay.
5. Coordinate with Metro Public Works prior to Final SP for appropriate roadside cross section on Eastland Ave, i.e. curb placement, grass strip, sidewalk, drainage, infrastructure, utilities, etc.
6. Right of Way dedication must be recorded prior to Metro Public Works sign off on the building permit.
7. Obtain Metro Water Services approval, during construction drawing review, for a bioretention area over a sewer line.
8. The final site plan shall label all internal driveways as "Private Driveways". A note shall be added to the final site plan that the driveways shall be maintained by the Homeowner's Association.
9. Uses within the SP shall be limited to a maximum of 24 detached residential units.

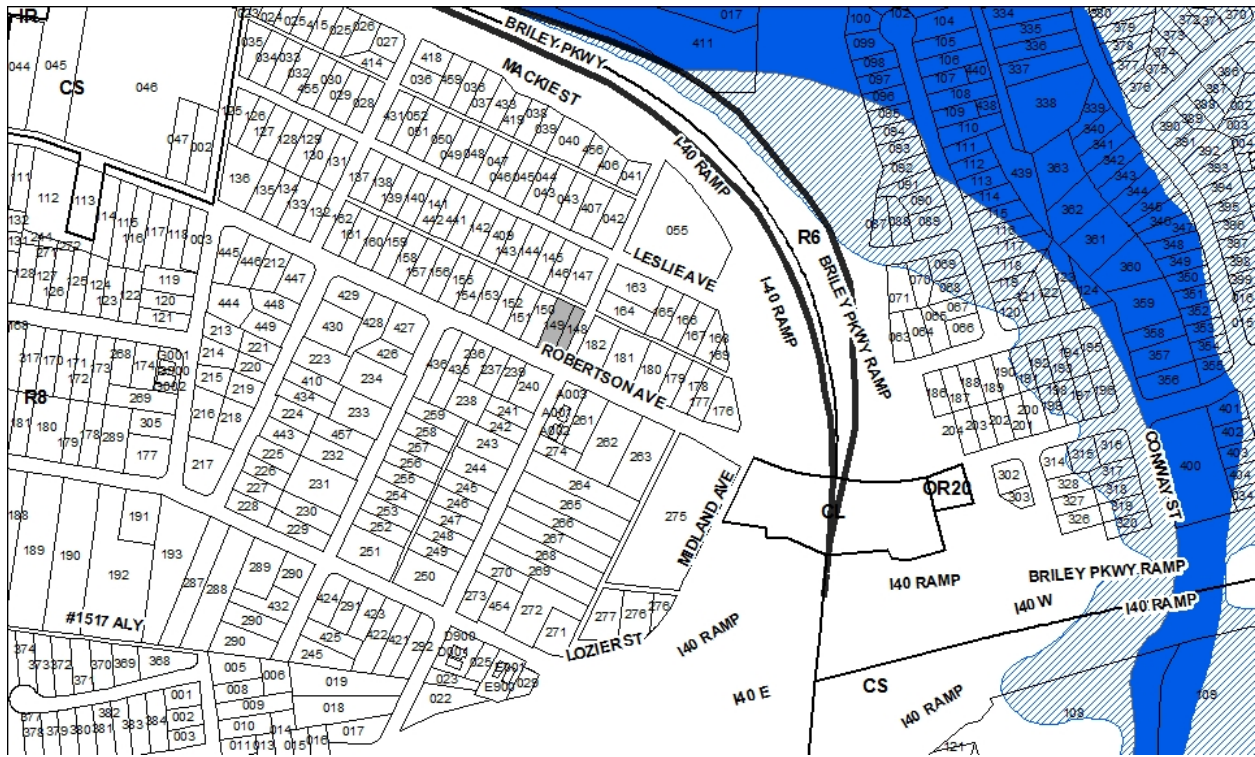


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10. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM9 zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
11. The final site plan shall include architectural elevations showing raised foundations of 18-36" for residential buildings.
12. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
13. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
14. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2015SP-014-001

SNYDER COURT

Map 091-10, Parcel(s) 148-149

07, West Nashville

20 (Buddy Baker)



Project No. 2015SP-014-001
Project Name Snyder Court
Council District 20 – Baker
School District 9 – Frogge
Requested by Dale and Associates, applicant; Nuck & Beal, LLC, owners.

Deferrals This case was deferred from the March 12, 2015, Planning Commission meetings.

Staff Reviewer Sajid
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

Preliminary SP to permit up to 4 detached residential units.

Preliminary SP

A request to rezone from One and Two-Family Residential (R8) to Specific Plan-Residential (SP-R) zoning for properties located at 5800 and 5802 Robertson Avenue, at the northwest corner of Robertson Avenue and Snyder Avenue, (0.28 acres), to permit up to 4 detached residential units.

Existing Zoning

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 5.79 dwelling units per acre including 25 percent duplex lots. *R8 would permit a maximum of 1 lot with 1 duplex lot for a total of 2 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

CRITICAL PLANNING GOALS

- Supports Infill Development

The proposed SP creates an opportunity for infill housing in an area that is served by existing infrastructure. Locating development in areas served by existing, adequate infrastructure does not burden Metro with the cost of upgrading or building new infrastructure.

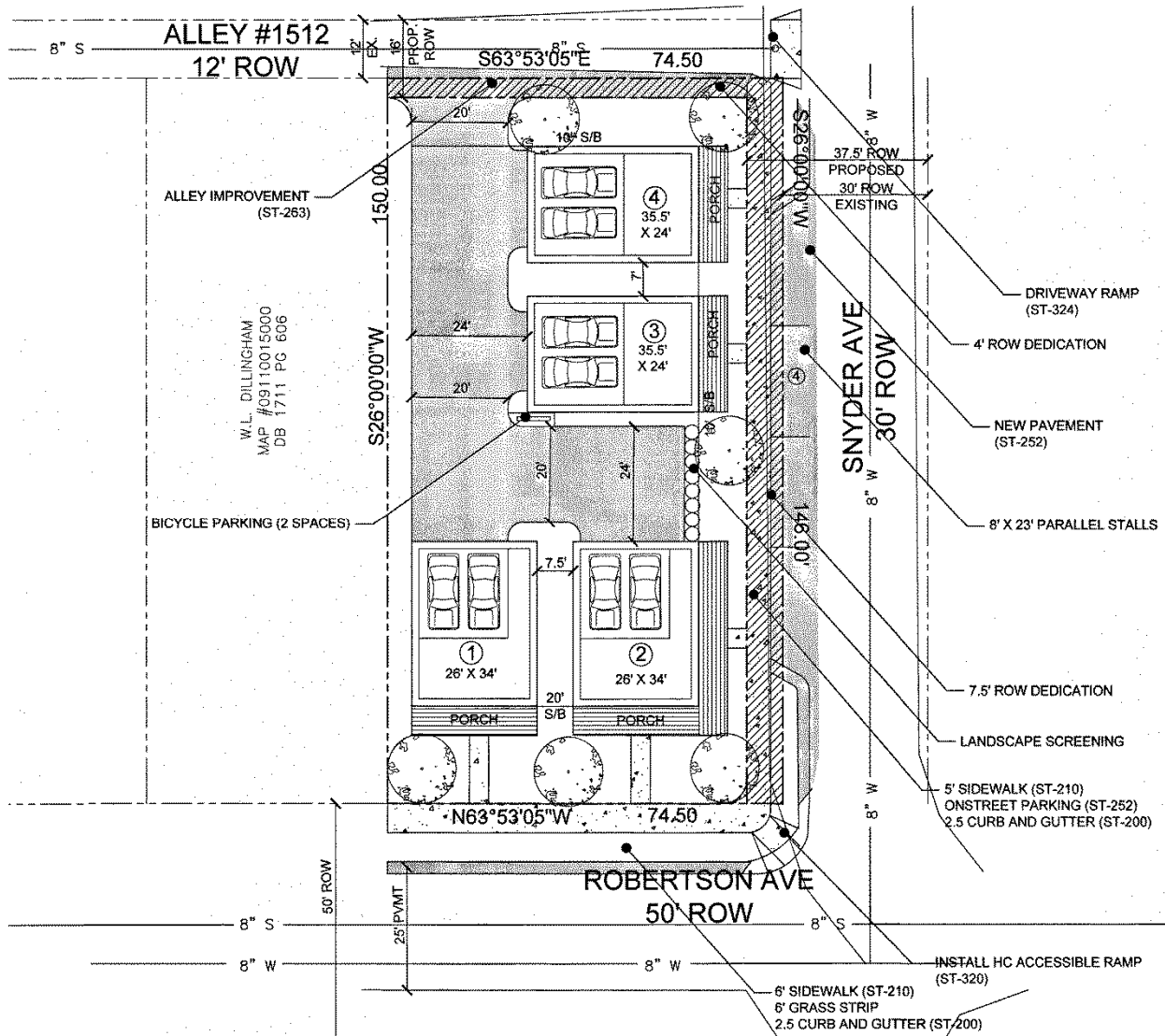
WEST NASHVILLE COMMUNITY PLAN

Existing Policy

Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This



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Proposed Site Plan



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reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

DRAFT Preferred Future Policy

No change proposed.

Consistent with Policy?

Yes, the SP is consistent with the Urban Neighborhood Evolving policy. The Urban Neighborhood Evolving policy is intended to create neighborhoods that are compatible with the general character of urban neighborhoods while anticipating changes such as additional density and the introduction of additional housing types. The neighborhood surrounding the site is characterized by a mixture of housing types. In addition, the subject property is located along Robertson Avenue which is classified as a collector street.

PLAN DETAILS

The site is located at the northwest corner of the intersection of Robertson Avenue and Snyder Avenue. Surrounding zoning includes R8, and the area is characterized by a mixture of housing types. Access to the property is from the existing improved alley that abuts the site to the north.

Site Plan

The plan proposes 4 detached residential units with two units fronting Robertson Avenue and two units fronting Snyder Avenue. Unit 2 includes a side façade oriented toward Snyder Avenue and incorporates a wraparound porch so that the building addresses both street frontages. Architectural images have not been included with the preliminary SP. The SP, however, includes notes that address design considerations for the SP. The design conditions address doorway placement, glazing, window orientation and porches. Also, EIFS and vinyl siding are not permitted as building materials. The maximum height for all units is 3 stories in 35' to the roof ridgeline. Staff recommends that the plan incorporate a Type A-3 landscape buffer yard between the site and the existing single-family residential to the west.

Parking for the units is provided in garages and surface parking, and on-street parking will be available for guests. The SP proposes to widen the road and construct sidewalks along the Snyder Avenue frontage and improve the existing sidewalk at Robertson Avenue to the standards of the Major and Collector Street Plan.

ANALYSIS

The proposed SP is well designed and consistent with Urban Neighborhood Evolving land use policy and meets a critical planning goal. Therefore, staff recommends approval with conditions and disapproval without all conditions.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review.



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STORMWATER RECOMMENDATION

Conditional if approved

- Offsite storm improvements may be required (to be determined during Construction Drawing Review).

TRAFFIC & PARKING RECOMMENDATION

No exception taken

WATER SERVICES RECOMMENDATION

Approve

- Approved as a Preliminary SP only. If needed, public construction plans must be submitted and approved prior to Final SP approval. The required capacity fees should be paid prior to Final SP stage.

PUBLIC WORKS RECOMMENDATION

No exception taken

Maximum Uses in Existing Zoning District: **R8**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Two-Family Residential (210) | 0.28 | 5.44 D | 2 U * | 20 | 2 | 3 |

*Based on one two-family lot.

Maximum Uses in Proposed Zoning District: **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|--------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Multi-Family Residential (210) | 0.28 | - | 5 U | 48 | 4 | 6 |

Traffic changes between maximum: **R8** and **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | + 3 U | +28 | +2 | +3 |

SCHOOL BOARD REPORT

Projected student generation existing R8 district: 0 Elementary 0 Middle 0 High

Projected student generation proposed SP-R district: 1 Elementary 0 Middle 0 High

The proposed SP-R zoning district would generate one more students than what is typically generated under the existing R6 zoning district. Students would attend Cockrill Elementary School, McKissack Middle School, and Pearl-Cohn High School. Cockrill Elementary School and Pearl-Cohn High School have been identified as over capacity. There is capacity within the cluster for



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additional elementary school students. There is capacity for high school students in an adjacent cluster. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

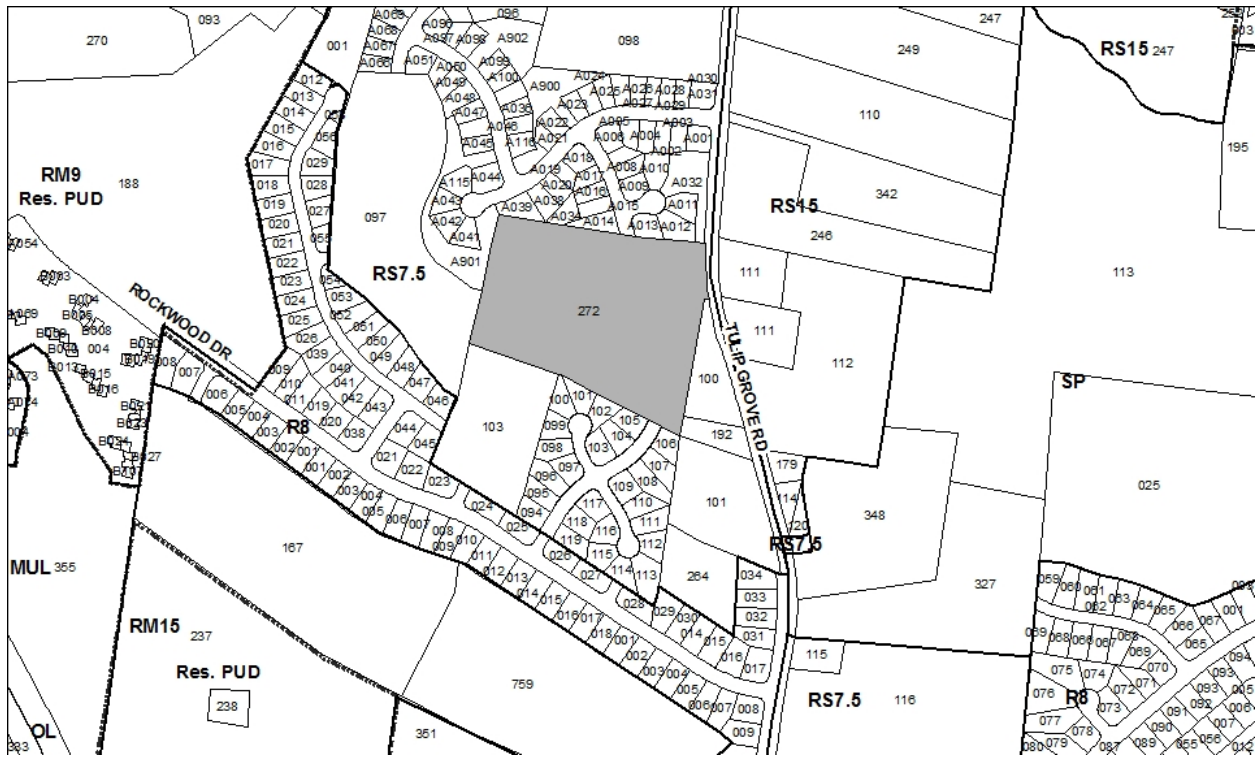
Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Uses within the SP shall be limited to 4 detached residential units.
2. A Type A-3 landscape buffer yard shall be provided along the western property line.
3. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM15-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
4. The final site plan shall include architectural elevations showing raised foundations of 18-36" for residential buildings.
5. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
6. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
7. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2015SP-028-001

TULIP GROVE ADDITION

Map 086, Parcel(s) 272

14, Donelson - Hermitage

12 (Steve Glover)



| | |
|-------------------------|--|
| Project No. | Zone Change 2015SP-028-001 |
| Project Name | Tulip Grove Addition |
| Council District | 12 – Glover |
| School District | 4 – Shepherd |
| Requested by | Dale & Associates, applicant; Southeastern Development Group, LLC, owners. |

Deferrals This case was deferred from the March 12, 2015, Planning Commission meeting.

Staff Reviewer Swaggart
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

Preliminary SP to permit up to 49 residential units.

Preliminary SP

A request to rezone from Single-Family Residential (RS7.5) to Specific Plan-Mixed Residential (SP-R) for property located at 1132 Tulip Grove Road, 400 feet south of Tulip Grove Point, (8.86 acres), to permit up to 49 detached residential dwelling units.

Existing Zoning

Single-Family Residential (RS7.5) requires a minimum 7,500 square foot lot and is intended for single-family dwellings at a density of 4.94 dwelling units per acre. *RS7.5 would permit a maximum of 51 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

CRITICAL PLANNING GOALS

- Supports Infill Development
- Provides a Range of Housing Choices

This area is located in an area that is served by adequate infrastructure. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure, such as substandard roads, water and sewer, because it does not burden Metro with the cost of upgrading or building new infrastructure. The request provides an additional housing option in the area. Additional housing options are important to serve a wide range of people with different housing needs.

DONELSON – HERMITAGE COMMUNITY PLAN

Suburban Neighborhood Evolving (T3 NE) policy is intended to create suburban neighborhoods that are compatible with the general character of classic suburban neighborhoods as characterized by their building form, land use and associated public realm, with opportunities for housing choice



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Proposed Site Plan



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and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern will have designed open space with smaller lot sizes and a broader range of housing types, providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing .

DRAFT Preferred Future Policy

No changes are proposed.

Consistent with Policy?

Yes. The plan is consistent with the policy as it supports a variety of residential types including the proposed detached multi-family development.

HISTORY

On January 23, 2014, the Commission deferred indefinitely a SP request to permit up to 48 multi-family units, at the request of the applicant. Later the applicant submitted a concept plan application for subdivision approval for 22 single-family lots. The Commission approved the concept plan on May 5, 2014.

PLAN DETAILS

The subject site is located on the west side of Tulip Grove Road, approximately 2,600 feet north of Central Pike. The site is between the Tulip Grove Pointe subdivision to the north and Rockwood Estates Subdivision to the south. The property is currently vacant, is heavily wooded and contains steep slopes near the western boundary. The Evans Hill cemetery is located at the top of the hill near the western boundary.

Site Plan

The plan calls for 49 detached residential units. A majority of the units front onto Elegance Way, Saddlestone Drive, Tulip Grove Road or active open space. Nine units along the eastern property line front onto the neighboring property. Units are limited to three stories in 35 feet. The plan also includes some architectural standards that pertain to entrances, glazing, porches, window orientation and materials.

Access to the site is proposed to be provided by private drives that connects Elegance Way to Saddlestone Drive. All units are accessed from the rear by private drives. The plan calls for Elegance Way to be extended from Tulip Grove Point Subdivision to Tulip Grove Road. The plan calls for Saddlestone Drive to be extended from Rockwood Estates Subdivision to the eastern property line. The plan calls for 125 parking spots and includes surface parking as well as single-car garages for each unit. An access easement is proposed to provide access to the cemetery and necessary parking for the cemetery. The plan also calls for a walkway to the cemetery.

The plan calls for a 10 foot wide "A" buffer yard between lots located in the Tulip Grove Point Subdivision and the Rockwood Estates Subdivision. The area with the steepest slopes (approximately 3.6 acres) is proposed to be in open space, which includes the cemetery at the top of the hill.



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ANALYSIS

The request is well designed and consistent with the T4 NE policy and meets two critical planning goals.

FIRE MARSHAL'S OFFICE

Approved with conditions

- Water flow requirements for single-family homes that do not exceed 3600 sq. ft. is a minimum of 1000 gpm @ 20 psi. Provide this data to pre-approve the future homes.

PUBLIC WORKS RECOMMENDATION

Conditions if approved

1. A monument sign shall be placed at each entrance at Elegance Way and Saddlestone Drive clearly indicating that the development is private. The final design of the sign shall be approved by the Planning Department prior to the approval of any final site plan.
2. All private drives shall be constructed to the ST-251 pavement schedule.
3. An engineer must present certification to Public Works that the private drives have been constructed to the ST-251 pavement schedule. Certification is to be conducted in the stages that construction occurs and NOT at the end of the project. The stages shall include subgrade grade elevations, proof rolling and compaction testing of the sub grade, certification of the proper amount of stone, certification on the proper amount of tack coat and pavement thickness and compaction. Certification shall include a statement that the engineer was present and observed the construction, and that it was in compliance with the approved plans.
4. All drives shall be maintained by the Home Owner's Association and shall not be maintained by Metro.
5. Public access shall be provided to the cemetery on the site.
6. The extension of Elegance Way to Tulip Grove Road shall be required with any final site plan. The removal of this extension shall require Council approval.
7. The private drives must be named prior to construction permitting, coordinate through Public Works.

STORMWATER RECOMMENDATION

Approved

WATER SERVICES

Approved with conditions

1. Prior to final site plan approval plans for public sewer and water must be submitted and approved.
2. The required capacity fees must be paid prior to final site plan approval.

METRO SCHOOL BOARD REPORT

Projected student generation existing RS7.5 district: 5 Elementary 4 Middle 4 High
Projected student generation proposed SP-R district: 6 Elementary 5 Middle 4 High

The proposed SP-R zoning district would generate two additional students than what is typically generated under the existing RS7.5 zoning district. Students would attend Dodson Elementary, Dupont Tyler Middle School, and McGavock High School. All three schools are over capacity; however, there is capacity within the cluster for additional elementary and middle school students



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and there is additional capacity for additional high school students in the adjacent Hunters Lane and Stratford clusters. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Uses shall be limited to a maximum of 49 residential units.
2. A monument sign shall be placed at each entrance at Elegance Way and Saddlestone Drive clearly indicating that the development is private. The final design of the sign shall be approved by Planning prior to the approval of any final site plan.
3. All private drives shall be constructed to the ST-251 pavement schedule.
4. An engineer must present certification to Public Works that the private drives have been constructed to the ST-251 pavement schedule. Certification is to be conducted in the stages that construction occurs and NOT at the end of the project. The stages shall include subgrade grade elevations, proof rolling and compaction testing of the sub grade, certification of the proper amount of stone, certification on the proper amount of tack coat and pavement thickness and compaction. Certification shall include a statement that the engineer was present and observed the construction, and that it was in compliance with the approved plans.
5. All drives shall be maintained by the Home Owner's Association and shall not be maintained by Metro.
6. The final site plan shall include paved public access to the cemetery on the site with minimal disturbance.
7. The extension of Elegance Way to Tulip Grove Road shall be required with any final site plan. The removal of this extension shall require Council approval.
8. The private drives must be named prior to construction permitting, coordinate through Public Works.
9. A raised foundation of 18"- 36" is required for all residential structures.
10. The following design standards shall be added to the plan:
 - a. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.
 - c. EIFS and vinyl siding shall be prohibited.
 - d. Porches shall provide a minimum of six feet of depth.
11. Side façades of units that face a public street shall have front façade requirements. Elevations of side façades units facing a public street shall be submitted with the final site plan.
12. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM9 zoning district as of the date of the applicable request or application.
13. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application
14. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise



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permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.

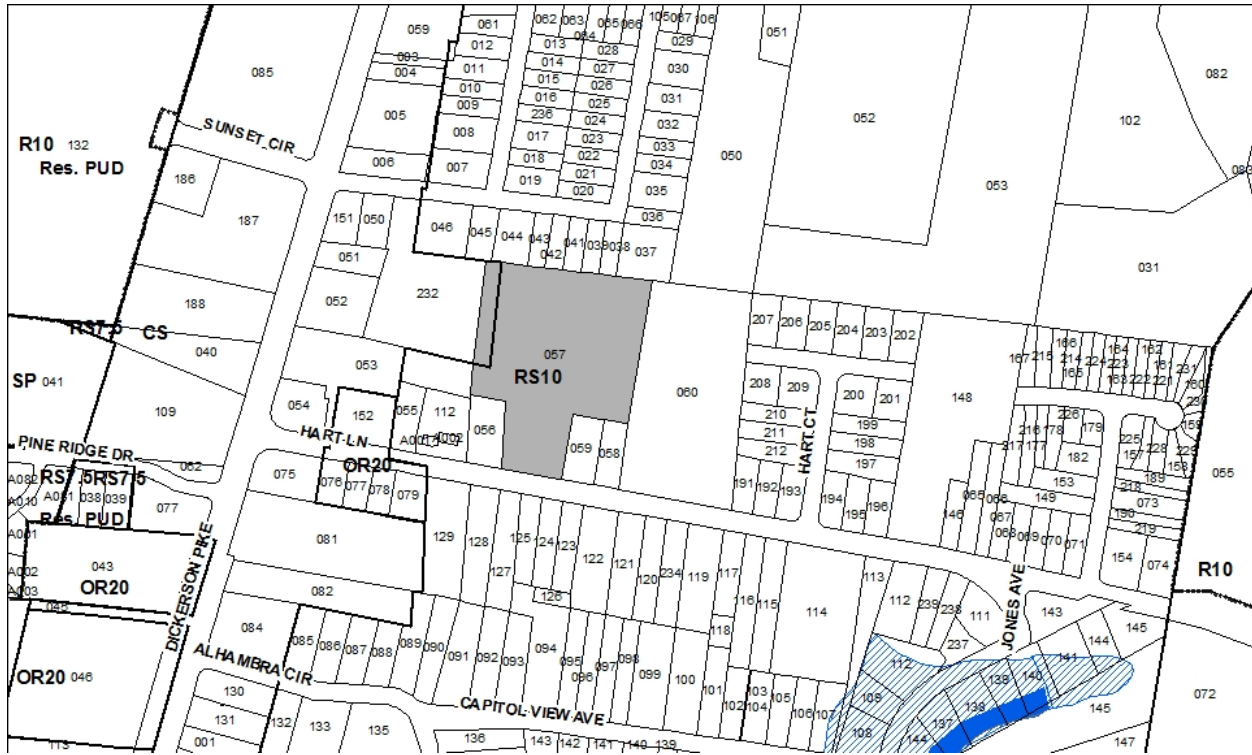
15. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



SEE NEXT PAGE



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2015SP-029-001
HART LANE COTTAGES
Map 060-12, Parcel(s) 057
05, East Nashville
08 (Karen Bennett)



Project No. 2015SP-029-001
Project Name Hart Lane Cottages
Council District 8 - Bennett
School District 3 - Speering
Requested by Dale & Associates, Inc., applicant; John Howard, owner.

Deferrals This case was deferred from the March 12, 2015, Planning Commission meeting.

Staff Reviewer Milligan
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST
Preliminary SP to permit up to 28 residential units

Preliminary SP
 A request to rezone from Single-Family Residential (RS10) to Specific Plan-Residential (SP-R) zoning for properties located at 115 Hart Lane (4.59 acres), to permit up to 28 multi-family residential units.

Existing Zoning
Single-Family Residential (RS10) requires a minimum of 10,000 square foot lot and is intended for single-family dwellings at a density of 3.7 dwelling units per acre. *RS10 would permit a maximum of 17 units.*

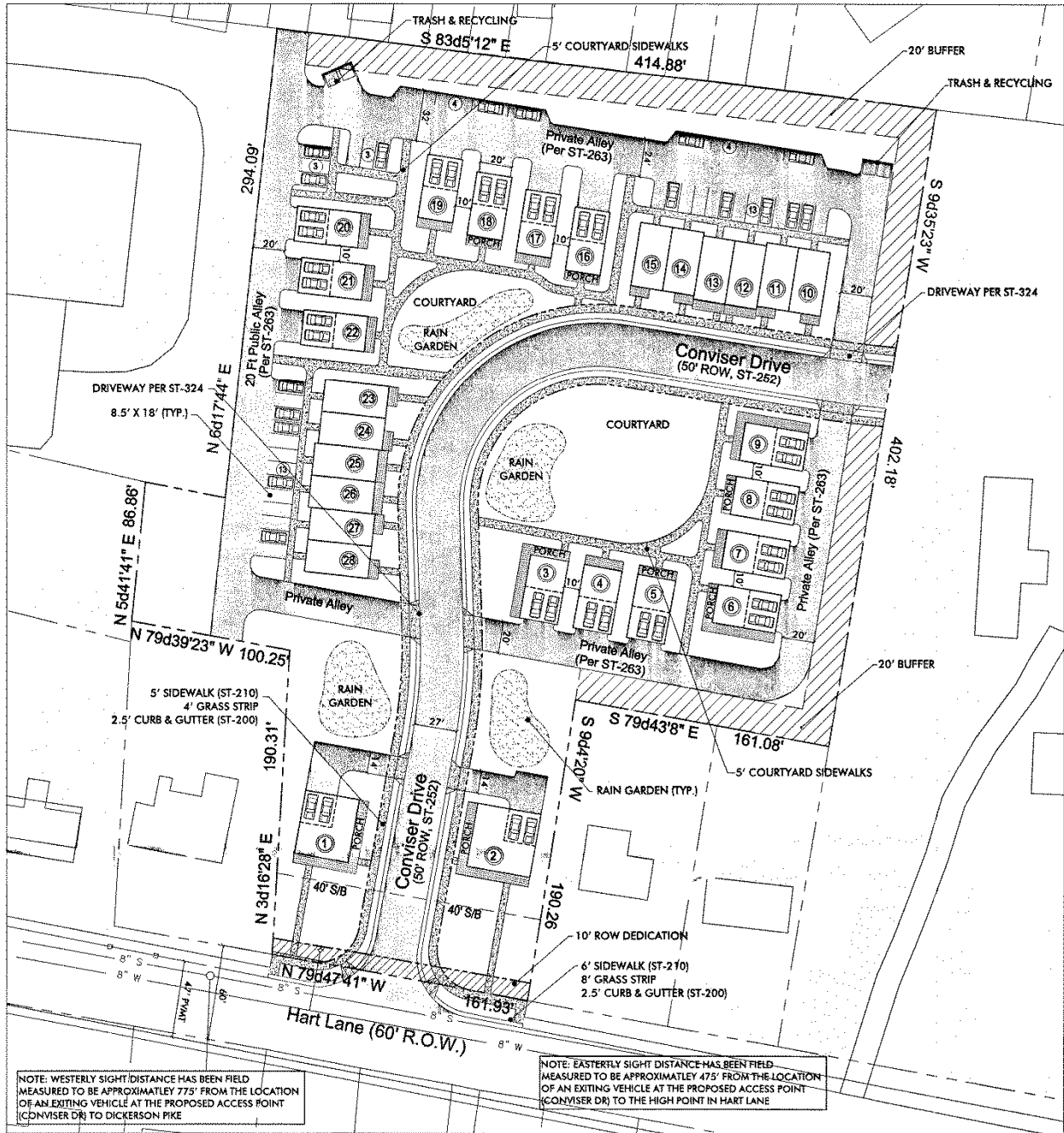
Proposed Zoning
Specific Plan-Residential (SP-R) is a zoning District category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes two residential building types.

- CRITICAL PLANNING GOALS**
- Supports Infill Development
 - Creates Walkable Neighborhoods
 - Provides a Range of Housing Choices

This proposal meets several critical planning goals. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure such as roads, water and sewer, because it does not burden Metro with the cost of maintaining new infrastructure. The project will intensify development on an infill site and provide for a different housing type than currently exists in the immediate area. Sidewalks are being provided along Hart Lane and internal roads to provide for a more walkable neighborhood.



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Proposed Site Plan



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EAST NASHVILLE COMMUNITY PLAN

Existing Land Use Policy

T4 Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

DRAFT Preferred Future Land Use Policy

No change proposed.

Consistent with Policy?

Yes. The proposed SP zoning is consistent with the proposed T4 NE policy. The proposed development is creating an additional housing option in this area while still being compatible with the general character of the area in regards to building placement. The units proposed along Hart Lane reflect the setbacks of the existing homes.

PLAN DETAILS

The site is located at 115 Hart Lane, north of Hart Lane and east of Dickerson Pike. The site is approximately 4.59 acres in size. The current use of the property is 1 single-family detached unit.

Site Plan

The plan proposes up to 28 multi-family residential units. The plan proposed two types of housing units: 16 single-family detached units along with 12 townhome units. There are two units proposed along Hart Lane. The Hart Lane units will have similar setbacks to the existing homes along Hart Lane.

The plan provides sidewalks along Hart Lane, along Conviser Drive and within the open space/courtyard area. Vehicular access to garages and parking is from a series of alleys, except for the Hart Lane units which will gain access from Conviser Drive. Street trees are proposed along Conviser Drive. Conviser Drive is stubbed out to the eastern property line, which will allow for a future connection to the existing Conviser Drive.

Architectural standards have been provided including specifications for raised foundations, window orientation, porches, materials, etc. Many units are planned with wraparound porches to provide for architectural interest.

ANALYSIS

The plan is well designed and consistent with the draft preferred future land use policy and adds housing choice to an existing urban neighborhood. The plan meets critical planning goals, including creating a more pedestrian friendly, walkable streetscape and providing an infill development on an underutilized urban lot.



Metro Planning Commission Meeting of 03/26/2015

FIRE DEPARTMENT RECOMMENDATION

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review

STORMWATER RECOMMENDATION

Approved

WATER SERVICES

Approved

- A revised availability study has been submitted, which matches the unit count in this Preliminary SP (28 units). Approved as a Preliminary SP only. Public construction plans must be submitted and approved prior to Final SP stage. The required capacity fees must be paid prior to Final SP approval.

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Indicate that the private alleys are to be dedicated into ROW prior to building permit approval

TRAFFIC AND PARKING RECOMMENDATION

No exception taken

Maximum Uses in Existing Zoning District: **RS10**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Single-Family Residential (210) | 4.59 | 4.35 D | 19 U | 182 | 15 | 20 |

Maximum Uses in Proposed Zoning District: **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|--------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Multi-Family Residential (230) | 4.59 | - | 28 U | 213 | 19 | 22 |

Traffic changes between maximum: **RS10** and **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | + 9 U | +31 | +4 | +2 |



Metro Planning Commission Meeting of 03/26/2015

METRO SCHOOL BOARD REPORT

Projected student generation existing RS10 district: 1 Elementary 1 Middle 1 High
Projected student generation proposed SP-R district: 1 Elementary 1 Middle 1 High

The proposed SP-R zoning district will generate no additional students that what could be generated under the existing RS10 zoning. Students would attend Tom Joy Elementary School, Jere Baxter Middle School and Maplewood High School. None of the schools have been identified as being over capacity. This information is based upon data from the school board last updated October 2015.

STAFF RECOMMENDATION

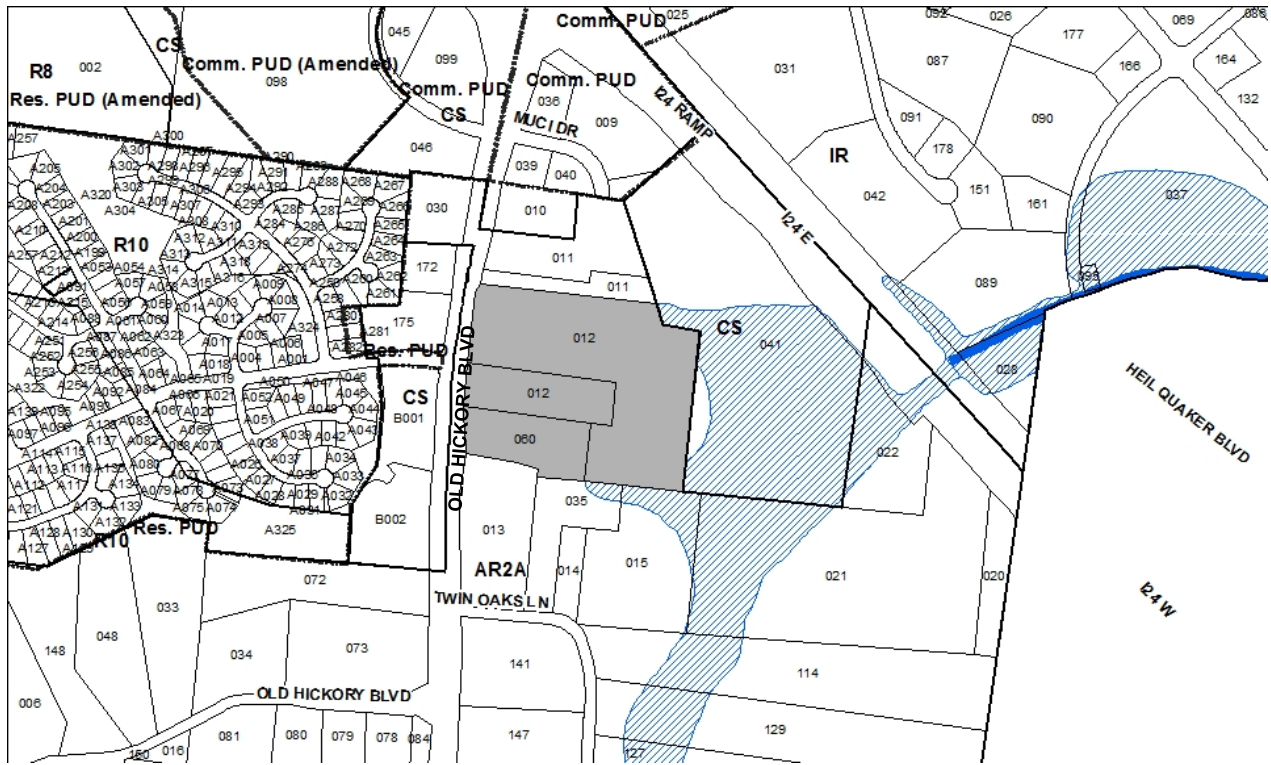
Staff recommends approval with conditions and disapproval without all conditions. The plan is consistent with the draft preferred future policy and meets several critical planning goals.

CONDITIONS

1. Permitted land uses shall be limited to up to 28 multi-family residential units.
2. For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations and requirements of the RM6 zoning district as of the date of the application request or application.
3. All alleys shown on the plan shall be dedicated as public alleys with a maximum right of way of 20 feet, and constructed to ST 263 standard with parking as shown on the plan.
4. Side facades of units that face a public street shall meet the following requirements: provide a minimum of 1 entry and provide a minimum of 25% glazing. Elevations for side façade units facing a public street shall be submitted with the final site plan.
5. Elevations of side façade units facing a public street shall be submitted with the final site plan.
6. A corrected copy of the preliminary SP plan incorporating the conditions of approval by the Metro Council shall be provided to the Planning Commission prior to or with the final site plan application.
7. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering, or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
8. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



Metro Planning Commission Meeting of 03/26/2015



2015SP-033-001
BURKITT STATION
Map 183, Parcel(s) 012.01, 012, 060
12, Southeast
33 (Robert Duvall)



| | |
|-------------------------|--|
| Project No. | Zone Change 2015SP-033-001 |
| Project Name | Burkitt Station |
| Council District | 33- Duvall |
| School District | 2 - Brannon |
| Requested by | Ragan-Smith Associates, Inc., applicant; Keach Investments, owner. |

Deferrals This case was deferred from the March 12, 2015, Planning Commission meeting.

Staff Reviewer Birkeland
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

Preliminary SP to permit a mixed use development.

Preliminary SP

A request to rezone from Agricultural/Residential (AR2A) to Specific Plan-Mixed Use (SP-MU) zoning for property located at 13153, 13159, and 13167 Old Hickory Boulevard, approximately 640 feet south of Muci Drive, (18.74 acres), to permit up to 244 residential units and mixed use development.

Existing Zoning

Agricultural/Residential (AR2A) requires a minimum lot size of two acres and intended for uses that generally occur in rural areas, including single-family, two-family, and mobile homes at a density of one dwelling unit per two acres. The AR2a District is intended to implement the natural conservation or rural land use policies of the general plan. *AR2a would permit a maximum of 9 lots with 2 duplex lots for a total of 13 units.*

Proposed Zoning

Specific Plan-Mixed Use (SP-MU) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes residential uses in addition to commercial uses.

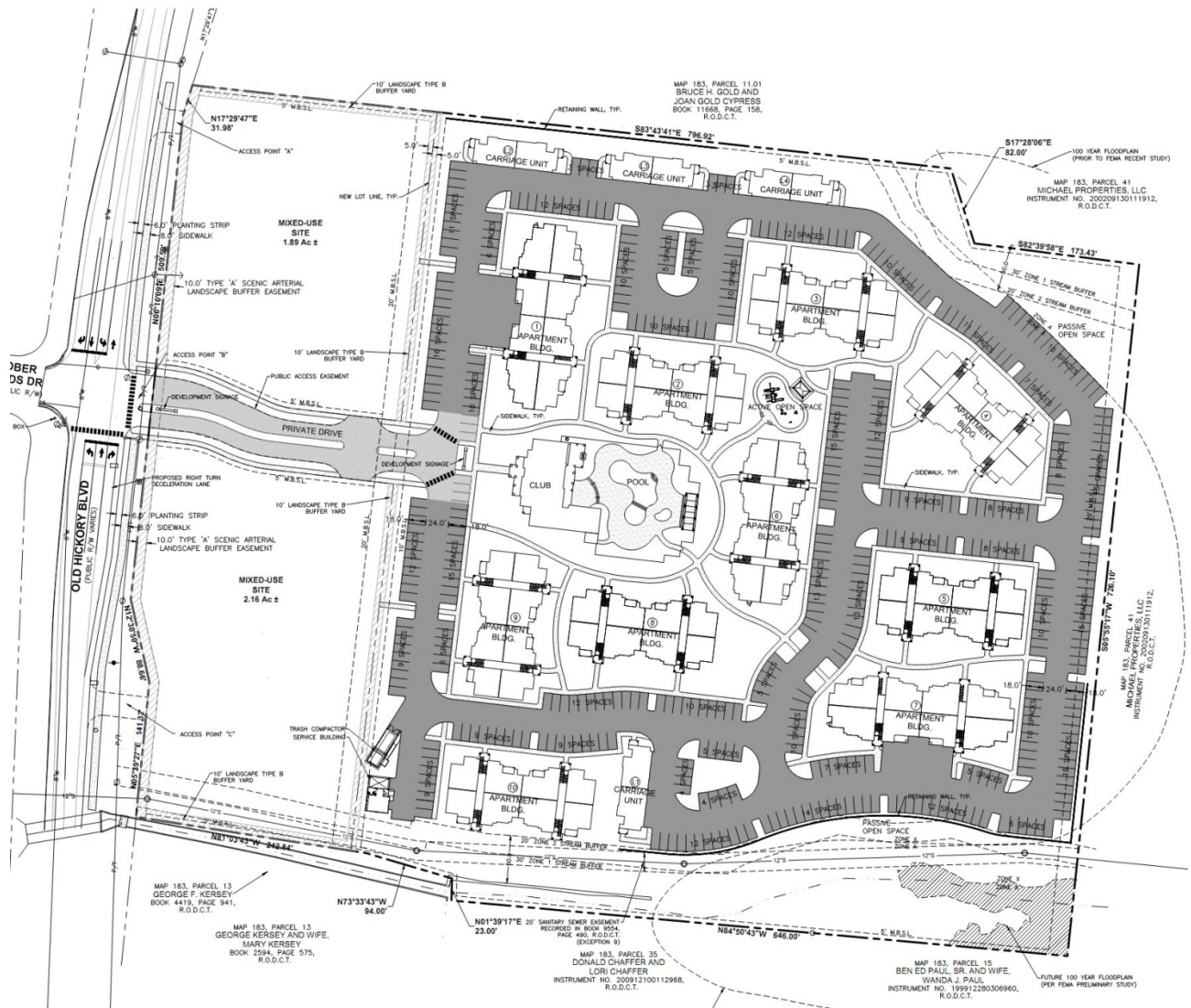
CRITICAL PLANNING GOALS

- Creates Walkable Neighborhoods

This site has frontage on Old Hickory Boulevard, an active corridor, providing retail and services in Southeast Nashville. The SP will project will intensify development on an infill site and provide for a different housing type than currently exists in the immediate area. Sidewalks are being provided along Old Hickory Boulevard to increase walkability in the area and within the development.



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Proposed Site Plan



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SOUTHEAST COMMUNITY PLAN

Current Policy

T3 Suburban Community Center (T3 CC) policy is intended to enhance suburban community centers encouraging their redevelopment as intense mixed use areas that are compatible with the general character of suburban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T3 Suburban Community Centers are pedestrian friendly areas, generally located at prominent intersections.

DRAFT Preferred Future Policy

No changes are proposed.

Consistent with Policy?

Yes. The SP would permit residential uses and a variety of commercial mixed use consistent with the T3 Community Center land use policy. The placement of the proposed commercial uses and multi-family residential uses will provide a transition between the center and less intense suburban residential surrounding the SP.

PLAN DETAILS

The proposed SP will permit up to 244 multifamily residential units and mixed commercial uses. The intent of the mixed commercial portion is to encourage a neighborhood mixed use center to serve nearby residents.

Uses allowed in the commercial portion will be based on uses allowed in the MUN zoning district, excluding cash advance and cash checking. Additional uses permitted include day car – parent’s days out, animal hospital, animal boarding facility, automobile convenience (permitted with conditions the same within MUL-A zoning district), fast-food restaurant and parking and recreation uses. Residential land uses will be based on uses allowed in the RM20 Zoning.

Commercial / Mixed Use Phase

The mixed use phase will occupy approximately four acres within the SP, along the western property line, with frontage along Old Hickory Boulevard. Standards have been established to ensure that the buildings will create a community center and provide a pedestrian friendly environment. These standards include that front yard setback, to the primary street, shall be 0 feet to 80 feet. The build shall occupy 45% of the lot and the first floor height of a one story building shall be 16 feet; multi-story shall be 14 feet in height. The intensity of development will be determined by the provided floor area ratio (0.60). Maximum height is three stories, within 45 feet. All buildings shall comply with glazing requirements and architectural standards.

This SP includes an eight foot sidewalk and a six foot planting strip along Old Hickory Boulevard as the Major and Collector Street Plan (MCSP) All internal sidewalks shall be a minimum of five feet. Sidewalk connections shall be made to the residential portion of the SP and Old Hickory Boulevard.

All ground and building signs must be approved under an overall signage plans for each phase of development with the SP District. Commercial signage will allow wall signs to be mounted flush or



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projecting, window signs, awning signs, ground signs, etc. The total sign area shall be determined based upon type of sign. Signs that are to be lighted shall be spotlighted, externally-lit, or back-lit with a diffused light source. Backlighting shall illuminate only the letters, character, or graphics on the sign, rather than the background of the sign. Backgrounds shall be opaque.

Multi-family signs shall be limited to the name of the multi-family development and/or insignia. Monument signs shall be allowed at the medium of the private access drive and the terminus of the central private drive.

Residential Phase

The proposed residential component of the SP will occupy 14 acres on the eastern portion of the site. The residential use would allow up to 244 multi-family units. These units will be in the form of apartment buildings and carriage units. The maximum height of the buildings will not exceed three stories. Amenities as a club house, pool and playground have been included. An extensive internal sidewalk network has been included to promote walkability.

Parking/Access

The required number of parking spaces shall be consistent with current requirements stipulated in the Metro Zoning Code. The amount of bedrooms per residential dwelling unit shall be submitted with the final site plan. It is possible that additional open space may be created by a reduction in parking stalls at that time. Three access points are proposed along Old Hickory Boulevard; access point "B" will provide the main access to the residential units and the mixed use space. Access points "A" and "C" will be used to access the commercial portion. Access locations will be determined with any final site plan and would be subject to Public Works approval.

Landscaping

A 10 foot type "A" scenic arterial landscape buffer yard will be installed along Old Hickory Boulevard. Parking areas will be separated from adjacent side and rear lot lines by a type "B" landscape buffer yard on commercial portion along the northern, eastern and southern property lines. All Landscaping shall meet the requirements in the Metro Code.

Infrastructure Deficiency Area

In 2004, a community plan update was prepared for the Southeast Community Plan. As part of the update, the Planning Commission noted that there were certain portions of the community that had insufficient infrastructure to meet development demands and expected growth. An Infrastructure Deficiency Area was established and any proposed developments within this area are required to improve major roadways, or construct an equivalent transportation improvement, to accommodate additional traffic volumes created by the development.

In order to accommodate traffic volumes, the applicant shall improve major roadways (or an equivalent transportation improvement) within the identified infrastructure deficiency area to a two-lane cross-section at a level acceptable to the Department of Public Works. Eligible roadways are identified in the Infrastructure Deficiency Area map. Such improvements shall be undertaken within available right-of-way and at a level commensurate with the development entitlements appropriate on the site. The rate of a commensurate requirement is calculated as 3,087 total required feet of roadway.



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ANALYSIS

The SP provides a preliminary site plan and provides a development plan with the necessary conditions that will be used to regulate future development within the SP boundary.

The proposed SP is consistent with the T3 Community Center land use policy and staff recommends approval with conditions.

FIRE MARSHAL'S OFFICE

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review. Applicant states grades are adequate to meet our requirements.

PUBLIC WORKS RECOMMENDATION

Conditions if approved

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Comply with MPW Traffic Engineer conditions.
- All roadway and ROW improvements are to meet MPW and AASHTO standards prior to Final SP.
- Submit a plan indicating the roadway improvements on separate sheet. Include note indicating signal modifications, existing vs proposed EOP, new vs existing striping, etc.
- All roadway improvements that are required for the operation of any of the 3 driveways shown (A, B, or C) must be completed with the construction of Phase 1 and must be completed prior to the first U/o for the development.

TRAFFIC AND PARKING RECOMMENDATION

Conditions if approved

In accordance with TIS findings, the developer shall construct and install the following road improvements.

- Developer of the proposed SP apartment parcel shall construct 1 SB left turn lane from northern property line to the main access road opposite October Woods and extend the existing NB left turn lane at October Woods with TWTL striping to access C and provide adequate lane transitions.
- Developer of the proposed SP apartment parcel shall construct 1 NB thru/right travel lane along SP Old Hickory Blvd frontage and provide adequate lane transition.
- Developer of the SP apartment parcel shall modify the existing signal as required and provide pedestrian signals and infrastructure per ADA standards. Developer shall submit signal plan and modify signal when approved by Metro traffic engineer. The existing signal at this intersection should be modified to accommodate southbound, eastbound, and westbound left turn phases. Signal timing at this signalized intersection should be reviewed.
- The Proposed Burkitt Station Access "B" should include one lane for traffic entering the site and two lanes for traffic exiting the site. The lanes exiting the site should be designated as one left turn lane and one thru/ right turn lane each having a storage length of 100 feet at a minimum. This access will create a new east approach at the signalized intersection.



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- Developer of the commercial parcels shall submit signal warrant analysis for the Old Hickory Blvd /BURKITT RD at Old Hickory Blvd intersection and provide signal plan and install traffic signal when directed by metro traffic engineer.
- Three access points are shown on the SP. No additional access drives will be allowed to OHB.
- Provide adequate sight distance at all access drives along OHB frontage. Proposed landscaping and development signage should be set back a minimum of 20 feet from the roadway edge to provide appropriate intersection sight distance from the proposed Burkitt Station Access.

STORMWATER RECOMMENDATION

Approved

WATER RECOMMENDATION

Approved

- The unit count in this SP package now matches the unit count in the latest availability study (244 units). Approved as a Preliminary SP only. Public construction plans must be submitted and approved prior to Final SP stage. The required capacity fees must be paid prior to Final SP approval.

Maximum Uses in Existing Zoning District: **AR2a**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Single-Family Residential (210) | 18.74 | 0.50 D | 9 U | 87 | 7 | 10 |

Maximum Uses in Proposed Zoning District: **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|--------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Multi-Family Residential (220) | 18.74 | - | 248 U | 1627 | 126 | 155 |

Traffic changes between maximum: **AR2a** and **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | + 239 U | +1,540 | +119 | +145 |

METRO SCHOOL BOARD REPORT

Projected student generation existing AR2A district: 8 Elementary 5 Middle 6 High
Projected student generation proposed SP-MU district: 26 Elementary 21 Middle 17 High

The proposed SP-MU zoning district could generate 45 more students than what is typically generated under the existing AR2A zoning district. Students would attend A.Z. Kelley Elementary School, Marshall Middle School, and Cane Ridge High School. Marshall Middle School and Cane Ridge High School have been identified as over capacity. There is capacity within the cluster for



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middle school students, but there is no capacity within the adjacent clusters for high school students. This information is based upon data from the school board last updated October 2014.

The fiscal liability of 11 high school students is \$396,000.00 (11 x \$36,000 per student). This is only for information purposes to show the potential impact of this proposal, this is not a staff condition of approval.

STAFF RECOMMENDATION

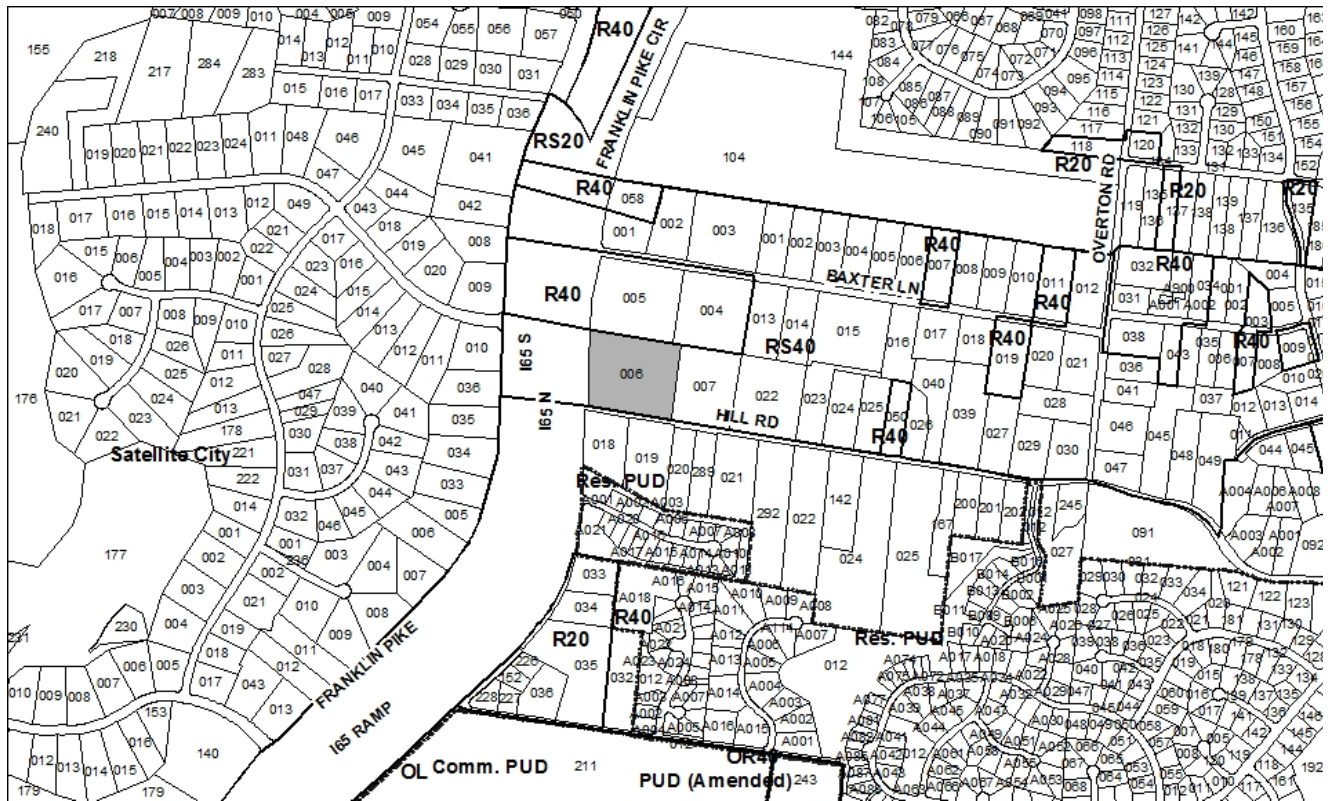
Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Commercial uses within Phase 2 shall be limited to uses allowed in the MUN zoning district, excluding cash advance and cash checking. Additional uses permitted include day care – parent's days out, animal hospital, animal boarding facility, automobile convenience (permitted with conditions the same within MUL-A zoning district), fast-food restaurant and parking and recreation uses. Uses in Phase 1 are limited to multi-family residential.
2. Roadway improvements that are a direct result of this specific project as determined by the approved Traffic Impact Study and the Department of Public Works shall be constructed.
3. Any required right-of-way within the project site that is identified as necessary to meet the adopted roadway plans shall be dedicated.
4. In order to accommodate traffic volumes, the applicant shall improve major roadways (or an equivalent transportation improvement) within the identified infrastructure deficiency area to a two-lane cross-section at a level acceptable to the Department of Public Works. Eligible roadways are identified in the Infrastructure Deficiency Area map. Such improvements shall be undertaken within available right-of-way and at a level commensurate with the development entitlements appropriate on the site. The rate of a commensurate requirement is calculated as 3,087 total required feet of roadway. IDA requirements shall be completed prior to the issuance of permits for the first phase.
5. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the MUN-A zoning district as of the date of the applicable request or application.
6. All signs must be approved under an overall signage plans for each phase of development with the SP District. Signage standards shall meet the requirements within the SP plan.
7. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
8. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2013S-132-001
HILL ROAD SUBDIVISION
Map 160-06, Parcel(s) 006
12, Southeast
26 (Chris Harmon)



| | |
|-------------------------|---|
| Project No. | Subdivision 2013S-132-001 |
| Project Name | Hill Road Subdivision |
| Council District | 26 – Harmon |
| School District | 8 – Pierce |
| Requested by | Mike May owner, Harrah & Associates, applicant. |

Deferrals This request was deferred from the February 12, 2015, Planning Commission meeting.

Staff Reviewer Swaggart
Staff Recommendation *Approve with conditions.*

APPLICANT REQUEST

Final Plat to create five single-family residential lots.

Final Plat

A request for final plat approval to create five lots on property located at 808 Hill Road, at the northeast corner of Hill Road and Franklin Pike Circle, (5.83 acres), zoned Single-Family Residential (RS40).

Existing Zoning

Single-Family Residential (RS40) requires a minimum 40,000 square foot lot and is intended for single-family dwellings at a density of .93 dwelling units per acre. *RS40 would permit a maximum of six lots.*

CRITICAL PLANNING GOALS

- Supports Infill Development

The subdivision creates additional residential development opportunity consistent with the land use policy in an area where infrastructure and services exist.

HISTORY

This application was originally filed on July 11, 2013, for the August 22, 2013, MPC meeting. During this time, Stormwater revised their policy regarding single-family development. Stormwater’s new policy requires that single-family development address stormwater issues, which resulted in an indefinite deferral.

After the application was filed, the Planning Commission began evaluating whether to review subdivisions under the One Tier or Two Tier Approach, which is explained later in this report. The new Subdivision Regulations for infill development had not yet been adopted. *This final plat application is being reviewed under the Subdivision Regulations in effect at the time the application was initiated.*



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INFILL SUBDIVISION REVIEW: ONE OR TWO-TIER APPROACH

In 2011, the Subdivision Regulations were amended. Included in the amendment was the replacement of Section 3-5, Lot Comparability with Section 3-5, Infill Subdivisions. The section applies to subdivision proposals in areas that are predominately developed.

The first section, Section 3-5.1, requires that new lots in areas that are predominately developed be generally comparable to surrounding lots and is written as follows:

1. *Infill Subdivisions.* In areas previously subdivided and predominately developed, residential lots resulting from a proposed subdivision within the R and RS zoning districts on an existing street shall be generally comparable with surrounding lots.

The subsequent section, Section 3-5.2, refers to criteria for determining comparability which is as follows:

2. **Criteria for Determining Comparability:** The following criteria shall be met to determine comparability of lots within infill subdivisions:
 - a. The resulting density of lots within the RL, RLM and RM land use policies do not exceed the prescribed densities of the polices.
 - b. For lots within NE, NM and NG policies, the lots fit into the community character as defined in Section 7-2 and are consistent with the general plan.
 - c. All minimum standards of the zoning code are met.
 - d. Each lot has street frontage or meets the requirements of Section 3-4.2.b for fronting onto an open space or meets the requirements of Sections 4-6.3 or 5-3.1 fronting onto open space.
 - e. The current standards of all reviewing agencies are met.

One-Tier Approach

Under the one-tier approach, staff read subsections 1 and 2 together and defined comparability by utilizing the language in Subsection 3-5.2. New lots would be comparable in the RL, RLM and RM land use policies if the resulting densities do not exceed the prescribed densities of the policies. The density calculation can be determined two ways:

1. Looking at the lot(s) proposed for subdivision;
2. Looking at a larger area.

The area for determining density is not defined; therefore, staff must use best judgment to define the area to use for the density calculation. It could include adjacent lots on both sides of the lot(s) proposed for subdivision, across the street or the entire block.

Two-Tier Approach

Under the two-tier approach, subsections 1 and 2 are considered separately, creating a two-part test for determining comparability. Staff must first determine if the proposed lots are generally comparable, as specified in subsection 1. The terms “generally comparable” and “surrounding lots” are not defined. If it is determined that the proposed lots are generally comparable to surrounding lots, then the new lots must also be consistent with subsection two.



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Since the regulations do not define the area for which proposed lots should be compared, staff must define an area for which to compare. Without guidance from the regulations, the defined area becomes subjective. To reduce subjectivity, staff defined the area to compare as lots on the same block face. If it is a corner lot then both block faces may be considered.

Planning Commission Action

The Planning Commission is the ultimate interpreter of the Subdivision Regulations. The Planning Commission has the ability to agree with either interpretation or provide a new interpretation. Prior to the December 12, 2013, Planning Commission meeting, the Planning Commission debated how to interpret the Subdivision Regulations. At the December 12, 2013, meeting, the Planning Commission evaluated a majority of the applications on the agenda using the One-Tier approach. Also at the December 12, 2013, meeting, the Commission heard proposed amendments to the infill sections of the Subdivision Regulations and took the following action:

“Defer all amendments ... until the January 9, 2014, Planning Commission meeting, direct staff to process all infill subdivision cases submitted before noon today [December 12, 2013] under the current language in Section 3-5 and direct staff not to process any additional infill subdivision applications until amendments to Section 3-5 are adopted...”

Note: references to non-infill Sections of the Subdivision Regulations removed.

Therefore, this application was reviewed using the One-Tier approach.

PLAN DETAILS

This subdivision proposes five single-family residential lots where one lot and an existing dwelling currently exist. The proposed density is less than one unit per acre (0.85 DU/AC). The subject lot is located on the northeast quadrant of Hill Road and Franklin Pike Circle. The lot is very large (approximately 5.8 acres) and there are no known environmental constraints. The lot does contain numerous mature trees.

As proposed all lots will contain the minimum lot area required by the RS40 zoning. The approximate lot areas and street frontages are as follows:

Lot 48: 72,684 sq. ft.; 104.76, along Hillwood and;

Lot 49: 43,901 sq. ft.; 95’;

Lot 50: 43,954 sq. ft.; 95’;

Lot 51: 44,007 sq. ft.; 95’;

Lot 49: 44,054 sq. ft.; 95’.

The plat provides a 170’ foot minimum setback along Hill Road and a 40’ setback along Franklin Pike Circle. As proposed all lots would be accessed by a shared drive that would access Hill Road at two locations.

ANALYSIS

One-Tier Approach

At the time the application was submitted, the land use policy that applied to the existing lot and a majority of the surrounding area was Residential Low (RL). RL policy supports low intensity



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development with a maximum density of two units per acre. The density for the proposed five lots is approximately 0.85 units per acre. This calculation is based on the subject site, not the surrounding area. *Because the density of the subdivision is under policy, the five proposed lots are comparable.* The site is also within a Special Policy, with which the proposed subdivision complies. The special policy supports lots no smaller than 40,000 sq. ft. It also calls for resubdivision of properties to protect views from the streets and from existing buildings by preserving trees that line the roads and by orienting new homes so that their rear yards are not in a direct line of sight from the fronts of existing homes.

Two-Tier Approach

First, staff determines whether the subdivision is “generally comparable.” The five proposed lots are not generally comparable in terms of lot size and width at the street. This is based on 12 lots on the same block face from Franklin Pike Circle to Overton Road.

Average Area: 108,900 sq. ft.

Average Frontage: 222 Feet

Sidewalks

There are no sidewalks in the immediate area along Hill Road or Franklin Pike Circle. There are four options to satisfy the sidewalk requirements which are as follows:

- The required sidewalks have been bonded;
- The required sidewalks have been constructed and accepted by Public Works;
- A contribution to the sidewalk fund has been made in lieu of constructing the required sidewalks;
- An equal length of sidewalk is constructed and accepted or bonded within the same Pedestrian Benefit Zone (PBZ).

When the application was submitted, the total contribution would be \$6,000. The policy required \$15 per linear foot or \$1,500 per lot, whichever is less.

STORMWATER RECOMMENDATION

Approve with conditions

1. Pay Plan Review Fee and Grading Permit Fee, Total Cost = \$1035.00.
2. Add NOC note with TNR #.
3. List amount of disturbed area.
4. Extend SF along Northern property line.
5. Add an EPSC Phase 2 drawing showing proposed contours and EPSC measures, or show the Phase 2 measures on the grading and drainage plan.
6. Suggest removing western ditch once it passes the set back line, and allow water to sheet flow to pond #3.
7. Add lot line swales to #49 and #52.
8. Suggest moving ponds to north side of shared drive.
9. Show underdrains in ponds.
10. Clarify how swale A-A ties into existing roadside ditch.



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WATER SERVICES RECOMMENDATION

Returned for corrections

- Awaiting construction plan submittal/approval and payment of the required capacity fees. A new availability request must also be submitted, since the previous study expired back in 2013.

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- If sidewalks are required, then they should be shown on the plan per Public Works standards with the required curb and gutter and grass strip.

STAFF RECOMMENDATION

Staff recommends approval with conditions under one-tier approach, as the proposed lots are generally comparable since they are in keeping with the previous Residential Low land use policy.

CONDITIONS

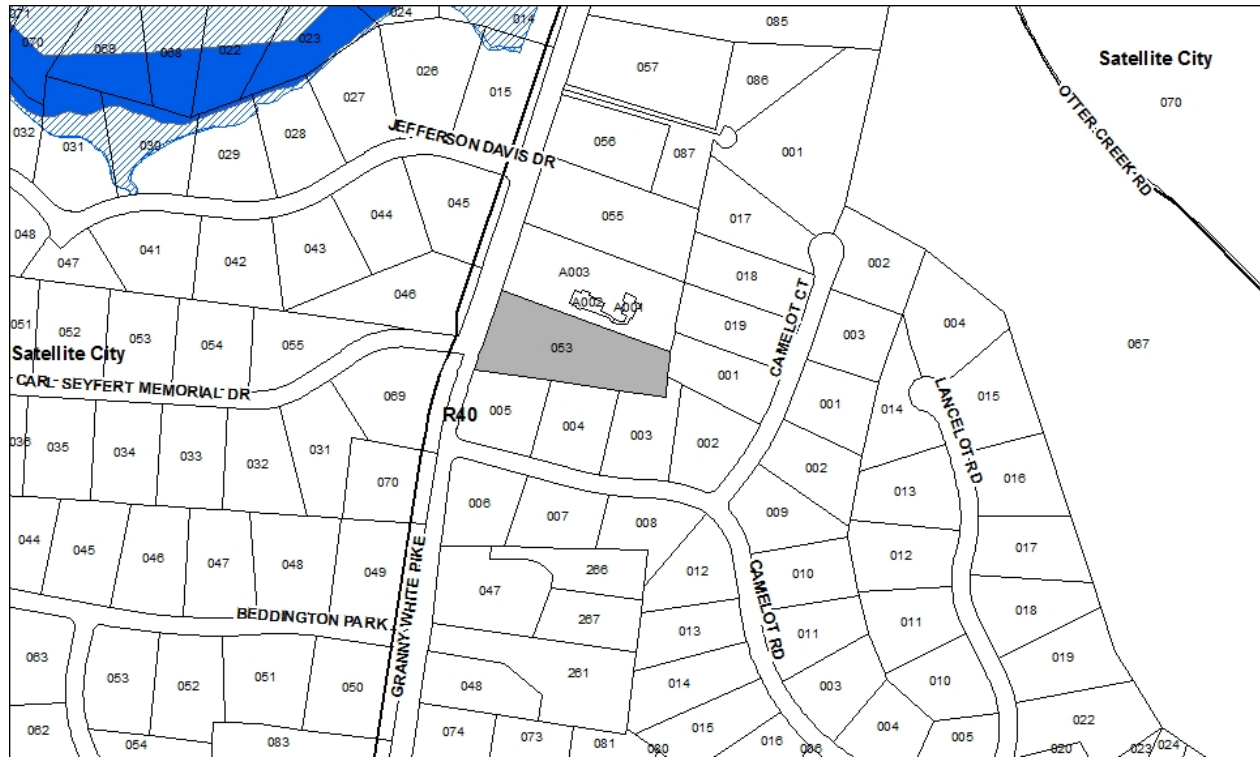
1. Provide proof of removal of the existing building on the site prior to recordation of the subdivision.
2. Final approval from Water Services is required prior to the recordation of the plat.
3. Final approval from Stormwater is required prior to the recordation of the plat.
4. Sidewalks are required along the Hill Road and Franklin Pike. Prior to final plat recordation, one of the options must be chosen related to sidewalks:
 - a. Submit a bond application and post a bond with the Planning Department,
 - b. Construct sidewalk and have it accepted by Public Works,
 - c. Submit contribution in-lieu of construction to the Planning Department, four additional lot will require a \$6,000 contribution to Pedestrian Benefit Zone 5-B.
 - d. Construct an equal length of sidewalk within the same Pedestrian Benefit Zone (5-B), in a location to be determined in consultation with the Public Works Department, or add the following note to the plat: "No building permit is to be issued on any of the proposed lots until the required sidewalk is constructed per the Department of Public Works specifications." Sidewalk shall be shown and labeled on the plan per Public Works Standards with the required curb and gutter.



SEE NEXT PAGE



Metro Planning Commission Meeting of 03/26/2015



2015S-024-001

RESUB. LOT 6, THE ROBERT H. DEMOSS 69- ACRE TRACT

Map 145, Parcel(s) 053

10, Green Hills - Midtown

34 (Carter Todd)



Project No. 2015S-024-001
Project Name Resub. Lot 6, The Robert H. Demoss 69 – Acre Tract
Council District 34 – Todd
School District 8 – Pierce
Requested by CK Surveyor, LLC, applicant; Phillip L. Bennett, et ux, owners.

Deferrals This case was deferred from the February 26, 2015, and the March 12, 2015, Planning Commission meetings.

Staff Reviewer Swaggart
Staff Recommendation Disapprove.

APPLICANT REQUEST

Create two lots.

Final Plat

A request for final plat approval to create two lots on property located at 5408 Granny White Pike, on the east side of Granny White Pike, approximately 170 feet north of Camelot Road, zoned One and Two-Family Residential (R40) (2.53 acres).

Existing Zoning

One and Two-Family Residential (R40) requires a minimum 40,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 1.16 dwelling units per acre including 25 percent duplex lots. *R40 would permit a maximum of two lots with two duplex lots for a total of four units.*

CRITICAL PLANNING GOALS

N/A

GREENHILLS – MIDTOWN COMMUNITY PLAN

Current Policy

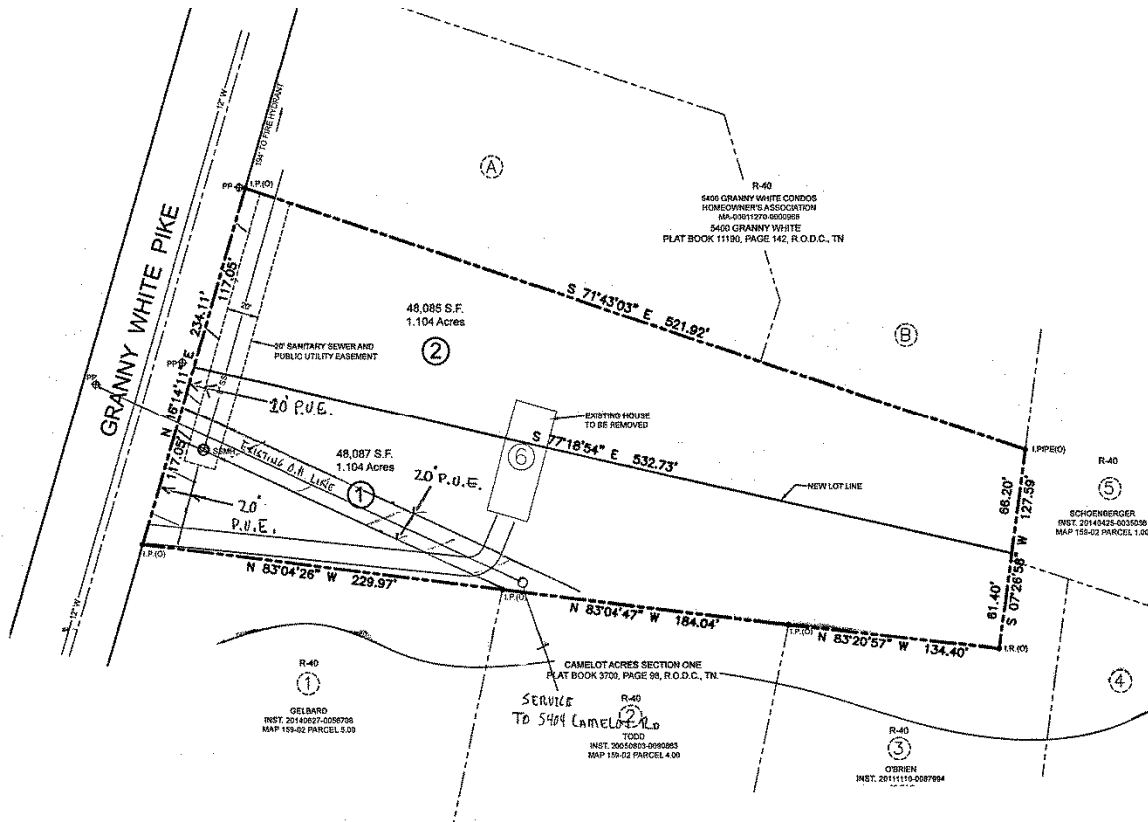
Suburban Neighborhood Maintenance (T3 NM) is intended to preserve the general character of suburban neighborhoods as characterized by their development pattern, building form, land use and associated public realm. T3 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and the public realm. Where not present, enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Current Special Policy 13

The special policy which was adopted in 2008, calls for new development to reflect the character and appearance of abutting lots in terms of lot widths and front setbacks.



Metro Planning Commission Meeting of 03/26/2015



Proposed Subdivision



Metro Planning Commission Meeting of 03/26/2015

DRAFT Preferred Future Policy

No changes are proposed.

REQUEST DETAILS

This final plat proposes to split one parcel into two lots. Each lot will have access from Granny White Pike. Proposed lots are as follows:

- Lot 1: 47,503 sq. ft. (1.09 acres), and 116.54' of frontage;
- Lot 2: 47,501 sq. ft. (1.09 acres), and 116.56' of frontage.

ANALYSIS

The proposed subdivision does not meet the special policy or the the infill compatibility analysis that is outlined in Section 3-5.2 of the Subdivision Regulations. Section 3-5.2 of the Subdivision Regulations outlines the criteria for reviewing infill subdivisions located within the sites Suburban Neighborhood Maintenance policy area.

Staff reviewed the final plat against the following criteria, as required by the Subdivision Regulations:

Zoning Code

Proposed lots meet the minimum standards of the R40 zoning district.

Street Frontage

Proposed lots have frontage on a public street.

Density

Urban Neighborhood Maintenance land use policy supports density up to 20 dwelling units per acre. The proposed infill subdivision would provide a density of less than one acre for one unit on two lots or approximately 1.8 units per acre for two units on each lot. Both fall within the range supported by policy.

Community Character along Granny White Pike

1. Lot frontage: The proposed lots must have frontage either equal to or greater than 70% of the average frontage of surrounding parcels or equal to or greater than the surrounding lot with the least amount of frontage, whichever is greater. *The proposed lots do not meet the minimum frontage requirement.*

| Lot Frontage Analysis | |
|-----------------------------|------|
| 70% of Average | 120' |
| Smallest Surrounding Parcel | 88' |

| Proposed | |
|----------|---------|
| Lot 1 | 116.54' |
| Lot 2 | 116.56' |



Metro Planning Commission Meeting of 03/26/2015

2. Lot size: The proposed lots must have lot area that is either equal to or greater than 70% of the lot size of the average size of surrounding parcels or equal to or larger than smallest surrounding lot, whichever is greater. *The proposed lots do not meet the minimum area requirement.*

| Lot Size Analysis | | Proposed | |
|-----------------------------|----------------|----------|----------------|
| 70% of Average | 57,934 sq. ft. | Lot 1 | 47,503 sq. ft. |
| Smallest Surrounding Parcel | 52,272 sq. ft. | Lot 2 | 47,501 sq. ft. |

3. Street Setback: The plat does not propose a platted setback to ensure that new homes would be consistent with adjacent homes or the special policy.
4. Lot Orientation: Both lots are oriented towards Granny White Pike.

Compatibility with Surrounding Area

Neither lot is compatible with the surrounding lots as outlined in the Subdivision Regulations. At time this report was sent out, the applicant has not provided any information demonstrating how the proposed lots would fit into the overall character of the area. Staff is recommending disapproval because the proposed lots do not meet the surrounding community character as outlined in the Subdivision Regulations.

FIRE MARSHAL RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

Conditions if approved

- A permit is required from The Department of Public Works prior to commencing any work within the right-of-way.
- If sidewalks are required by Planning and the applicant chooses to construct rather than pay the in-lieu fee, they should be shown and labeled on the plan per Public Works standards. This includes curb and gutter, grass strip, sidewalk, and a minimum of 20 feet pavement width if on a minor local road. Final construction plans must be submitted and address any related drainage improvements, utility relocation(s), and tree removal.

STORMWATER RECOMMENDATION

Returned for corrections

1. The 20 foot "Sanitary Sewer and Public Utility Easement" should be changed to a "Sanitary Sewer and Public Utility and Drainage Easement."
2. Change P.U.E to P.U.D.E along Granny White pike.

WATER SERVICES RECOMMENDATION

Returned for corrections

- The required capacity fees must be paid.



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STAFF RECOMMENDATION

Staff recommends disapproval as the request is not consistent with the surrounding community character as outlined in the Subdivision Regulations.

CONDITIONS (If approved)

1. The plat shall not be recorded until capacity fees have been paid.
2. The plat shall not be recorded until it has been approved by Stormwater.
3. A permit is required from The Department of Public Works prior to commencing any work within the right-of-way.
4. If sidewalks are required by Planning and the applicant chooses to construct rather than pay the in-lieu fee, they should be shown and labeled on the plan per Public Works standards. This includes curb and gutter, grass strip, sidewalk, and a minimum of 20 feet pavement width if on a minor local road. Final construction plans must be submitted and address any related drainage improvements, utility relocation(s), and tree removal.



SEE NEXT PAGE

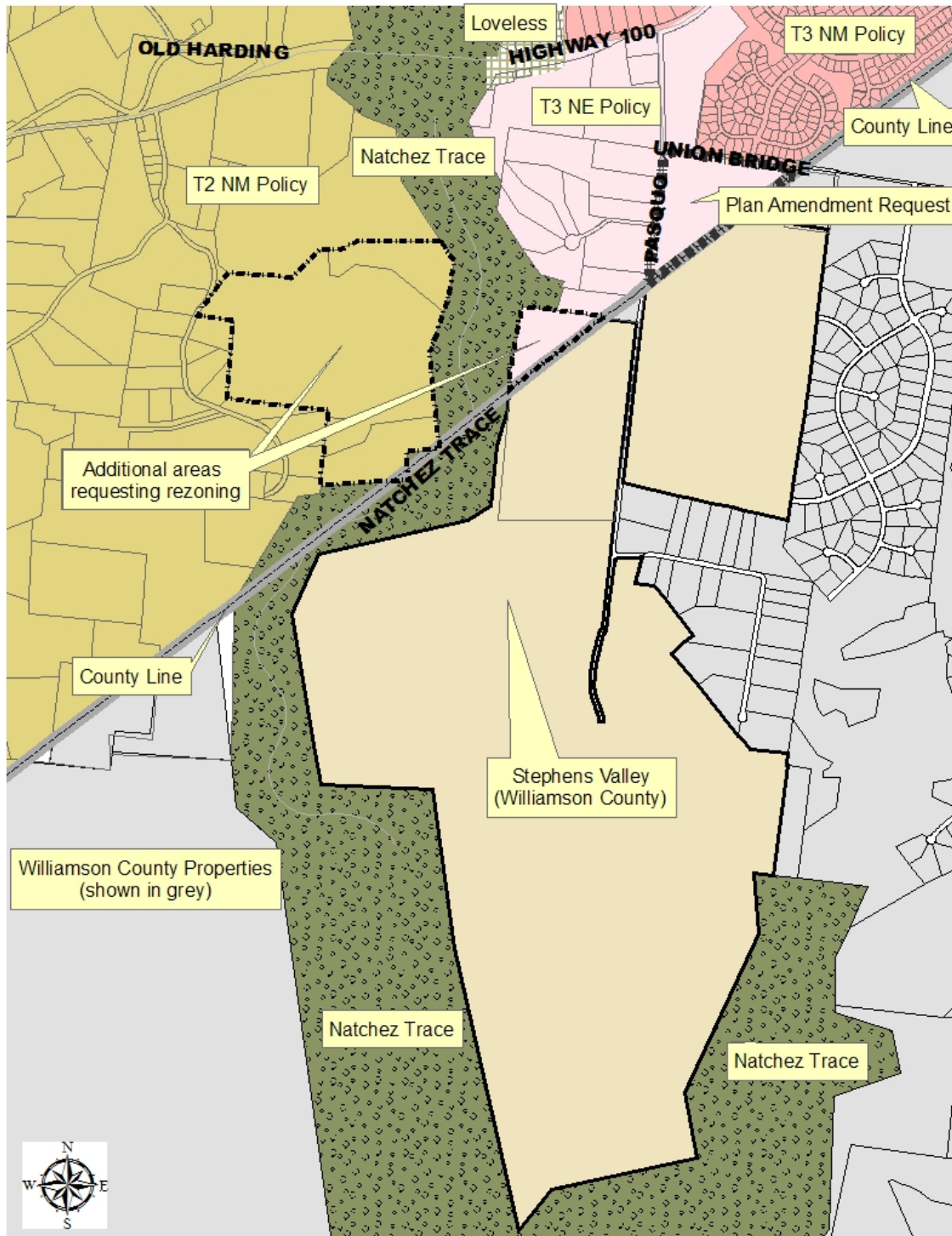


COMMUNITY PLAN POLICY CHANGES and ASSOCIATED CASE

- **Plan Amendment**
- **Specific Plan**



Metro Planning Commission Meeting of 03/26/2015



2015CP-006-001
BELLEVUE COMMUNITY PLAN AMENDMENT
Map 169, Parcel(s) 069
06, Bellevue
35 (Bo Mitchell)



| | |
|-----------------------------|---|
| Project No. | Major Plan Amendment 2015CP-006-001 |
| Project Name | Bellevue Community Plan: 2011 Update – Stephens Valley |
| Associated Case | 2015SP-013-001 |
| Council District | 35 – Mitchell |
| School District | 9 – Frogge |
| Requested by | Ragan-Smith Associates, Inc., applicant; Natchez Associates, L.P., owner. |
| Staff Reviewer | McCaig |
| Staff Recommendation | <i>Approve.</i> |

APPLICANT REQUEST

Change the policy from Suburban Neighborhood Evolving to Suburban Neighborhood Center.

Major Plan Amendment

A request to amend the Bellevue Community Plan by changing the Community Character policy from Suburban Neighborhood Evolving policy to Suburban Neighborhood Center policy for property located at 441 Union Bridge Road, at the southeast corner of Pasquo Road and Union Bridge Road (23.83 acres).

CRITICAL PLANNING GOALS

- Creates Walkable Neighborhoods
- Provides a Range of Housing Choices

The application of Suburban Neighborhood Center policy on this property creates a mixed use center that provides opportunities for neighborhood-scaled services, shops, restaurants, offices, and residences and creates a destination that can be walked to from nearby residential uses.

BELLEVUE COMMUNITY PLAN

Current Policy

Suburban Neighborhood Evolving (T3 NE) policy is intended to create suburban residential neighborhoods that provide more opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern will have higher densities than many surrounding neighborhoods and/or smaller lot sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land without sensitive environmental features and the cost of developing housing. These are challenges that were not faced years ago when original suburban neighborhoods were built.

DRAFT Preferred Future Policy

No change proposed.

Proposed Policy

Suburban Neighborhood Center (T3 NC) policy is intended to enhance and create suburban neighborhood centers that fit in with the general character of suburban neighborhoods.



Metro Planning Commission Meeting of 03/26/2015

Infrastructure and transportation networks may be enhanced to improve pedestrian, bicycle and vehicular connectivity. T3 Suburban Neighborhood Centers are pedestrian-friendly areas, generally located at intersections of suburban streets that contain commercial, mixed use, residential, office, and civic land uses.

BACKGROUND

The property at 441 Union Bridge Road, the subject of this plan amendment request, is part of an 859 acre development proposal referred to as Stephens Valley with properties in both Davidson and Williamson Counties. Planning staff has been working with Williamson County planning staff. The property at 441 Union Bridge Road is the only portion of the property in Davidson County where a plan amendment is needed.

COMMUNITY PARTICIPATION

Community meeting and public hearing notices were mailed to property owners within 1,300 feet in Davidson County in February 2015. At the request of staff, the applicant also mailed the same notice to property owners in Williamson County. A total of 415 notices were sent out between the two counties. Copies of the notices were also placed on the Planning Department website.

A community meeting was held on February 23, 2015, to discuss the plan amendment request. Approximately 40 people attended the meeting, along with the applicants, Williamson County Planning staff, and the area councilmember.

Several attendees voiced concerns and left written comments regarding this proposal. Attendees were mainly concerned that the proposed amendment and rezoning, if approved, would:

- create additional traffic in an already congested area, especially along Sneed Road and Highway 100;
- create additional cut-through traffic in adjacent neighborhoods, including Traceside, Temple Hills and Timberline;
- negatively affect the existing quality of life;
- allow commercial and office uses to intrude into a residential area; and,
- place additional pressure on infrastructure – water, schools, fire protection, utilities.

In addition, for neighbors that live in adjacent rural areas, there were concerns that this is changing the character of their area to suburban.

The applicant began working with the community in September of 2014, months before submitting their plan amendment and rezoning request. The applicant has met with the Traceside Homeowners Association (in Davidson County) and has continued working through issues with Traceside's traffic calming committee. The applicant also met with the Temple Hills Homeowners Association and the Natchez Bend Road neighbors (both in Williamson County). Another neighborhood adjacent to the project, Timberline (also in Williamson County), does not have an organized homeowners/neighborhood association. In total, the applicant has met with the community seven times, with approximately 300 people attending across all the meetings.

ANALYSIS

Currently, the property is an agricultural and wooded property that contains a few houses scattered throughout. The property that is the subject of the plan amendment request is not visible from the



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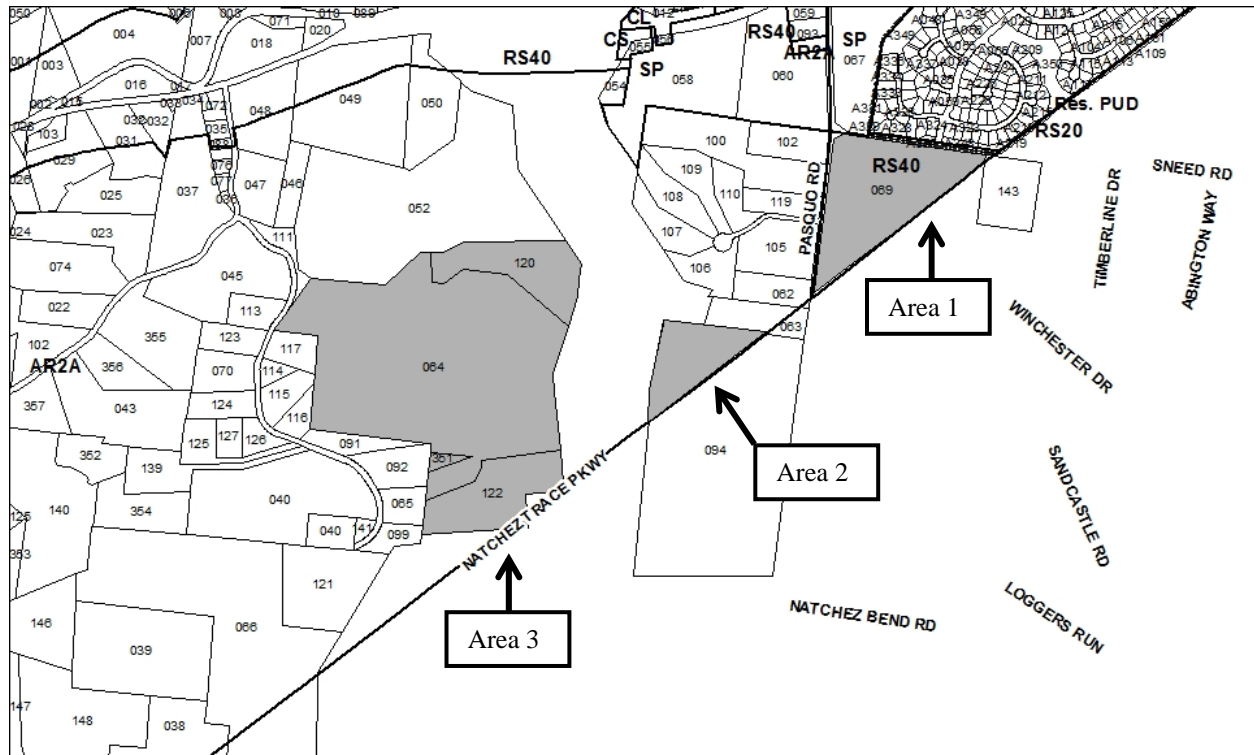
Natchez Trace Parkway and does not contain any sensitive environmental features. Ideally, the proposed suburban neighborhood center component would be located along a more prominent corridor, such as Highway 100. However, staff feels that providing convenient neighborhood services and inclusive gathering places in a residential area helps to create a more livable community that appeals to a greater variety of people, enriches civic life, and promotes a more walkable environment – opportunities to walk to the corner store or to dinner. It also provides these opportunities to the surrounding neighborhoods

STAFF RECOMMENDATION

Staff recommends approval of the amendment application because neighborhood center policy in this location creates opportunities for neighborhood-scaled services, shops, restaurants, offices, and residences and creates a destination that can be walked to from nearby residential uses.



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2015SP-013-001

STEPHENS VALLEY

Map 169, Parcel(s) 064, 069, Part of 094, 120, 122, 351

06, Bellevue

35 (Bo Mitchell)



Project No. 2015SP-013-001
Associated Case No. 2015CP-006-001
Project Name Stephens Valley
Council District 35 - Mitchell
School District 9 – Frogge
Requested by Ragan-Smith Associates Inc, LLC, applicant; Natchez Trace Associates, LP, owner.

Staff Reviewer Milligan
Staff Recommendation *Approve with conditions and disapprove without all conditions if a recommendation of approval is received from all reviewing agencies and subject to approval of the associated policy change.*

APPLICANT REQUEST

Preliminary SP to permit a mixed use development.

Preliminary SP

A request to rezone from Agricultural/Residential (AR2a) and One-Family Residential (RS40) to Specific Plan-Mixed Use (SP-MU) zoning for properties located at 441 Union Bridge Road, Natchez Trace Parkway (unnumbered), Haselton Road (unnumbered), and Pasquo Road (unnumbered), south of Highway 100 (133.15 acres) to permit a mixed use development.

Existing Zoning

Agricultural/Residential (AR2a) district requires a minimum lot size of two acres and intended for uses that generally occur in rural areas, including single-family, two-family, and mobile homes at a density of one dwelling unit per two acres. The AR2a District is intended to implement the natural conservation or rural land use policies of the general plan. *AR2a would permit a maximum of 54 lots with 13 duplex lots for a total of 67 units.*

One-Family Residential (RS40) district RS40 requires a minimum 40,000 square foot lot and is intended for single-family dwellings at a density of .93 dwelling units per acre. *RS40 would permit a maximum of 22 units.*

Proposed Zoning

Specific Plan-Mixed Use (SP-MU) district provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes residential uses in addition to office and/or commercial uses.

CRITICAL PLANNING GOALS

- Preserves Sensitive Environmental Features
- Creates Open Space
- Creates Walkable Neighborhoods
- Provides a Range of Housing Choices



Metro Planning Commission Meeting of 03/26/2015



Proposed Site Plan – Area 1



Proposed Site Plan – Area 2



Metro Planning Commission Meeting of 03/26/2015

The proposed plan meets several critical planning goals. The development, particularly Area 3, has been designed in such a way as to be sensitive to the Natchez Trace Parkway and the viewsheds of the parkway, preserving open space and places of natural beauty. Additionally, the development is protecting existing streams. By clustering the lots in Area 3, a minimum amount of the land is being disturbed creating large open space that will remain tree covered. A sidewalk network is being established that will link all parts of the development and allow for the residents to walk to retail and office uses within the town center area. Nearby residents in existing neighborhoods will also have the opportunity to walk to the town center utilizing sidewalks being installed as part of upgraded existing local roads. A variety of housing types are being included in the neighborhood providing for various housing options for future residents and allowing for residents to live in the community through all stages of life.

BELLEVUE COMMUNITY PLAN

Current Policy

T2 Rural Neighborhood Maintenance (T2 NM) policy is intended to preserve the general character of rural neighborhoods as characterized by their development pattern, building form, land use, and associated public realm. T2 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and public realm. Where not present, enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

T3 Suburban Neighborhood Evolving (T3 NE) policy is intended to create suburban neighborhoods that are compatible with the general character of classic suburban neighborhoods as characterized by their building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern will have higher densities than classic suburban neighborhoods and/or smaller lot sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing - challenges that were not faced when the original classic, suburban neighborhoods were built.

DRAFT Preferred Future Policy

No changes proposed.

Proposed Policy

T3 Suburban Neighborhood Center (T3 NC) policy is intended to enhance and create suburban neighborhood centers that are compatible with the general character of suburban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T3 Suburban Neighborhood Centers are pedestrian friendly areas, generally located at intersections of suburban streets that contain commercial, mixed use, civic and public benefit land uses, with residential present only in mixed use buildings. T3 Suburban Neighborhood Centers serve suburban neighborhoods within a 5 minute drive.



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Proposed Site Plan – Area 3



Metro Planning Commission Meeting of 03/26/2015

Consistent with Policy?

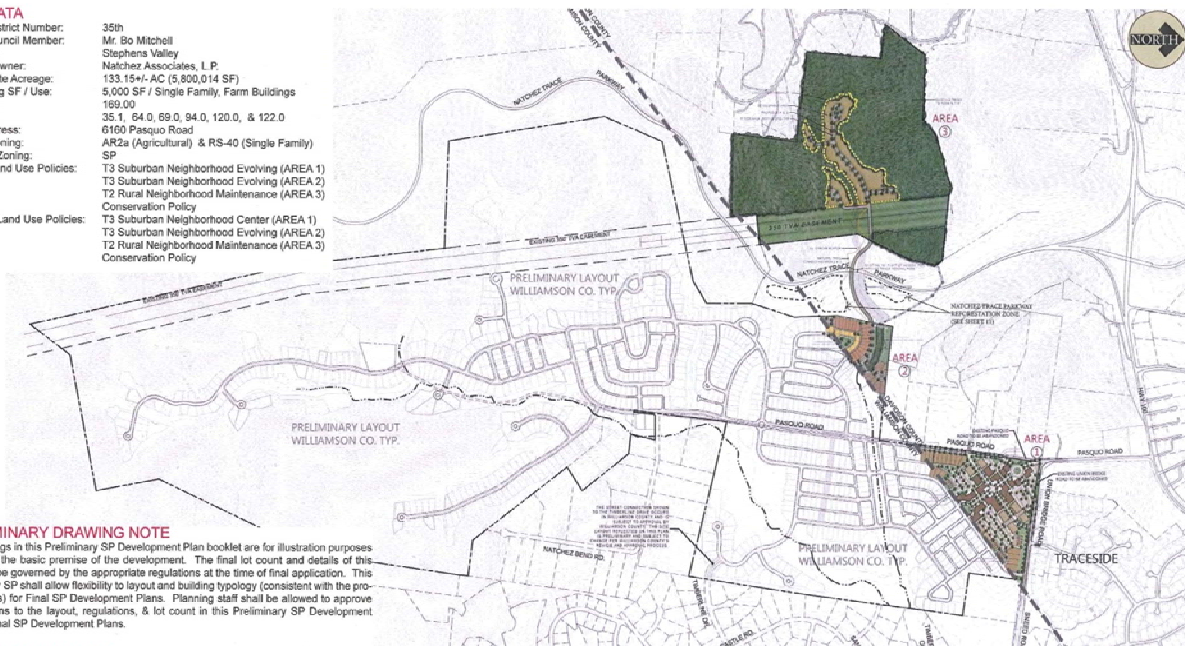
Yes. The proposed SP is consistent with the T2 Rural Neighborhood Maintenance, T3 Suburban Neighborhood Evolving, and T3 Suburban Neighborhood Center policies. Area 3 is being developed at a very low density and is preserving the majority of the land as undisturbed open space. This protects the rural nature of the property and respects the form of surrounding developments. Area 2 is providing housing types that do not currently exist in the area, providing for additional housing choice. Area 1 is creating a new suburban town center to serve the proposed development as well as existing residential developments in the area. The town center is located at a prominent intersection and includes a mixture of uses including retail, office and residential. Sidewalks are being provided on new and existing streets to encourage walking by future residents as well as residents of existing neighborhoods within the immediate area.

PLAN DETAILS

The site is located at at 441 Union Bridge Road, Natchez Trace Parkway (unnumbered), Haselton Road (unnumbered), and Pasquo Road (unnumbered), south of Highway 100. The site is approximately 133.15 acres in size. The overall Stephens Valley development is approximately 859 acres in size. The remaining portion of the development, outside of the 133.15 acres within Davidson County, is located within Williamson County. The Metro Planning staff has been working with Williamson County staff in reviewing the project to coordinate the development between the two jurisdictions.

SITE DATA

| | |
|-----------------------------|---|
| Council District Number: | 35th |
| District Council Member: | Mr. Bo Mitchell |
| SF Name: | Stephens Valley |
| Property Owner: | Natchez Associates, L.P. |
| Parcel's Site Acreage: | 133.15 +/- AC (5,800,014 SF) |
| Ex. Building SF / Use: | 5,000 SF / Single Family, Farm Buildings |
| Tax Map: | 199.00 |
| Parcels: | 35.1, 64.0, 69.0, 94.0, 120.0, & 122.0 |
| Street Address: | 6100 Pasquo Road |
| Existing Zoning: | AR2a (Agricultural) & RS-40 (Single Family) |
| Proposed Zoning: | SP |
| Existing Land Use Policies: | T3 Suburban Neighborhood Evolving (AREA 1) T3 Suburban Neighborhood Evolving (AREA 2) T2 Rural Neighborhood Maintenance (AREA 3) Conservation Policy |
| Proposed Land Use Policies: | T3 Suburban Neighborhood Center (AREA 1) T3 Suburban Neighborhood Evolving (AREA 2) T2 Rural Neighborhood Maintenance (AREA 3) Conservation Policy |



PRELIMINARY DRAWING NOTE

The drawings in this Preliminary SP Development Plan booklet are for illustration purposes to indicate the basic premise of the development. The final lot count and details of this plan shall be governed by the appropriate regulations at the time of final application. This Preliminary SP shall allow flexibility to layout and building typology (consistent with the proposed uses) for Final SP Development Plans. Planning staff shall be allowed to approve modifications to the layout, regulations, & lot count in this Preliminary SP Development Plan for Final SP Development Plans.

Overall Stephens Valley Concept Plan

Site Plan

The Plan is broken into 3 distinct areas within the Davidson County portion of the project. Area 1 is the town center area located at the intersection of Sneed Road/Union Bridge Road and Pasquo Road. Area 1 is approximately 23.83 acres in size. Area 1 proposes a mixture of uses including residential, office, and commercial uses. Area 1 is limited to up to 243 residential dwelling units. These units may be one-family, two-family, multi-family, or live/work units. Additionally,



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accessory dwelling units are allowed throughout the development. Area 1 allows up to 88,000 square feet of non-residential uses, with up to 45,000 square feet of the non-residential designated for retail uses. The non-residential uses are limited to buildings primarily along newly extended Pasquo Road, wrapping around the corner to the new portion of Union Bridge Road.

Non-residential uses are required to create a minimum contiguous area of no less than 15,000 square feet. Maximum height of buildings within Area 1 shall be 3 stories in 48'. All residential uses within Area 1 are to be rear loaded. Parking areas have been located behind buildings, to the extent possible. Architectural guidelines have been provided for commercial as well as residential buildings. Street trees are proposed along all streets.

Area 2 is approximately 8.96 acres in size and includes only residential uses. There are up to 45 residential units allowed and they may be one-family or two-family. The layout of Area 2 allows for cottage style units that front onto a shared open space along with more traditional single-family lots, the majority of which will be alley loaded. There is some allowance for front-loaded garages within this area, on a limited basis. An existing stream within Area will be protected and trees will remain in the area.

Area 3 is approximately 100.36 acres and will include up to 50 residential dwelling units. Units may be one-family, two-family or townhomes. A minimal amount of the land in Area 3 is being disturbed for development area. This allows for the existing trees to remain and allows for a more sensitive development that creates less impact on the surrounding area.

Sidewalks are being provided throughout the development and will be provided along existing roadways as well. A trail connection is being provided between Area 2 and Area 3. Some realignment of existing roadways is proposed with the development and a section of Union Bridge Road and Pasquo Road will be closed and rerouted. The current plan shows a street connection to the existing Timberline Road, which is within Williamson County. Staff feels that this connection is very important not only from the standpoint of providing emergency services but also from a neighborhood connectivity standpoint. This will allow opportunities for travel between existing neighborhoods and the proposed neighborhood for future residents. It will provide an option for existing residents to access the proposed town center. Timberline Road was built as a stub street and intended for a future connection which can now be completed with the development of Stephens Valley.

The project site is surrounded by the Natchez Trace Parkway and parkland. For the most part, the project is screened from view of the Trace by existing wooded areas. There is one area of the development that is not currently screened. However, the applicant has been working with the National Park Service on a reforestation plan for this area that will provide screening of the development in a way that is complimentary to similar existing portions of the Trace. Native trees and shrubs will be planted in a way that is not seen as a planned, formal buffer but will act more as a natural transition and buffer to the development.

ANALYSIS

The plan is consistent with the existing T2 Rural Neighborhood Maintenance and T3 Suburban Neighborhood Evolving policies along with the proposed T3 Suburban Neighborhood Center policy, which staff is also recommending for approval. The development is being proposed in a



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way that is sensitive to the environment of the area and is being particularly careful in regards to views from the Natchez Trace Parkway. The town center provides for non-residential uses within close proximity to existing and future residents allowing for some non-automobile trips to occur. Based on the design of the plan, staff recommends approval with conditions and disapproval with conditions subject to the approval of the associated plan amendment.

FIRE DEPARTMENT RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review. Grades not to exceed 10%

STORMWATER RECOMMENDATION

Returned for corrections

- Show Undisturbed Buffers, a hydrologic determination showing the conveyances as a wet weather conveyance, or provide variance. See Metro Maps for all stream locations.
- Add Buffer Note to plans:
- (The buffer along waterways will be an area where the surface is left in a natural state, and is not disturbed by construction activity. This is in accordance with the Stormwater Management Manual Volume 1 - Regulations.)
- Add Access Note to plans:
- (Metro Water Services shall be provided sufficient and unencumbered access in order to maintain and repair utilities in this site.)
- Provide a Water Quality Concept and room for detention for Area 3. Also, provide documentation from Williamson County stating that water quality will be managed within their jurisdiction.

HARPETH VALLEY UTILITY DISTRICT

Approved as a preliminary SP only

- Prior to the approval of the Final SP, the design engineer must submit construction plans along with a review fee for review and approval.

PUBLIC WORKS RECOMMENDATION

Conditions if approved

This development will require Public Works approval of detailed construction plans prior to grading the site that comply with the design regulations established by the Department of Public Works.

Final design and improvements may vary based on actual field conditions

- The proposed roundabout shall comply with AASHTO and FHWA design criteria and have an inscribed circle diameter of no less than 90 feet.
- Minimum lane widths shall be 11 feet for all streets.
- Install a center median on Timber Gap Drive from Union Bridge Road to 100' north.
- All new streets shall use the ST-200 curb and gutter, except the rural park road that may be eligible for a non-curb section if a sidewalk is not installed.
- On the rural park road, install a sidewalk on one side or a multi- use path.
- A mandatory referral application and approval will be required for any existing street relocations.



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- The street names shown are not approved at this time and will require coordination with the Public Works Department.
- Comply with all conditions of the Public Works Traffic Engineer.
- Any right-of-way plantings must be coordinated with the Public Works Horticulturist.

TRAFFIC AND PARKING RECOMMENDATION

Approve with conditions

In accordance with the TIS and addendum findings the developer shall install the following roadway improvements.

Highway 100 at McCrory Lane

- The developer shall construct westbound right turn lane on Highway 100 at McCrory Lane with adequate transition. The combined length of the storage and taper length for this turn lane may be limited to approximately 200 feet due to existing driveways and right-of-way constraints on the north side of Highway 100. The timing of the right turn lane installation should coincide with construction of the Stephens Valley SP (Stephens Valley Phase 3).

Highway 100 at Pasquo Road

- The developer shall construct an eastbound right turn lane on Highway 100 at Pasquo Rd with adequate transition. The combined length of the storage and taper length for this turn lane may be limited to approximately 250 feet due to the presence of a cemetery, existing driveway, and underground fiber optic utility lines on the south side of Highway 100. The timing of the right turn lane installation should coincide with construction of the Stephens Valley SP (Stephens Valley Phase 3).

Highway 100 at Temple Road

- Developer shall construct additional roadway improvements at this location. Additional traffic analysis shall be conducted prior to the final SP plan to determine the required improvements. The timing of the intersection improvement should occur with Stephens Valley Phase 3.

Williamson Co. Intersections - Sneed Road at Timberline, Sneed Road Temple Rd, Hillsboro Road at Sneed Road

- MPW supports the recommended improvements at the Williamson county intersections as identified in the TIS.

Pasquo Road at Stephens Valley Access E

- The approach of Stephens Valley Access E to Pasquo Road should have a minimum width of 24 feet. Stephens Valley Access E will be a private drive, therefore, a commercial driveway ramp per Metro Nashville - Davidson County Public Works standard drawing ST-324 should be included in the design for Stephens Valley Access E.
- A southbound left turn lane on Pasquo Road at Stephens Valley Access E with adequate storage and transition should be included in the final design of Pasquo Road improvements.

Pasquo Road at Stephens Valley Access G

- The approach of Stephens Valley Access G should include two (2) westbound lanes (one right turn lane, one left turn lane) and one (1) eastbound lane for traffic entering Stephens Valley. Additional ROW for a WB thru lane may be required when the property opposite Stevens Valley is developed. A Southbound lane on Pasquo Rd at nearby access driveways and access G may be warranted at final SP. Additional ROW shall be reserved.



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Pasquo Road at Union Bridge Road/Stephens Valley Access F

- The intersection of Pasquo Road and Union Bridge Road will be moved approximately 300 feet to the south to allow mitigation of the 90 degree curve with the construction of a roundabout intersection.
- The design for this roundabout and the roadway approaches to it should include the methods and practices shown in the *National Cooperative Highway Research Program (NCHRP) Report 672 -Roundabouts: An Informational Guide*. Additional design guidelines and criteria from AASHTO and the MUTCD should be utilized where applicable.
- All access drives on the approaches to the roundabout shall be located at an adequate distance from the roundabout to prevent traffic queue into the roundabout. These approaches may also require left turn lanes with a 3 lane x-section for the relocated Union Bridge Rd.

Union Bridge Road at Stephens Valley Access C & D

- The approach of Stephens Valley Access C & D to Union Bridge Road should have a minimum width of 24 feet. Stephens Valley Access C & D will be private drives, therefore, a commercial driveway ramp per Metro Nashville - Davidson County Public Works standard drawing ST-324 should be included in the design for Stephens Valley Access C & D.
- A westbound left turn lane on Union Bridge Road at Stephens Valley Access C should be included in the final design of Union Bridge Road improvements. Access C and D shall be located to prevent traffic queuing into roundabout.

Union Bridge Road at Stephens Valley Access B

- The approach of Stephens Valley Access B should include two (2) northbound lanes (one right turn lane, one left turn lane) and one (1) southbound lane for traffic entering Stephens Valley.
- A left turn lane should be constructed on Sneed Road at Stephens Valley Access B. Due to the proximity with Stephens Valley Access A, the design of this left turn lane should consist of a three lane section with a two-way continuous left turn lane on Union Bridge Road between Stephens Valley Access A and Stephens Valley Access B.
- At final SP a right turn lane at access B may be required; therefore additional ROW shall be reserved.

Union Bridge Road at Timber Gap Drive

- The construction of Stephens Valley Access B will result in a two-way left turn lane being added to Union Bridge Road at this intersection.
- There has been discussion of improvements to Timber Gap Drive including a short median to improve the perception that Timber Gap Drive is a low speed, low volume residential street. This improvement and any other traffic calming measures discussed or proposed within the Traceside subdivision will require the approval of Metro Nashville - Davidson County Public Works staff and may require coordination through the Metro Nashville - Davidson County Neighborhood Traffic Management Program.

Union Bridge Road at Stephens Valley Access A

- Stephens Valley Access A will provide access to approximately 200 single family lots before any other access to Stephens Valley is available.
- Analysis of this access in the horizon year 2021 indicates that 207 single family units can be accessed at this location with level of service B or better on all movements. Additional analysis of the available capacity at this intersection indicates that over 500 single family lots can be accessed at this location with level of service D or better on all movements in the horizon year 2021 conditions. Even though the maximum capacity of the minor street at this location can serve over 500 single family homes, it is reasonable to expect that a second route of access will be perceived as a need by residents prior to reaching that maximum capacity.



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- Developer shall submit signal warrant analysis and signal plans and install traffic signal with pedestrian infrastructure when approved by Metro traffic engineer.
- The approach of Stephens Valley Access A should include two (2) northbound lanes (one right turn lane, one left turn lane) and one (1) southbound lane for traffic entering Stephens Valley.
- A left turn lane should be constructed on Sneed Road at Stephens Valley Access A. The storage length of this turn lane should be 225 feet with a bay taper length of 110 feet. Approach and departure tapers should be designed per TDOT design guidelines and MUTCD lane transition.
- A second route of access to Stephens Valley should be required when more than 350 single family units have received final approval. While this threshold is based on approximately 60% of the maximum capacity of Stephens Valley Access A, it is likely that additional routes of access will already be in place prior to Stephens Valley reaching the threshold of 350 single family units.

Stephens Valley Roadway Infrastructure

- The Stephens Valley Preliminary SP submittal includes a roadway infrastructure plan that identifies the right-of-way width and other cross-sectional elements for roadways within the limits of the Preliminary SP.
- The ROW of the through route between Hwy 100 and Williamson County line may require 3 lane cross sections at driveways and road intersections with adequate transitions. Appropriate ROW shall be reserved.
- Focused TIS may be required as the commercial mixed use area 1 develops in order to identify any additional roadway improvements, signage and traffic control. Access B and access F may require additional ROW in order to construct left turn lanes and appropriate ROW shall be reserved.
- Developer shall provide adequate sight distance at all road and driveway intersections.
- Developer shall bond off- site improvements in Davidson County and area 1 of Specific plan road network with 1st Final SP Plat.
- Prior to final site plan for commercial or residential construction in area 1 of the specific plan, construction documents for the Roundabout and Union Bridge relocation shall be approved for area 1 roadway network. Union Bridge Rd shall remain a through route until a focused TIS determines when through traffic can be re-routed through proposed roundabout.

Maximum Uses in Existing Zoning District: **AR2a**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------------------|--------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Single-Family Residential (210) | 109.32 | 0.5 D | 54 U | 590 | 48 | 62 |

Maximum Uses in Existing Zoning District: **RS40**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Single-Family Residential (210) | 23.83 | 1.08 D | 25 U | 240 | 19 | 26 |



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Maximum Uses in Proposed Zoning District: **SP-MU**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------------------|--------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Single-Family Residential (210) | 133.15 | - | 95 U | 992 | 77 | 103 |

Maximum Uses in Proposed Zoning District: **SP-MU**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|--------------------------------|--------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Multi-Family Residential (230) | 133.15 | - | 243 U | 1393 | 106 | 125 |

Maximum Uses in Proposed Zoning District: **SP-MU**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|--------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Retail (820) | 133.15 | - | 88,000 SF | 6250 | 143 | 584 |

Traffic changes between maximum: **AR2a and RS40** and **SP-MU**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | | +7,805 | +259 | +724 |

METRO SCHOOL BOARD REPORT

Projected student generation existing AR2a & RS40 district: 29 Elementary 15 Middle 13 High
Projected student generation proposed SP-MU district: 28 Elementary 14 Middle 11 High

The proposed SP-MU zoning district could generate 24 fewer students than would be generated under the existing AR2a and RS40 zoning districts. Students would attend Harpeth Valley Elementary School, Bellevue Middle School and Hillwood High School.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions if a recommendation of approval is received from all reviewing agencies and subject to the approval of the associated policy change. If the associated policy change is not approved, staff recommends disapproval.

CONDITIONS

1. Permitted land uses shall be limited as follows:

- Area 1 – limited to up to 243 multi-family residential dwelling units and other uses as indicated on the Preliminary SP
- Area 2 – limited to up to 45 residential units, single-family and two-family only. Single-family units shall also be permitted to have 1 accessory dwelling unit.
- Area 3 – limited to up to 50 residential units, single-family, two-family and townhome only. Single-family units shall also be permitted to have 1 accessory dwelling unit.



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2. On the corrected set of plans, add a standard that a raised foundation of a minimum of 18” and a maximum of 36” is required for all residential units within the development.
3. On the corrected set of plans, correct the reference to the roadway notes on page 21.
4. On the corrected set of plans, revise the street section for the improved portion of Union Bridge Road to show sidewalks and planting strip as per the Major and Collector Street plan (4 foot planting; 8 foot sidewalk).
5. The Final Plat for Area 2 shall not be recorded until such time that the adjoining Williamson County streets are recorded.
6. A connection should be provided to the existing stub street of Timberline Drive within Williamson County. The connection should tie into the overall Stephens Valley development.
7. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the MUN-A zoning district (Area 1); R10 zoning district (Area 2); and RS40 zoning district (Area 3) as of the date of the applicable request or application. Uses are limited as described in the Council approved plan.
8. Add the following note to plan: Ownership for units may be divided by a Horizontal Property Regime or a subdivision with a minimum lot size as shown on preliminary SP plan.
9. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
10. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
11. The requirements of the Metro Fire Marshal’s Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



RECOMMENDATIONS TO METRO COUNCIL

- **Zoning Text Amendments**
- **Specific Plans**
- **Zone Changes**
- **Urban Design Overlays**
- **Planned Unit Developments**
- **Institutional Overlays**



NO SKETCH



| | |
|-----------------------------|---|
| Project No. | Text Amendment 2015Z-003TX-001 |
| Project Name | DTC Housekeeping Text Amendment |
| Council Bill | BL2015-1053 |
| Council District | 19 – Gilmore |
| School District | 5 – Kim |
| Requested by | Councilmember Erica Gilmore and Metro Planning Department |
| | |
| Staff Reviewer | Collins |
| Staff Recommendation | <i>Approve.</i> |

APPLICANT REQUEST

Amend the Zoning Code to update review processes, update maps, revise typographic errors, and refine the urban design standards of the Downtown Code.

TEXT AMENDMENT

A request to amend Chapters 17.37 and 17.40 of the Metropolitan Code to update the Downtown Code (DTC) standards.

ANALYSIS

This text amendment updates and refines the Downtown Code (DTC) design standards and review processes, to continue to encourage high quality development downtown. The amendments correct discrepancies and errors in the text and graphics; updates maps, as well as the review process for final site plan review, variances and modifications to DTC standards.

The general standards section of the DTC is also updated in order to continue providing opportunities for business to thrive, while enhancing the character of Downtown through thoughtful development, with care given to the public realm. For instance, the step-back provisions will be updated to allow for greater facade variation by-right, giving flexibility to project designers; the building step-back will be allowed to occur between a range of stories (so long as the minimum step-back depth is achieved by the upper story bound of the range), rather than the current mandate of requiring a building to fully step-back 15’ at a single story. Another update will require an active ground floor use on tertiary streets, if the tertiary street is a project’s only frontage. This will aid in further activating the public streetscape.

Direct references to the Major and Collector Street Plan are also added, strengthening the sidewalk corridors and streetscape language. Additional guidance is also provided on requiring parking structures to have architectural cladding or facade treatment to eliminate exposed parking decks downtown, if visible. Guidance is also provided on vehicular / pedestrian conflicts (such as loading docks and valet pull-offs), so that the pedestrian realm is adequately maintained at those points.

STAFF RECOMMENDATION

Staff recommends approval.

Please see Exhibit A at the end of this report for proposed edits to the DTC.



Metro Planning Commission Meeting of 03/26/2015

Ordinance No. BL2015-1053

An Ordinance amending Chapters 17.12, 17.37 and 17.40 of the Metropolitan Code to update the Downtown Code (DTC) standards. (Proposal No. 2015Z-003TX-001).

WHEREAS, The Council of The Metropolitan Government of Nashville and Davidson County adopted the Downtown Code (DTC) on February 2, 2010; and

WHEREAS, The Metropolitan Government recognizes the importance of updating and refining the Downtown Code design standards and process to encourage high quality, sustainable urban development; and

WHEREAS, The Downtown Code strives to encourage active and interactive streetscapes; and

WHEREAS the DTC standards provide opportunities for businesses to thrive, while enhancing the character of Downtown through mixed-use development, housing choices, and commercial and entertainment opportunities; and

WHEREAS, the refinement of the DTC standards will continue to ensure a quality urban experience for the citizens and businesses of Metropolitan Nashville and Davidson County.

NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:

Section 1. That Section 17.12.060.F of the Metropolitan Code is hereby amended by inserting the following after the words “Urban Zoning Overlay District”:

“, but not including the DTC district:”

Section 2. That Chapter 17.37 of the Metropolitan Code is hereby amended by replacing “Exhibit A” in its entirety with the attached Exhibit; which updates all maps with the most recently available property lines; revises any typographic and mapping errors; refines the modifications, variances and special exception process; updates the sidewalk requirements by referencing the Major and Collector Street Plan; and updates the General Standards by providing additional guidance on vehicular / pedestrian conflicts, parking structure facades, and street frontages.

Section 3. That Section 17.40.170.B of the Metropolitan Code is hereby amended by deleting the words:

“DTC district,”

Section 4. That Section 17.40.170 of the Metropolitan Code is hereby amended by renumbering “C” as “D and inserting a new “C” as follows:



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C. Final Approval by the Planning Department within the DTC district. Prior to the issuance of a building permit, the Planning Department shall review site plans in the DTC district to verify conformance with the provisions of Chapter 17.37 of this title. A site plan shall be submitted in form and content established by the Planning Department, and shall specifically describe the nature and scope of development to serve as the basis for determination of compliance with the DTC district standards.

Section 5. That this Ordinance shall take effect five (5) days from and after its passage and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.

Sponsored by: Erica Gilmore



NO SKETCH



| | |
|-----------------------------|--|
| Project No. | Zone Change 2015Z-004TX-001 |
| Project Name | Commercial Usage (Inside and Outside) |
| Council Bill | BL2015-1055 |
| Council District | Countywide |
| School District | Countywide |
| Requested by | Councilmember Scott Davis |
| | |
| Staff Reviewer | Sajid |
| Staff Recommendation | <i>Disapprove.</i> |

APPLICANT REQUEST

Modify Zoning Code to permit commercial amusement (inside and outside) as land uses permitted with conditions in the IG zoning district.

TEXT AMENDMENT

A request to amend Chapters 17.08 and 17.16 of the Metropolitan Code, Zoning Regulations, to designate commercial amusement as a use permitted with conditions and adding conditions applicable to such land uses.

CRITICAL PLANNING GOALS

N/A

EXISTING ZONING CODE

The Zoning Code defines commercial amusement (inside) as “the provision of entertainment or games of skill to the general public for a fee and that is wholly enclosed in a building, including but not limited to a bowling alley or billiard parlor. This use does not include an arena.” Currently “commercial amusement (inside)” is permitted by right in all mixed use, commercial, DTC, and shopping center districts. The use is also permitted by right in ORI, ORI-A, IWD and IR districts.

The Zoning Code defines commercial amusement (outside) as “the provision of entertainment or games of skill to the general public for a fee where any portion of the activity takes place outside of a building, including but not limited to a golf driving range, archery range or miniature golf course. This use does not include a stadium.” This use is permitted by right in all DTC and shopping center districts in addition to MUL, MUL-A, MUG, MUG-A, MUI, MUI-A, CL, CS, CA and CF districts. The use is also permitted by special exception in IWD and IR districts.

The special exception conditions for commercial amusement (outside) in IWD and IR are as follows:

1. Buildings. Any new structure constructed on the property shall be no greater in size than one thousand, five hundred square feet.
2. Setback. Where any building or outdoor storage area, excluding passenger car parking lots, abuts a residential zone district or district permitting residential use, there shall be a minimum setback of one hundred feet from the property line.
3. Landscape Buffer. Along all residential zone districts and districts permitting residential use, screening in the form of landscape buffer yard standard A shall be applied along common property lines.



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4. Lighting. All light and glare shall be directed on-site to ensure surrounding properties are not adversely impacted by increases in direct or indirect ambient lighting levels.
5. Street Standard. At a minimum, driveway access shall be from a collector street.
6. The board of zoning appeals may stipulate, based on the zoning pattern and nature of the land uses in the immediate area, whether a maximum of sixty or seventy decibels noise level on the A-weighted scale shall be permitted to occur at the site boundary.

PROPOSED ZONING CODE

The proposed text amendment would permit commercial amusement (inside and outside) with conditions in the IG zoning district and includes conditions that would be applicable.

The proposed conditions are as follows:

1. Location. The facility shall be limited to locate only in the industrial area bounded by the Cumberland River, by interstate I-65 and by interstate I-24.
2. Lighting. All light and glare shall be directed on-site to ensure surrounding properties are not adversely impacted by increases in direct or indirect ambient lighting levels.
3. Fencing. Adequate fencing shall be provided to ensure golf balls or other airborne sports equipment do not fly into abutting or adjacent properties and streets.
4. Retail/Restaurant. The facility may have retail, restaurant, and similar commercial uses as accessory uses so long as such uses are operated by the same business establishment operating the principal use.
5. Traffic Management Study. The traffic engineer may require a traffic management study to determine what, if any, improvements may be necessary to accommodate projected traffic to and from the facility.

ANALYSIS

The proposed amendment was introduced as an alternative to a recently filed rezoning request (2015Z-014PR-001) to rezone properties on Cowan Street from IG to MUL and MUG-A for Top Golf. Staff has recommended approval of this rezoning request as both MUL and MUG-A are consistent with the Urban Mixed Use Neighborhood policy, whereas, the existing IG zoning is not consistent with the policy.

Staff finds that the requested text amendment is inappropriate as it is tailored to permit a specific use on a specific property with conditions that are tailored to a particular business and location. The text amendment is the wrong process to permit a use on a specific property. There are other tools that are more appropriate to permit specific uses on a given property. The property could be rezoned to a district that permits the desired use or to an SP. Both of these options would be preferable from a policy standpoint since they would involve an analysis of the land use policy for the given site. Furthermore, a more appropriate process is currently underway and received a recommendation of approval from the Metro Planning Commission on March 12, 2015. Furthermore, allowing commercial amusement uses in the IG zoning district is not appropriate since it would introduce the possibility of having a high volume of people in a zoning district that is intended for industrial uses where it could be unsafe for patrons of the commercial establishment.

STAFF RECOMMENDATION

Staff recommends disapproval.



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ORDINANCE NO. BL2015-1055

An Ordinance amending Chapters 17.08 and 17.16 of the Metropolitan Code, Zoning Regulations, to designate commercial amusement as a use permitted with conditions and adding conditions applicable to such land uses (Proposal No. 2015Z-004TX-001).

WHEREAS, commercial amusement as recreation and entertainment uses are currently permitted in two of the three industrial zoning districts;

WHEREAS, commercial amusement is a less intensive use of flood prone areas zoned for heavy industrial use; and

WHEREAS, it is fitting and appropriate to allow commercial amusement uses to occupy a limited area of industrial zoned property in close proximity to Nashville's entertainment district.

NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:

Section 1. That Section 17.08.030, District Land Use Tables, is hereby amended by designating "commercial amusement (inside)" and "commercial amusement (outside)" as land uses permitted with conditions (PC) in the Industrial General (IG) zoning district.

Section 2. That Section 17.16.120 is hereby amended by establishing a new subsection to read as follows:

D. Commercial Amusement (inside and outside)

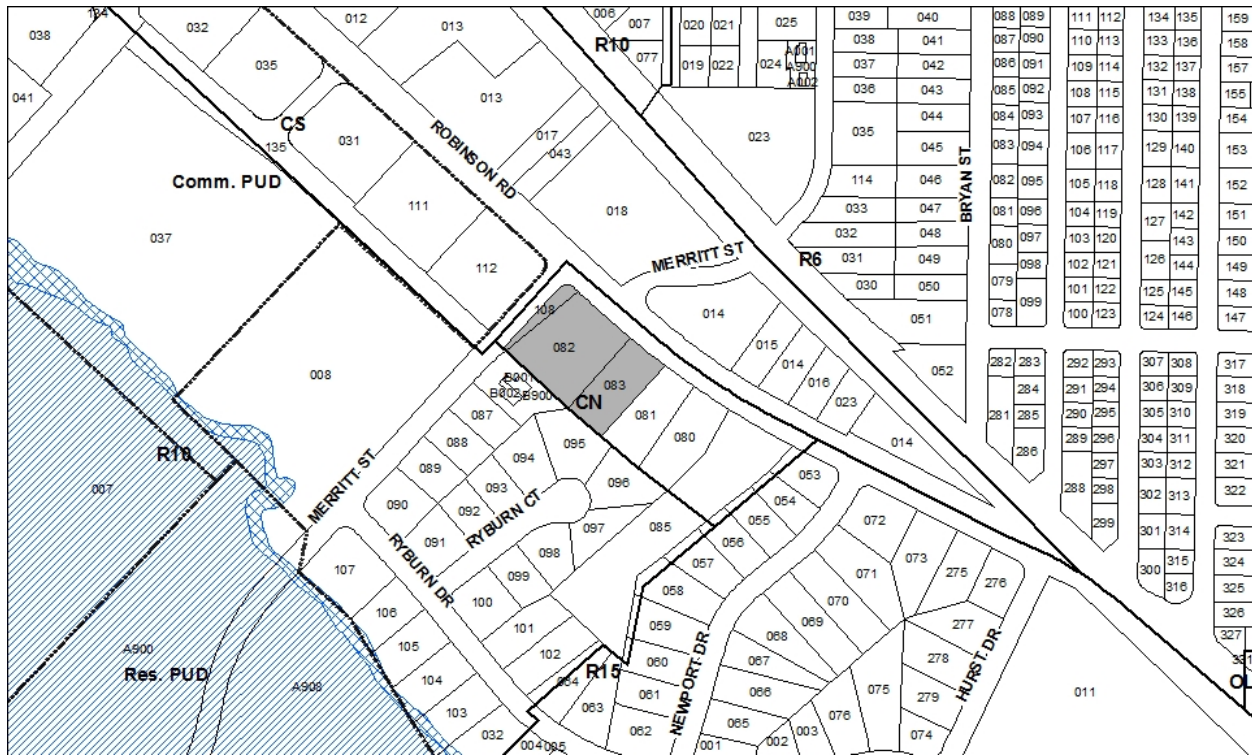
1. Location. The facility shall be limited to locate only in the industrial area bounded by the Cumberland River, by interstate I-65 and by interstate I-24.
2. Lighting. All light and glare shall be directed on-site to ensure surrounding properties are not adversely impacted by increases in direct or indirect ambient lighting levels.
3. Fencing. Adequate fencing shall be provided to ensure golf balls or other airborne sports equipment do not fly into abutting or adjacent properties and streets.
4. Retail/Restaurant. The facility may have retail, restaurant, and similar commercial uses as accessory uses so long as such uses are operated by the same business establishment operating the principal use.
5. Traffic Management Study. The traffic engineer may require a traffic management study to determine what, if any, improvements may be necessary to accommodate projected traffic to and from the facility.

Section 3. Be it further enacted that this Ordinance shall take effect immediately after its passage and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.

Sponsored by: Scott Davis



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2015SP-036-001

MAPCO EXPRESS, INC.

Map 053-02, Parcel(s) 082-083, 108

14, Donelson - Hermitage

11 (Larry Hagar)



| | |
|-----------------------------|---|
| Project No. | 2015SP-036-001 |
| Project Name | Mapco Express, Inc. |
| Council District | 11 – Hagar |
| School District | 4 – Shepherd |
| Requested by | Fulmer Engineering, LLC, applicant; Freda Cox, Thomas & Alice Williams, and John Dean Norris, owners. |
| Staff Reviewer | Sajid |
| Staff Recommendation | <i>Approve with conditions and disapprove without all conditions.</i> |

APPLICANT REQUEST

Preliminary SP to permit a convenience store with fuel sales.

Preliminary SP

A request to rezone from Commercial Neighborhood (CN) to Specific Plan-Commercial (SP-C) zoning for property located at 1500 and 1504 Robinson Road and Merritt Street (unnumbered), at the southeast corner of Merritt Street and Robinson Road (1.23 acres), to permit a 4,522 square foot automobile convenience.

Existing Zoning

Commercial Neighborhood (CN) is intended for very low intensity retail, office, and consumer service uses which provide for the recurring shopping needs of nearby residential areas.

Proposed Zoning

Specific Plan-Commercial (SP-C) is a zoning District category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes commercial uses.

CRITICAL PLANNING GOALS

- Supports Infill Development

The proposed SP creates an opportunity for infill development in an area that is served by existing infrastructure. Locating development in areas served by existing, adequate infrastructure does not burden Metro with the cost of upgrading or building new infrastructure.

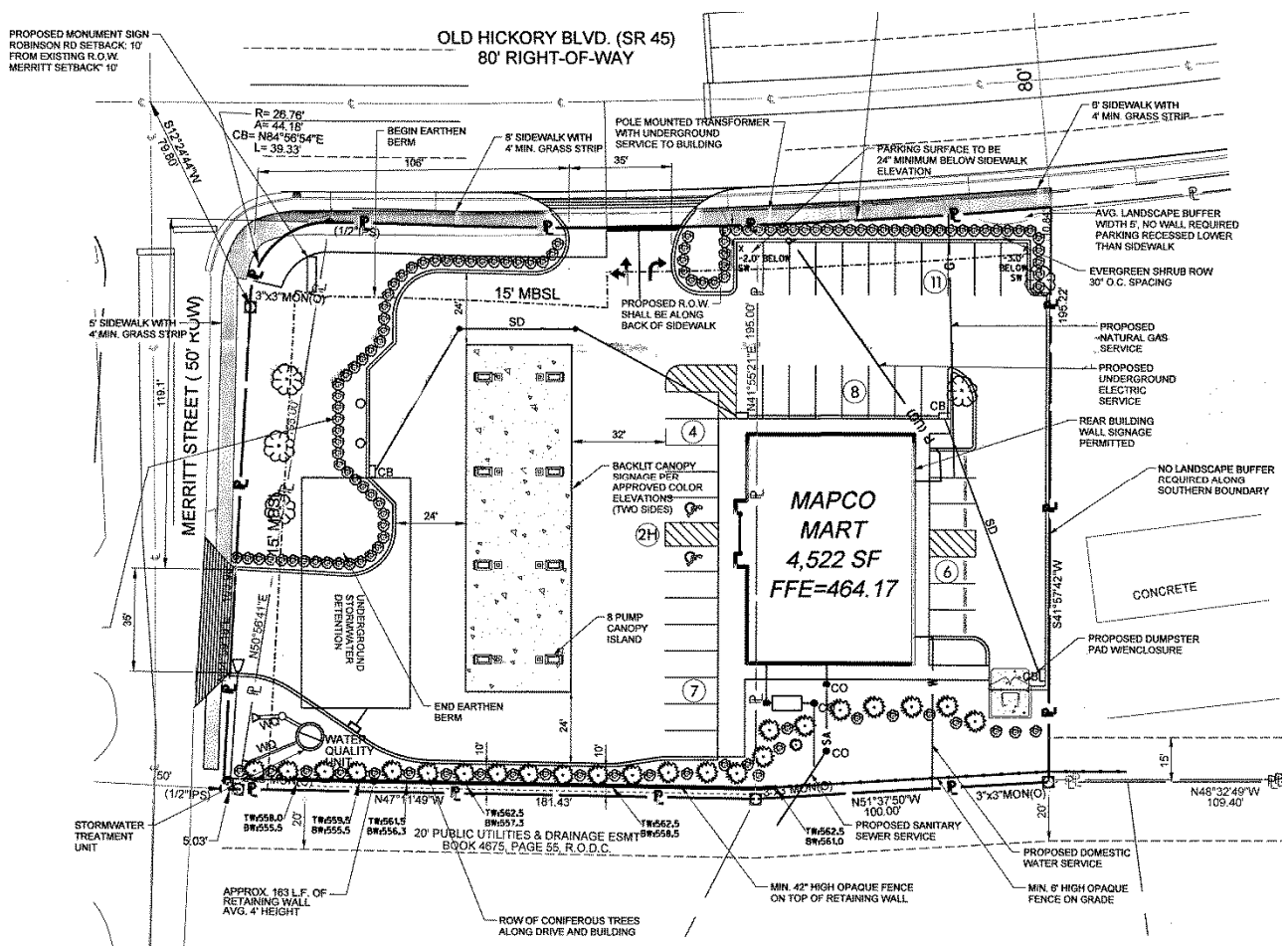
DONELSON-HERMITAGE COMMUNITY PLAN

Existing Policy

Urban Community Center (T4 CC) policy is intended to preserve, enhance, and create urban community centers encouraging their development and redevelopment as intense mixed use areas that are compatible with the general character of urban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T4 Urban Community Centers are pedestrian friendly areas, generally located at intersections of prominent urban streets. T4 Urban Community Centers serve urban communities within a 5 minute drive or a 5 to 10 minute walk.



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Proposed Site Plan



Metro Planning Commission Meeting of 03/26/2015

DRAFT Preferred Future Policy

No change proposed.

Consistent with Policy?

Yes, the SP is consistent with the Urban Community Center policy. The Urban Community Center policy is intended to create intense mixed use areas that serve nearby urban communities. The SP proposes a commercial use that will provide a service to the neighborhood. The site is also in close proximity to other properties zoned CS which permit the proposed use. In addition, the SP proposes to provide sidewalks in an area where they are currently not present.

PLAN DETAILS

The site is located at the southeast corner of the intersection of Robinson Road and Merritt Street. Surrounding zoning includes CN, CS and R10, and the area is characterized by a mixture of residential, commercial and office uses. Access is proposed from one driveway on Robinson Road and one driveway on Merritt Street.

Site Plan

The SP proposes a 4,522 SF automobile convenience store with fuel sales that includes 8 gas pumps. The plan includes a rectangular building that is oriented to Merritt Street which is necessary to provide security at the gas pumps. The side façade of the building oriented toward Robinson Road incorporates additional glazing in order to address the street.

Parking is dispersed throughout the site, and the dumpster is shown at the rear of the building. A 9' landscape buffer yard that includes densely planted evergreen trees and shrubs and an opaque is provided to screen the site from the existing residential to the southwest. Also, the façade of the gas canopy facing the adjacent residential property shall not be illuminated. The plan incorporates an earthen berm along the Merritt Street frontage and the Robinson Road frontage west of the driveway to that public street. Staff recommends that a knee wall constructed of brick or stone be located along the Robinson Road frontage west of the driveway to the public street. The frontage east of the driveway onto Robinson Road includes a grade change so that the parking is recessed lower than the sidewalk.

Sidewalks are required along both the Robinson Road and Merritt Street frontages. Since Merritt Street is a local road, the plan incorporates a 5' sidewalk and 4' planting strip. Part of Robinson Road along the site is constrained and part is not. Along the constrained part of the street, the final plan includes a 6' sidewalk and 4' planting strip while an 8' sidewalk and 4' planting strip along are shown on the non-constrained part of the Robinson Road frontage. Signage will be limited to a monument sign with a maximum height of 8' and a maximum area of 68 SF.

ANALYSIS

The proposed SP is consistent with Urban Community Center land use policy and meets a critical planning goal. Therefore, staff recommends approval with conditions and disapproval without all conditions.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review.



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STORMWATER RECOMMENDATION

Approve

TRAFFIC & PARKING RECOMMENDATION

Conditional if approved

- Relocate Merritt St access drive near western property line opposite existing commercial drive.
- Public Works will allow a second driveway onto SR45/Robinson Rd. Applicant shall continue working with Public Works to determine the appropriate location.

WATER SERVICES RECOMMENDATION

Approve

- Approved as a Preliminary SP only. Applicant shall ensure proposed landscaping does not impact any existing public sewer and water mains.

PUBLIC WORKS RECOMMENDATION

Approve with conditions

1. The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
2. Sidewalks along public streets are to be located within ROW and are to meet the MCSP, may require ROW dedication.

Maximum Uses in Existing Zoning District: CN

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Retail (814) | 1.23 | 0.25 F | 13,394 SF | 611 | 18 | 54 |

Maximum Uses in Proposed Zoning District: SP

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Automobile Convenience (945) | 1.23 | - | 16 Pumps 4,500 SF | 2605 | 161 | 215 |

Traffic changes between maximum: CN and SP

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | - | +1,994 | +143 | +161 |

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.



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CONDITIONS

1. Uses within the SP shall be limited to automobile convenience.
2. Note 1 shall be updated to reflect the use shown on the plan: “The purpose of this SP is to permit automobile convenience.”
3. The brick on the gas island pillars shall extend to the canopy.
4. The ground sign shall incorporate elements of a monument sign and shall be limited to 8’ in height and 68 square feet in area.
5. The plan shall incorporate a minimum 2 foot tall knee wall constructed of brick or stone along the Robinson Road frontage west of the driveway to that public street.
6. The canopy and signage on the canopy shall not be illuminated on the façade facing the existing residential property located southwest of the site.
7. The SP shall comply with the landscaping requirements of the Zoning Code with the exception of the landscape buffer yard which shall be 9’ in width and shall consist of evergreen trees and shrubs and an opaque fence.
8. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the CS zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
9. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
10. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
11. The requirements of the Metro Fire Marshal’s Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2015SP-037-001
SOUTHGATE STATION
Map 105-11, Parcel(s) 019-020, 309-310
11, South Nashville
17 (Sandra Moore)



| | |
|-----------------------------|--|
| Project No. | 2015SP-037-001 |
| Project Name | Southgate Station |
| Council District | 17 - Moore |
| School District | 5 - Kim |
| Requested by | Civil Site Design Group, PLLC, applicant; William Smallman and Alpha One, LLC, owners. |
| Staff Reviewer | Milligan |
| Staff Recommendation | <i>Approve with conditions and disapprove with all conditions.</i> |

APPLICANT REQUEST

Preliminary SP to permit a multi-family residential development.

Preliminary SP

A request to rezone from One and Two-Family Residential (R6) to Specific Plan-Residential (SP-R) for properties located at 514, 518, and 520 Southgate Avenue and 1608 Pillow Street (1.67 acres), to permit a multi-family residential development with up to 41 units.

Existing Zoning

One and Two-Family Residential (R6) requires a minimum 6,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 7.71 dwelling units per acre including 25 percent duplex lots. *R6 would permit a maximum of 12 lots with 3 duplex lots for a total of 15 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

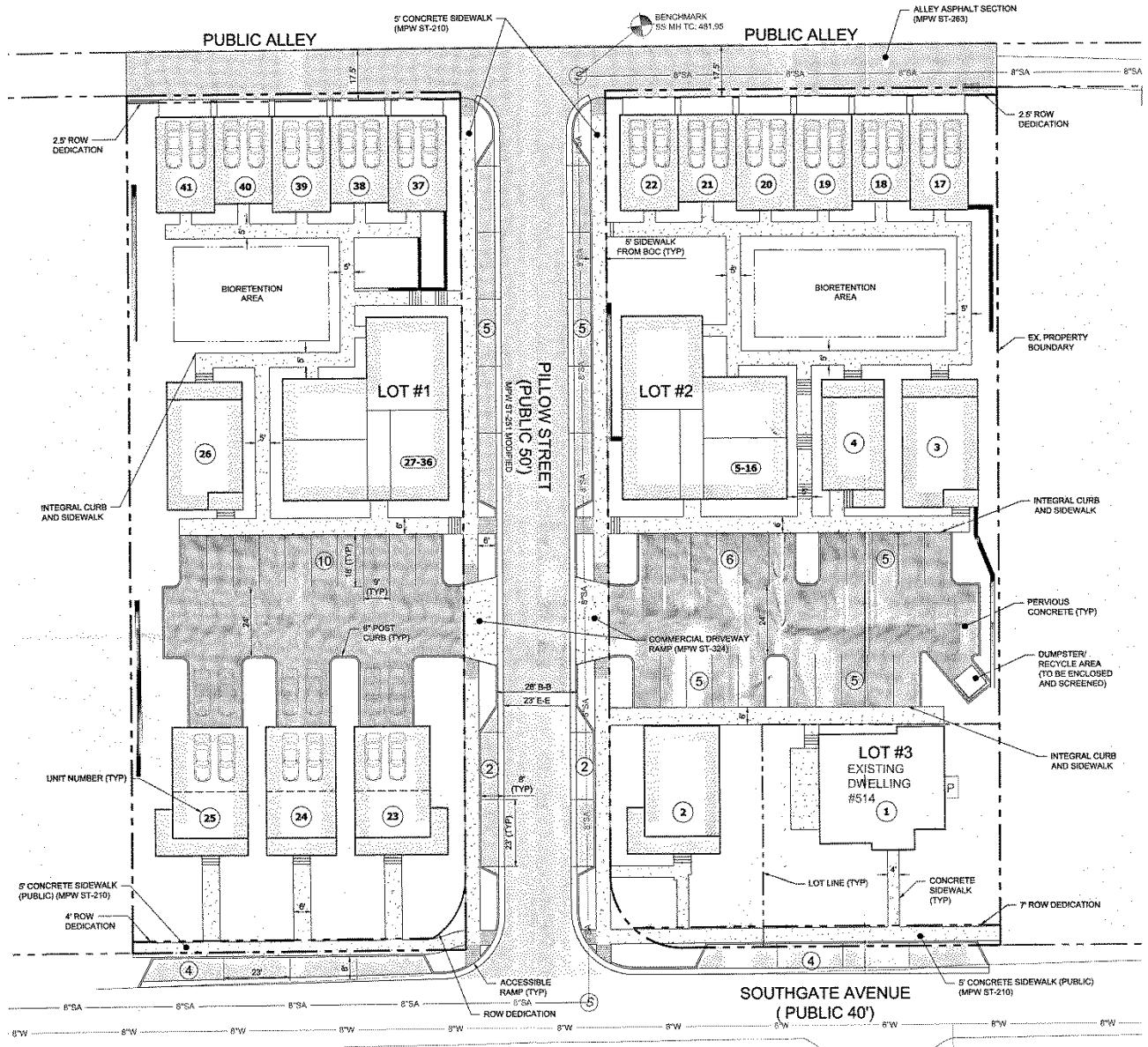
CRITICAL PLANNING GOALS

- Creates Walkable Neighborhoods
- Provides a Range of Housing Choices
- Supports Infill Development

This proposal meets several critical planning goals. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure such as roads, water and sewer, because it does not burden Metro with the cost of maintaining new infrastructure. The project will intensify development on an infill site and provide for a different housing type than currently exists in the immediate area. Sidewalks are being provided along Southgate Avenue and Pillow Street to create a more walkable neighborhood.



Metro Planning Commission Meeting of 03/26/2015



Proposed Site Plan



Metro Planning Commission Meeting of 03/26/2015

SOUTH NASHVILLE COMMUNITY PLAN

Current Policy

T4 Urban Neighborhood Maintenances (T4 NM) policy is intended to preserve the general character of urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use and the public realm. Where not present, enhancements are made to improve pedestrian, bicycle and vehicular connectivity.

DRAFT Preferred Future Policy

T4 Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

Consistent with Policy?

Yes. The proposed SP is consistent with the proposed T4 Neighborhood Evolving policy. The proposed development is intensifying an underutilized urban infill lot and providing three housing types within the development allowing for additional housing choice within the neighborhood. The development is also respectful of the existing land use patterns in the area by keeping the existing single-family home on Southate Avenue on the eastern edge of the development and providing only single-family detached units along Southgate Avenue. The proposed single-family detached units along Southgate Avenue will have similar setbacks to the existing homes along the street.

PLAN DETAILS

The site is located at 514, 518 and 520 Southgate Avenue and 1608 Pillow Street, at the northeast and northwest corners of Pillow Street and Southgate Avenue. The site is approximately 1.69 acres in size. The property is currently in use as 3 single-family detached units.

Site Plan

The plan proposes a multi-family residential development with up to 41 residential dwelling units. There are 3 residential dwelling types included in the proposed plan: single-family detached (8); single-family attached (11); and stacked flats (22).

Vehicular access will be from Pillow Street and from the existing public alley. Pillow Street runs through the middle of the development and provides access to both the west and to the east. The Pillow Street accesses are to surface parking lots that are located behind the single-family detached units fronting on Southgate Avenue. There is also access to the attached residential units from the existing public alley. Sidewalks are being provided along Southgate Avenue and along Pillow Street. Internal sidewalks are also provided throughout the development. The plan will be required meet the standards of the Bicycle Parking Ordinance and provide 10 bicycle parking spaces.



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The developer has proposed architectural guidelines for the project. All buildings must address the street frontage and green space with architectural treatments that may include windows, stoops and entrances, balconies, and porches. Street facing fronts of stacked flat buildings are proposed to have a minimum of 15% glazing. Raised foundations are proposed at a minimum of 18". Due to site topography, the developer is proposing that some foundations may be up to 5 feet in height. Staff recommends that raised foundations not exceed 4 feet, with limited exceptions. The maximum height of the units is 3 stories in 35 feet for the single-family detached units and 3 stories in 45 feet for the stacked flats and single-family attached units.

ANALYSIS

The plan is consistent with the draft preferred T4 Neighborhood Evolving policy by providing for additional housing options on an underutilized urban site while respecting the existing form of the neighborhood. By providing single-family detached units along Southgate Avenue and tucking the surface parking, stacked flats, and attached units behind, the development is able to provide housing choice while maintaining the pattern of the neighborhood.

FIRE DEPARTMENT RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review

STORMWATER RECOMMENDATION

Approved

WATER SERVICES

Approved

- Approved as a Preliminary SP only. The required public water and sewer construction plans must be submitted and approved prior to Final SP stage. The required 30% capacity fees must be paid prior to Final SP approval.

PUBLIC WORKS RECOMMENDATION

No exception taken

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.

TRAFFIC AND PARKING RECOMMENDATION

No exception taken

Maximum Uses in Existing Zoning District: R6

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Two-Family Residential (210) | 1.69 | 7.26 D | 15 U * | 144 | 12 | 16 |

*Based on three two-family lots.



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Maximum Uses in Proposed Zoning District: **SP-MR**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|--------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Multi-Family Residential (220) | 1.69 | - | 41 U | 373 | 24 | 41 |

Traffic changes between maximum: **R6** and **SP-MR**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | + 26 U | +229 | +12 | +25 |

METRO SCHOOL BOARD REPORT

Projected student generation existing R6 district: 1 Elementary 1 Middle 1 High

Projected student generation proposed SP-R district: 0 Elementary 0 Middle 0 High

The proposed SP-R zoning district could generate fewer students than could be generated under the current zoning.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions as the plan is intensifying an underutilized urban site and providing a mix of housing options in an area with existing transit service.

CONDITIONS

1. Permitted land uses shall be limited to up to 41 multi-family residential units.
2. On the corrected set, provide Bicycle Parking as per the adopted Bicycle Parking Ordinance (10 spaces; 2 of these must be publicly available)
3. Provide detailed landscaping plans with the submittal of the Final SP.
4. Provide architectural elevations for review and approval with the submittal of the Final SP.
5. Raised foundations shall be a minimum of 18” and shall not exceed 48”, except where additional foundation is needed due to topography. Foundations over 48” without pedestrian entrances shall include articulation and landscaping elements.
6. On the corrected set, revise the height note as follows: Single-family detached units limited to 3 stories in 35 feet. Stacked flat and single-family attached units limited to 3 stories in 45 feet.
7. On the corrected set, add the following standards:
 - a. All detached and attached units shall provide a minimum of 1 principal entrance and a minimum of 25% glazing along all facades fronting a street or open space.
 - b. On detached and attached units, windows shall be vertically oriented at a ratio of 2:1 or greater.
 - c. EIFS and vinyl siding shall be prohibited.
 - d. Stacked flat buildings must address Pillow Street through use of balconies, glazing, or other methods.
8. On the correct set, remove Note 8 from Architectural Notes.



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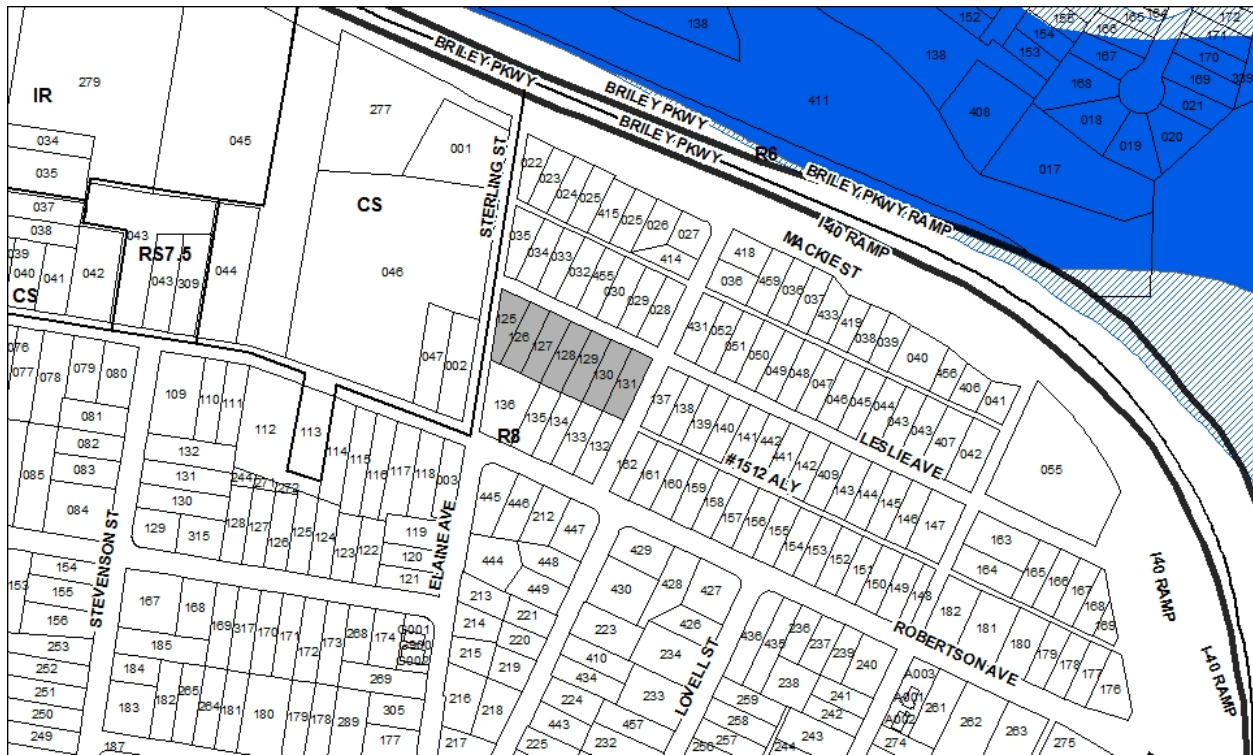
9. On the correct set, update Architectural Note 4 to apply only to the stacked flat buildings. Revise the note to apply to facades along street frontages as well as facing open space, courtyards and parking areas.
10. For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations and requirements of the RM20-A zoning district as of the date of the application request or application.
11. A corrected copy of the preliminary SP plan incorporating the conditions of approval by the Metro Council shall be provided to the Planning Commission prior to or with the final site plan application.
12. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering, or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
13. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



SEE NEXT PAGE



Metro Planning Commission Meeting of 03/26/2015



2015SP-038-001
ETHEL & LESLIE
Map 091-10, Parcel(s) 125-131
07, West Nashville
20 (Buddy Baker)



| | |
|-----------------------------|---|
| Project No. | Zone Change 2015SP-038-001 |
| Project Name | Ethel & Leslie |
| Council District | 20 - Baker |
| School District | 9 - Frogge |
| Requested by | Dale and Associates, applicant; Leslie Avenue, G.P., owner. |
| Staff Reviewer | Birkeland |
| Staff Recommendation | <i>Approve with conditions and disapprove without all conditions.</i> |

APPLICANT REQUEST

Zone change to permit 15 detached residential units.

Preliminary SP

A request to rezone from One and Two-Family Residential (R8) to Specific Plan-Residential (SP-R) zoning for property located at 5825 and 5827 Leslie Avenue and five parcels of Leslie Avenue (unnumbered), on the south side of Leslie Avenue between Sterling Street and Ethel Street, (1.26 acres), to permit up to 15 detached residential units.

Existing Zoning

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 5.79 dwelling units per acre including 25 percent duplex lots. *There are currently 7 lots, none of which are eligible for duplexes, for a total of 7 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes attached residential buildings.

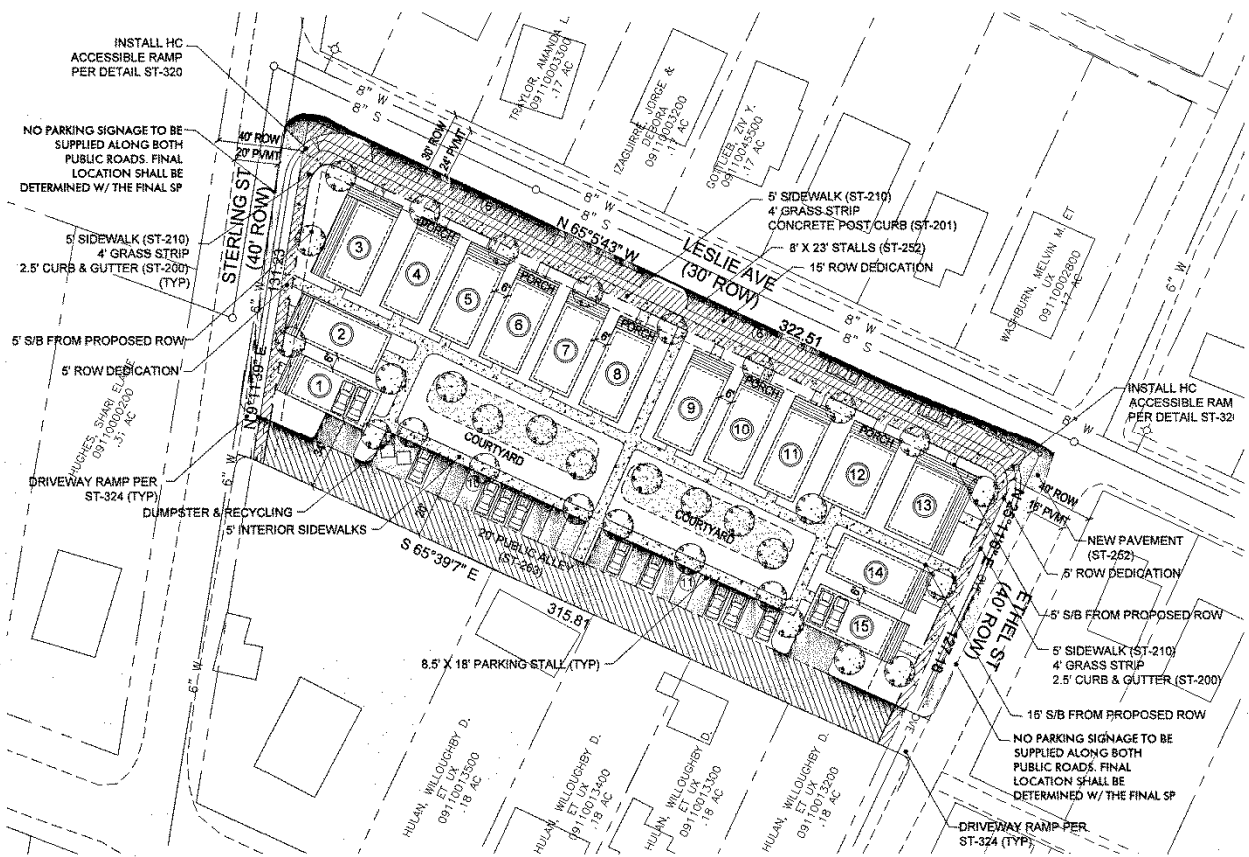
CRITICAL PLANNING GOALS

- Creates Walkable Neighborhoods
- Provides a Range of Housing Choices
- Supports infill development

The proposed SP creates an opportunity for infill housing in an area that is served by existing infrastructure. The request provides additional housing opportunities in the area. Housing options are important to serve a wide range of people with different housing needs. A new sidewalk along Ethel Street, Leslie Avenue and Sterling Street that will provide an improved pedestrian connection by providing pedestrians safe and access to other areas.



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Proposed Site Plan



Metro Planning Commission Meeting of 03/26/2015

WEST NASHVILLE COMMUNITY PLAN

Current Policy

T4 Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

Proposed Policy

No change proposed.

Consistent with Policy?

Yes. The policy supports the proposed detached residential units through form and interaction with the public realm. The SP will provide a mixture of housing types in a strategic location within West Nashville while providing improved pedestrian connectivity. The Neighborhood Evolving Policy also encourages improved alley access, which already exists along the rear of the property.

PLAN DETAILS

The site encompasses the entire block between Ethel and Sterling Streets, along Leslie Avenue in West Nashville. The site consists of seven existing parcels located at 5825 and 5827 Leslie Avenue and five parcels of Leslie Avenue (unnumbered). 5825 and 5827 Leslie Avenue have existing dwellings located on them; the other five parcels are vacant. The proposed SP includes 15 detached dwelling units; 11 units front Leslie Avenue, two units will front Ethel Street and two units will front Sterling Street.

The existing alley, between Sterling Street and Ethel Street, will be improved to public alley standards and will provide vehicular access from Ethel and Sterling Streets to the proposed dwelling units. Units 1 and 15 will have a two-car garage accessed from the alley. Additional surface parking for 21 vehicles has been provided on-site, off the alley. Right-of-way will be dedicated along Leslie Avenue that will allow for 12 additional parking stalls along the street. This SP has met and exceeded the Metro Zoning Code parking requirements. A courtyard will be established between the units and the on-site parking area to provide an active open space and landscaping. Additional landscaping will be placed throughout the SP.

Ethel, Leslie and Sterling Streets are identified as local streets in the Major and Collector Street Plan (MCSP). A new five foot sidewalk and a four foot planting street will be installed along each street; each residential unit will have a connection to these sidewalks. Additionally, interior sidewalks, five feet in width, shall provide a pedestrian connection to the courtyard and the on-site parking area.

Architectural standards been included on the plan and shall be provided with the final site plan. The standards include that buildings 3 and 13 shall have wraparound porches on each street side, as well as the standard façade requirements. The proposed residential units shall have a maximum height limitation of three stories within 35 feet, measured to roofline.



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ANALYSIS

The SP is consistent with the T4 Neighborhood Evolving policy and meets two critical planning goals. The 15 detached residential units will provide a well-designed development within West Nashville that needs additional housing.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review

STORMWATER RECOMMENDATION

Approved

WATER SERVICES RECOMMENDATION

Approved

- Approved as a Preliminary SP only. The required public sewer construction plans must be submitted and approved prior to Final SP stage. The required 30% capacity fees must be paid prior to Final SP approval.

TRAFFIC AND PARKING RECOMMENDATION

Approved with conditions

- Apply to Traffic and Parking to restrict on street parking along Ethel, Sterling, and Leslie street frontage.

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Prior to Final SP, revise the plans to indicate that all utility crossings within the ROW are at 90 degree angles with the centerline.
- Prior to building permit approval dedicate ROW to the back of the sidewalk along all public streets.

Maximum Uses in Existing Zoning District: **R8**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Two-Family Residential (210) | 1.26 | 5.44 D | 7 U * | 67 | 6 | 8 |

*Based on one two-family lot.



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Maximum Uses in Proposed Zoning District: **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|--------------------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| Multi-Family Residential (210) | 1.26 | - | 15 U | 144 | 12 | 16 |

Traffic changes between maximum: **R8** and **SP-R**

| Land Use (ITE Code) | Acres | FAR/Density | Total Floor Area/Lots/Units | Daily Trips (weekday) | AM Peak Hour | PM Peak Hour |
|---------------------|-------|-------------|-----------------------------|-----------------------|--------------|--------------|
| - | - | - | + 8 U | +77 | +6 | +8 |

METRO SCHOOL BOARD REPORT

Projected student generation existing R8 district: 1 Elementary 1 Middle 1 High

Projected student generation proposed SP-R district: 3 Elementary 2 Middle 2 High

The proposed SP-R zoning district could generate 4 more students than what is typically generated under the existing R8 zoning district. Students would attend Cockrill Elementary School, McKissack Middle School, and Pearl-Cohn High School. Cockrill Elementary School and Cane Pearl-Cohn High School have been identified as over capacity. There is capacity within the cluster for middle school and high school students. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions. The proposed SP is consistent with the T4 Neighborhood Evolving policy of the West Nashville Community Plan.

CONDITIONS

1. Uses within this SP shall be limited to a maximum of 15 detached residential units.
2. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM15-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
3. The final site plan shall include architectural elevations showing raised foundations of 18-36" for residential buildings.
4. The dumpster and recycling area shall be screened by a six foot opaque fence.
5. No structure shall be more than three stories and shall be limited to a maximum height of 35 feet, measured to the roofline. Building elevations for all street and courtyard facing facades shall be provided with the final site plan. Buildings 3 and 13 shall have wraparound porches on each street side. The following standards shall be met:
 - a. Building facades fronting a street and courtyard shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except in dormers.
 - c. EIFS and vinyl siding shall be prohibited.
6. Ownership for units may be divided by a Horizontal Property Regime or a subdivision with a minimum lot size of 1,000 square feet.



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7. The preliminary SP plan approved by the metropolitan council is of such detail that the executive director of the planning department or his designee may waive the submittal of a final site plan.
8. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
9. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
10. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



SEE NEXT PAGE



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2015Z-008PR-001

DTC DISTRICT

Map 081-16, Parcel(s) 213, 216-218, 255, 256, 260, 262.01, 262-264, 267, 277, 278, 283, 284, 287-294, 301, 303, 305, 306, 309-312, 368-372, 374-378, 380, 381, 383-385, 387-393, 395, 397, 399-401, 403-406, 409-410

Map 082-13, Parcel(s) 001-004, 008-011, 017-018

08, North Nashville

19 (Erica S. Gilmore)



| | |
|-----------------------------|---|
| Project No. | Zone Change 2015Z-008PR-001 |
| Project Name | Jefferson St. Rezoning to Downtown Code |
| Council District | 19 – Gilmore |
| School District | 1 – Gentry |
| Requested by | Councilmember Erica Gilmore and Metro Planning Department |
| Staff Reviewer | Collins |
| Staff Recommendation | <i>Approve.</i> |

APPLICANT REQUEST

Zone change from CS, MUL, OR20, RM40, and R6 zoning to DTC zoning and the Hope Gardens DTC subdistrict.

Zone Change

A request to rezone from Commercial Service (CS), Mixed-Use Limited (MUL), Office/ Residential (OR-20), Multi-family (RM40), and One and Two-Family Districts (R6) zoning to Downtown Code (DTC) zoning, and the Hope Gardens DTC subdistrict, for various properties located west of Rosa L Parks Boulevard and north of Jefferson Street (approximately 16.76 acres) and located in the Phillips-Jackson Street Redevelopment District; and to update associated maps within Chapter 17.37 of the Zoning Code, to reflect the proposed DTC and Hope Gardens subdistrict boundaries.

Existing Zoning

Commercial Service (CS) is intended for a diverse range of commercial uses that include retail, consumer service, auto sales and repair, financial, restaurant, office, self-storage, light manufacturing, and allows cash advance, check cashing, title loan and pawn shops.

Mixed-Use Limited (MUL) is intended for moderate intensity mixed commercial uses, and allows cash advance, check cashing, title loan and pawn shops.

Office/ Residential (OR-20) is intended for a mixture of compatible office and multi-family residential use at medium high density levels of intensity. Permitted uses include cash advance, check cashing, and title loan.

Multi-family (RM40) is intended for a high intensity residential development typically characterized by mid and high-rise structures and structured parking.

One and Two-Family Districts (R6) is intended to provide higher intensity one and two-family development for established residential subdivisions where the pattern for development contains both one and two-family development.

Proposed Zoning

Downtown Code (DTC) is intended to provide for and encourage a mix of compatible land uses that provide opportunities to live, work and shop within neighborhoods of Downtown. In order to create



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a more sustainable Downtown, the DTC emphasizes regulating the height, bulk and location of a building and the context of the building in relationship to its surroundings or other nearby buildings.

Hope Gardens Subdistrict is a subdistrict of the DTC, where development along the major streets – Jefferson Street and Rosa L. Parks Boulevard – should be low-rise and transition in height and mass near the single family areas. The Hope Gardens Subdistrict currently adjoins the subject properties south of Jefferson Street. The proposed zone change would expand the subdistrict to the north side of Jefferson St. The existing commercial, residential and industrial can be maintained while providing opportunities for new mixed-use development. The harmonization of these many uses – through the regulation of the building forms - will ensure the vitality of this mixed-use neighborhood.

CRITICAL PLANNING GOALS

- Creates Walkable Neighborhoods
- Provides a Range of Housing Choices
- Supports Infill Development
- Encourages Community Participation

The Downtown Code (DTC) is a form-based code that emphasizes creation of a strong public realm, and would enhance the pedestrian environment by requiring sidewalk improvements with redevelopment. The DTC is also flexible to allow a variety of housing opportunities, both single and multifamily.

The proposed DTC rezoning would bring consistent zoning to the area. The current zoning for these properties is a mix of commercial, residential, and mixed-use zoning districts, meanwhile the properties south of Jefferson Street are already zoned DTC, Hope Gardens Subdistrict. The proposed rezoning would simply extend that DTC zoning and the Hope Gardens Subdistrict, to the northside of Jefferson Street. By simplifying the zoning and streamlining the development process under a form-based code, the rezoning will encourage more infill development of vacant and underutilized properties.

NORTH NASHVILLE COMMUNITY PLAN

Current Policy

Urban Community Center (T4 CC) policy is intended to preserve, enhance, and create urban community centers encouraging their development and redevelopment as intense mixed use areas that are compatible with the general character of urban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T4 Urban Community Centers are pedestrian friendly areas, generally located at intersections of prominent urban streets. T4 Urban Community Centers serve urban communities within a 5 minute drive or a 5 to 10 minute walk.

Urban Mixed Use Corridor (T4 CM) policy is intended to enhance urban mixed use corridors by encouraging a greater mix of higher density residential and mixed use development along the corridor, placing commercial uses at intersections with residential uses between intersections; creating buildings that are compatible with the general character of urban neighborhoods; and a



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street design that moves vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

DRAFT Preferred Future Policy

No change proposed.

Consistent with Policy?

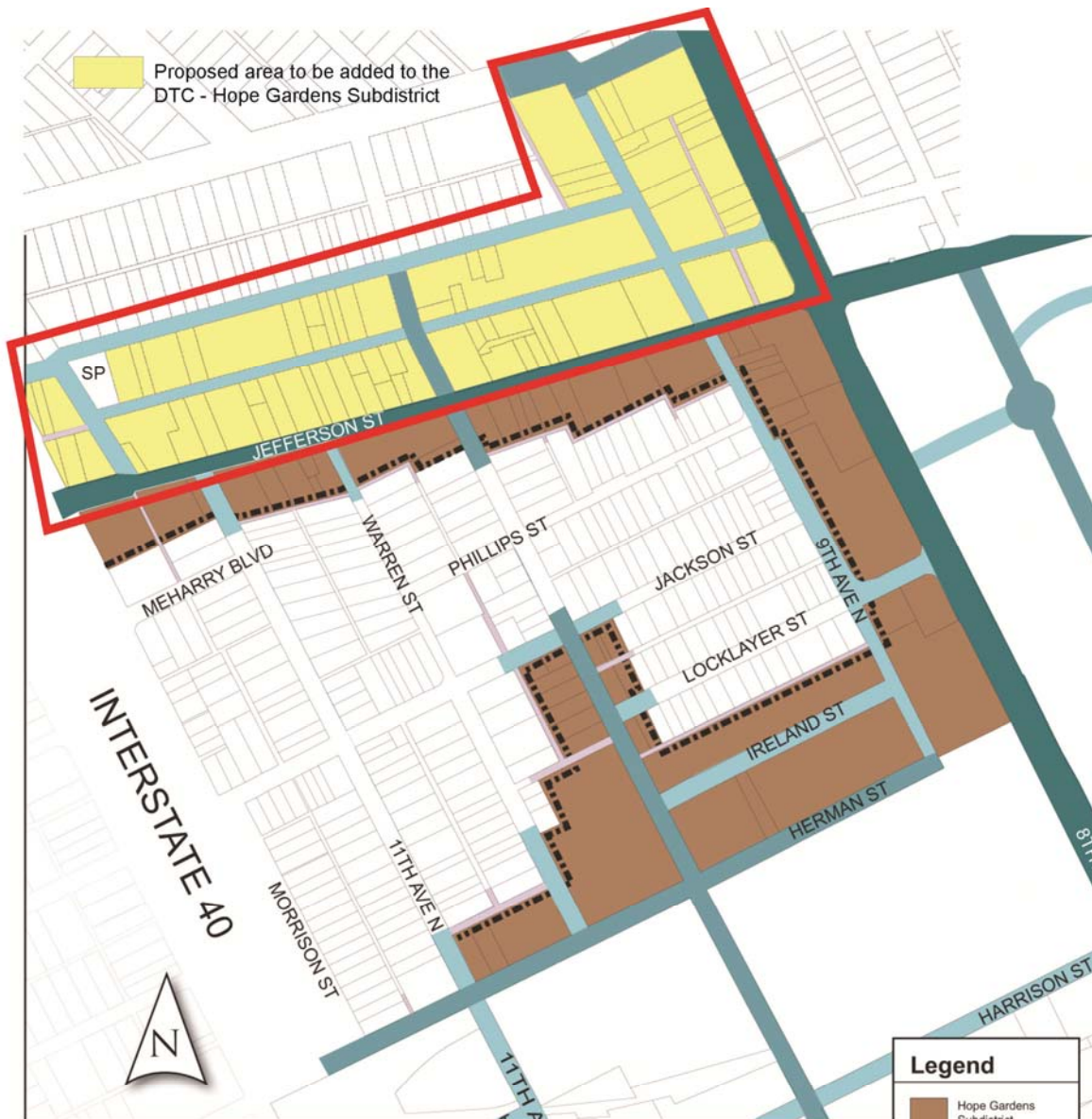
Yes. The proposed DTC district is a mixed-use form based code, emphasizing the creation of a strong public realm, mixed-use development, and enhancing the pedestrian environment. The DTC Hope Gardens subdistrict requires development be constructed in context with the adjacent existing development. The proposed rezoning would allow more intense development along Rosa L. Parks Boulevard and Jefferson Street, with less intense development on Scovel Street, 9th Avenue, 10th Avenue and 11th Avenue.



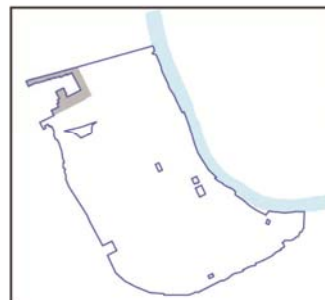
Aerial of the proposed rezoning area and existing DTC - Hope Gardens Subdistrict.



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The Hope Gardens subdistrict includes the commercial and multi-family areas surrounding the single-family residential at the center of the Hope Gardens neighborhood. Development along the major streets – Jefferson Street and Rosa Parks Boulevard – should be low-rise and should transition in height and mass near the single-family areas. The existing commercial, residential and industrial uses are all important factors within this area and can be maintained while providing opportunities for mixed-use. The harmonization of these many uses – through the regulation of the building forms – will ensure the vitality of this mixed-use neighborhood.



| Legend | |
|--------|---|
| | Hope Gardens Subdistrict |
| | Property lines adjacent to single family neighborhood |
| | Primary Street |
| | Secondary Street |
| | Tertiary Street |
| | Alley |

Proposed rezoning area and existing DTC - Hope Gardens Subdistrict.



Section II: Subdistrict Standards

Hope Gardens Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|---------|
| Primary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |

| | |
|-----------------------|---------|
| Tertiary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |

Industrial Frontage is allowed in this Subdistrict along Herman Street only.

B Facade width

| | |
|------------------|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 60% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

C Min. building depth 15' from building facade

A building liner is required surrounding parking structures on the ground floor facing public streets and Open Space.

Height

D Max.

| | |
|--------------------|-----------|
| • Primary Street | 7 stories |
| • Secondary Street | 4 stories |
| • Tertiary Street | 3 stories |

Additional height available through the Bonus Height Program

Step-back

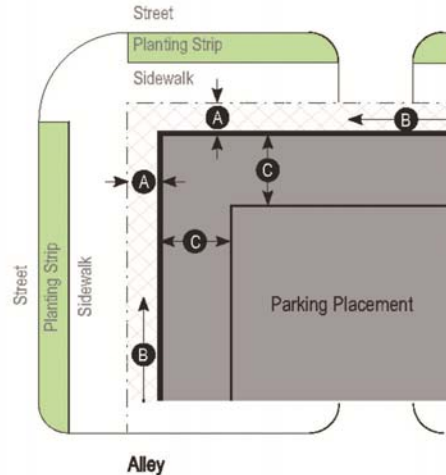
Step-back required for properties abutting the single family neighborhood. Step-back shall be measured from the abutting property line.

| | |
|----------------------------|-----------|
| E Step-back required after | 3 stories |
| F Minimum step-back depth | 30' |

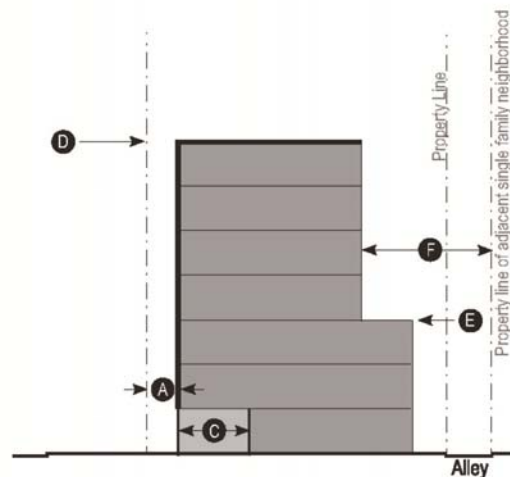
Buffer

Landscape buffer

A landscaped buffer in accordance with 17.24.240 B-5 shall be required along any property line directly abutting a single or two-family zone district.



Building Plan



Building Section



Metro Planning Commission Meeting of 03/26/2015

PROPOSE DTC ZONING DISTRICT COMPARISON

| Key Land Use Comparison | Existing Zoning | | | | | Proposed |
|-------------------------|-----------------|-----|------|------|----|-----------|
| | CS | MUL | OR20 | RM40 | R6 | DTC-North |
| Key Uses: | | | | | | |
| Cash Advance | P | P | P | | | |
| Check Cashing | P | P | P | | | |
| Title Loan | P | P | P | | | |
| Pawn Shop | P | P | | | | |
| Self Service Storage | PC | | | | | P |

Bulk Standards Comparison

| | Street Setback | Side Setback | Rear Setback | Height at Setback |
|--------------------------|----------------|--------------|--------------|---|
| DTC | | | | |
| Hope Gardens Subdistrict | 0'-10' | na | na | 3 - 7 stories, depending on frontage |
| Existing Zoning | | | | |
| CS | 15' | na | 20' | 30' then 1.5 to 1 height control plane |
| MUL | 10' | na | 20' | 3 stories (45' max.) then 1.5 to 1 height control plane |
| OR20* | 20' | 5' | 20' | 30' then 2 to 1 height control plane |
| RM40* | 30' | 5' | 20' | 45' then 2 to 1 height control plane |
| R6** | 20' | 5' | 20' | 3 stories |

*For Multi-Family & Nonresidential Uses

**For Single/Two Family Dwellings on local streets

ANALYSIS

The purpose of the rezoning is to create a stronger Jefferson Street corridor and to encourage quality urban development through the standards of the DTC. Currently the properties south of Jefferson Street are already zoned DTC (Hope Gardens Subdistrict), while properties north of Jefferson Street are a random mix of commercial, residential, and mixed-use zoning districts. The proposed rezoning would expand the DTC zoning district across to the north side of Jefferson Street, so that both sides of the street are in the DTC zoning district. The DTC zoning would streamline the development process under a form-based code, emphasizing creation of a strong public realm and enhancing the pedestrian environment. The proposed zoning would allow for a mix of compatible land uses that will provide for more opportunities to live, work, and shop. Predatory uses such as Cash Advance, Check Cashing, Title Loan, and Pawn Shops would not be permitted. However, existing businesses would be grandfathered in, and allowed to continue as legally non-conforming uses. The area proposed to be rezoned would be within the Hope Gardens Subdistrict of the DTC.

This area is also regulated by the MDHA Phillips-Jackson Redevelopment District. The review process for MDHA will remain the same as it is today; MDHA must review and approve all projects within their Redevelopment Districts. Additionally within the DTC, the planning review process is simplified so that an MDHA approval also acts as a DTC approval, with the MDHA Design Review



Metro Planning Commission Meeting of 03/26/2015

Committee able grant modifications to the DTC standards, for projects within their Redevelopment Districts.

MDHA RECOMMENDATION

MDHA is aware of expanding of the DTC north of Jefferson. We have no issues with this. MDHA approves this proposal.

HISTORIC ZONING COMMISSION RECOMMENDATION

Approved

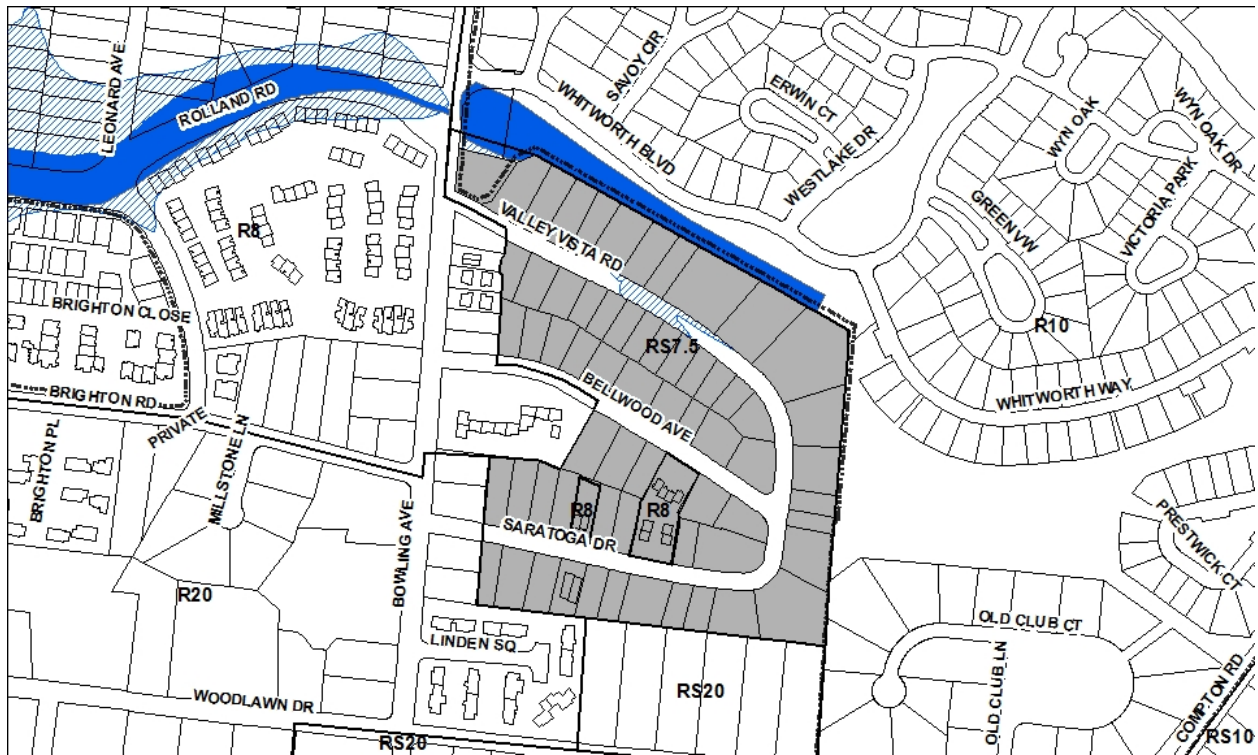
No exception taken

STAFF RECOMMENDATION

Staff recommends approval. Rezoning to the DTC would encourage a mix of compatible land uses to provide more opportunities to live, work and shop in this area.



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2015UD-001-001

BELLWOOD UDO

Map 104-13, Parcel(s) 189-195, 197-200, 202-207, 209-211, 218-224, 226-229, 232-234, 238-241, 253, 268, 279

Map 104-13-0-C, Parcel(s) 001-004

Map 104-13-0-G, Parcel(s) 001-002

Map 104-14, Parcel(s) 002-004, 006, 314

Map 104-14-0-G, Parcel(s) 149

Map 117-01, Parcel(s) 013-014, 017, 018, 020, 021, 023-026, 028-030, 201

Map 117-01-0-B, Parcel(s) 015-016, 256-257

Map 117-01-0-H, Parcel(s) 001-003

Map 117-02, Parcel(s) 001

10, Green Hills - Midtown

24 (Jason Holleman)



| | |
|-----------------------------|--|
| Project No. | Urban Design Overlay 2015UD-001-001 |
| Project Name | Bellwood Urban Design Overlay |
| Council District | 24 – Holleman |
| School District | 8 – Pierce |
| Requested by | Councilmember Jason Holleman |
| Staff Reviewer | Diaz |
| Staff Recommendation | <i>Approve.</i> |

APPLICANT REQUEST

Adopt the Bellwood Urban Design Overlay (UDO) to apply design standards along Bowling Avenue, Valley Vista Road, Bellwood Avenue and Saratoga Drive.

Urban Design Overlay

A request to apply the Bellwood Urban Design Overlay to various properties located east of Bowling Avenue, along Valley Vista Road, Bellwood Avenue, and Saratoga Drive (20.5 acres), zoned Single-Family(RS7.5) and One and Two-Family (R8), to apply design standards regarding building height, setbacks, frontage, and driveways.

Existing Zoning

Single-Family (RS7.5) medium density residential, requiring a minimum 7,500 square foot lot and intended for single-family dwellings.

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes.

Proposed Zoning

Urban Design Overlay (UDO) is a zoning overlay category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan.

CRITICAL PLANNING GOALS

- Preserves the character of neighborhoods
- Supports Infill Development
- Encourages Community Participation

The Bellwood UDO will support appropriate infill by ensuring that new development is consistent in scale and mass with existing homes.

The UDO will also preserve housing choice by allowing one- and two-family homes to continue to be built, but under the UDO standards. The Bellwood neighborhood, like many inner-ring neighborhoods, has faced tear down and reconstruction trends with larger two-family homes replacing smaller existing homes. Presented with this change, many of these neighborhoods have chosen to rezone to single-family only zoning.

The Bellwood Neighborhood studied the options available to them and approached the Planning Department about preparing an Urban Design Overlay that would still allow infill housing to be built,



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but would preserve the scale and massing of the neighborhood. A committee of neighbors proposed the standards in the UDO.

GREEN HILLS/MIDTOWN COMMUNITY PLAN

Current Policy

Urban Neighborhood Maintenance (T4 NM) is intended to preserve the general character of existing urban neighborhoods. Areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Conservation Policy (T4 CO) identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what transect the area is in and whether or not they have already been disturbed.

Consistent with Policy?

Yes. The proposed UDO is consistent with the policy for the area and will ensure that future infill is compatible with the existing character of the neighborhood.

PLAN DETAILS

The original homes that were constructed in the neighborhood are of similar character, generally 1.5 stories tall, with single width driveways. The neighborhood is currently zoned RS7.5 and two properties are zoned R8, but many of the standards of the existing zoning permit development that is out of character with the scale of the original homes.

This Urban Design Overlay (UDO) has been created to maintain the scale of the existing homes. The UDO is not intended to dictate style, architecture or require new construction to exactly replicate the existing homes. The standards of the UDO focus primarily on the front of the house and yard - through the standards for height, setbacks and driveways/garages.

Height

The standards for height will have the most impact in ensuring compatibility of new development. The current RS7.5 and R8 zoning standards allow a maximum height of 3 stories. The 3 stories are in addition to a foundation as tall as 7 feet. This standard would allow a new structure to stand twice as tall as the average existing home in the Bellwood neighborhood.

The UDO will regulate height based on one criterion. The height standard will depend on the width of the build-to line. Three categories have been established based on potential build-to line dimensions; 55-74 feet, 75 feet and greater, 110 feet and greater. Each category has different height and side setback standards to fit with the size of the lot. For example, a larger lot gets additional height, but also a little wider side setback.

The UDO standard proposes to regulate height at three points of the front façade- the maximum eave height from top of foundation 16-22 feet, and a height of foundation that is 18 inches minimum and five feet maximum. This would allow for a maximum height of 29-35 feet from grade to the top of the roof.



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An exception is proposed for existing houses exceeding the maximum building height specified in the Bulk Standards Table. They may use their existing height as the maximum building height for future expansions or construction. Exceptions to foundation height may be made to those properties affected by the floodway and required to follow FEMA regulations.

Build-to Line and Rear Setback

Setbacks are varied from the standards of the RS7.5 and R8 districts. To allow for contextual front setbacks the minimum build-to line shall be average of the street setback of the two developed lots abutting each side of the subject lot. When one or more of the abutting lots is vacant, the next developed two lots on the same block face shall be used. Where there is only one abutting lot on the same block face, it shall be used for this calculation. All new construction will be required to build within 5 feet from the build-to line. Balconies, porches, stoops and other architectural protrusions can be built to the build-to line and cannot exceed 60% of the primary structure's façade width. Steps may encroach beyond the build-to line up to 5 feet.

The rear setback is consistent with the Metro Zoning Code for the RS7.5 and R8 zoning districts (20 feet).

Side Setbacks

Minimum side setbacks are 5-10 feet. For build-to lines 55-74 feet wide the side setback is 5 feet; for lots 75 feet and greater is 7 feet; and for lots 110 feet and greater is 10 feet.

Corner Lot Frontages

Corner lots shall incorporate continuity of design in architectural details and materials that address both streets and construction design to avoid long, monotonous, uninterrupted walls or roof planes facing side streets.

New Construction and Additions

Materials, texture, details, and material color of a new building's public facades shall be visually compatible, by not contrasting greatly, with surrounding buildings. Vinyl and aluminum siding are not appropriate.

Garage and Accessory Buildings Location/Setback/Height

Garages are to either be detached and located behind the principal structure, or attached and accessed from the side or rear, behind the front façade. This is to maintain the current location of garages in this area and to prevent the creation of front loaded garages with new home construction. Detached garages or accessory buildings cannot be placed in the side and rear setbacks except as noted in Metro Code 17.12.040 E1b.

Detached garages and accessory buildings must be less than 50% of the total building footprint of the primary structure.

The eave of the garage or accessory building shall not exceed the height of the eave line of the primary structure. The roof ridge shall be no higher than 27 feet or 2 feet below roof ridge of the primary structure whichever is lower.



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Driveways and Parking

One driveway is allowed per unit with the exception for a circular driveway. A circular driveway is limited to a maximum of two curb cuts not to exceed 10 feet in width. The driveway width standard is 8 feet minimum – 12 feet maximum, with a requirement that it must be 12 feet wide through the right-of-way to allow for turning movements. Driveways are required to be setback 2 feet from side and rear property lines, with a requirement that it must be setback 4 feet from the side property line through the right-of-way per Metro Code 13.12.110. This standard is to prevent paving right up to the property line, which can cause stormwater runoff and erosion problems for adjoining property owners. Driveways on new builds should not be adjacent to each other or an existing driveway. Lastly, driveways must be of a hard surface dustless material.

Compliance

Full compliance with the Development Standards shall be required when:

- Property is redeveloped or vacant property is developed.
- The building's occupiable square footage is being expanded; the expansion shall be in compliance with all applicable Development Standards.
- When a new structure is built on a lot with multiple structures, the new structure shall be in compliance with all Development Standards.

Compliance with the parking and driveways standards will become effective when the UDO is adopted. Existing non-compliant situations will be "grandfathered," but changes after the effective date of the UDO to parking or driveways must be consistent with the standards of the UDO.

Any building permits that have been pulled prior to the UDO will be able to construct their plan without compliance of the UDO standards.

Applicability

Base zoning district standards that are not varied by the provisions set forth in the UDO shall apply as applicable to all property within the UDO boundary.

Modifications

Based on site-specific issues, modifications to the standards may be necessary. Any standard within the UDO may be modified, insofar as the intent of the standard is being met; the modification results in better urban design for the neighborhood as a whole; and the modification does not impede or burden existing or future development of adjacent properties. The process for approving modifications is as follows:

Minor modifications, deviations of 10 percent or less, may be approved by the Planning Commission's designee. Major modifications, deviations of 11 percent or more, shall be approved by the Planning Commission.

STAFF RECOMMENDATION

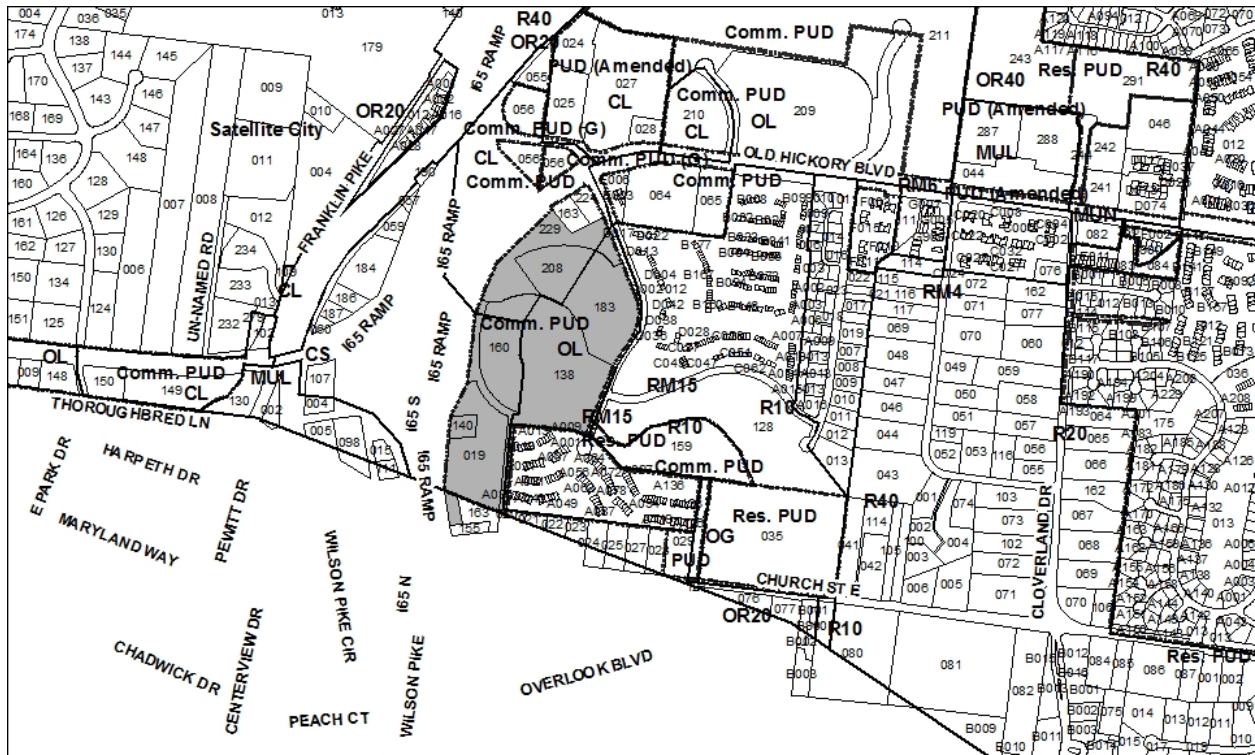
Staff recommends approval of the Bellwood UDO.



SEE NEXT PAGE



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55-85P-002

THE SUMMIT

Map 160, Parcel(s) 183, 208, 229

Map 171, Parcel(s) 019, 138, 140, 160

12, Southeast

04 (Brady Banks)



| | |
|-----------------------------|--|
| Project No. | Planned Unit Development 55-85P-002 |
| Project Name | The Summit |
| Council District | 4 – Banks |
| School District | 8 – Pierce |
| Requested by | Goodwyn, Mills, Cawood, Inc., applicant; Advent Properties, Inc., owner. |
| Staff Reviewer | Sajid |
| Staff Recommendation | <i>Approve with conditions.</i> |

APPLICANT REQUEST

Revise preliminary plan for a portion of the Summit PUD.

Revise Preliminary PUD and Final Site Plan

A request to revise the preliminary plan for a portion of the Summit Commercial Planned Unit Development Overlay District on property located at 201, 202, 205 & 305 Summit View Drive and Summit View Drive (unnumbered), Frierson Street (unnumbered) and 520 Summit View Place, approximately 640 feet south of Old Hickory Boulevard (19.36 acres), zoned Commercial Limited (CL), Commercial Services (CS) and Office Limited (OL).

Existing Zoning

Commercial Limited (CL) is intended for retail, consumer service, financial, restaurant, and office uses.

Commercial Services (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.

Office Limited (OL) is intended for moderate intensity office uses.

CRITICAL PLANNING GOALS

- Preserves Sensitive Environmental Features

The proposed PUD revision reduces the overall building footprint on the site and relocates development away from areas on the site with the steepest slopes.

REQUEST DETAILS

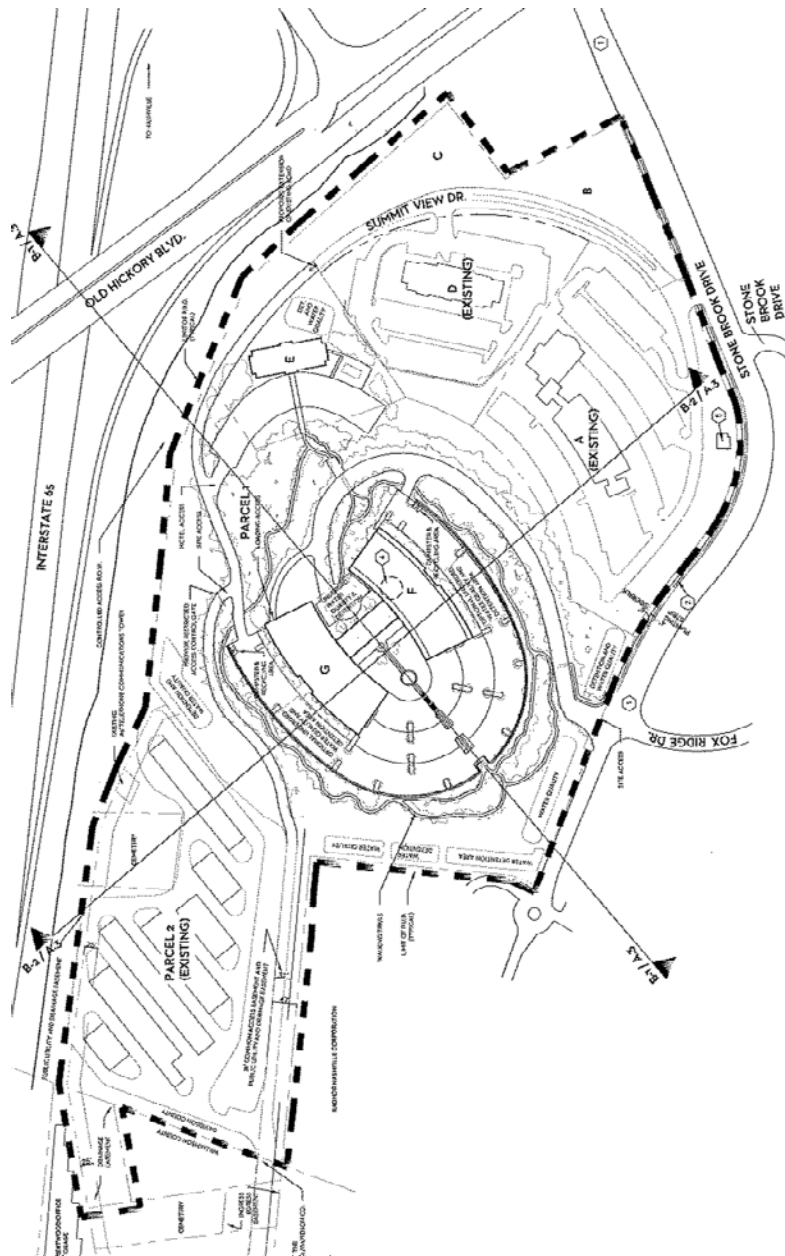
The subject property is located southwest of the intersection of Old Hickory Boulevard and Stone Brook Drive in Brentwood. Surrounding zoning includes R10, RM15, CL, and CS. The zoning of the subject property is OL, CL and CS and PUD overlay.

ANALYSIS

The PUD was original approved by Metro Council in 1985 for commercial development. The current request would revise the PUD to relocate several planned buildings to areas outside of the steepest slopes on the site, which would reduce grading into the hillside in comparison to the currently approved plan. In addition, the overall building area on the site on Parcel 1 is reduced by 27,000 SF (from 480,000 SF to 453,000 SF). The improvements shown on Parcel 2 have already been constructed.



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Proposed Site Plan



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No changes are being proposed that conflict with the concept of the Council approved plan. The revised site layout is consistent with the concept of the PUD and does not include any unapproved uses or increases in gross floor area. Consequently, staff finds that the proposed revision is a minor modification.

Section 17.40.120.F permits the Planning Commission establishes types of changes that require Metro Council concurrence. Staff finds that the request does not meet the threshold for Metro Council concurrence and may be approved by the Planning Commission as a revision to the PUD. Section 17.40.120.F is provided below for review.

F. Changes to a Planned Unit Development District.

1. Modification of Master Development Plan. Applications to modify a master development plan in whole or in part shall be filed with and considered by the planning commission according to the provisions of subsection A of this section. If approved by the commission, the following types of changes shall require concurrence by the metropolitan council in the manner described:
 - a. Land area being added or removed from the planned unit development district shall be approved by the council according to the provisions of Article III of this chapter (Amendments);
 - b. Modification of special performance criteria, design standards, or other requirements specified by the enacting ordinance shall be authorized by council ordinance;
 - c. A change in land use or development type beyond that permitted by the specific underlying zoning district shall be authorized only by council ordinance; or
 - d. An increase in the total number of residential dwelling units above the number last authorized by council ordinance or, for a PUD district enacted by council ordinance after September 1, 2006, an increase in the total number of residential dwelling units above the number last authorized by council ordinance or above the number last authorized by the most recent modification or revision by the planning commission; or
 - e. When a change in the underlying zoning district is associated with a change in the master development plan, council shall concur with the modified master development plan by ordinance.
 - e.[f.] Any modification to a master development plan for a planned unit development or portion thereof that meets the criteria for inactivity of Section 17.40.120.H.4.a.

The proposal is for a revision to the preliminary plan to relocate buildings E, F and G on Parcel 2.in order to lessen the environmental impact on the site as the subject property is encumbered by substantial steep slopes. The plan also proposes a reduction in the previously approved gross floor area on Parcel 2. The previously approved plan was for a total of 480,000 SF, and the current proposal includes 453,000 SF. While the overall floor area is to be reduced, the plan does propose to increase the height of building F from 8 stories to 10 stories. However, the proposed 10 stories would be permitted under OL which is the base zone on this part of the site. Access is from Stone Brook Drive, and the plan incorporates a 6' sidewalk and 4' planting strip along that street frontage.

As the proposed revision keeps with the overall intent of the PUD, planning staff recommends approval of the request.



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FIRE MARSHAL RECOMMENDATION

N/A

STORMWATER RECOMMENDATION

Conditional if approved

- Add Access Note to plans:
(Metro Water Services shall be provided sufficient and unencumbered access in order to maintain and repair utilities in this site.)

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Comply with MPW Traffic Engineer
- With the submittal of the Final PUD a detailed review of the site plan will occur. The concept appears to be feasible, but a detailed review has not occurred.
- All sidewalks along public streets are to be constructed within ADA Compliance and be located within ROW.

TRAFFIC & PARKING RECOMMENDATION

Conditional if approved

In accordance with the findings of the TIS, The developer shall construct the following roadway improvements.

Stone Brook Drive

- Developer shall widen Stone Brook Drive between Old Hickory Boulevard and Site Access 1 to provide a four-lane cross-section with additional widening as described below for the intersection of Old Hickory Boulevard and Stone Brook Drive. Due to the topography, significant grading and retaining walls will be necessary along Stone Brook Drive. The recommended cross-section, at a minimum, will require approximately 59 feet of ROW, assuming four 11-foot travel lanes, 2.5-foot curb and gutter, and a 6-foot sidewalk with 4-foot green zone.
- The widening and reconstruction of Stone Brook Drive should provide a 6-foot wide sidewalk with a 4-foot planting strip on the west side of the street adjacent to the project site.

Old Hickory Boulevard and Franklin Pike Circle/Stone Brook Drive

- The developer shall widen the northbound approach of Stone Brook Drive at the intersection to provide an additional lane for the approach. Then the lanes should be striped to provide two left turn lanes, one through lane, and a separate right turn lane.
- Developer shall submit signal plan to modify the signal to accommodate a wider cross-section of Stone Brook Drive and install signal modification when approved by Metro traffic engineer.



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Old Hickory Boulevard and Oakes Drive/American General Way

- Developer shall install pavement striping for the northbound approach of Oakes Drive to provide one left turn lane and one shared through/right turn lane. Loop detectors are currently provided for two northbound lanes although pavement markings are not provided. The striping should include a double solid yellow line and approximately 130 feet of storage for each northbound lane. A left turn arrow and a shared through/right turn arrow should be provided. Developer shall submit signal modification plan including pedestrian signals and associated ped infrastructure and install signal modifications when directed by Metro traffic engineer.

Old Hickory Boulevard and I-65 Southbound Ramps

This signalized intersection currently experiences long queues during peak hours particularly for the westbound left turn lane. Major improvements are needed at this location based on existing traffic volumes and conditions. The following improvements will increase the capacity of the intersection and are needed for current conditions. These improvements require significant investment and should be programmed by TDOT and Metro-Nashville. The 2015 TDOT RSAR for the I-65 interchange at Old Hickory Blvd is limited to adding a WB 4th lane at the I-65 NB exit 74A ramp. Therefore, the developer shall coordinate and participate with TDOT regarding the recommended roadway improvements at the SB on ramp to construct the items listed below.

- Provide an additional westbound left turn lane on Old Hickory Boulevard with at least 200 feet of storage.
- Widen the I-65 Southbound On-ramp in order to accommodate dual westbound left turn lanes onto the ramp.
- Modify the traffic signal design to provide protected-only left turn signal phasing for the westbound approach of Old Hickory Boulevard.

Stone Brook Drive and Fox Ridge Drive/Site Access 1

- Site Access 1 should be aligned with Fox Ridge Drive.
- Stop-control should be provided for the Site Access 1 approach to Stone Brook Drive.
- Site Access 1 should be designed to include one exiting lane for left, through, and right turn movements and one entering lane.
- The final design of Site Access 1 should be completed such that departure sight triangles, as specified by AASHTO, will be clear of all potential sight obstructions, including horizontal and vertical curvature, landscaping, monument signs, etc.

Summit View Drive and Site Access 2

- Site Access 2 at Summit View Drive should be designed with sufficient width to include one exiting lane and one entering lane.
- The final design of Site Access 2 should be completed such that departure sight triangles, as specified by AASHTO, will be clear of all potential sight obstructions, including horizontal and vertical curvature, landscaping, monument signs, etc.

Summit View Drive and Hotel Access

- The Hotel Access at Summit View Drive should be designed with sufficient width to include one exiting lane and one entering lane.



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- The final design of the Hotel Access should be completed such that departure sight triangles, as specified by AASHTO, will be clear of all potential sight obstructions, including horizontal and vertical curvature, landscaping, monument signs, etc.

Parking

- A minimum of 1,483 parking spaces are needed to accommodate the proposed development based on 400,000 square feet of office space and 200 hotel rooms, assuming 30 employees for each of the two proposed hotels. This minimum number of spaces accounts for a five percent reduction for shared parking between the two land uses.

Phasing plan

- Developer shall prepare a phasing plan and identify installation schedule of the specific roadway improvements triggered by specific land use construction.
- Developer shall submit signal plans, striping and signage plans with construction documents.

WATER SERVICES RECOMMENDATION

Approved

- Approved as a Preliminary plan only. Public water and sewer construction plans must be submitted and approved prior to Final Site Plan Approval. The required 30% capacity fees must be paid prior to Final Site Plan approval.

STAFF RECOMMENDATION

Staff recommends approval with conditions.

CONDITIONS

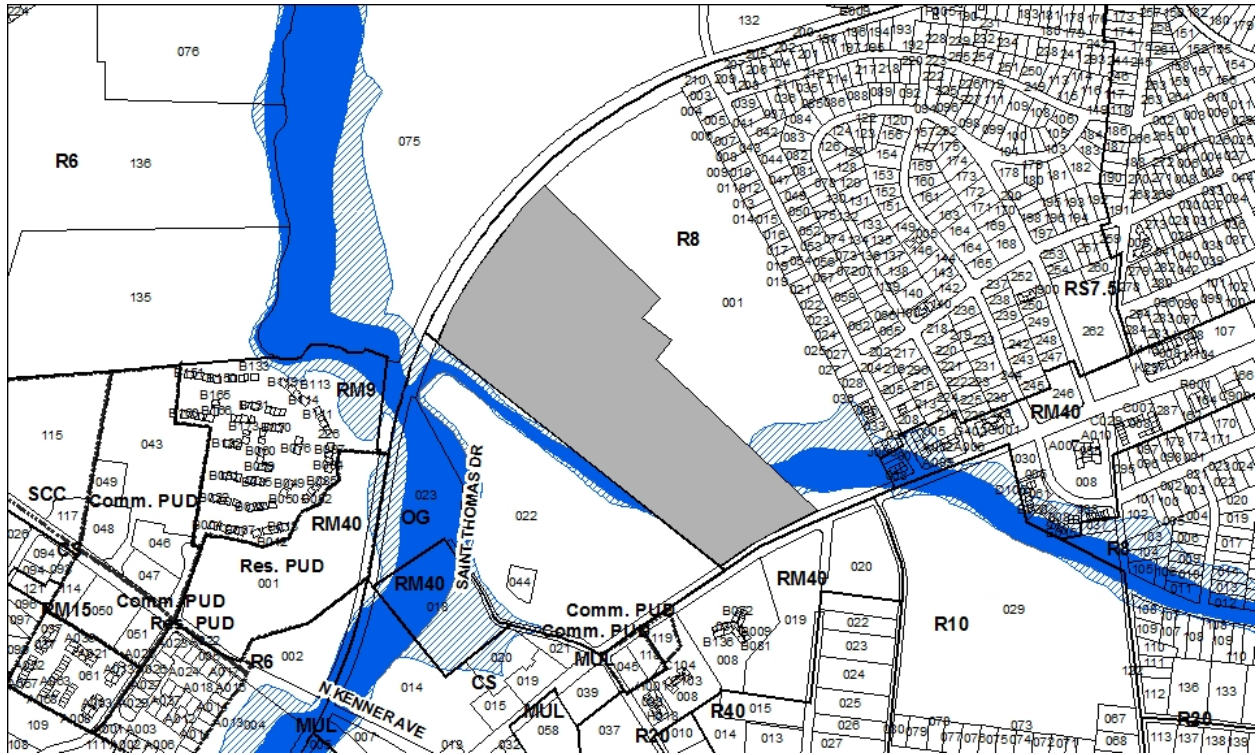
1. The final site plan shall include a 4' planting strip with 6' sidewalks along the west side of Stone Brook Boulevard frontage to Old Hickory Boulevard.
2. This approval does not include any signs. Signs in planned unit developments must be approved by the Metro Department of Codes Administration except in specific instances when the Metro Council directs the Metro Planning Commission to review such signs.
3. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
4. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Stormwater Management division of Water Services.
5. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Traffic Engineering Sections of the Metro Department of Public Works for all improvements within public rights of way.
6. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
7. Authorization for the issuance of permit applications will not be forwarded to the Department of Codes Administration until four additional copies of the approved plans have been submitted to the Metro Planning Commission.
8. The PUD final site plan as approved by the Planning Commission will be used by the Department of Codes Administration to determine compliance, both in the issuance of permits for construction and field inspection. Significant deviation from these plans may require reapproval by the Planning Commission and/or Metro Council.



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2008IN-001-002
AQUINAS COLLEGE
Map 103-12, Part of Parcel(s) 001
10, Green Hills - Midtown
24 (Jason Holleman)



| | |
|-----------------------------|--|
| Project No. | Institutional Overlay 2008IN-001-002 |
| Project Name | Aquinas College |
| Council District | 24- Holleman |
| School District | 9 - Frogge |
| Requested by | 906 Studio Architects, LLC, applicant; St. Cecilia Congregation, owner. |
| Staff Reviewer | Birkeland |
| Staff Recommendation | <i>Approve with conditions.</i> |

APPLICANT REQUEST
Amend the Institutional Overlay.

Institutional Overlay Amendment

A request to amend the Aquinas College Institutional Overlay district for a portion of property located at 4210 A Harding Pike, approximately 225 feet west of Cherokee Road (46.5), zoned One and Two-Family Residential (R8), to maintain existing buildings and add temporary modular buildings for swing space during renovations.

Existing Zoning

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 5.79 dwelling units per acre including 25 percent duplex lots. *R8 would permit a maximum of 251 lots with 62 duplex lots for a total of 313 units.*

Institutional Overlay (IO) District is intended for colleges, universities, and other specialized community uses within or near residential areas.

CRITICAL PLANNING GOALS

N/A

GREEN HILLS -MIDTOWN COMMUNITY PLAN

Current Policy

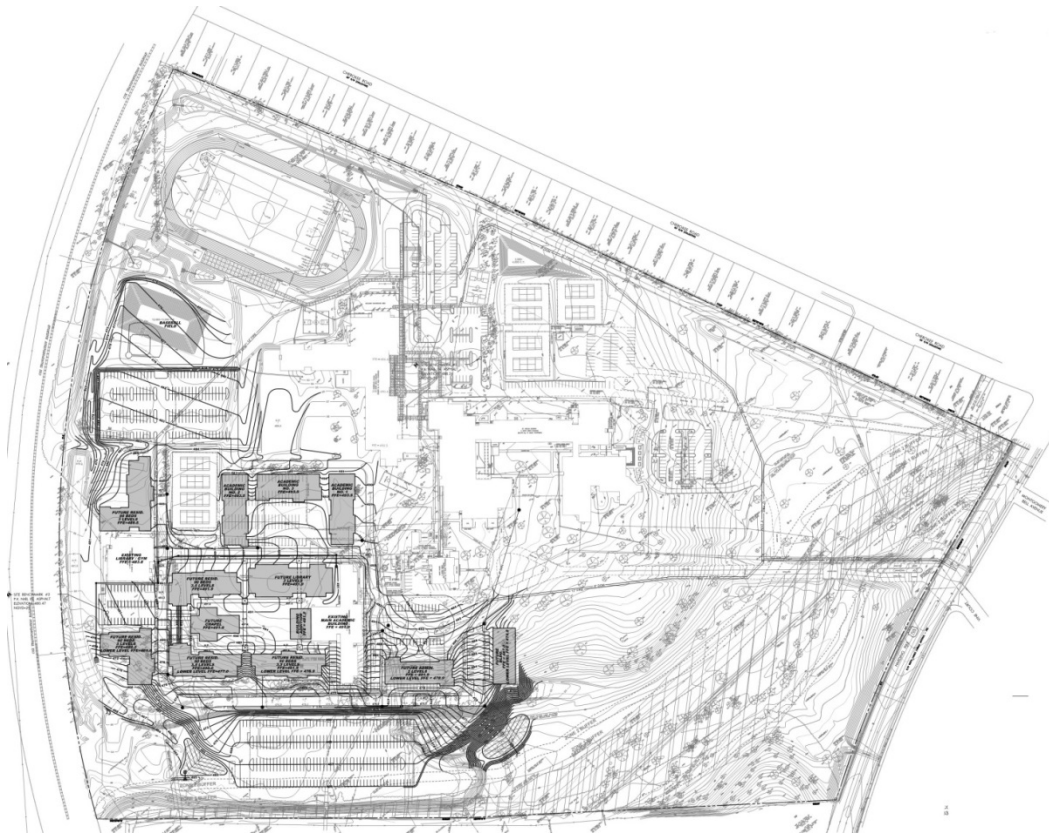
D Major Institutional (D MI) policy is intended to preserve, enhance, and create Districts where major institutional uses are predominant and where the development and redevelopment of such Districts occurs in a manner that is complementary of the varying character of surrounding communities as characterized by development patterns, building form, land use, and associated public realm. Land uses include large institutions such as medical campuses, hospitals, colleges and universities, and government community facilities as well as uses that are ordinarily ancillary to the principal use.

Special Policy Area #7

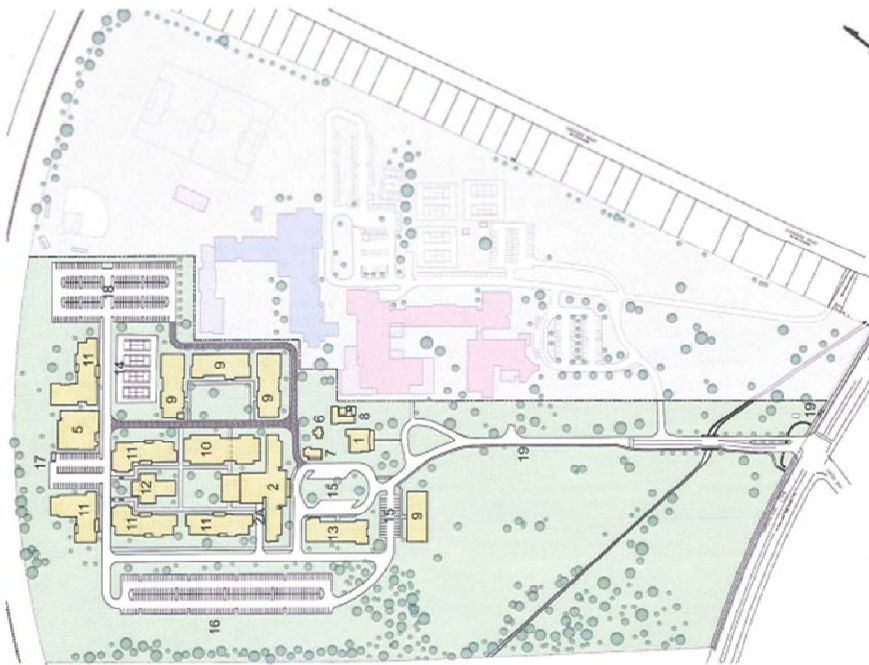
1. Development intended in this MI area is limited to the existing institutional uses. Health care and neighborhood convenience services, in particular, are not intended. Other than the existing



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Proposed Site Plan



Proposed Master Plan



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development, the only appropriate use without a change in policy is one and two-family residential at low-medium intensity.

2. Additional development is appropriate only when it is determined that it will not have any adverse impacts on the adjoining Cherokee Park neighborhood.

3. Future development of the college campus portion of this area should be governed by "Institutional Overlay" [IO] zoning.

4. A generous amount of green space should be preserved along and near West End Avenue and Cherokee Avenue in conjunction with the development of the area between those streets and the existing facilities.

5. Nonresidential base zoning is not recommended for any of this MI area.

DRAFT Preferred Future Policy

No changes are proposed.

Consistent with Policy?

Yes. The Aquinas College is currently located within the Institutional Overlay. As required by in the special policy, the Institutional Overlay will govern the future development of the Aquinas College Campus.

PLAN DETAILS

Aquinas College encompasses a little over 46-acres of the 83-acre Dominican Campus, which also includes St. Cecilia Academy (high school) and the Overbrook School (elementary and middle school).

The proposed amendment to the Original Institutional Overlay includes the following:

- Modification to reduce the scope of the campus and further define the separation between the college and the lower schools.
- Maintain existing historic structures in their original locations
- Allow the use of temporary modular buildings to serve as swing space for the renovation of the existing buildings on campus.

The plan includes approximately 475,000 square feet of new floor area; totaling approximately 546,800 of square footage. The proposed buildings include a chapel, four - academic buildings, five – residence halls, dining hall addition, a library and an administration building. The two historic structures, the White House and Little White House, are proposed to remain and will be renovated accordingly.

In order to facilitate the renovation of the existing Main Academic Building, the college has proposed on-site swing space in the form of modular buildings that will be removed intermittently as new permanent structures are built. The modular buildings are proposed to be located in such a way to facilitate the college campus feel prior to the permanent buildings are constructed.

Architectural Guidelines

The plan describes the architectural standards of each of the proposed buildings; including architectural elements, intended uses and potential floor area for each building. A statement regarding the architectural standards has been provided:



Metro Planning Commission Meeting of 03/26/2015

“The intent of this master plan is to develop an esthetically cohesive campus for Aquinas College. The buildings are visualized as bring with limestone or precast concrete details. The buildings will typically be two or three stories. Buildings on the western side of campus have the option for an additional, partially concealed, basement floor as the grade recedes towards St. Thomas West Hospital. Final elevations and site plans will be submitted to the Metropolitan Planning Commission for review and approval before the building permits are procured.”

Setback Requirements

The R8 base zoning requires a setback of 86’ from the centerline of Harding Road. In addition to the setback, there is also a 75’ buffer that is required to the north of Richland Creek, which runs through the front of the property. The plan also proposed a rear setback of 25’ and a side setback on 75’.

Landscaping Guidelines

Landscape plans for each phase of campus development shall be included with final site plan applications. The master plan identifies that effort will be taken to preserve the matures existing tree buffers located on the campus edges adjacent to the Cherokee Park neighborhood and the St. Thomas West Hospital campus.

Access

The Dominican campus will continue to use the existing ingress/egress to Harding Road and existing egress to Cherokee Road. This plan does not propose any additional access points. The plan includes a statement, “The Dominican Campus schools, per the filing of this Institutional Overlay, have agreed to surrender their privilege of acquiring any new or additional access pint to Cherokee Road. The acquisition of additional access to Cherokee Road requires amendment to the Aquinas Institutional Overlay/Master Plan as well as approved from the Metropolitan Nashville City Council.”

The original master plan from 2008 included the construction of a loop road around the perimeter of the three schools. The loop road has been removed in the proposed amendment. A new interior drive is proposed to provide vehicular access to the proposed parking lots along the western and northern borders of the campus. A service/pedestrian path is also proposed to bisect the campus and continue along the eastern border of the campus. These drives will also provide service and emergency access for the campus.

Pedestrian access will be provided from the campus, along the entry drive, to Harding Pike. An eight foot sidewalk and four foot planting strip shall be constructed prior to the issuance for the use and occupancy permit for the combination of dorms that would accommodate three hundred students.

Signage

With this amendment, Aquinas College is proposes to install a free-standing, ground mounted signage on Harding Pike to replace the existing temporary sign panels. All signage shall comply with Metro Zoning sign regulations for the MUL-A Zoning District.



Metro Planning Commission Meeting of 03/26/2015

ANALYSIS

The proposed Institutional Overlay amendment is provides a cohesive campus plan while maintaining the existing historical structures. The proposed amendment is consistent with the D- Major Institutional Policy therefore staff recommends approval with conditions.

FIRE MARSHAL'S OFFICE

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review

PUBLIC WORKS RECOMMENDATION

No exception taken

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.

TRAFFIC AND PARKING RECOMMENDATION

Conditions if approved

- An updated TIS addendum was received on 2/18/15 and reviewed.

STORMWATER RECOMMENDATION

Approved

METRO HISTORICAL ZONING COMMISSION STAFF RECOMMENDATION

N/A

- Project does not include demolition

WATER RECOMMENDATION

Approved with conditions

- Approved as an Institutional Overlay only. The following statement has nothing to do with this approval (just information for the applicant) Public sewer abandonment/relocation will be required for this development. Please submit these plans for review and approval before Final Site Plans are submitted or construction is started.

STAFF RECOMMENDATION

Staff recommends approval with conditions.

CONDITIONS

1. Prior to the submittal of the first final site plan, coordinate with MTA to provide an upgrade bus stop, which may include a shelter, along the property frontage.
2. The sidewalk along Harding Road and the pedestrian path shall be constructed prior to the issuance for the use and occupancy permit for the combination of dorms that would accommodate three hundred students.
3. One on-premise ground sign shall be allowed within the Institutional Overlay. Signage shall comply with the Metro Zoning Code standards for on-premise signs within the MUL-A Zoning District.
4. Building elevations and landscaping plans shall be submitted with each final site plans.



Metro Planning Commission Meeting of 03/26/2015

5. A corrected copy of the Institutional Overlay plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
6. Minor modifications to the Institutional Overlay Master Plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
7. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.

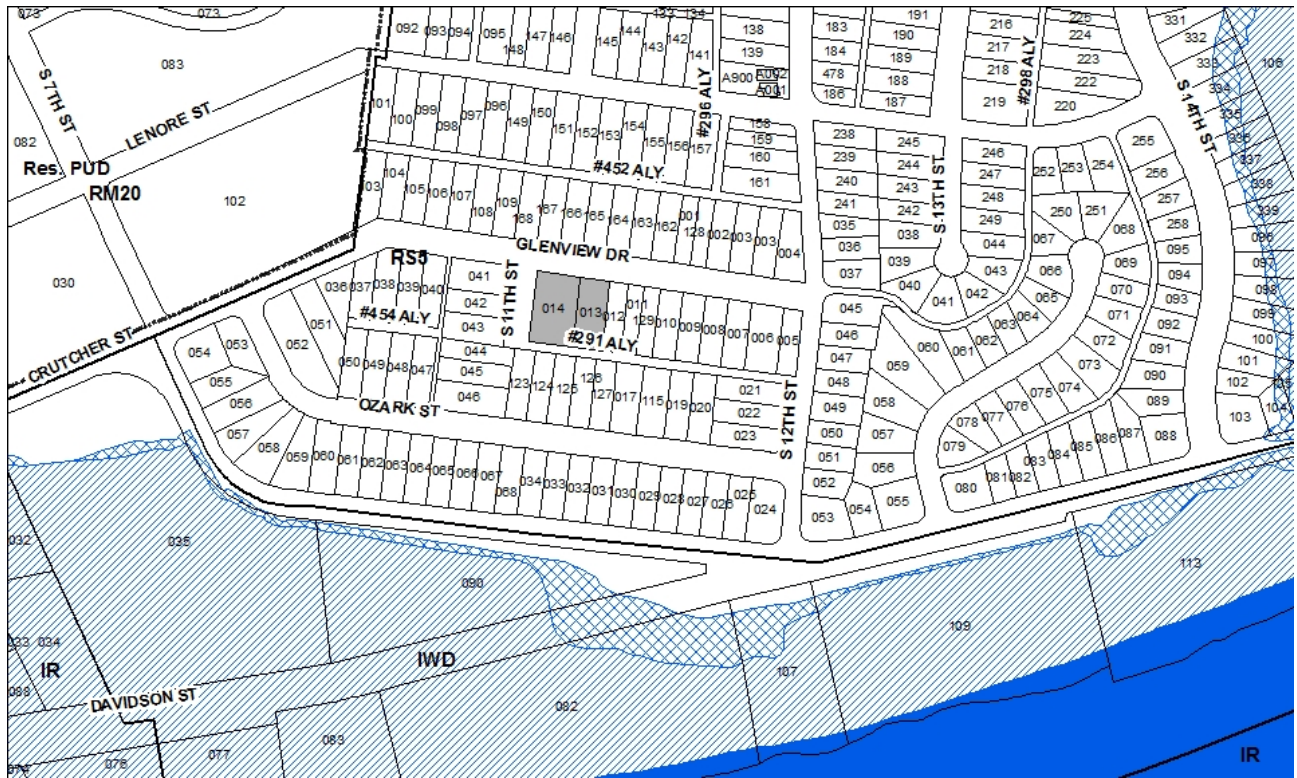


PLANNING COMMISSION ACTIONS

- **Subdivision (Final Plats)**



Metro Planning Commission Meeting of 03/26/2015



2015S-036-001
HAYNIE'S CENTRAL PARK, RESUB LOT 86
Map 094-05, Parcel(s) 013-014
05, East Nashville
06 (Peter Westerholm)



| | |
|-----------------------------|--|
| Project No. | 2015S-036-001 |
| Project Name | Haynie's Central Park, Resub Lot 86 |
| Council District | 6 – Westerholm |
| School District | 5 – Kim |
| Requested by | Dale & Associates, applicant; Sherry Phillips Hopwood and Robert & Betty O'Malley, owners. |
| Staff Reviewer | Sajid |
| Staff Recommendation | <i>Defer to the April 23, 2015, Planning Commission meeting.</i> |

APPLICANT REQUEST

Create 4 lots.

Preliminary SP

A request for final plat approval to create four lots on properties located at 1100 and 1104 Glenview Drive, at the southeast corner of Glenview Drive and South 11th Street, zoned Single-Family Residential (RS5) (0.55 acres).

STAFF RECOMMENDATION

Staff recommends deferral to the April 23, 2015, Planning Commission meeting at the request of the applicant.



SEE NEXT PAGE



EXHIBIT A

Nashville

DOWNTOWN CODE

PROPOSED TEXT AMENDMENT TO DTC - EXHIBIT

2015Z-003TX-001
CB# 2015CB-1053

PROPOSED CHANGES ARE SHOWN IN RED

Amended by Ordinance No. BL2013-376
as adopted on April 02, 2013


Amended by Ordinance No. BL2012-142
as adopted on May 21, 2012

Amended by Ordinance No. BL2011-896
as adopted on May 26, 2011

Attachment to Ordinance No. BL2009-586
as adopted on February 02, 2010

Document Contact Information

Planning Department staff provides consultations for developing within the Downtown Code. Call (615) 862-7190 or email planningstaff@nashville.gov to schedule a meeting.

 The Planning Department does not discriminate on the basis of age, race, sex, color, national origin, religion or disability in access to, or operation of, its programs, services, and activities, or in its hiring or employment practices. For ADA inquiries, contact Josie Bass, ADA Compliance Coordinator, at 862-7150 or e-mail her at josie.bass@nashville.gov. For Title VI inquiries contact Shirley Sims-Saldana or Denise Hopgood of Human Relations at 880-3370. For all employment-related inquiries call 862-6640.

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Section I: Introduction

Section I: Introduction

The Downtown Code and the Downtown Plan

The Downtown Code (DTC) implements the community vision set forth in the Downtown Community Plan: 2007 Update. Through the community planning process, stakeholders reached a common vision for the future of Downtown. The Plan envisions multiple distinct neighborhoods within Downtown, each with its own character and scale, which contribute to the vitality of Downtown as the center of the city and the region.

The DTC regulates the physical form of buildings to ensure each makes a positive contribution to a complete urban environment. By ensuring a specific and predictable urban form, the DTC ensures that all new construction makes a positive contribution to the public realm – streets and open space – and that all investments are held to the same standard.

While the DTC is a regulating document, the Downtown Plan contains placemaking tools and guidelines and should be used as a companion document to the DTC.

The Guiding Principles of the Downtown Plan have directed the standards in the DTC and are as follows:

Ensure that Downtown remains the civic, commercial and entertainment center for Nashville, Middle Tennessee and the Southeast.

- The regulations of the DTC clarify the vision for each neighborhood in Downtown. Under the DTC, certain areas are allowed to have high-rise towers, while others are zoned for neighborhood-scale development, while still others are allowed to be more transitional with support uses necessary to sustain urban business.
- In recent years, nearly all projects in Downtown have sought rezoning or variances to existing zoning. The DTC alleviates this need by allowing significantly greater development rights, in forms based on construction norms and urban design objectives, than existing zoning. The DTC also allows the modification of standards for site-specific issues.

Provide opportunities for continued growth while preserving and enhancing the character that inspires residents and businesses to move Downtown.

- The Downtown Plan called for areas of increased height and density within Downtown. The Plan extended the boundaries of the Core – the tallest and most intense neighborhood – and provided guidance regarding additional height in other neighborhoods. The DTC codifies these opportunities in the subdistrict standards for these neighborhoods.
- Similarly, the Downtown Plan acknowledged the difficulty of developing within the sky exposure plane in some neighborhoods. The DTC modified the allowable form of build-

ings by allowing additional height at the street in exchange for an overall height-cap. The result is that properties, and thus neighborhoods, will have the same intensity as in CF zoning but in a form that is aligned with typical construction methods and creates a more predictable urban environment.

- The form and shape of development under CC and CF zoning is unpredictable. The DTC provides clear direction on minimum development and maximum development. For instance, the Gulch is zoned for a specific character, while Sulphur Dell is zoned for something different. This ensures certainty about the character and scale of development in each neighborhood.
- In an urban environment, the street level design and function of a building is of the utmost importance. The interaction of the building with the street should enliven the street, making it comfortable, safe and interesting for pedestrians. The DTC is based on frontage design – storefront, stoop, porch, industrial, and civic – and includes standards on glazing, vehicular access, landscaping, and active uses on the ground level. Correctly designed, these attributes will contribute to safe and interesting streets to result in vibrant neighborhoods and a healthy Downtown.
- A safe and interesting urban environment attracts people. People who feel comfortable in Downtown – enjoying available activities, prospering from the businesses and services, and lingering in the spaces and places – want to be Downtown. The DTC fosters this desirable urban environment through the attention to pedestrian-oriented design, appropriate scale and massing, and neighborhood creation.

Section I: Introduction

The Downtown Code and the Downtown Plan

Create strategic mixed use to facilitate Downtown's transformation into a 24/7 community.

- The Plan calls for a “thoughtful mixture of uses including residential, retail and office, to ensure that Downtown doesn't close at 5 p.m. or even after the concert ends or the restaurants close, but is instead a welcoming home for a diverse residential population.” Multiple stories encourage multiple uses within each building, increasing the likelihood that one of the uses will be active at any given hour. Compare this to one story buildings which lack activity during the off-hours of the single use.
- To be viable, Downtown must accommodate numerous services and functions. By promoting form over use, the DTC allows for a mixture of uses. Because various functions occur at different times of the day – working, shopping and recreating during the day, entertainment and home-life at night – mixed-use, 24/7 neighborhoods use existing infrastructure more efficiently and function in a more sustainable way.

Create and nurture urban neighborhoods.

- As stated in the Plan, “While residential *development* has flourished in recent years, the creation and enhancement of urban *neighborhoods* is still a goal. Residential living will thrive in Downtown when residents feel that they are part of a neighborhood, supported by shared public spaces and the services and amenities needed for daily life.” The DTC directly addresses the development characteristics of neighborhood design: building mass and scale, frontage design, and open space design.
- To create these distinctive urban neighborhoods, the DTC aligns the regulations of each subdistrict with the intended character of the neighborhood. For instance, the South Gulch is envisioned to continue as a high-rise and mid-rise, mixed-use neighborhood. The DTC codifies mid-rise height in the general subdistrict and allows high-rise buildings on key intersections and along important streets. In contrast, the North Gulch is envisioned to be a low-rise neighborhood – to preserve Capitol views and transition into the Hope Gardens and John Henry Hale neighborhoods. The DTC codifies this vision by capping the overall

height, allowing for less intense development such as two story houses and townhouses, and encouraging porch and stoop frontages. These are two examples of how the DTC aligns the zoning of neighborhoods with the vision cast during the community planning process.

- “Since 2000, Downtown has experienced an unprecedented residential boom. In 2000, there were approximately 1500 dwelling units in Downtown. As of 2006, over 2600 new residential units were under construction, planned or proposed... Since 2000 there has been over \$500 million in capital investments in residential construction, with another \$400 million planned or proposed.” In addition, “since 2000, Downtown office building development has grown modestly with \$140 million of development completely or under construction.” These strong numbers increase the viability of Downtown. To continue this good momentum, the emphasis must be on place-making as well as development. The DTC encourages the creation of mixed-use, sustainable neighborhoods that have flexibility to address the needs of citizens over time.

Create active, attractive streets and streetscapes.

- “...the Downtown Plan encourages walking as a primary mode of transportation in Downtown. Walking is encouraged in the Downtown Plan by making the walk safe, interesting, and comfortable...” Streets are the most plentiful open space in Downtown and should prioritize the pedestrian experience while appropriately accommodating vehicular traffic. The DTC emphasizes frontage design, requires active ground level uses, and sets standards for vehicular activity.
- The DTC prioritizes the location of vehicular access points per street type – Primary, Secondary, Tertiary, Other, and Alley. By prioritizing, instead of regulating, the DTC allows flexibility for site-specific solutions to be reached in collaboration between the developer and Metro departments.
- While the DTC sets standards on the development of private property, active and attractive streets must be created by a collaborative process with all Metro Departments. As development proposals are offered, Metro will retain a commitment to creating a strong pedestrian-oriented urban

Section I: Introduction

The Downtown Code and the Downtown Plan

environment. “As Downtown becomes home to many of its workers, and mass transit options into Downtown improve, providing welcoming routes for pedestrians will provide benefits in terms of improved mobility and reduced traffic congestion.”

- The DTC references the *Downtown Streetscape Design Guidelines*, and the *Major and Collector Street Plan including constrained right-of-ways and local streets*, created by Metro Public Works and Metro Planning, and **encourages requires** their use.

Protect and reuse historic structures and districts.

- The DTC has a subdistrict for 2nd Avenue and Lower Broadway that reinforces the historic zoning overlay for these streets. To encourage the adaptive reuse of the historic structures, this subdistrict is eligible for transfer of development rights through the Bonus Height Program.
- The creation of the Core Historic subdistrict encourages the preservation of the existing historic buildings between 3rd and 5th Avenues North, while allowing for appropriately scaled and appropriately detailed infill.
- The creation of the Rutledge Hill subdistrict encourages the preservation of the existing historic buildings in this historic residential area, while allowing for appropriately scaled and appropriately detailed infill.
- During the community planning process, the preservation of views to the Capitol building was identified as important to the development of neighborhoods north of Capitol Hill. The maximum building heights in subdistricts north of Capitol Hill are limited to the elevation of the base of the Capitol building to ensure this that this important civic view is preserved.

Create environmentally sustainable and energy efficient development.

- In order to meet the sustainability goals of the Downtown Plan and to achieve Metro Government’s vision of Nashville as the greenest city in the US, the DTC encourages urban infill and energy efficient development.
- By location alone, urban infill is more energy efficient than green-field development because it utilizes existing streets and infrastructure. By emphasizing mixed-use, walkable neighborhoods within Downtown, the DTC reinforces Metro Government’s commitment to sustainability and

responsible use of resources. Part of being sustainable is using existing under-utilized infrastructure – water lines, sewer lines, electricity, and streets – in lieu of creating new infrastructure and continuing green-field consumption.

- Location, however, does not ensure sustainability. The creation of the DTC emphasizes the need for a denser Downtown to provide citizens with all daily needs within walking distance, the need for buildings that can be adapted for new uses over time, the preservation and adaptive reuse of existing buildings, and the addition of street trees and open spaces.
- In addition to encouraging sustainable urbanity, the DTC encourages the measurement of energy efficient development through the US Green Building Council’s LEED program. (LEED may be substituted for a different nationally-recognized, third-party system of overseeing green building and/or sustainable development practices.)
- The Bonus Height Program of the DTC provides height bonuses for new construction that meets the standards of LEED silver, gold or platinum. LEED takes credits a project for its urban environment, but also for the sustainability of the building itself. The public benefit associated with LEED certification is significant. The lighter the building treads on the infrastructure of the city, the lighter the burden on the city to maintain the infrastructure. This helps the city function better as a whole. The bonuses are appropriately scaled for each subdistrict and are offered in exchange for the level of contribution of this important public benefit.

Create “great spaces” throughout Downtown for the enjoyment of citizens and visitors.

- In some areas of Downtown, open space is appropriately scaled and designed for the envisioned intensity of the neighborhood. In most areas, however, open space is dramatically lacking. The DTC identifies ¼ mile radius neighborhoods (about a five minute walk from edge to center) within Downtown to show the open space deficiencies. For every quarter mile neighborhood, there should be at least a quarter acre of well-designed public open space. This open space may be provided by public or private initiatives.
- The Downtown Plan recommended that Downtown have unique types of open space available to meet the needs

Section I: Introduction

The Downtown Code and the Downtown Plan

of citizens. The DTC provides standards for the creation of these open spaces: greens, squares, plazas, courts and pocket parks/playgrounds. The open spaces will serve as important “great spaces” to help create the vital and functioning neighborhoods envisioned by the Downtown Plan.

- The Bonus Height Program of the DTC provides height bonuses for the development of public open spaces. To be eligible for the height bonus, the development must follow the standards for open space design. The bonuses are appropriately scaled for each subdistrict and are offered in exchange for this important public amenity.

Provide for improved mobility in and through Downtown to support other principles for healthy growth in Downtown.

- “The Downtown Plan encourages walking as a primary mode of transportation in Downtown.” The DTC standards focus on the interaction between the building and street – the frontage of the building – to make the pedestrian realm safe, comfortable and interesting. This goal will be reached when public and private entities remain committed to creating a pedestrian-oriented Downtown.
- The Downtown Plan notes that “as Downtown becomes home to many of its workers, and mass transit options into Downtown improve, providing welcoming routes for

pedestrians will provide benefits in terms of improved mobility and reduced traffic congestion.” While the DTC sets standards on the development of private property, improved transportation options must be created by a collaborative process between the public and private sectors.

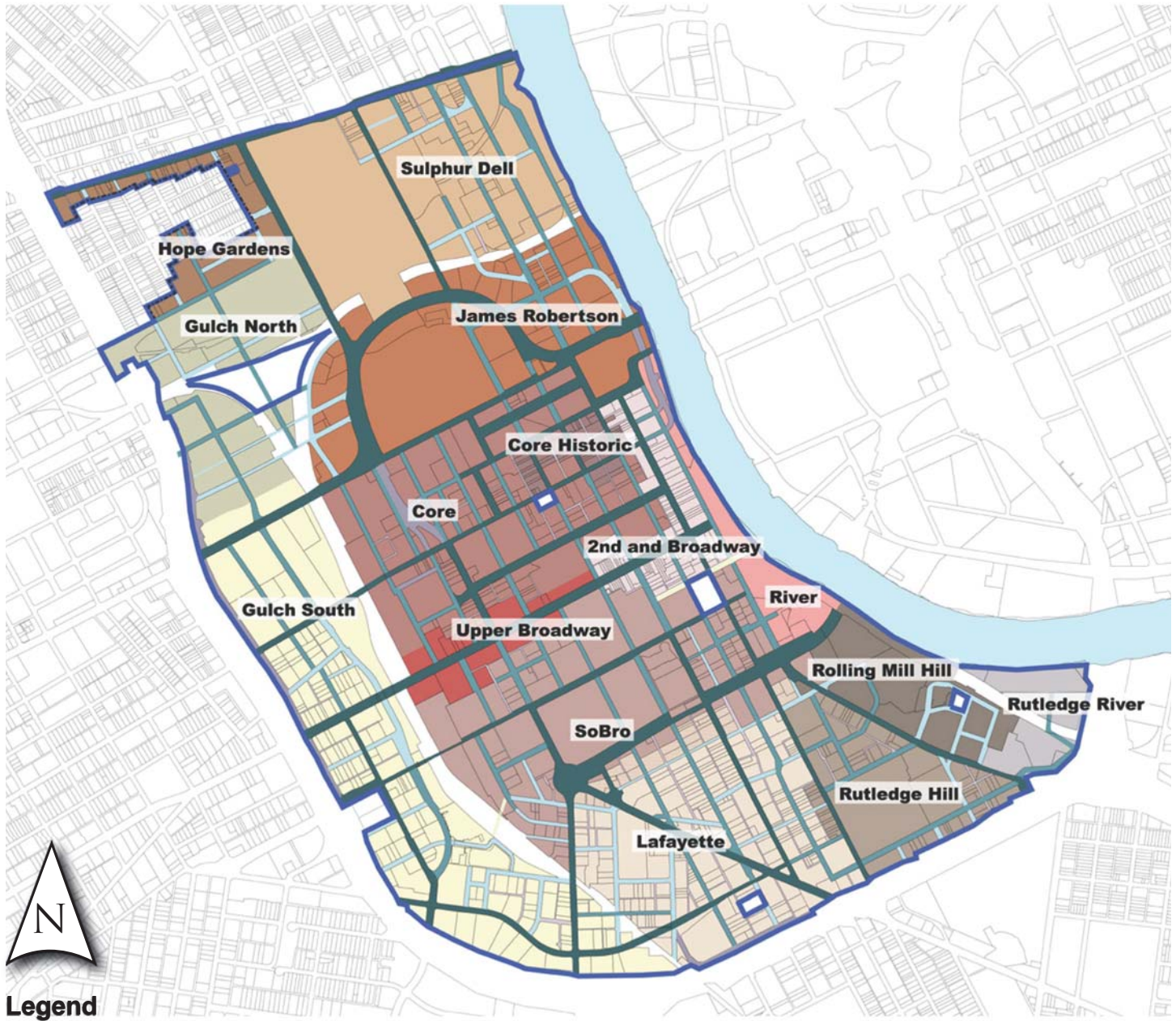
The Downtown Plan sets forth the common vision for the future of Downtown, and acknowledges that “the creation of the Downtown envisioned by the community can only be achieved through cooperative efforts of the public and private sectors and through the informed involvement of residents, businesses and investors in Downtown. Adherence to these guiding principles in the development actions of both the public and private sector will create the Downtown Nashville that the community has envisioned – an expanding, vibrant Downtown with opportunities for growth and development that embody the urban experience of a great city.” The DTC is one of several tools to strengthen Downtown through public and private investments.

By focusing on the creation of distinctive neighborhoods, pedestrian-oriented development, the DTC reaches toward the goal of an economically healthy, socially vibrant, and sustainable Downtown.

















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DTC Regulating Plan: Subdistrict Boundaries



Legend

| | | | |
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|  James Robertson Subdistrict - page 18 |  Upper Broadway Subdistrict - page 26 |  Lafayette Subdistrict - page 38 |  Gulch North Subdistrict - page 46 |
|  Core Subdistrict - page 20 |  SoBro Subdistrict - page 34 |  Rutledge Hill Subdistrict - page 40 |  Hope Gardens Subdistrict - page 50 |
|  Core Historic Subdistrict - page 22 |  River Subdistrict - page 32 |  Rutledge River Subdistrict - page 44 |  Sulphur Dell Subdistrict - page 52 |
|  2nd and Broadway Subdistrict - page 28 |  Rolling Mill Hill Subdistrict - page 42 |  Gulch South Subdistrict - page 48 |  DTC Boundary |

Section I: Introduction

Application of the DTC

General Provisions

If necessary, to adhere to the laws and regulations of Federal, State, or local departments or agencies, the regulations in this chapter may be modified. Such modifications may be approved by the Planning Commission, the DTC Design Review Committee or Planning Staff, in accordance with the Modifications section of this Chapter.

To the extent that the provisions of the Downtown Code is inconsistent or in conflict with the provisions of the Gateway Urban Design Overlay District that is also zoned DTC, the provisions of the DTC zoning shall be controlling; however, any provisions of the Gateway UDO may be used provided that the standards of the DTC zoning are met.

Applicable Chapters and Sections of the Zoning Code

In addition to the standards set forth within this document, the following Chapters and Sections of the Metro Zoning Code shall apply to properties with DTC zoning.

- All of Chapter 17.04 GENERAL PROVISIONS AND DEFINITIONS
- Within Chapter 17.08 ZONING DISTRICTS AND LAND USE TABLES
 - Section - 17.08.010 Zoning districts established.
 - Section - 17.08.020 Zoning districts described.
 - Section - 17.12.120 Transfer of development rights.
- All of Chapter 17.16 LAND USE DEVELOPMENT STANDARDS
- Within Chapter 17.20 PARKING, LOADING AND ACCESS
 - Section - 17.20.050 Handicapped parking.
 - Section - 17.20.060 Parking area design standards.
 - Section - 17.20.070 Queuing requirements for drive-through facilities.
 - Section - 17.20.130 Loading space requirements.
- Within Chapter 17.24 LANDSCAPING, BUFFERING AND TREE REPLACEMENT
 - Section - 17.24.010 Purpose and intent.
 - Section - 17.24.020 Landscape plan required.
 - Section - 17.24.030 Standards for form and quality of plants.
 - Section - 17.24.040 Spacing standards.
- Article II. Tree Protection and Replacement
 - Section - 17.24.090 Removal of protected trees.
 - Section - 17.24.100 Replacement of trees.
 - Section - 17.24.110 Protection of trees during development activities.
 - Section - 17.24.120 Less desirable trees.
 - Section - 17.24.160 Interior planting requirements.
 - Section - 17.24.170 Nonconforming parking areas.
- All of Chapter 17.28 ENVIRONMENTAL AND OPERATIONAL PERFORMANCE STANDARDS
- All of Chapter 17.32 SIGN REGULATIONS
- All of Chapter 17.36 OVERLAY DISTRICTS, except Article XII, Urban Zoning Overlay (UZO) District.
- All of Chapter 17.40 ADMINISTRATION AND PROCEDURES, except as otherwise provided for within this document.

Section I: Introduction

Application of the DTC: How to Use this Document

How to Use this Document

The Downtown Code is organized by Subdistricts and Street Types, as identified on the Regulating Plan.

To determine the standards which apply to a particular property:

- On the Regulating Plan, identify the Subdistrict in which the property is located and on what type of street(s) it fronts.
- Consult the Building Regulations for development standards relevant to the Subdistrict.
- Consult the Use Table for uses allowed in each Area.
- Consult the General Standards section for guidance on development standards for all Subdistricts.

Subdistricts and Areas

- Downtown Nashville consists of numerous neighborhoods with unique character. The development standards for the DTC are organized by Subdistricts, which establish

the zoning of each neighborhood to create or maintain the envisioned character.

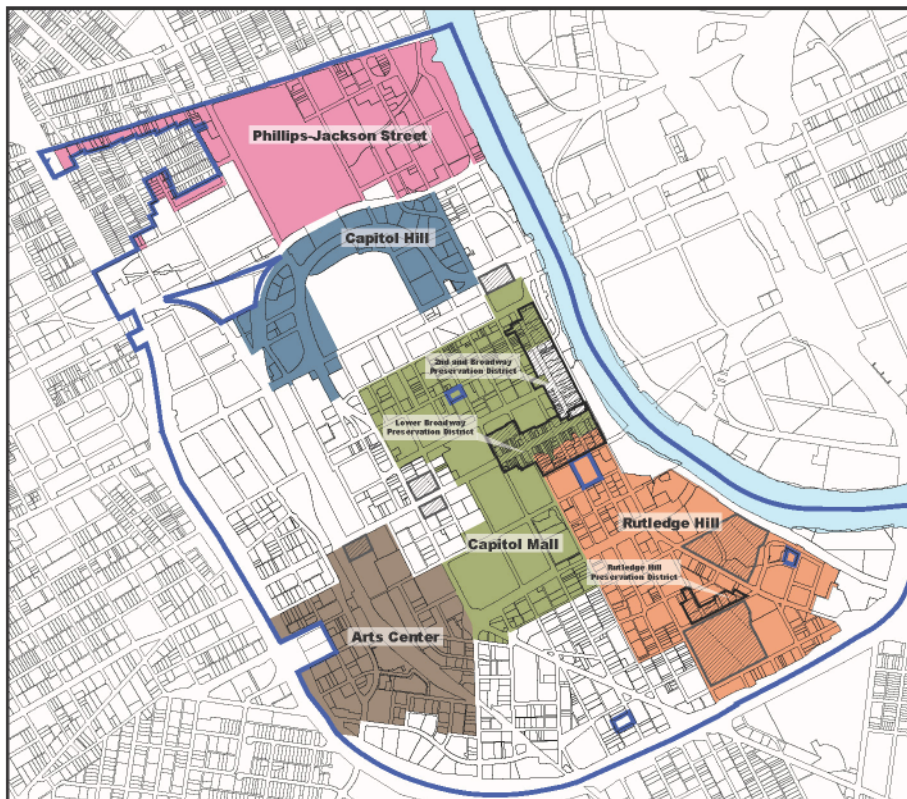
- The DTC is divided into 4 Areas (North, South, West and Central), which establish the allowed uses.
- The DTC includes General Standards that apply to all Subdistricts.

Regulating Plan

- The Regulating Plan is the official zoning map of the DTC. The Subdistrict boundaries are shown on the series of maps which comprise the Regulating Plan.

Overlapping Plans

Within the area governed by the DTC, there exist other regulations and design guidelines intended to work in conjunction with the DTC. The DTC does not exempt development from complying with the regulations of other Federal, State, and Local departments and agencies. These departments and agencies should be contacted during the development process to address their rules, regulations and policies.



MDHA redevelopment districts and Historic overlays at the time of adoption of the DTC.

Section I: Introduction

Application of the DTC: Modifications and Design Review

Modifications to the Standards

Based on site-specific issues, an applicant may seek modifications to the standards of this document. Any standard within the DTC may be modified, insofar as the intent of the standard is being met, the modification results in better urban design for the neighborhood as a whole, and the modification does not impede or burden existing or future development of adjacent properties.

The DTC, the Downtown Plan and any other policies and regulations from governing agencies shall be consulted when considering modifications. Any standards that shall not be modified are explicitly noted in this document.

Modifications may be approved by Planning staff, the Downtown Code Design Review Committee (DTC DRC) or the Planning Commission.

- Minor modifications – deviations of 20 percent or less – may be approved by Planning Staff.
- Any determination made by the Planning Staff may be appealed to the DTC DRC.
- Major modifications – deviations of more than 20 percent – and modifications of standards without numbers may be approved by the DTC DRC. Within MDHA redevelopment districts, the MDHA DRC shall act as the DTC DRC.
- Any determination made by the DTC DRC or the MDHA DRC regarding standards of the DTC may be appealed to the Planning Commission.
- **No modification shall be granted for overall building height.**

Variances and special exceptions

Variances and special exceptions that are not specifically for standards of the DTC zoning district shall follow the procedures of Chapter 17.40, Articles VII and VIII of the zoning code.

Additionally, ~~variances and special exceptions~~ to the DTC standards on ~~height at the street and~~ overall height shall follow the procedures of Chapter 17.40, Articles VII and VIII. The DTC DRC shall provide a recommendation to the Board of Zoning Appeals (BZA) on such cases.

Standards specific to the DTC zoning district may be modified based on the modifications section of this document.

Design Review Committee

Applications that meet all applicable standards of the DTC shall be reviewed by staff before building permits are granted. Such “by-right” applications will not be reviewed by the DTC DRC. The DTC DRC will review applications seeking modifications.

The DTC DRC is subject to the rules and procedures adopted by the Planning Commission. The DTC DRC will consist of eight (8) voting members.

One member shall be nominated by each of the following with confirmation by the Planning Commission:

- Chamber of Commerce
- Civic Design Center
- Downtown Partnership
- Urban Residents Association

One member shall be appointed by the following:

- Mayor
- Vice-Mayor, on behalf of the Metro Council
- Historic Commission
- Planning Commission

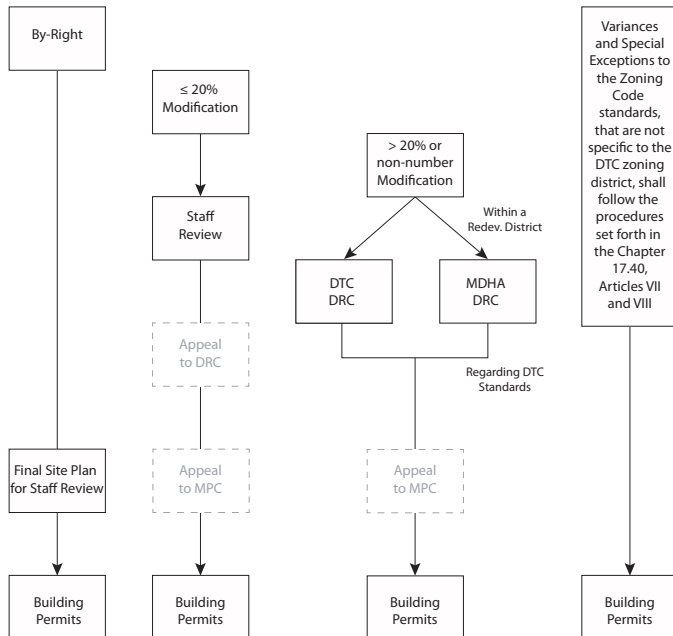
Each member shall be a design professional, with a degree or several years of experience in architecture, landscape architecture, planning or urban design. The members nominated by the Nashville Area Chamber of Commerce and the Nashville Downtown Partnership shall be exempt from this requirement.

Each term shall be four years, with the appointees of the Mayor, Vice-Mayor and Planning Commission serving an initial two-year term upon adoption of the DTC.

Any vacancy occurring during the unexpired term of any members shall be filled in the manner prescribed herein for the original selection of the members of the DTC DRC.

Section I: Introduction

Application of the DTC: Compliance



Compliance with the DTC shall be required according to the following:

- Level 1: An addition of twenty-five percent or less of the square footage of the existing building
 - To the extent practicable, any addition shall be in compliance with applicable standards of the DTC.
- Level 2: An addition of more than twenty-five percent of the square footage of the existing building or 1000 square feet, whichever is greater
 - Improvements to the sidewalk **corridor** according to the *Downtown Streetscape Design Guidelines Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning)*
 - Where possible, the addition of street trees
 - Parking lot landscaping standards
 - To the extent practicable, any addition shall be in compliance with applicable standards of the DTC.
- Level 3: Redevelopment after the demolition or destruction of more than five percent and less than fifty percent of the existing building
 - Improvements to the sidewalk **corridor** according to the *Downtown Streetscape Design Guidelines Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning)*
 - Where possible, the addition of street trees
 - Parking lot landscaping standards
 - To the extent practicable, new construction shall be in compliance with applicable standards of the DTC.
- Level 4: Redevelopment after the demolition or destruction of more than fifty percent of the existing building
 - All standards of the DTC
 - Improvements to the sidewalk **corridor** according to the *Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning)*
- Level 5: New construction of buildings, parking, open space, etc
 - All standards of the DTC
 - Improvements to the sidewalk **corridor** according to the *Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning)*

Signage Compliance

- For those properties within Historic Zoning Overlays, the sign standards of the CF zoning district shall apply, along with historic zoning district design guidelines.
- For those properties not within Historic Zoning Overlays, the sign standards of the DTC zoning district shall apply.
- No new billboards are allowed within the DTC boundaries.

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Section II: Subdistrict Standards

Amended by Ordinance No. BL2012-142
as adopted on May 21, 2012

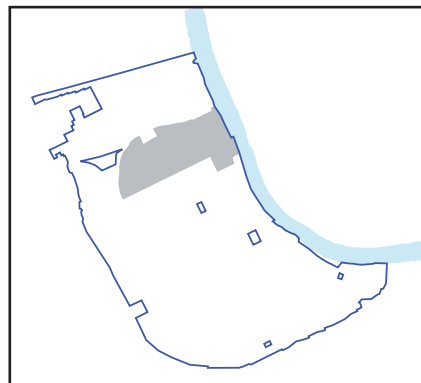
Amended by Ordinance No. BL2011-896
as adopted on May 26, 2011







Section II: Subdistrict Standards

James Robertson Subdistrict: Regulating Plan



The James Robertson Subdistrict is the primarily civic area surrounding Capitol Hill. The Downtown Plan emphasizes “preserving the James Robertson neighborhood’s treasured civic and open space resources, while encouraging redevelopment to offer a mixture of uses...while recognizing that the area’s uses will continue to be dominated by government activities.” The importance of the State Capitol as a Nashville and Tennessee landmark also warrants maintaining views of the Capitol from all vantage points to the west, north and east. The DTC allows a maximum height of 560’ above sea level (the elevation of the base of the capitol building) to preserve these views.



| Legend | |
|---|-----------------------------|
|  | James Robertson Subdistrict |
|  | Primary Street |
|  | Secondary Street |
|  | Tertiary Street |
|  | Other Street |
|  | Alley |

Section II: Subdistrict Standards

James Robertson Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

Primary Street

- Storefront Frontage
 - James Robertson Boulevard
 - West of 3rd Ave 20'-30'
 - East of 3rd Ave 0'-10'
 - Charlotte Avenue 0'-10'
- Stoop Frontage
 - James Robertson Boulevard
 - West of 3rd Ave 20'-30'
 - East of 3rd Ave 5'-10'
 - Charlotte Avenue 5'-10'

Secondary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'

Tertiary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'

B Facade width

- | | |
|------------------|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 80% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |
- Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

C Min. building depth 15' from building facade

A building liner is required surrounding parking structures on the all floors facing James Robertson Blvd.

Height

D Max. elevation of 560'

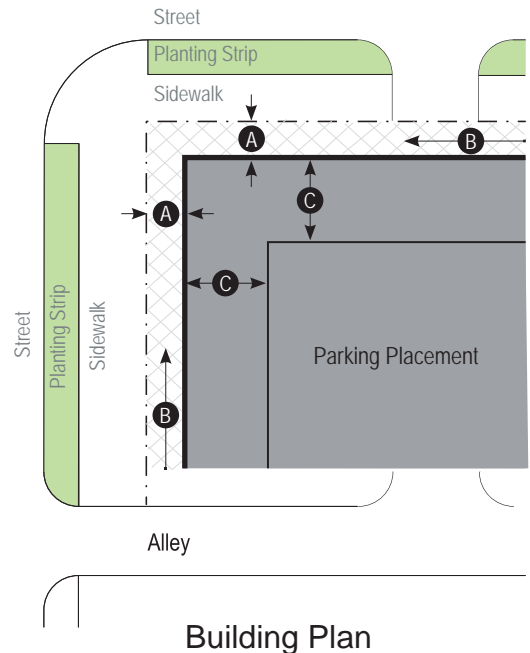
Step-back

Step-back required on all public streets and Open Space

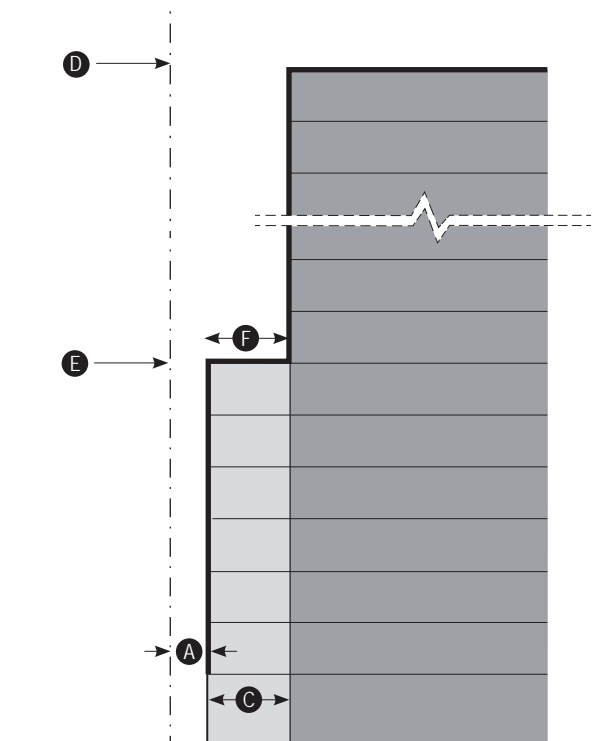
- E Step-back after between 4th and 8th 7 stories
- F Min. step-back depth 15'

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



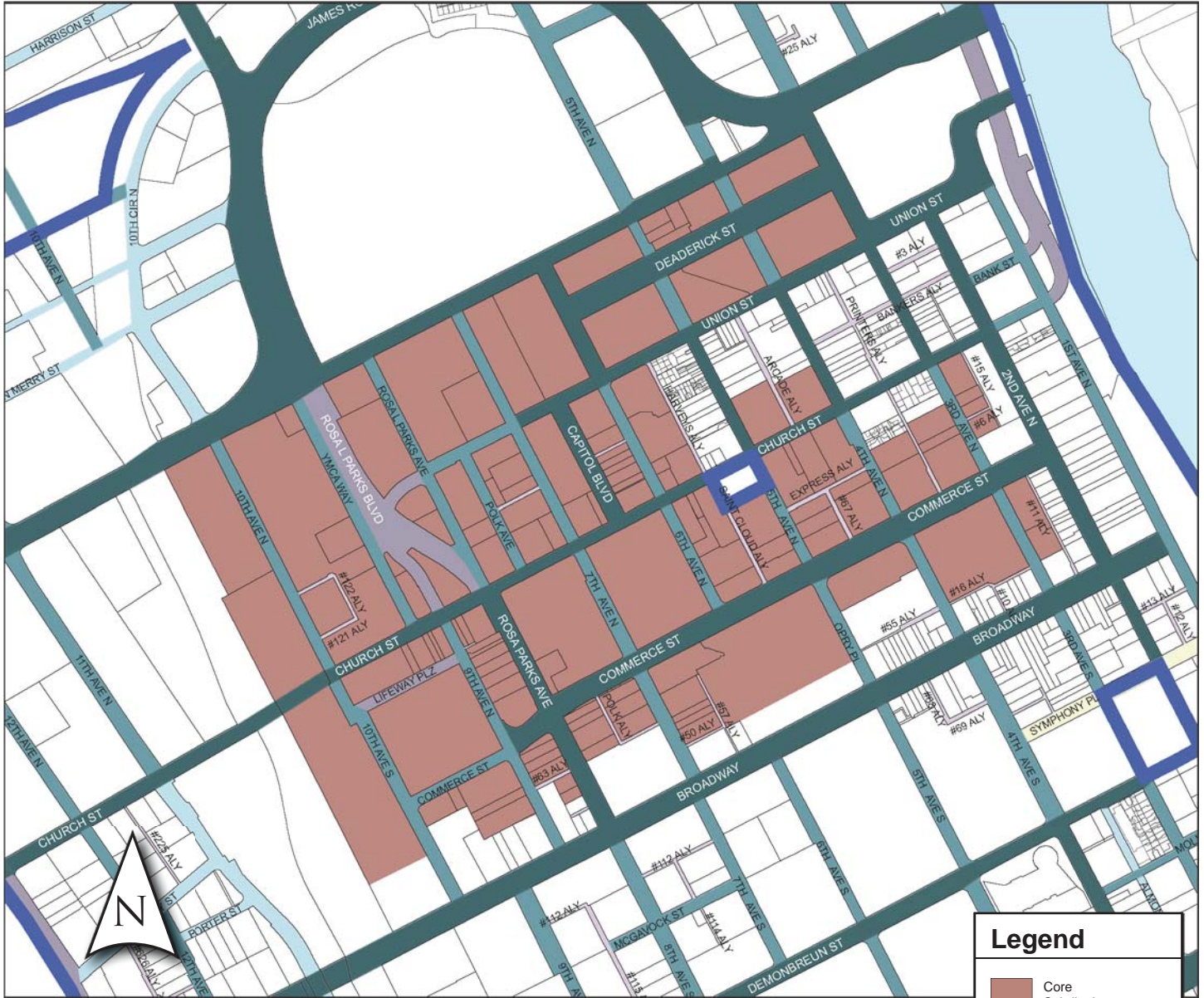
Building Plan



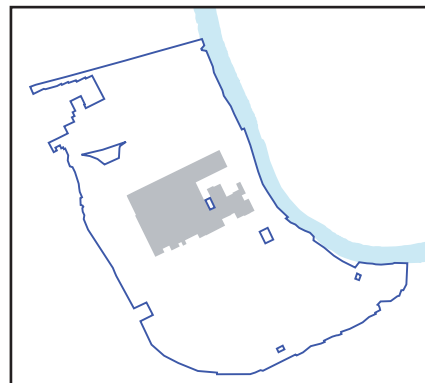
Building Section

Section II: Subdistrict Standards

Core Subdistrict: Regulating Plan



The Core is the heart of the Downtown business district, the economic engine of the Middle Tennessee region, and a significant economic force in the Southeast. It is the densest neighborhood in Downtown and has the greatest height allowances. Pedestrian comfort and safety should be prioritized with an interesting sidewalk realm, activity on the ground level of buildings, and controlled vehicular access.



| Legend | |
|--------|------------------|
| | Core Subdistrict |
| | Primary Street |
| | Secondary Street |
| | Tertiary Street |
| | Other Street |
| | Alley |

Section II: Subdistrict Standards

Core Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|--------|
| Primary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Tertiary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |

B Facade width

| | |
|--|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 80% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |
| Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking. | |

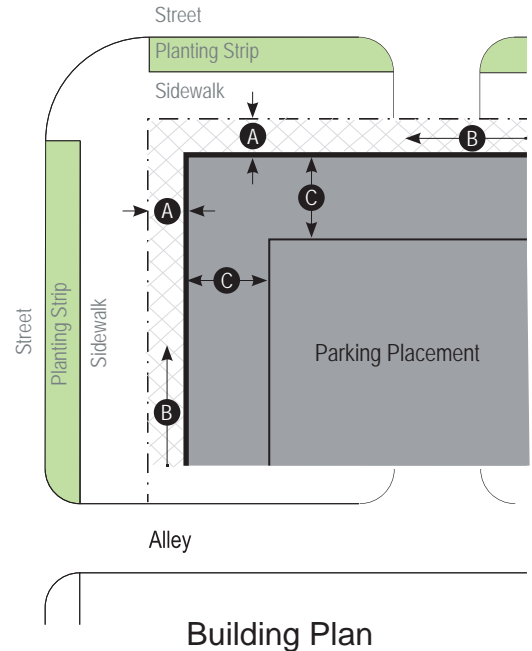
C Min. building depth 15' from building facade

Height

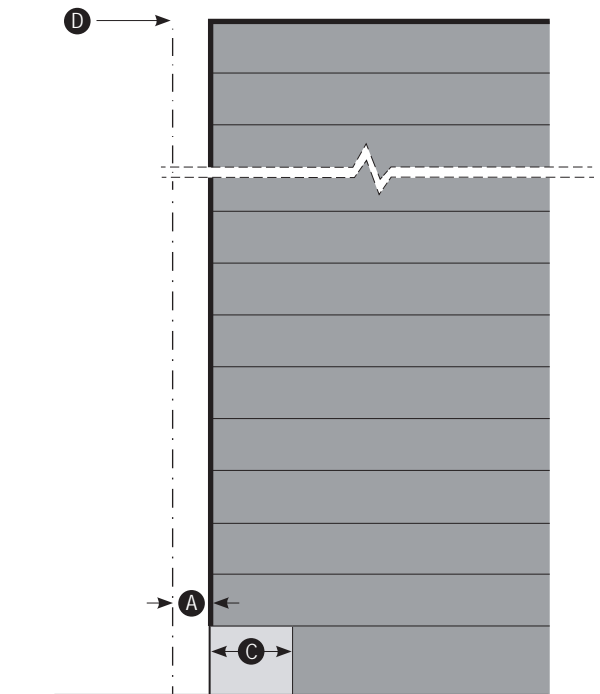
D Max. 30 stories Additional height available through the Bonus Height Program

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



Building Plan



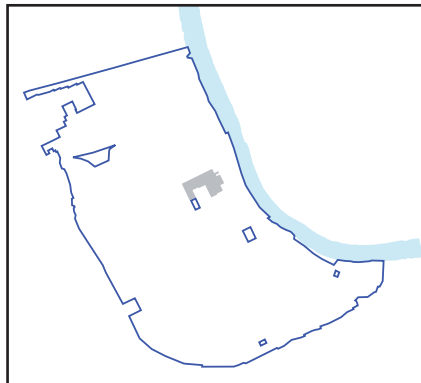
Building Section

Section II: Subdistrict Standards

Core Historic Subdistrict: Regulating Plan



The Core Historic neighborhood has two historic urban spaces – the Arcade and Printers’ Alley. This neighborhood is comprised several historic buildings, many of which have been recently renovated. The height maximums for this subdistrict reflect historic urban design features – lower buildings mid-block and taller buildings to “book-end” the blocks at the corners. The adaptive reuse of historic buildings is encouraged and new construction should be of appropriate scale and detailing, maintaining the existing storefront rhythm. Pedestrian comfort and safety should be prioritized with an interesting sidewalk realm, activity on the ground level of buildings, and controlled vehicular access.



| Legend | |
|--|---------------------------|
| | Core Historic Subdistrict |
| | Primary Street |
| | Secondary Street |
| | Tertiary Street |
| | Other Street |
| | Alley |
| | Pedestrian Frontage |

Section II: Subdistrict Standards

Core Historic Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|--------|
| Primary Street | |
| • Storefront Frontage | 0' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0' |
| • Stoop Frontage | 5'-10' |

B Facade width

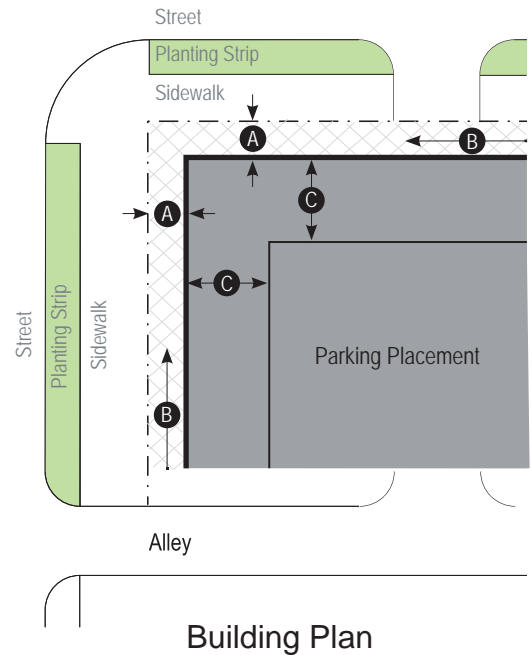
| | |
|------------------|--------------------------|
| Primary Street | 95% of lot frontage min. |
| Secondary Street | 95% of lot frontage min. |

C Min. building depth 15' from building facade

A building liner is required surrounding parking structures on all floors facing public streets and Open Space.

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



Section II: Subdistrict Standards

Core Historic Subdistrict: Building Regulations

Height

| | |
|---------------|------------|
| D Max. | |
| On Corners | 10 stories |
| Mid-Block | 6 stories |

Step-back

Step-back after

| | | |
|----------|----------------------|-----------|
| E | • On Printer's Alley | 4 stories |
| F | • All Others | 6 stories |

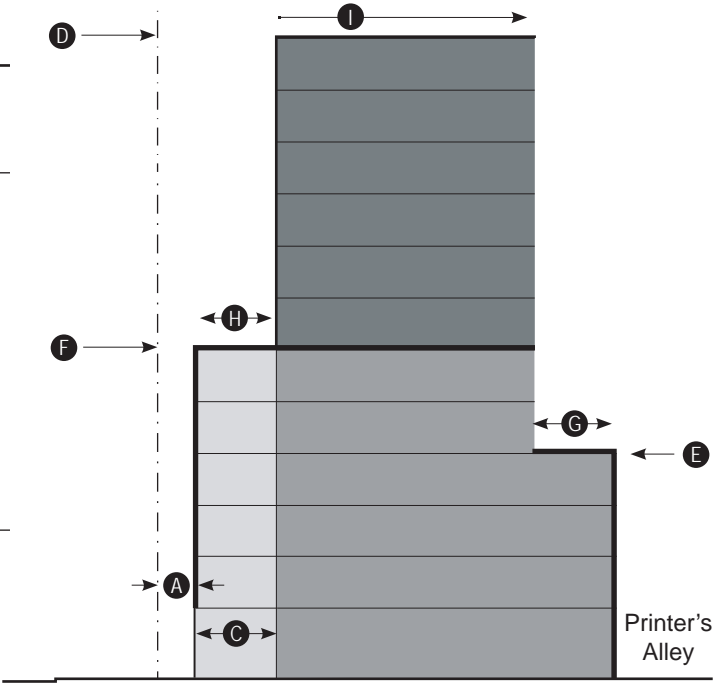
Depth

| | | |
|----------|----------------------|-------------------|
| G | • On Printer's Alley | 10' min. |
| H | • All Others | 10' min. and max. |

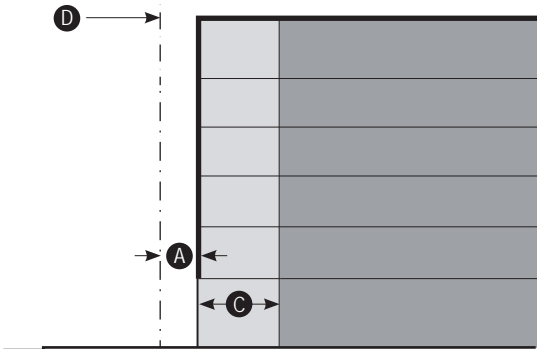
Step-back not required for buildings fronting Church Street.

Buildings 6 stories or less shall not step-back and all stories shall occupy the Build-to Zone.

| | |
|--------------------------------|-----------|
| I Max. tower dimensions | 90' x 90' |
|--------------------------------|-----------|



Block Corner Section



Mid-Block Section

Section II: Subdistrict Standards

Core Historic Subdistrict: Building design and Facade articulation

Building design and Facade articulation

- | | |
|---------------------------------------|--|
| L Primary building divisions | 20'-50' wide |
| M Secondary building divisions | 5'-25' wide Secondary building divisions are defined by solid vertical elements that consist of changes in materials or planes within the facade. |
| N First Floor height | 16' min. |
- Windows**
- O** Ground floor - 60% glazing required from 2 feet above grade to the finished floor of the 2nd story
 - P** Window sill height 18"-24"
- Q** Upper Floors - Windows shall be vertically oriented at a ratio of 2:1 or greater.

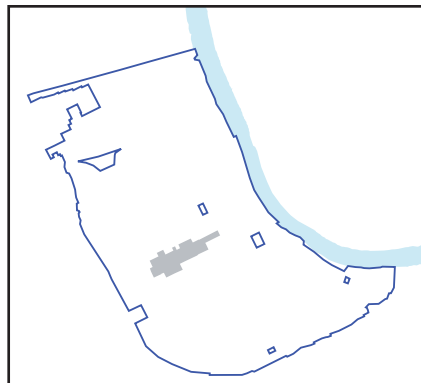


Section II: Subdistrict Standards

Upper Broadway Subdistrict: Regulating Plan



The Upper Broadway area is one of the most important gateways into Downtown. Several civic and cultural buildings front this urban corridor. Maintenance and adaptive reuse of historic buildings is encouraged, and the height and scale of new buildings should be in-keeping with the existing urban pattern. When properties front more than one street, Broadway should be the Principal frontage.



Legend

- Upper Broadway Subdistrict
- Primary Street
- Secondary Street
- Other Street
- Alley

Section II: Subdistrict Standards

Upper Broadway Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

Primary Street

- Storefront Frontage 0-5'

Secondary Street

- Storefront Frontage 0'-5'

** Civic Frontages are encouraged in this subdistrict.

B Facade width

Primary Street 80% of lot frontage min.

Secondary Street 80% of lot frontage min.

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

C Min. building depth 15' from building facade

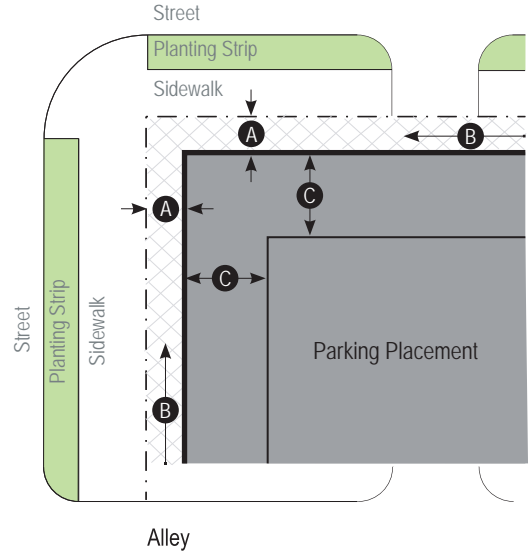
A building liner is required surrounding parking structures on all floors facing Broadway and Open Space.

Height

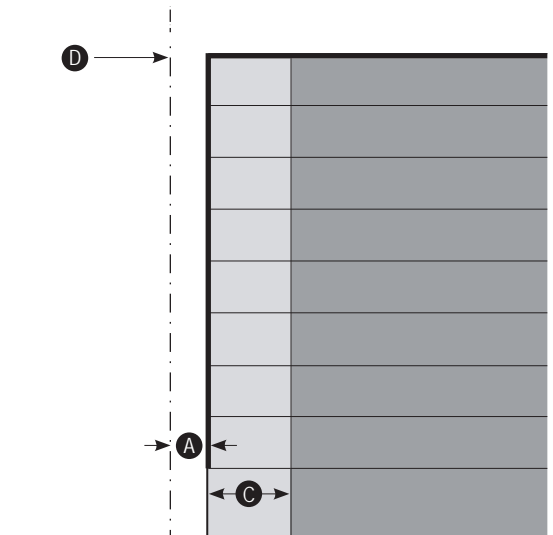
D Max. 100'

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



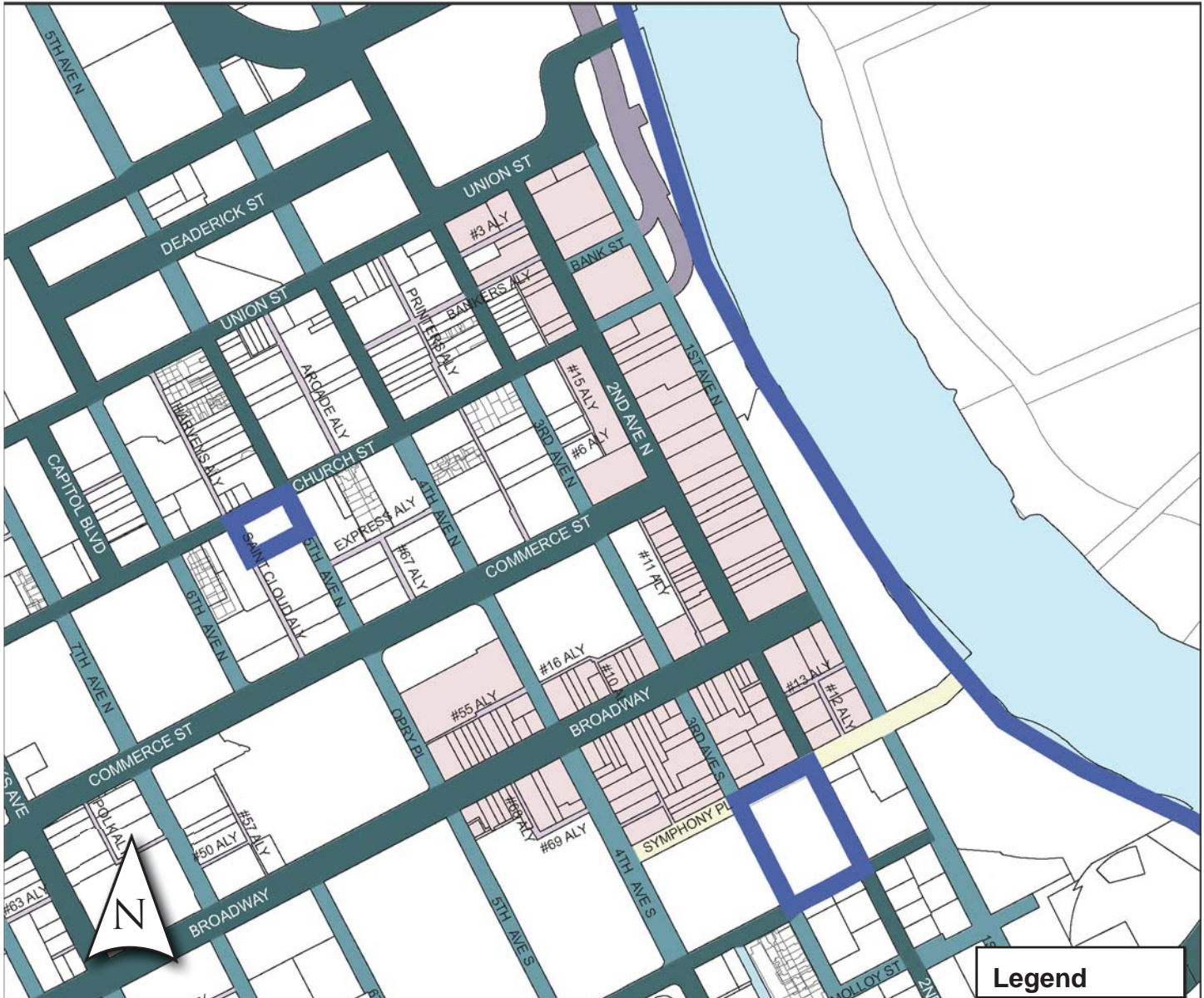
Building Plan



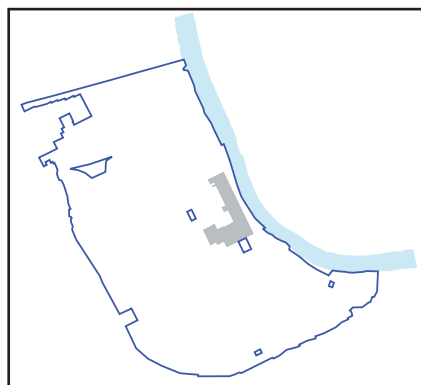
Building Section

Section II: Subdistrict Standards

2nd and Broadway Subdistrict: Regulating Plan



Second and Broadway is the heart of Downtown; where the main street of the city meets the Cumberland River. The Downtown Plan encourages maintaining “the low-scale, pedestrian-friendly historic character” and adaptively reusing existing historic structures “in order to respect, maintain, and enhance not only individual structures, but the existing character of the Second and Broadway neighborhood as a whole.” With the exception of the Stahlman building on the northeast corner of 3rd and Union, this neighborhood is overseen by two historic zoning overlays: *The Second Avenue Historic Zoning Overlay* and *The Broadway Historic Zoning Overlay*. Property owners must contact the Metro Historic Commission for additional details.



| Legend | |
|--------|------------------------------|
| | 2nd and Broadway Subdistrict |
| | Primary Street |
| | Secondary Street |
| | Other Street |
| | Alley |
| | Pedestrian Frontage |

Section II: Subdistrict Standards

2nd and Broadway Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|----|
| Primary Street | |
| • Storefront Frontage | 0' |
| Secondary Street | |
| • Storefront Frontage | 0' |

B Facade width

| | |
|------------------|---------------------------|
| Primary Street | 100% of lot frontage min. |
| Secondary Street | 100% of lot frontage min. |

C Min. building depth 15' from building facade

Height

D Min. 40'

E Max. at the street

| | |
|---------------------|-------------------------------------|
| • On Broadway | 5 stories to a max. height of 65' |
| • On 2nd Ave | 8 stories to a max. height of 105' |
| • On Union St | 12 stories to a max. height of 180' |
| • All other streets | 8 Stories to a max. height of 105' |

F Min. step-back depth

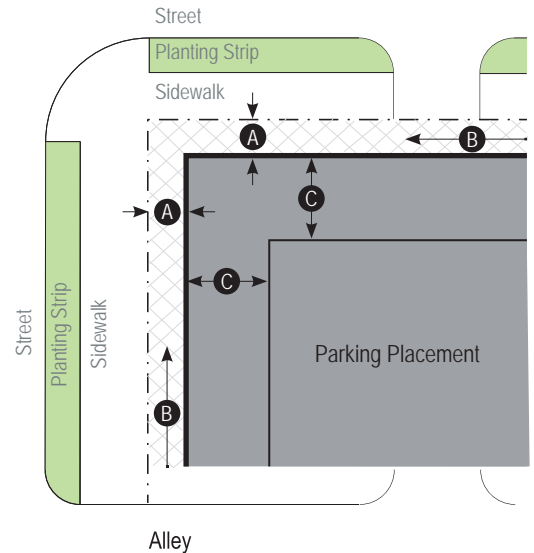
| | |
|------------------------|-----|
| • On Broadway | 30' |
| • On all other streets | 20' |

G Max. height 1 additional story

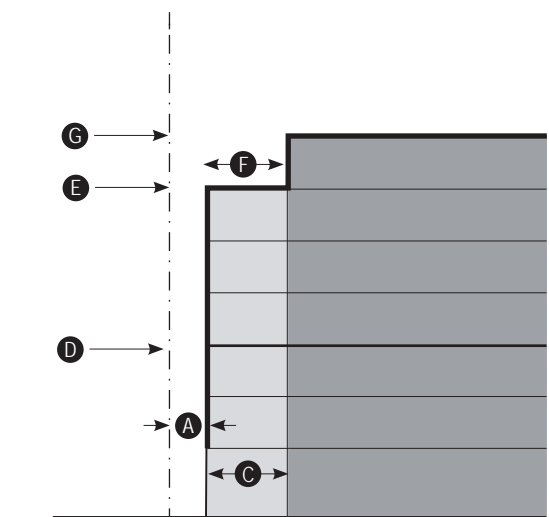
| |
|--|
| • Within 150' of the right-of-way of Broadway, height shall not exceed 6 stories or 80' |
| • Between 150' and 200' of the right-of-way of Broadway, height shall not exceed 7 stories or 90'. |

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



Building Plan



Building Section

Section II: Subdistrict Standards

2nd and Broadway Subdistrict: Building design and Facade articulation

Building design and Facade articulation

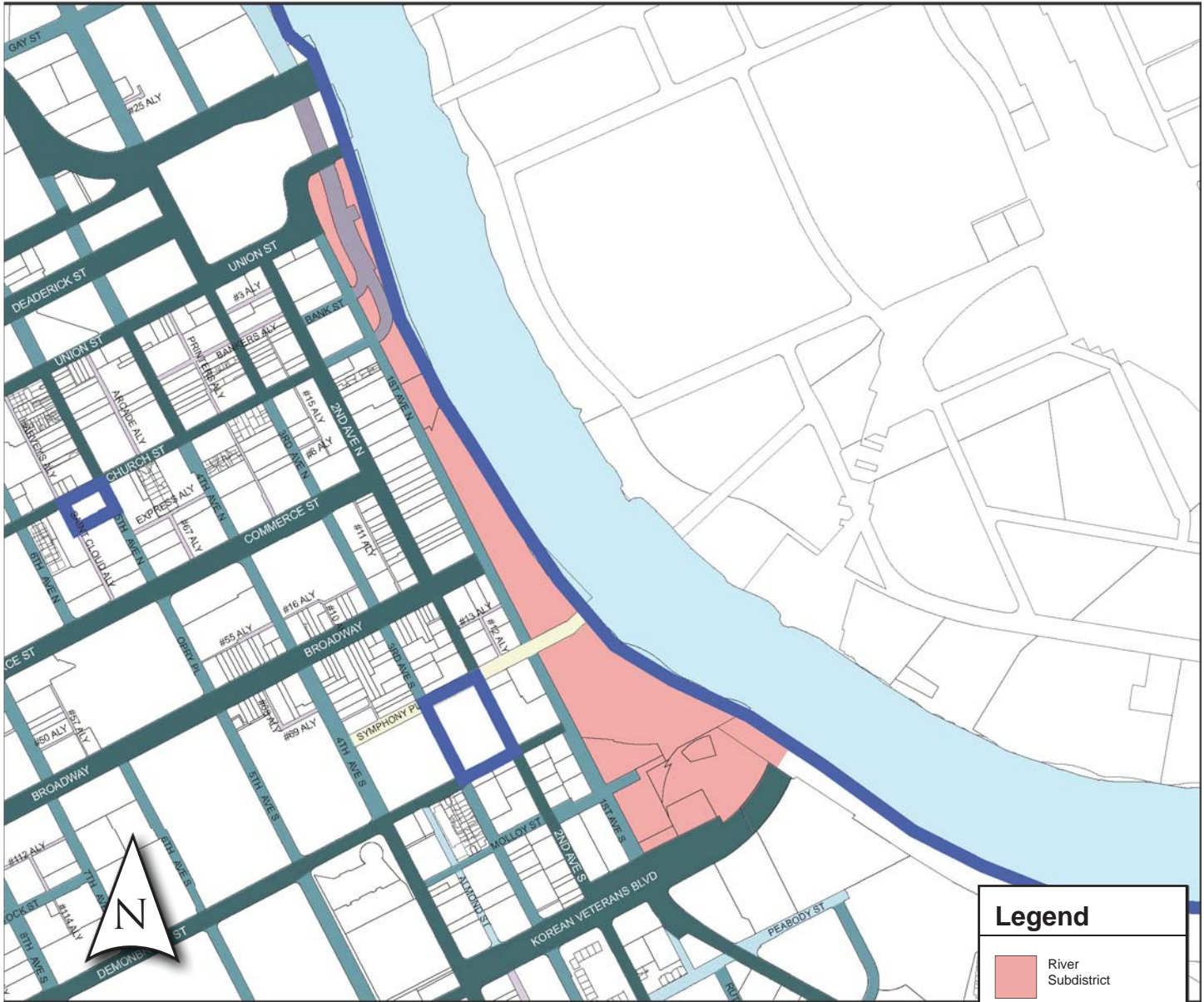
-
- L Primary building divisions** 20'-50' wide
-
- M Secondary building divisions** 5'-25' wide
Secondary building divisions are defined by solid vertical elements that consist of changes in materials or planes within the facade.
-
- N First Floor height** 16' min.
-
- Windows**
- O Ground floor - 60% glazing required from 2 feet above grade to the finished floor of the 2nd story**
 - P Window sill height** 18"-24"
-
- O Upper Floors - Windows shall be vertically oriented at a ratio of 2:1 or greater.**



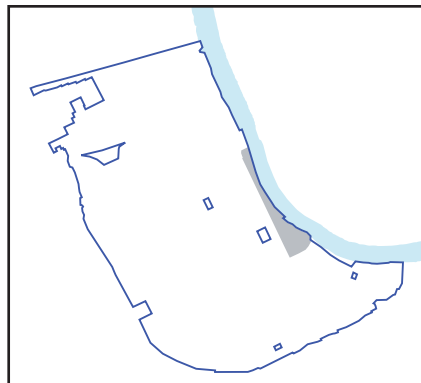
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Section II: Subdistrict Standards

River Subdistrict: Regulating Plan



The River subdistrict is a unique area between 1st Avenue South and the Cumberland River. This area is owned by the city and is envisioned to be an amenity for all residents and visitors. New development should be done in conjunction with the Parks Department's *Riverfront Redevelopment Plan* and should treat the river as an amenity while keeping a strong urban edge along 1st Avenue South and the boulevard. The maximum height is low in order to maintain views of the river from many vantage points in Downtown.



| Legend | |
|--|---------------------|
| | River Subdistrict |
| | Primary Street |
| | Secondary Street |
| | tertiary Street |
| | Other Street |
| | Alley |
| | Pedestrian Frontage |

Section II: Subdistrict Standards

River Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|--------|
| Primary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Tertiary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-15' |

B Facade width

| | |
|--|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 80% of lot frontage min. |
| 1st Avenue | 70% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |
| Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking. | |

C Min. building depth 15' from building facade

Height

D Max. 10 stories ~~to a max. height of 145'~~

Step-back

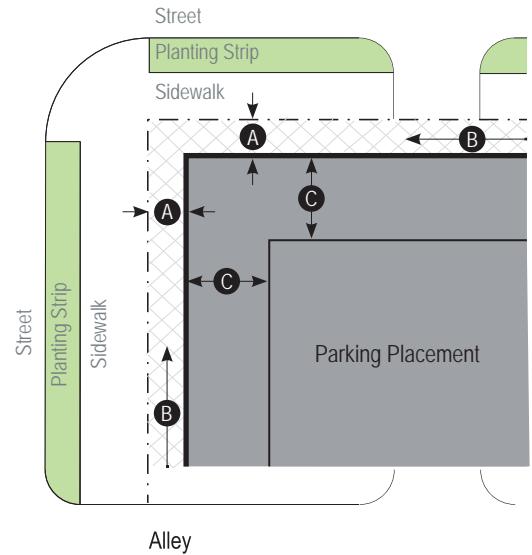
Step-back required on 1st Avenue frontage

E Step-back ~~after between 4th and 8th stories within a max. height of 105'~~

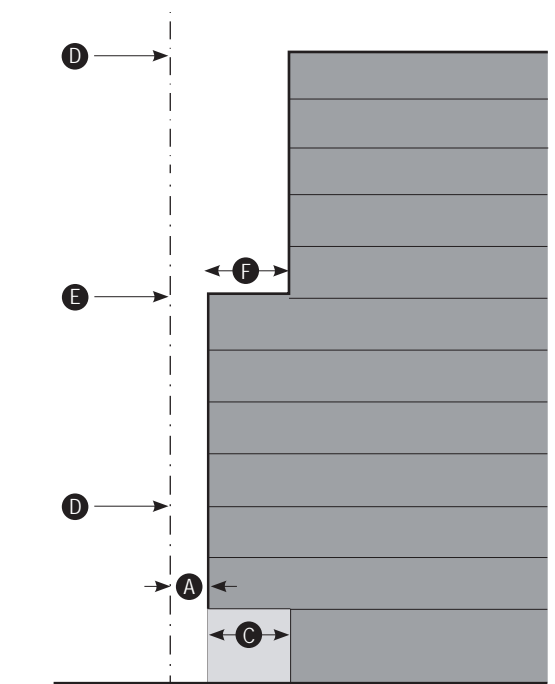
F Min. step-back depth 15'

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



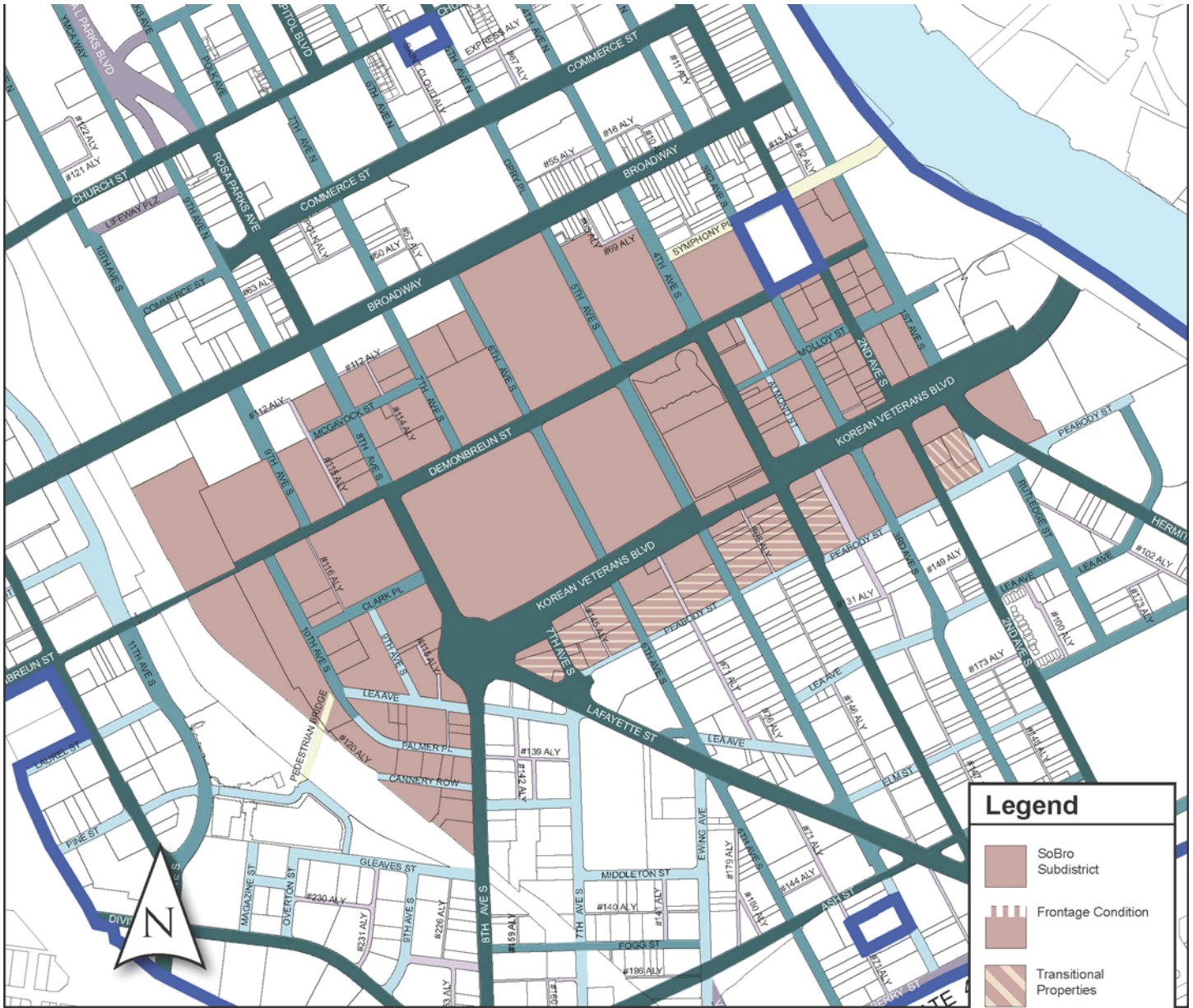
Building Plan



Building Section

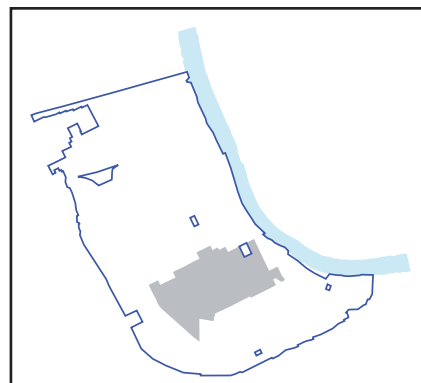
Section II: Subdistrict Standards

SoBro Subdistrict: Regulating Plan



The SoBro neighborhood is intended to be a high-intensity, mixed-use neighborhood emphasizing cultural and entertainment uses with a mix of residential and office uses. SoBro is an extension of the Core in height and intensity, but it is also a transition to the lower, mid-rise Lafayette neighborhood.

Properties with frontage on the boulevard, and Transitional Properties that consolidate to have frontage on the boulevard, shall be part of the SoBro Subdistrict. Properties south of the boulevard – Transitional Properties – without frontage on the boulevard, shall be part of the Lafayette Subdistrict.



| Legend | |
|--------|-------------------------|
| | SoBro Subdistrict |
| | Frontage Condition |
| | Transitional Properties |
| | Primary Street |
| | Secondary Street |
| | Tertiary Street |
| | Other Street |
| | Alley |
| | Pedestrian Frontage |

Section II: Subdistrict Standards

SoBro Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|--------|
| Primary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Tertiary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-15' |

B Facade width

| | |
|--|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 80% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |
| Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking. | |

C Min. building depth

15' from building facade

Height

D Min.

| | |
|--------------------|------------------|
| • On the Boulevard | 3 stories or 35' |
|--------------------|------------------|

E Max.

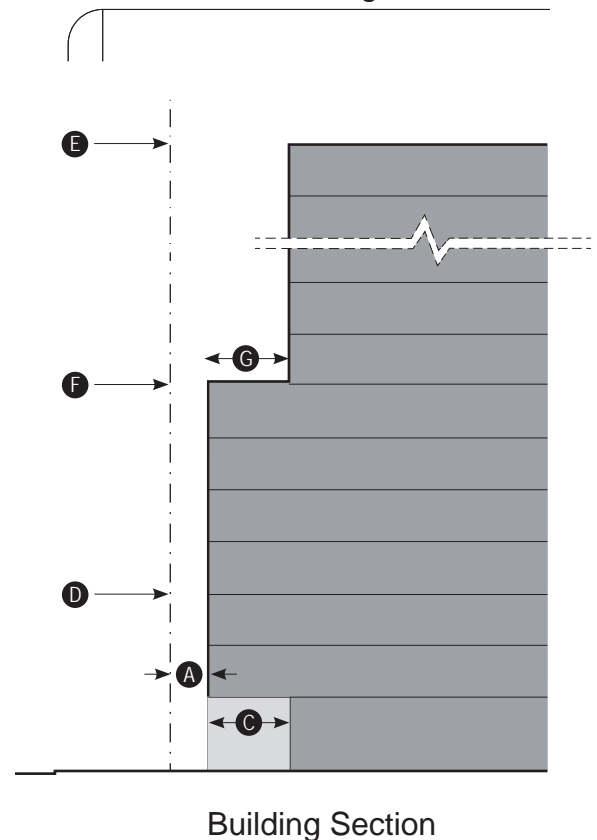
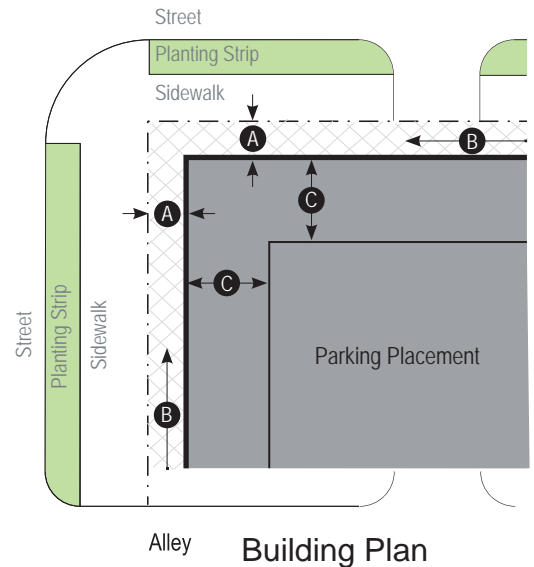
| | |
|---|---|
| • West side of 1st Ave and east side of 2nd Ave unless fronting the Boulevard | 15 stories to a max. height of 220' |
| • 8th Avenue Frontage, south of the roundabout | 8 stories within 100' of 8th Ave; 30 stories beyond 100' of 8th Ave stories |
| • Subdistrict general | 30 stories |
| Additional height available through the Bonus Height Program | |

F Step-back

| | |
|--------------------------------|-------------------------------|
| Step-back after between | |
| • Buildings taller than 150' | 4th and 8th stories-105' feet |

G ~~Properties on 1st Ave, 2nd Ave, and the Boulevard~~

| | |
|----------------------|-----|
| Min. step-back depth | 15' |
|----------------------|-----|



Notes

All standards of the Gateway UDO shall apply to development along the boulevard except maximum height at the street, step-back depth, overall height, and floor area ratio. The review process for the DTC and the UDO will be consolidated.

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

Building Regulations - Fronting Roundabout



- Buildings fronting the future roundabout shall respond to the circle with a facade perpendicular to the radius of the roundabout or a curved facade concentric with the roundabout.
- Curvilinear architectural elements such as arcades are encouraged.
- Only Storefront Frontage shall be permitted fronting the roundabout.

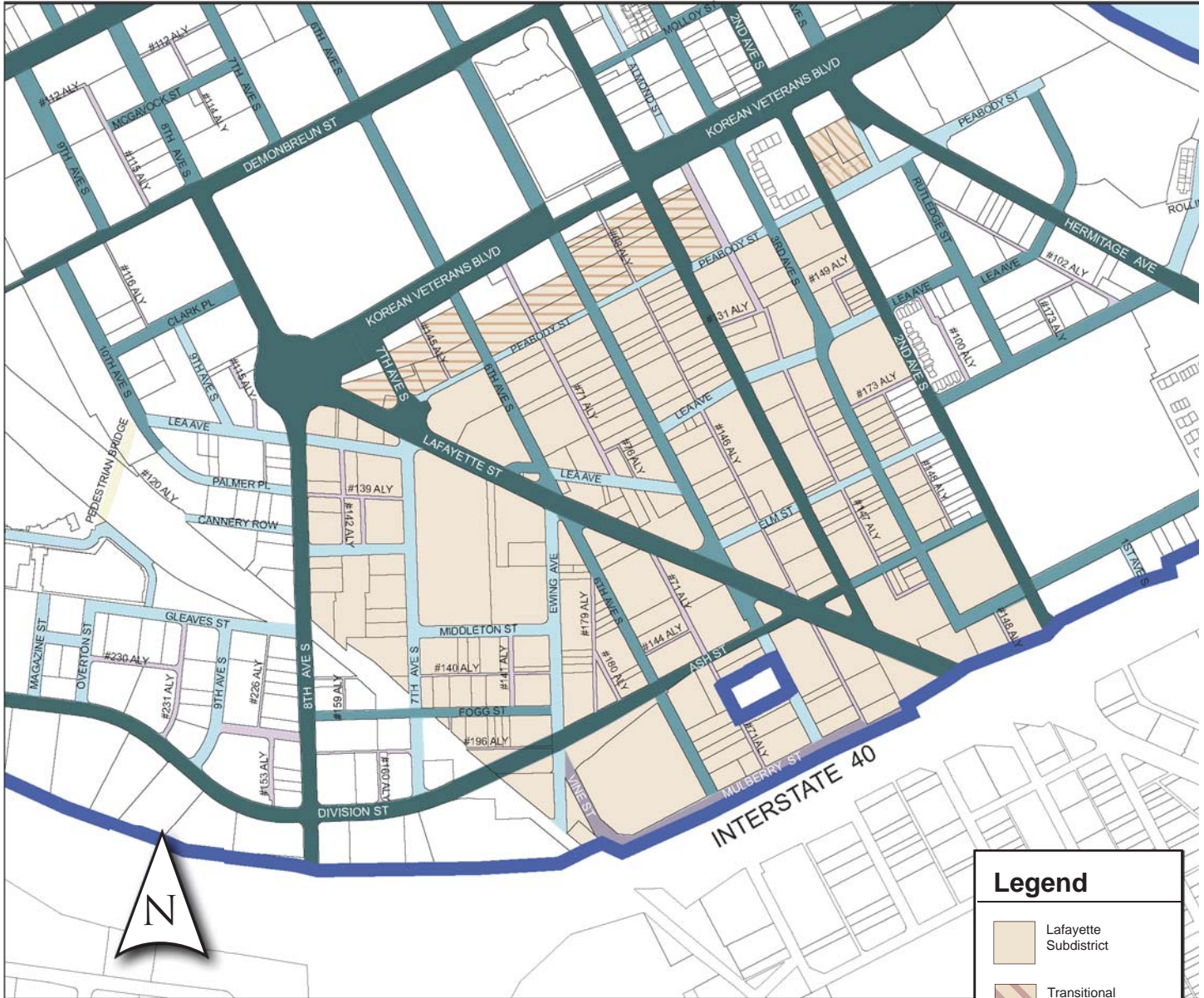
Legend

| | |
|---|------------------|
|  | Primary Street |
|  | Secondary Street |
|  | tertiary Street |
|  | Other Street |
|  | Alley |

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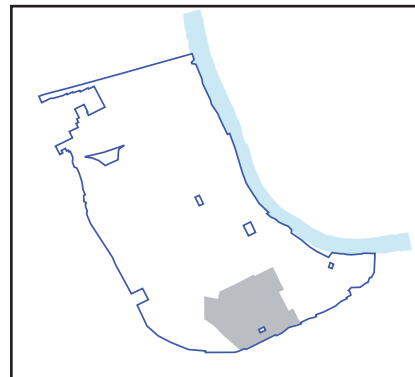
Section II: Subdistrict Standards

Lafayette Subdistrict: Regulating Plan



The Lafayette neighborhood is currently a primarily industrial and business services environment with strong transportation connections to the Gulch, SoBro, Midtown, and South Nashville neighborhoods. This neighborhood will likely retain many of the industrial and business service uses while accommodating a greater variety of uses. The neighborhood is primarily low-rise and should act as a transition from the height of the Core and SoBro to the single-family neighborhoods to the South.

Transitional properties that consolidate to have frontage on the boulevard shall be part of the SoBro Subdistrict. Transitional properties that do not consolidate to have frontage on the boulevard shall be part of the Lafayette Subdistrict.



Section II: Subdistrict Standards

Lafayette Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|---------|
| Primary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |
| Tertiary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |

Industrial Frontage is allowed in this Subdistrict.

B Facade width

| | |
|------------------|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 80% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

C Min. building depth 15' from building facade

Height

D Max.

| | |
|-----------------------|------------|
| • On Lafayette St | 12 stories |
| • Subdistrict general | 8 stories |

Additional height available through the Bonus Height Program

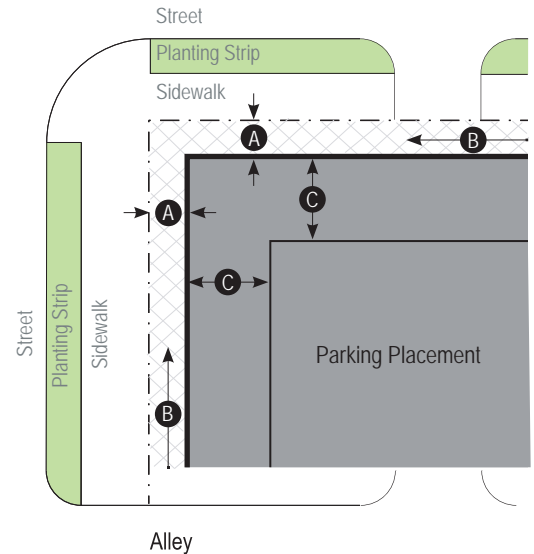
Step-back

Step-back required for all buildings fronting public streets

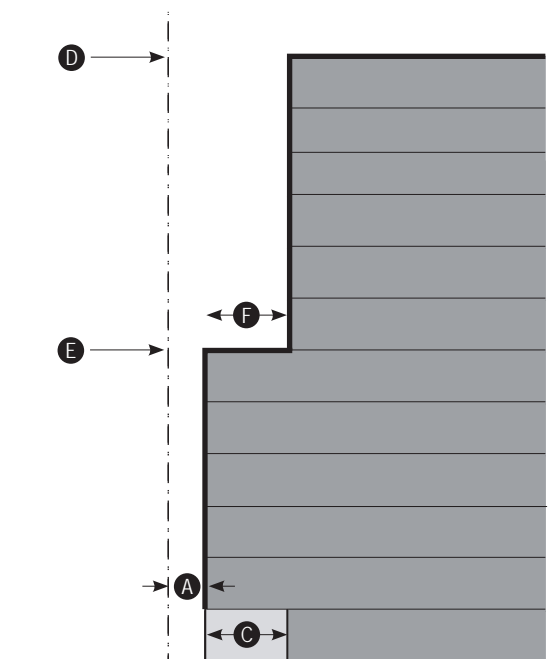
| | |
|----------------------------------|-----------------------|
| E Step-back after between | 6 4th and 7th stories |
| F Min. step-back depth | 15' |

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



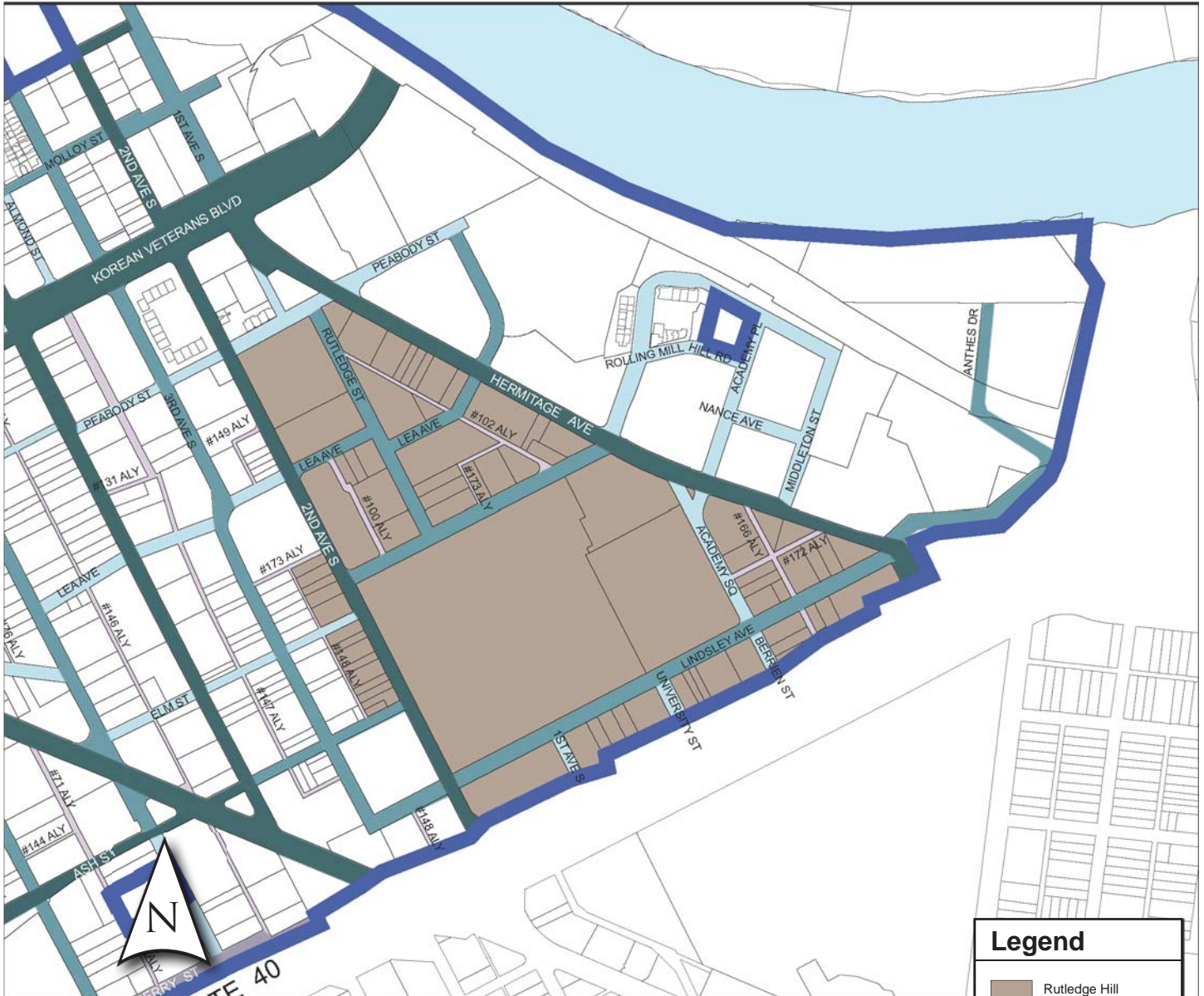
Building Plan



Building Section

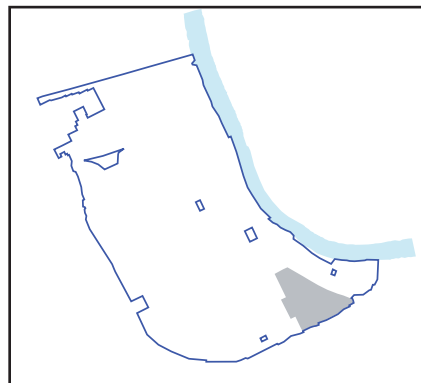
Section II: Subdistrict Standards

Rutledge Hill Subdistrict: Regulating Plan



The Rutledge Hill neighborhood includes a variety residential and civic historic buildings. This area is largely intact and new buildings of complimentary height, scale and massing.

Some of these properties are governed by a National Register District that regulates development, restoration and demolition. Property owners must contact the Metro Historic Commission for additional details.



| Legend | |
|--------|------------------------------------|
| | Rutledge Hill Subdistrict |
| | Rutledge Hill Historic Subdistrict |
| | Primary Street |
| | Secondary Street |
| | Tertiary Street |
| | Alley |

Section II: Subdistrict Standards

Rutledge Hill Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|---------|
| Primary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |
| Tertiary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |

B Facade width

| | |
|--|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 60% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |
| Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking. | |

C Min. building depth 15' from building facade

Height

D Max.

| | |
|---------------------------------|-----------|
| • Primary and Secondary streets | 6 stories |
| • Tertiary streets | 4 stories |

Additional height available through the Bonus Height Program

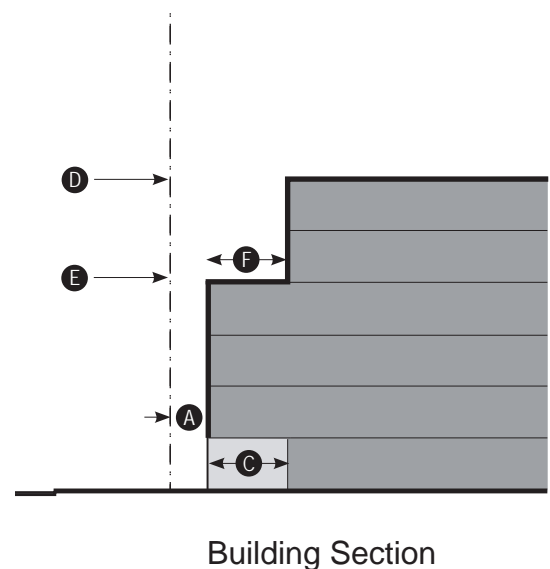
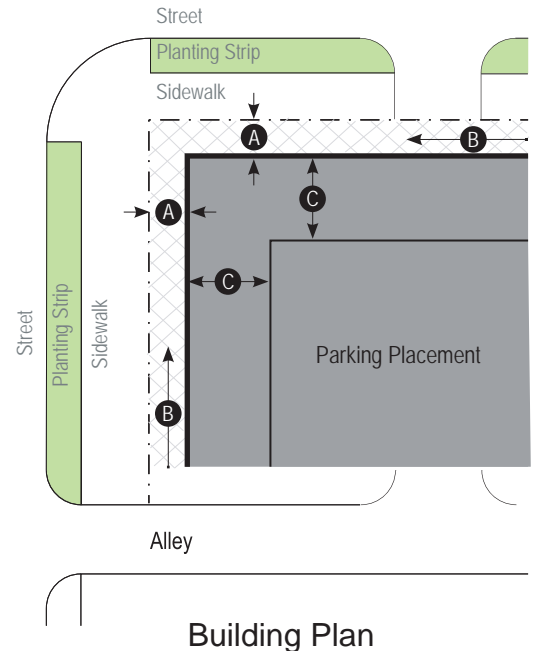
Step-back

Step-back required for all buildings fronting public streets

| | |
|------------------------|-----------|
| E Step-back after | 4 stories |
| F Min. step-back depth | 15' |

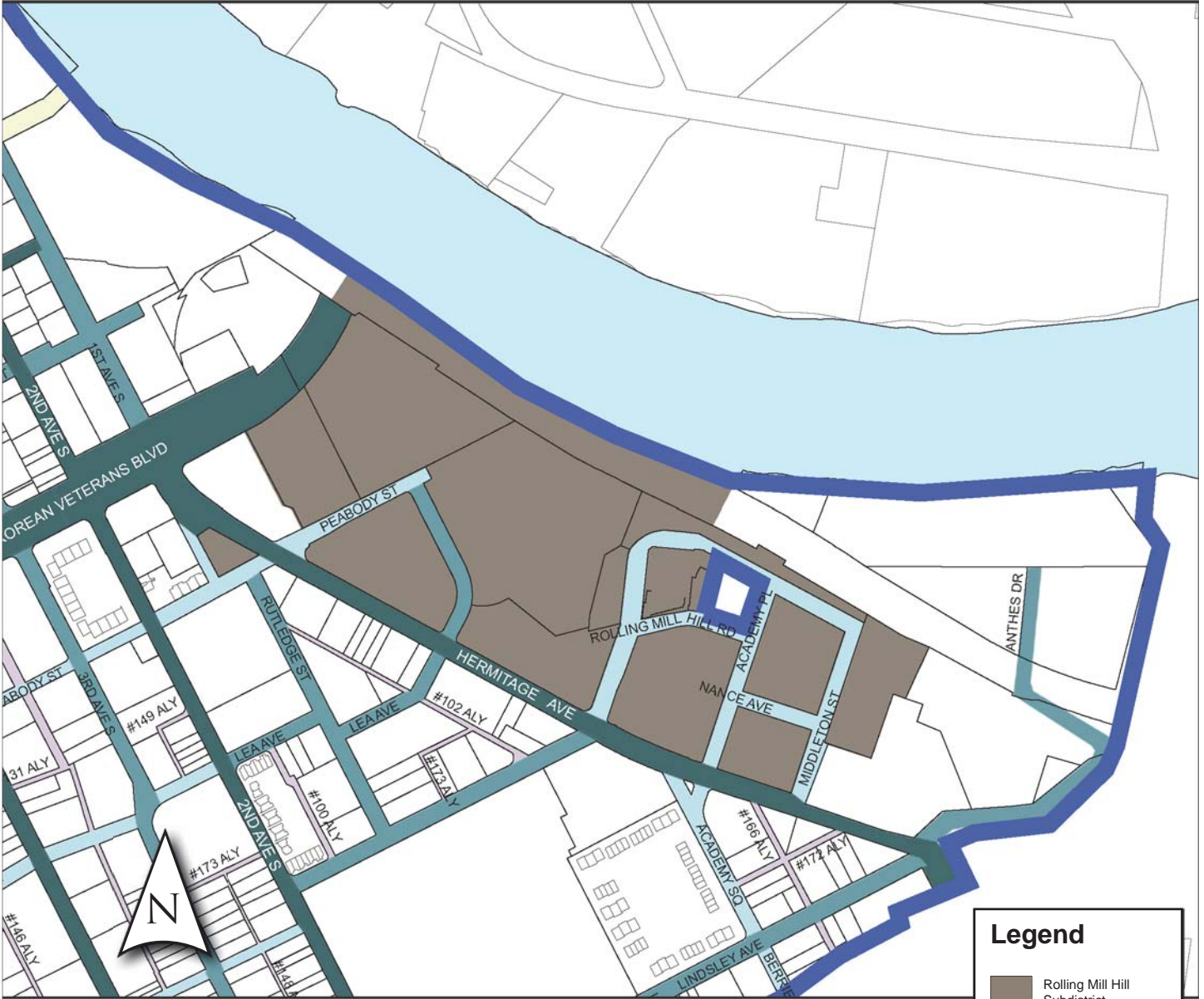
Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

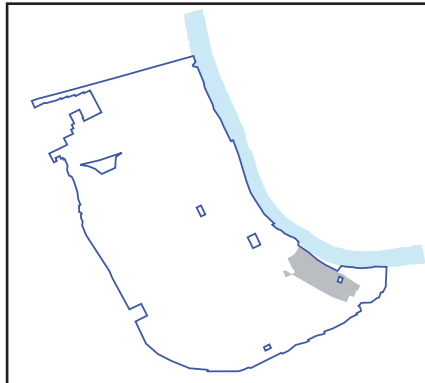








Section II: Subdistrict Standards

Rolling Mill Hill Subdistrict: Regulating Plan



The redevelopment of the Rolling Mill Hill neighborhood is overseen by the Metropolitan Development and Housing Agency (MDHA) through the *Master Plan and Development Guidelines for Rolling Mill Hill*.



| Legend | |
|---|--|
|  | Rolling Mill Hill Subdistrict |
|  | Rolling Mill Hill Historic Subdistrict |
|  | Primary Street |
|  | Secondary Street |
|  | Tertiary Street |
|  | Alley |

Section II: Subdistrict Standards

Rolling Mill Hill Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

All streets 0-20'

B Facade width

Minimum 25% of the lot frontage or 25', which ever is greater.
Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

C Min. building depth 15' from building facade

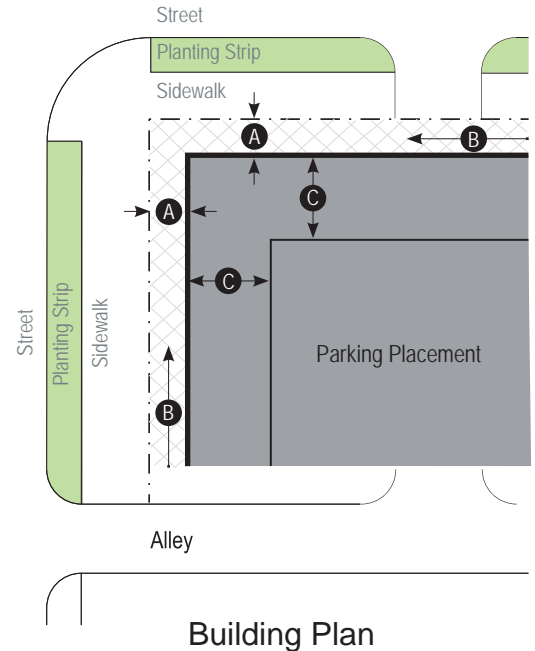
A building liner is required surrounding parking structures on the ground floor facing public streets and Open Space.

Height

D Max. height at the street 65'

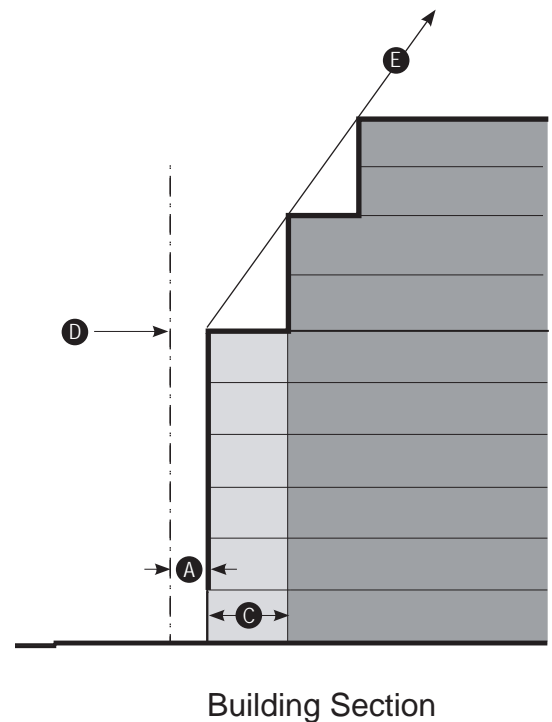
E Height Control Plane

Applies from all streets and Open Space.
1 foot horizontal to 1.5 feet vertical



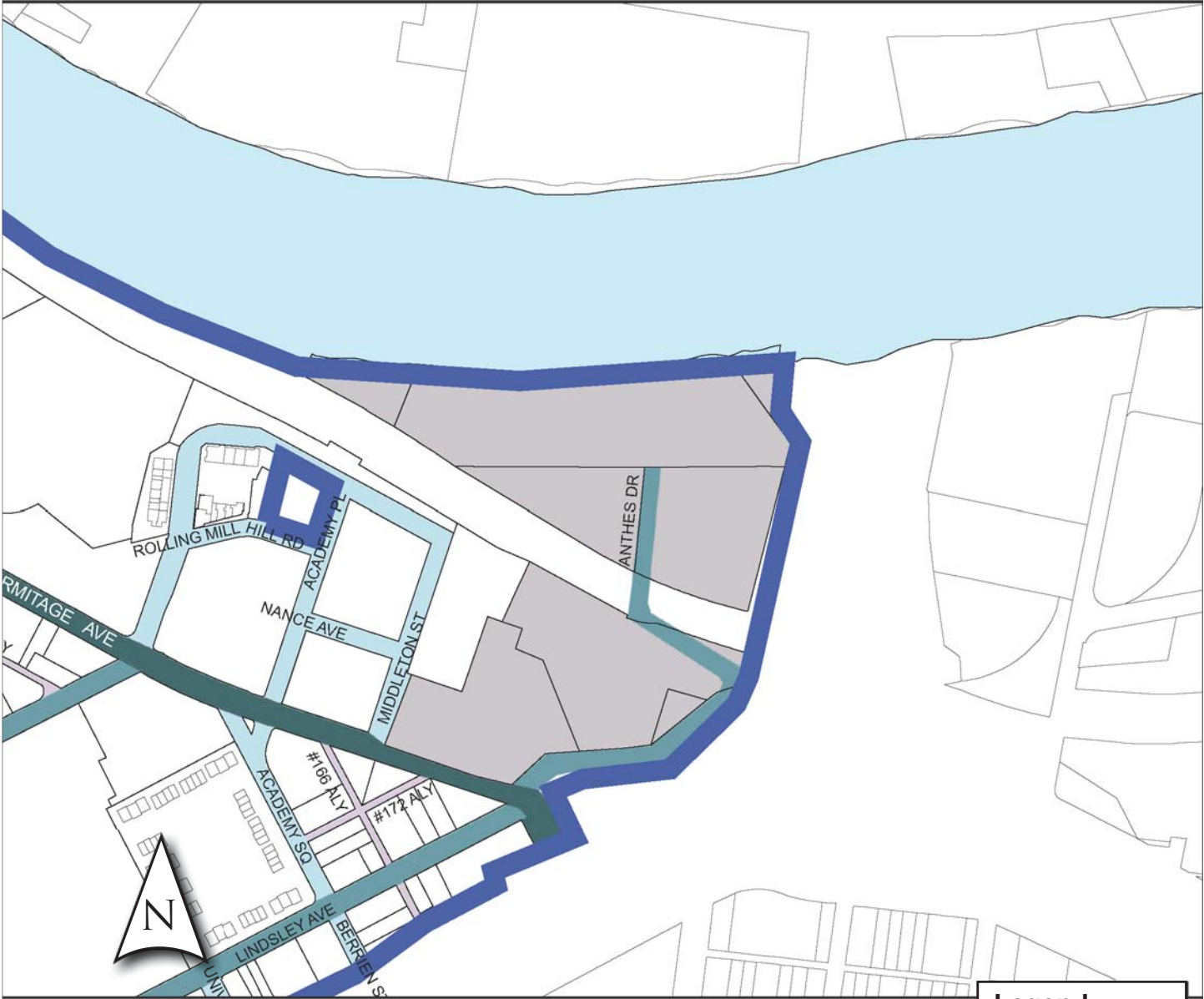
Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



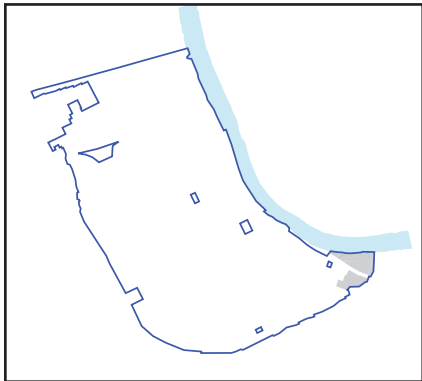
Section II: Subdistrict Standards

Rutledge River Subdistrict: Regulating Plan



The Rutledge River neighborhood is situated in a somewhat isolated area along the Cumberland River. Redevelopment of this area should make the most of the riverside location and the adjacent commuter rail line, while preserving the historic building on Hermitage Avenue.

New streets within the Rutledge River Subdistrict shall be Secondary or Tertiary Streets.



| Legend | |
|--------|----------------------------|
| | Rutledge River Subdistrict |
| | Primary Street |
| | Secondary Street |
| | Tertiary Street |
| | Alley |

Section II: Subdistrict Standards

Rutledge River Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

Primary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'

Secondary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'
- Porch Frontage 10'-15'

Tertiary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'
- Porch Frontage 10'-15'

Industrial Frontage is allowed in this Subdistrict.

B Facade width

- Primary Street 80% of lot frontage min.
- Secondary Street 80% of lot frontage min.
- Tertiary Street 60% of lot frontage min.

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

C Min. building depth 15' from building facade

Height

- D Max. 9 stories
Additional height available through the Bonus Height Program

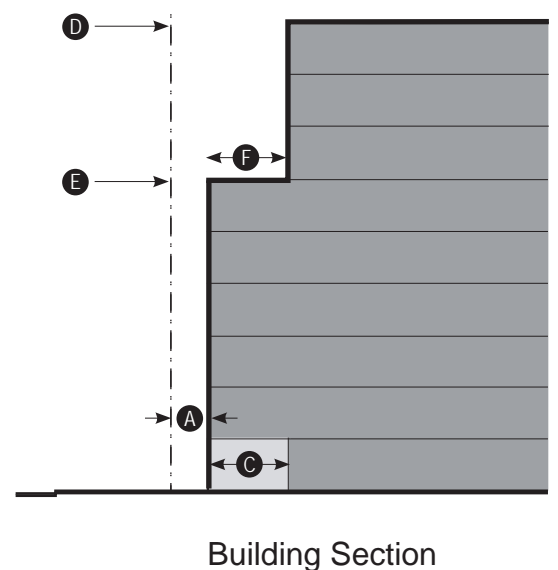
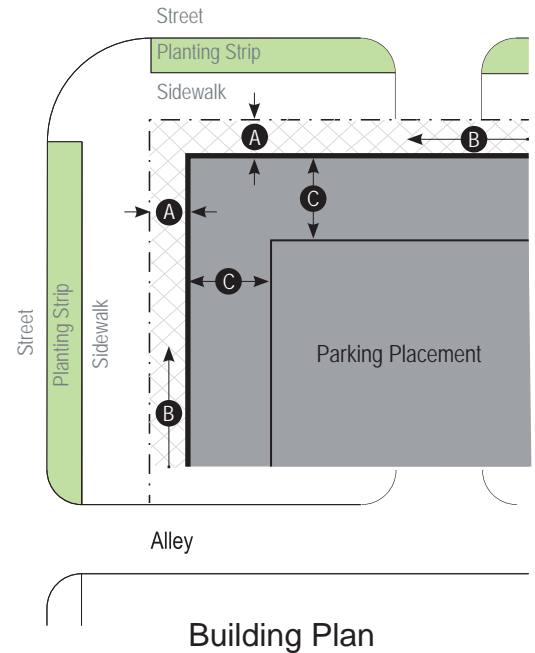
Step-back

Step-back required for all buildings fronting public streets

- E Step-back after between 6 4th and 7th stories
- F Min. step-back depth 15'

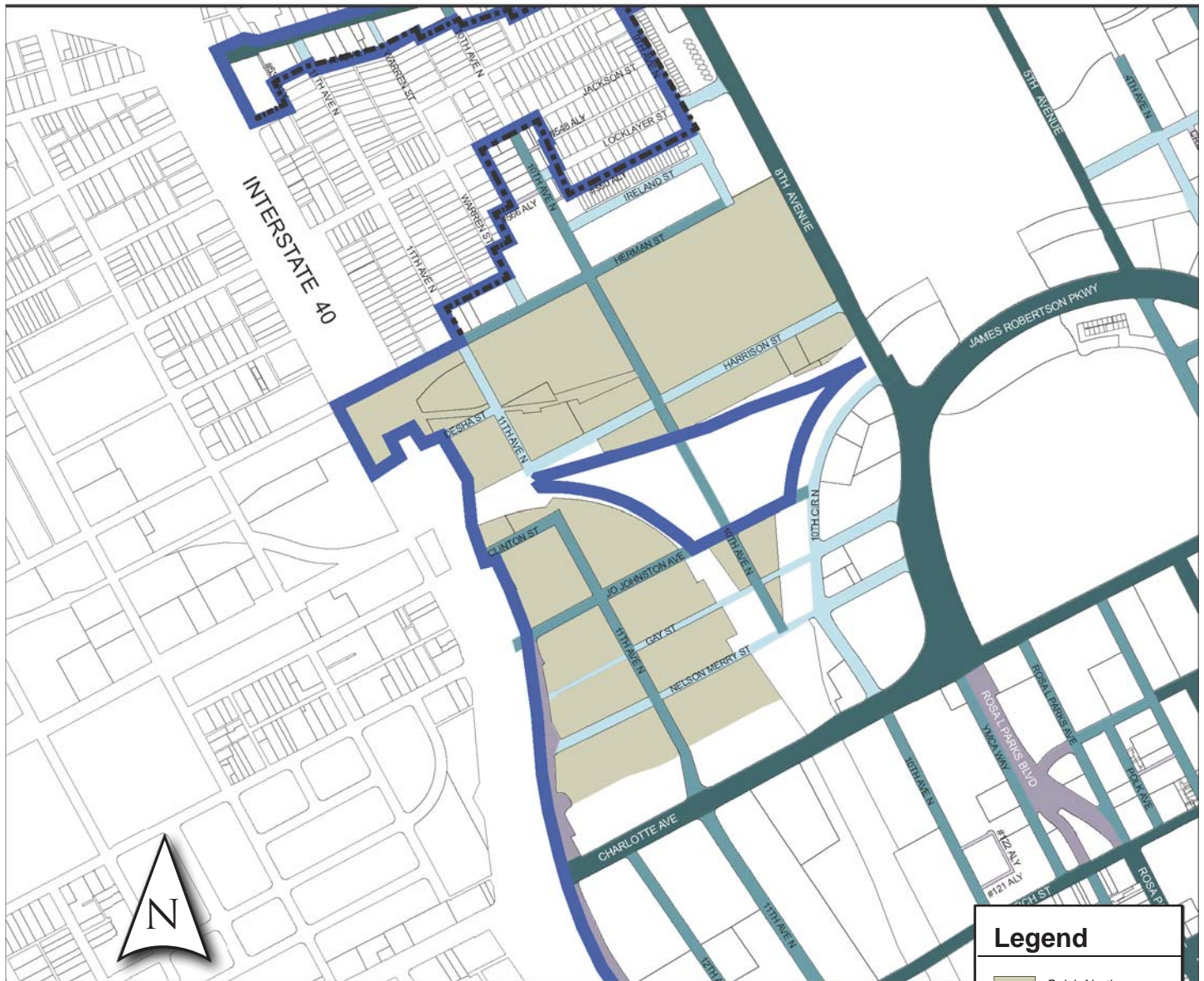
Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

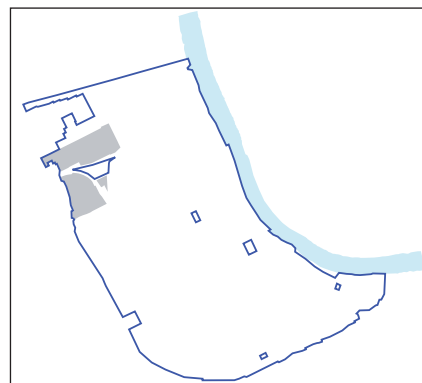


Section II: Subdistrict Standards

Gulch North: Regulating Plan



The North Gulch neighborhood is an area of transition within Downtown. Bordered on the north and west by single-family residential neighborhoods, on the east by prominent state government landmarks, and on the south by industrial, The North Gulch is envisioned to be a unique area that integrates and harmonizes these diverse uses and building types. Redevelopment of this area should make the most of the existing industrial buildings, interstate access and the internal railroad lines.



| Legend | |
|--------|-------------------------|
| | Gulch North Subdistrict |
| | Primary Street |
| | Secondary Street |
| | Tertiary Street |
| | Other Street |
| | Alley |

Section II: Subdistrict Standards

Gulch North: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|---------|
| Primary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |
| Tertiary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |

Industrial Frontage is allowed in this Subdistrict on streets north of Harrison Street, including Harrison Street.

B Facade width

| | |
|------------------|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 60% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

C Min. building depth 15' from building facade

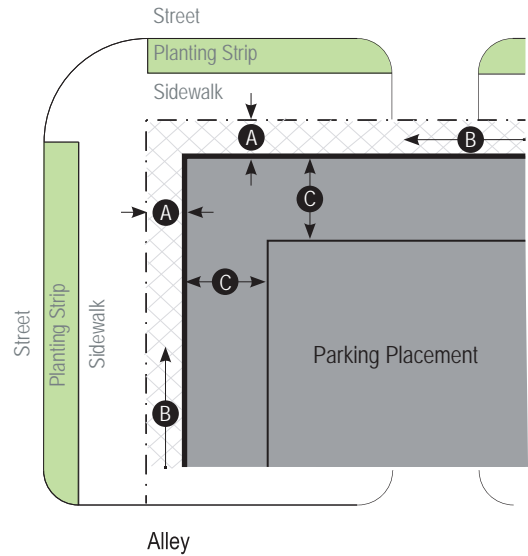
Height

| | |
|--------------------------------|-----------|
| D Max. | 7 stories |
| • Buildings fronting Herman St | 4 stories |

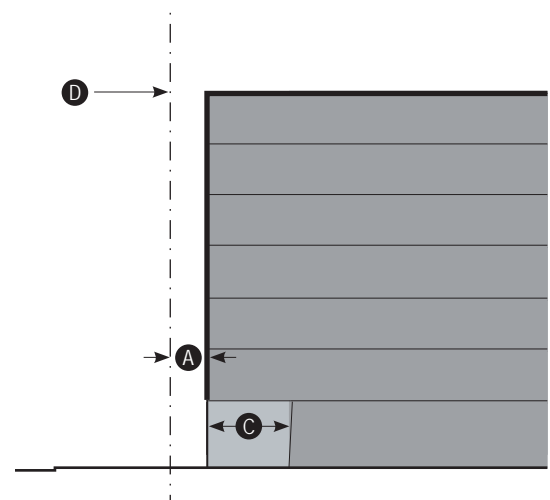
Additional height available through the Bonus Height Program

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



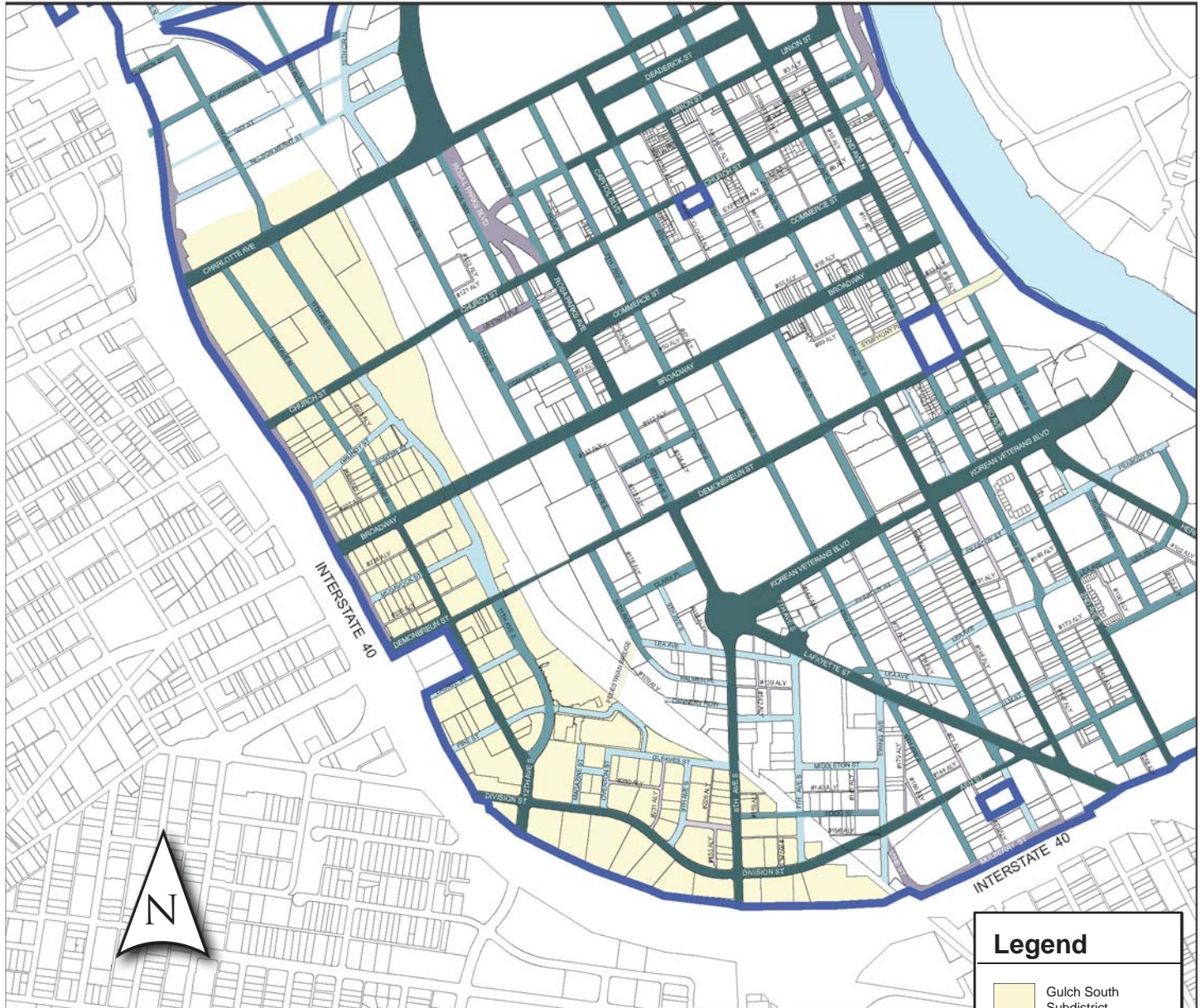
Building Plan



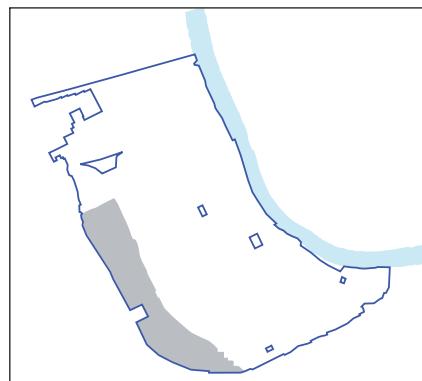
Building Section

Section II: Subdistrict Standards

Gulch South: Regulating Plan



The Gulch South neighborhood is an eclectic neighborhood of business services, restaurant, and retail with many new residential buildings. The area is a link between Downtown and Midtown and is envisioned to be mixed-use mid-rise buildings with opportunities for additional height at key intersections and along important streets. Connectivity – vehicular, rail, bicycle and pedestrian – should be preserved and additional connectivity is strongly encouraged.



| Legend | |
|--------|-------------------------|
| | Gulch South Subdistrict |
| | Primary Street |
| | Secondary Street |
| | Tertiary Street |
| | Other Street |
| | Alley |

Section II: Subdistrict Standards

Gulch South: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

Primary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'

Secondary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'
- Porch Frontage 10'-15'

Tertiary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'
- Porch Frontage 10'-15'

Facade width

- B** Primary Street 80% of lot frontage min.
 Secondary Street 80% of lot frontage min.
 Tertiary Street 60% of lot frontage min.
 Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

- C** Min. building depth 15' from building facade

Height

D Max.

- On Church St, Broadway, Demonbreun 15 stories
- At the intersection of 12th Ave and Demonbreun, 12th Ave and Division, 8th Ave and Division 20 stories
- Subdistrict general 10 stories

Additional height at intersections applies to frontage within 150 feet of the intersection

Additional height available through the Bonus Height Program

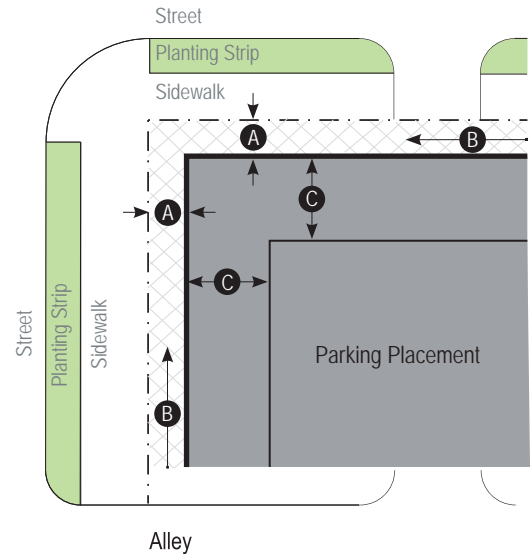
Step-back

Step-back required along all frontages in Subdistrict general and along Broadway.

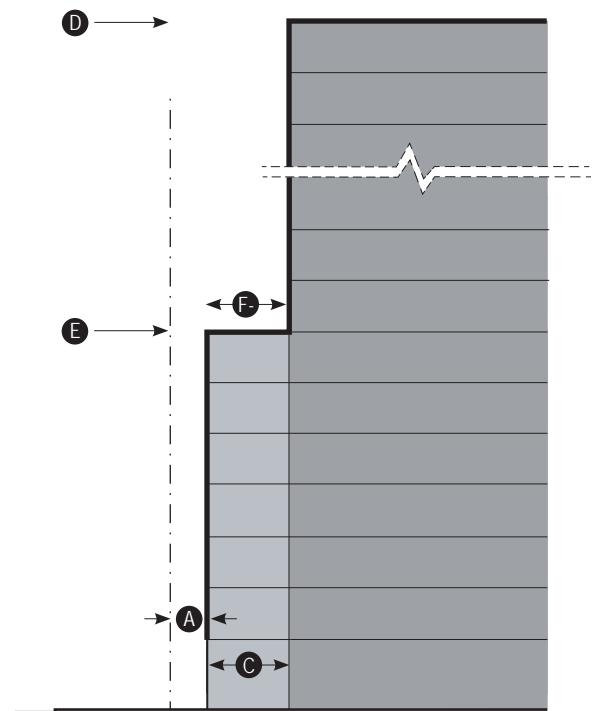
- E** Step-back after between 7 4th and 8th stories
F Min. step-back depth 15'

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



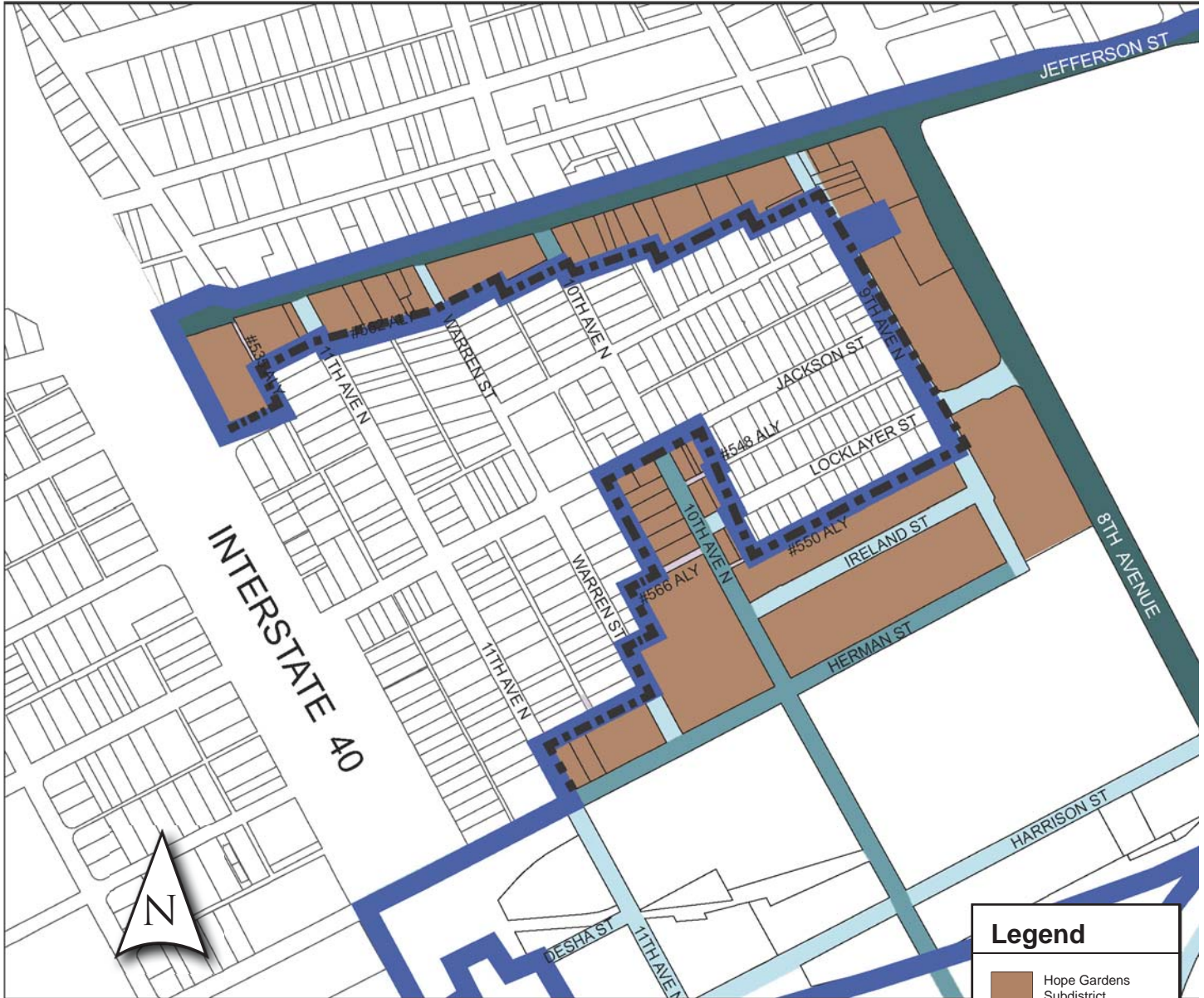
Building Plan



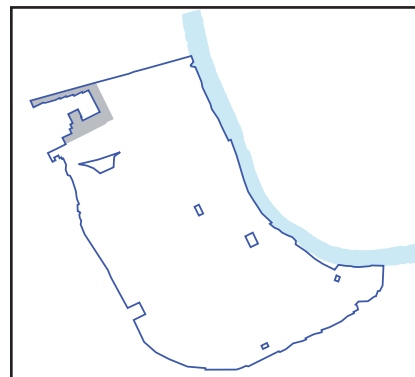
Building Section

Section II: Subdistrict Standards

Hope Gardens Subdistrict: Regulating Plan



The Hope Gardens subdistrict includes the commercial and multi-family areas surrounding the single-family residential at the center of the Hope Gardens neighborhood. Development along the major streets – Jefferson Street and Rosa Parks Boulevard – should be low-rise and should transition in height and mass near the single-family areas. The existing commercial, residential and industrial uses are all important factors within this area and can be maintained while providing opportunities for mixed-use. The harmonization of these many uses – through the regulation of the building forms – will ensure the vitality of this mixed-use neighborhood.



Section II: Subdistrict Standards

Hope Gardens Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|---------|
| Primary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |
| Tertiary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |

Industrial Frontage is allowed in this Subdistrict along Herman Street only.

B Facade width

| | |
|------------------|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 60% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

C Min. building depth 15' from building facade

A building liner is required surrounding parking structures on the ground floor facing public streets and Open Space.

Height

D Max.

| | |
|--------------------|-----------|
| • Primary Street | 7 stories |
| • Secondary Street | 4 stories |
| • Tertiary Street | 3 stories |

Additional height available through the Bonus Height Program

Step-back

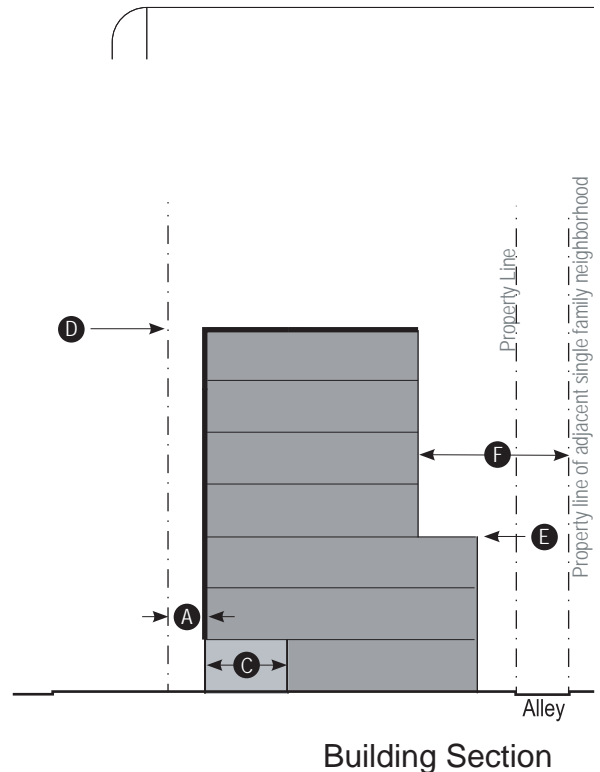
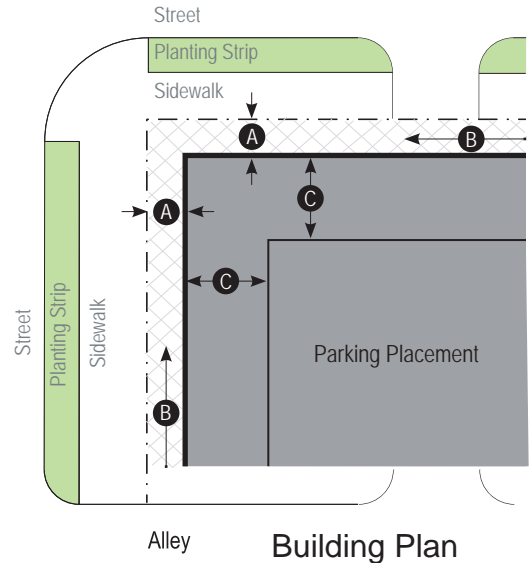
Step-back required for properties abutting the single family neighborhood. Step-back shall be measured from the abutting property line.

| | |
|-----------------------------------|-----------|
| E Step-back required after | 3 stories |
| F Minimum step-back depth | 30' |

Buffer

Landscape buffer

A landscaped buffer in accordance with 17.24.240 B-5 shall be required along any property line directly abutting a single or two-family zone district.

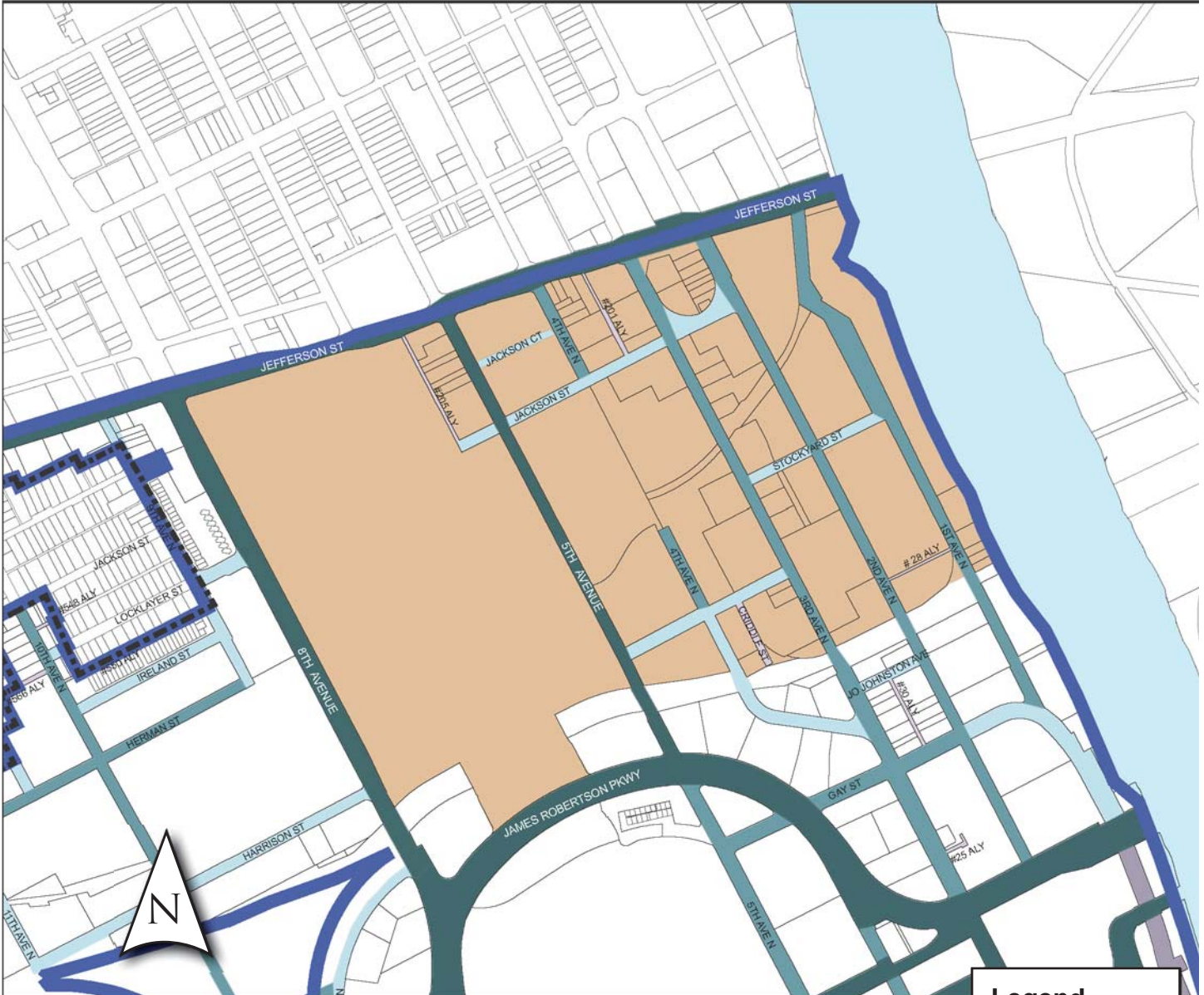


Sidewalk & Planting

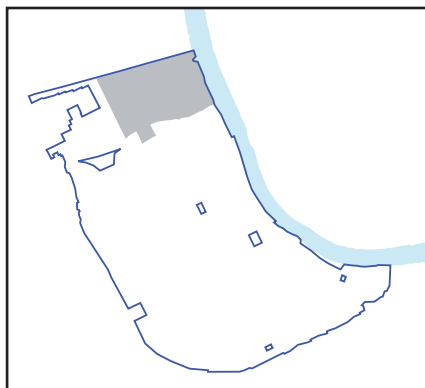
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).






Section II: Subdistrict Standards

Sulphur Dell: Regulating Plan



The Sulphur Dell neighborhood is a mixed-use neighborhood surrounding the Bicentennial Mall on the north side of Downtown. This area includes many state-owned properties and is envisioned to be a cultural and civic destination within the State. Mixed-use and residential buildings will diversify the neighborhood and provide a transition in height and use into neighborhoods to the north.



| Legend | |
|---|--------------------------|
|  | Sulphur Dell Subdistrict |
|  | Primary Street |
|  | Secondary Street |
|  | Tertiary Street |
|  | Alley |

Section II: Subdistrict Standards

Sulphur Dell Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

| | |
|-----------------------|---------|
| Primary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| Secondary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |
| Tertiary Street | |
| • Storefront Frontage | 0'-10' |
| • Stoop Frontage | 5'-10' |
| • Porch Frontage | 10'-15' |

B Facade width

| | |
|--|--------------------------|
| Primary Street | 80% of lot frontage min. |
| Secondary Street | 80% of lot frontage min. |
| Tertiary Street | 60% of lot frontage min. |
| Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking. | |

C Min. building depth 15' from building facade

Height

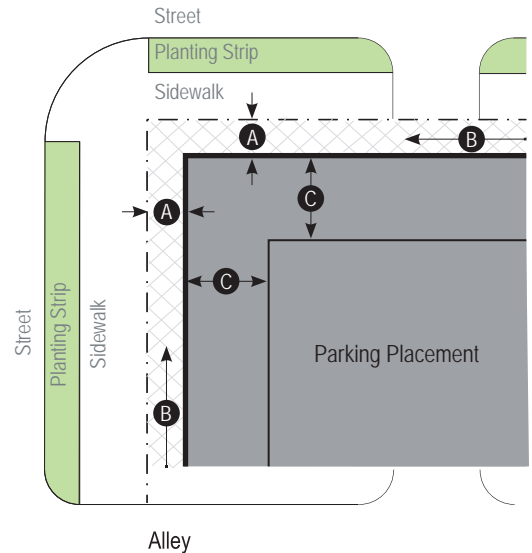
D Max.

| | |
|--------------------|-----------|
| • Primary Street | 7 stories |
| • Secondary Street | 5 stories |
| • Tertiary Street | 4 stories |

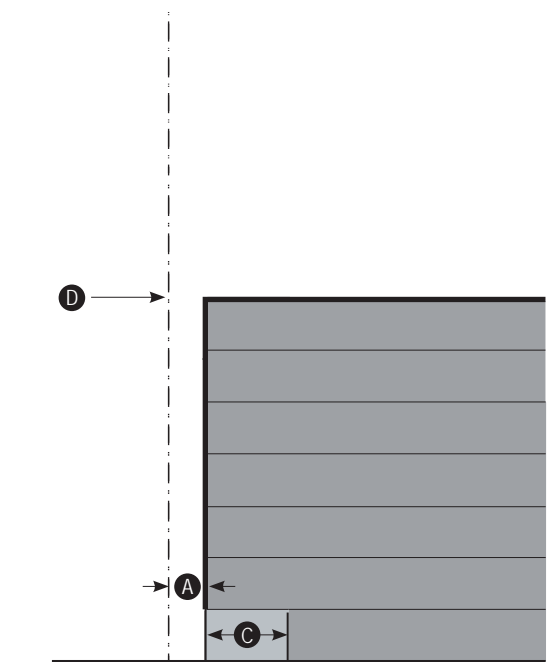
Additional height available through the Bonus Height Program

Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



Building Plan



Building Section

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Section III: Uses

Amended by Ordinance No. BL2012-142
as adopted on May 21, 2012

Amended by Ordinance No. BL2011-896
as adopted on May 26, 2011

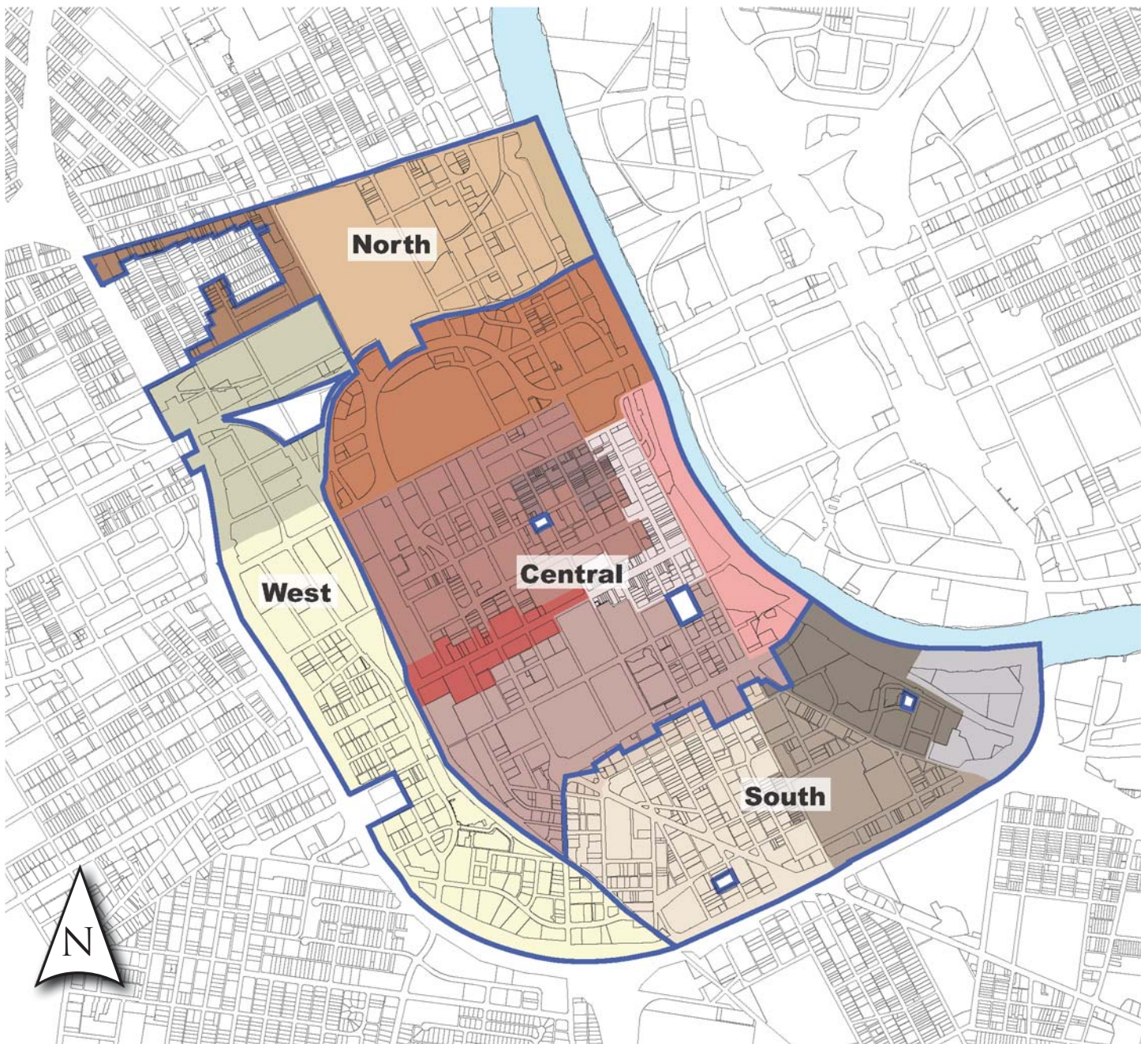
Section III: Uses

Use Areas

Land uses within the DTC are determined by Area – Central, South, West, and North. To create a sustainable and mixed-used Downtown, the form-based zoning of each subdistrict regulates the shape, scale, and placement of the buildings, and allows a variety of uses.

Uses Area boundaries are the same as Subdistrict boundaries. To determine the allowed land uses, locate the property on the Area Plan, and refer to the Area column on the Land Use Chart for the allowed uses.

Uses Permitted with Conditions or Permitted by Special Exceptions or Accessory shall follow the standards of Chapter 17.16. If standards within Chapter 17.16 and the DTC conflict, the stricter shall apply.



Section III: Uses

Use Tables

| | North | South | West | Central |
|-------------------------------------|-------|-------|------|---------|
| P Permitted by right | | | | |
| PC Permitted with conditions | | | | |
| SE Special Exception | | | | |
| A Accessory | | | | |
| O Overlay District | | | | |
| Residential Uses: | | | | |
| Single-family | P | P | P | P |
| Two-family | P | P | P | P |
| Multi-Family | P | P | P | P |
| Mobile home dwelling | | | | |
| Accessory apartment | P | P | P | P |
| Boarding house | P | P | P | P |
| Consignment sale | P | P | P | P |
| Garage sale | A | A | A | A |
| Historic bed and breakfast homestay | P | P | P | P |
| Historic home events | P | P | P | P |
| Home occupation | P | P | P | P |
| Rural bed and breakfast homestay | | | | |
| Security residence | | | | |
| Institutional Uses: | | | | |
| Correctional facility | P | P | P | P |
| Cultural center | P | P | P | P |
| Day care center (up to 75) | P | P | P | P |
| Day care center (over 75) | P | P | P | P |
| Day care home | P | P | P | P |
| Day care--parent's day out | A | A | A | A |
| School day care | P | P | P | P |
| Monastery or convent | P | P | P | P |
| Orphanage | P | P | P | P |
| Religious institution | P | P | P | P |
| Educational Uses: | | | | |
| Business school | P | P | P | P |
| College or university | P | P | P | P |
| Community education | P | P | P | P |
| Dormitory | P | P | P | P |
| Fraternity/sorority house | P | P | P | P |
| Personal instruction | P | P | P | P |
| Vocational school | P | P | P | P |
| Office Uses: | | | | |
| Financial institution | P | P | P | P |
| General office | P | P | P | P |
| Leasing/sales office | P | P | P | P |
| Medical Uses: | | | | |
| Animal hospital | | | | |
| Assisted-care living | P | P | P | P |
| Hospice | P | P | P | P |
| Hospital | P | P | P | P |
| Medical appliance sales | P | P | P | P |
| Medical office | P | P | P | P |

| | North | South | West | Central |
|--|-------|-------|------|---------|
| P Permitted by right | | | | |
| PC Permitted with conditions | | | | |
| SE Special Exception | | | | |
| A Accessory | | | | |
| O Overlay District | | | | |
| Medical or scientific lab | P | P | P | P |
| Nonresidential drug treatment facility | P | P | P | P |
| Nursing home | P | P | P | P |
| Outpatient clinic | P | P | P | P |
| Rehabilitation services | P | P | P | P |
| Residence for handicapped (8 or more) | P | P | P | P |
| Veterinarian | P | P | P | P |
| Commercial Uses: | | | | |
| After-hours establishment | PC | PC | PC | PC |
| Animal boarding facility | P | P | P | P |
| ATM | P | P | P | P |
| Automobile convenience | PC | PC | PC | PC |
| Automobile parking | P | P | P | P |
| Automobile repair | | P | | |
| Automobile sales, new | | P | P | P |
| Automobile sales, used | | | | |
| Automobile service | P | P | P | P |
| Bar or nightclub | P | P | P | P |
| Bed and breakfast inn | P | P | P | P |
| Business service | P | P | P | P |
| Carpet cleaning | P | P | P | |
| Car wash | | PC | | |
| Community garden (commercial) | P | P | P | P |
| Community garden (non-commercial) | P | P | P | P |
| Custom assembly | P | P | P | P |
| Donation center, drop-off | PC | PC | PC | PC |
| Funeral home | P | P | P | P |
| Furniture store | P | P | P | P |
| Home improvement sales | P | P | P | P |
| Hotel/motel | P | P | P | P |
| Inventory stock | A | A | A | A |
| Kennel/stable | | | | |
| Laundry plants | | P | P | |
| Liquor sales | P | P | P | P |
| Major appliance repair | P | P | P | |
| Microbrewery | P | P | P | P |
| Mobile storage unit | PC | PC | PC | PC |
| Mobile vendor | | | | |
| Personal care services | P | P | P | P |
| Restaurant, fast-food | P | P | P | P |
| Restaurant, full-service | P | P | P | P |
| Restaurant, take-out | P | P | P | P |
| Retail | P | P | P | P |
| Self-service storage | P | P | P | P |
| Vehicular rental/leasing | PC | PC | PC | PC |

Section III: Uses

Use Tables

| P Permitted by right PC Permitted with conditions SE Special Exception A Accessory O Overlay District | North | South | West | Central |
|---|-------|-------|------|---------|
| Vehicular sales & services, limited | | P | | |
| Wrecker service | | P | | |
| Communication Uses: | | | | |
| Amateur radio antenna | P | P | P | P |
| Audio/video tape transfer | P | P | P | P |
| Multi-media production | P | P | P | P |
| Printing and publishing | P | P | P | P |
| Radio/TV/satellite tower | PC | PC | PC | PC |
| Radio/TV studio | P | P | P | P |
| Satellite dish | P | P | P | P |
| Telephone services | PC | PC | PC | PC |
| Industrial Uses: | | | | |
| Artisan distillery | P | P | P | P |
| Building contractor supply | PC | PC | PC | |
| Distributive business/wholesale | PC | PC | PC | |
| Fuel storage | A | A | A | |
| Heavy equipment sales & service | | | | |
| Hazardous operation | | | | |
| Manufacturing, heavy | | | | |
| Manufacturing, medium | | | | |
| Manufacturing, light | PC | PC | PC | |
| Research service | P | P | P | |
| Scrap operation | | | | |
| Tank farm | | | | |
| Warehouse | PC | PC | PC | |
| Transportation Uses: | | | | |
| Airport/heliport | | | | |
| Boatdock (commercial) | P | P | | P |
| Bus station/landport | P | P | P | P |
| Bus transfer station | P | P | P | P |
| Commuter rail | P | P | P | P |
| Helistop | SE | SE | SE | SE |
| Motor freight | | | | |
| Park and ride lot | | | | |
| Railroad station | SE | SE | SE | |
| Railroad yard | | | | |
| Water taxi station | P | P | | P |
| Utility Uses: | | | | |
| Power/gas substation | P | P | P | P |
| Power plant | A | A | A | A |
| Reservoir/water tank | P | P | P | P |
| Safety services | P | P | P | P |
| Waste water treatment | SE | SE | SE | SE |
| Water/sewer pump station | P | P | P | P |
| Water treatment plant | SE | SE | SE | SE |

| P Permitted by right PC Permitted with conditions SE Special Exception A Accessory O Overlay District | North | South | West | Central |
|---|-------|-------|------|---------|
| Waste Management Uses: | | | | |
| Collection center | | | | |
| Construction/demolition landfill | | | | |
| Medical waste | A | A | A | A |
| Recycling collection center | P | P | P | P |
| Recycling facility | | | | |
| Sanitary landfill | | | | |
| Waste transfer | | | | |
| Recreation and Entertainment Uses: | | | | |
| Adult entertainment | O | O | O | O |
| Camp | | | | |
| Club | P | P | P | P |
| Commercial amusement (inside) | P | P | P | P |
| Commercial amusement (outside) | P | P | P | P |
| Country club | P | P | P | P |
| Drive-in movie | | | | |
| Driving range | | | | |
| Fairground | | | | |
| Golf course | | | | |
| Greenway | P | P | P | P |
| Park | P | P | P | P |
| Racetrack | | | | |
| Recreation center | P | P | P | P |
| Rehearsal hall | P | P | P | P |
| Stadium arena/convention center | P | P | P | P |
| Temporary festival | P | P | P | P |
| Theater | P | P | P | P |
| Zoo | | | | |
| Other Uses: | | | | |
| Agricultural activity | | | | |
| Cemetery | P | P | P | P |
| Domestic animals / wildlife | | | | |
| Mineral extraction | | | | |
| Pond/lake | P | P | P | P |

Section IV: General Standards

Amended by Ordinance No. BL2012-142
as adopted on May 21, 2012

Amended by Ordinance No. BL2011-896
as adopted on May 26, 2011

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Section IV: General Standards

Calculations

Measurement from “Grade”

- Unless otherwise indicated, reference to measurements from “grade” shall be calculated using the average elevation along the public right-of-way fronting the property. Thus, grade will generally be measured from the public sidewalk, not from grade on site.
- When buildings are set back from the property line more than 15 feet, grade shall be measured as the average existing elevation at the building facade.
- In the event that the base flood elevation, as established by FEMA, is higher than the sidewalk or grade elevations, the height of the first story, shall be measured from 1 foot above the base flood elevation.

Measurement of Height

- Unless otherwise specified herein, the height of buildings shall be measured in stories.
- The maximum height for an individual story shall not exceed 25 feet from finished floor to finished floor for each of the first 2 stories, 18 feet floor to floor above the second story, and 25 feet for the top story of buildings greater than 5 stories.
- The minimum **multi-story** building height shall be 25 feet. This applies to all buildings except those designed for single-family use, two-family use, or multi-family use with residential on the ground floor.
- The maximum height for a raised foundation is 6 feet above grade.
- Basements are not considered stories for the purposes of determining building height.
- Building height shall be measured from each Street Frontage (excluding Other streets alleys) or Open Space.
- The height of a parking structure concealed by a building liner may be equal to the height of the liner, regardless of the number of stories. If there is no liner to conceal the parking structure, its height is limited by the maximum number of stories allowed.
- The height of fences, walls and hedges shall be measured in feet from the average sidewalk elevation.

Step-backs

- If a building step-back is specified to occur between a range of stories, that range shall include the lower and upper story bounds. For example, the James Robertson Subdistrict specifies a 15’ step-back between the 4th and 8th stories. Therefore the required stepback can occur on the 4th, 5th, 6th, 7th, or 8th story.
- To allow for facade variation, lower stories may be permitted to step-back to a lesser extent, or not at all, so long as the minimum step-back depth is met by the upper story bound of the required step-back range.

Fenestration and Glazing

- Except as specifically referenced herein, facade glazing and opening standards shall include windows, doors and openings in parking structures and shall apply to all areas of the building facade facing a public street or open space (excluding Other streets and alleys) as follows:
 - First Floor: facade area measured from the finished floor to a height 14 feet above the finished floor.
 - Upper Floors: facade area from finished floor to finished floor.
 - Openings for vehicular access to parking structures on the first floor shall not be included in calculation of total facade area or glazed area.
 - The DTC recognizes the need for building systems and functionality including interior mechanical systems, fire safety egress, other building code issues and their impact of the feasibility of building fenestration. Areas of the facade affected by these elements shall not be counted toward minimum glazing requirements.

Section IV: General Standards

Street Character

The public right-of-way, including streets, sidewalks and public utility infrastructure, plays both a functional and social role in the life of the city and its citizens. Streets organize the city, help to define space, and link destinations. The street is also a public place where people congregate, shop, socialize and live. Active, attractive streets are critical to the continued growth and success of Downtown. The DTC includes urban design tools to make working, living and playing in Downtown lively, safe and comfortable.

The DTC uses Street Types as an urban design and organizing tool. All streets are classified on the Regulating Plan as Primary, Secondary, Tertiary, Other, or Alley.

Where alleys exist and are in working condition, or where new alleys can be created, the DTC prioritizes alleys for access and loading. The location of vehicular access from all other streets shall be determined on a case-by-case basis.

The Downtown Plan: 2007 Update calls for “a strong emphasis on expanding other modes of transportation including walking, cycling and transit.” The DTC emphasizes walking, cycling and transit as primary modes of transportation within Downtown through the urban design of individual buildings, blocks, and neighborhoods.

All Streets

- Streets refer to publicly or privately owned right-of-way. They are intended for use by pedestrian, bicycle, transit and vehicular traffic and provide access to property.
- Streets consist of vehicular lanes and the Sidewalk Corridor. The vehicular lanes, in a variety of widths, provide traffic and parking capacity and may include bicycle paths. The Sidewalk Corridor contributes to the urban character of each neighborhood. It may include pedestrian paths, landscaped planters, street furnishings and street trees.
- Pedestrian safety, comfort, and accessibility should be a primary consideration of street design and dimensioning.
- When alleys are present, vehicular access from alleys is preferred. Vehicular access from public streets shall be considered in the following order: Other Streets, Secondary Streets, Tertiary Streets and then Primary Streets as approved by Metro departments.



Section IV: General Standards

Street Character

Street Types

- **Primary Street:** Primary Streets accommodate high levels of pedestrian activity and high levels of vehicular traffic. On Primary Streets, active uses – residential, retail, restaurant or office – lining parking structures and on the first floor of buildings, and restricted vehicular access enhance the pedestrian experience. Primary streets provide the opportunity for more intense, urban development including shallow Build-to Zones and, in some cases, increased building height. Pedestrian comfort on these streets is of highest importance. Primary streets should have a continuous street wall, wide sidewalks between 15 and 20 feet to provide room for street furniture such as benches, trash receptacles, and bicycle parking. Primary Streets have the highest level of urban activity such as, outdoor dining, retail displays, and community activities like markets, parades, and music. Street trees provide protection from the sun and rain, reduce stormwater runoff and air pollution, and provide aesthetic value to the city. Trees should be planted in wells with tree grates to allow for the uninterrupted flow of pedestrian traffic.
- **Secondary Street:** Secondary Streets have moderate levels of pedestrian activity and moderate levels of vehicular traffic. Secondary Streets may be mixed-use or more residential in character. The Build-to Zone is generally shallow, and building heights are limited. In mixed-use areas, a continuous street wall should be maintained and sidewalks should be between 12 and 15 feet wide to accommodate pedestrian traffic. In residential areas, the required minimum façade width is limited – allowing for more space between buildings – and sidewalks may be narrower. Both tree wells and open landscaped planters are appropriate depending on sidewalk width.
- **Tertiary Street:** Tertiary Streets are the less important than Primary and Secondary streets. They may function as “back of house” for buildings with multiple street frontages. Care should be taken to make these streets as pedestrian-friendly as possible while accommodating loading and access needs.



Broadway is a Primary Street due to the high intensity of urban activity.



Third Avenue south of Broadway is a Secondary Street within a mixed-use area.



Ninth Avenue North is a Secondary Street within a residential area.

Section IV: General Standards

Street Character

- **Other Street:** Other Streets are streets that do not fall into any of the other street categories. They may have high or moderate levels of vehicular traffic, but often have no access to property and limited pedestrian activity. Building height along these streets is regulated by the other property frontages. Buildings do not front on these streets and may be built up to the property line.
- **Alley:** Alleys are service roads that provide shared access to property. Public utilities as well as access to mechanical equipment and trash should be located off an alley whenever possible. Where alleys exist and are in working condition, or where new alleys can be created through the dedication of new right-of-way, alleys are prioritized for access and loading.

Sidewalk Corridor

- The Sidewalk Corridor is the portion of the right-of-way between the vehicular lanes and the property line or building façade.
 - The primary function of the Sidewalk Corridor is to provide a safe, comfortable, and convenient route for pedestrian travel that is separated from vehicular movements.
 - The Sidewalk Corridor is a public space that should include pedestrian amenities such as seating, shade trees, places to congregate, trash receptacles and outdoor dining.
 - The Sidewalk Corridor may accommodate public utilities such as electric poles and vaults, water and sewer lines, bus stops and traffic signals.
- As property develops within the DTC boundaries, property owners shall consult with **Metro Planning and Public Works** to make the necessary improvements to the streetscape in accordance with *the Major and Collector Street Plan*, *the Downtown Streetscape Elements Design Guidelines*, *the Strategic Plan for Sidewalks and Bikeways* and Title 17.20.120 Provision of sidewalks.



Other Streets are less traveled by pedestrians and have limited access for vehicles from adjoining lots.



Alleys provide access for parking and service areas.

Section IV: General Standards

Street Character

Street Trees

Shade-producing street trees shall be planted in the public right-of-way along the length of the lot frontage at a maximum spacing of fifty feet or in accordance with the regulations of Metro departments and agencies.

Tree Quality

Tree species shall be chosen from the *Urban Forestry Recommended and Prohibited Tree and Shrub List* based on tree size and planting area provided or an alternative species deemed appropriate by the Urban Forester.

- At planting trees, shall meet the requirements for street trees set out in the *American Standard for Nursery Stock*.
- All nursery stock used as street trees shall be vigorous, healthy and free of diseases or infestation.
- Planting Area Dimension
 - The following standards are minimum standards. All development is encouraged to provide street trees with the largest area of pervious surface and volume of soil that can be accommodated.
 - Trees shall be accommodated in planting areas with a minimum depth of 3 feet and a minimum soil volume of 400 cubic feet.
 - The minimum pervious opening at grade shall be 25 square feet.
 - Tree vaults shall have the capability to drain water.
 - Planting areas shall not inhibit ingress/egress from buildings or pedestrian traffic along the Sidewalk Corridor.

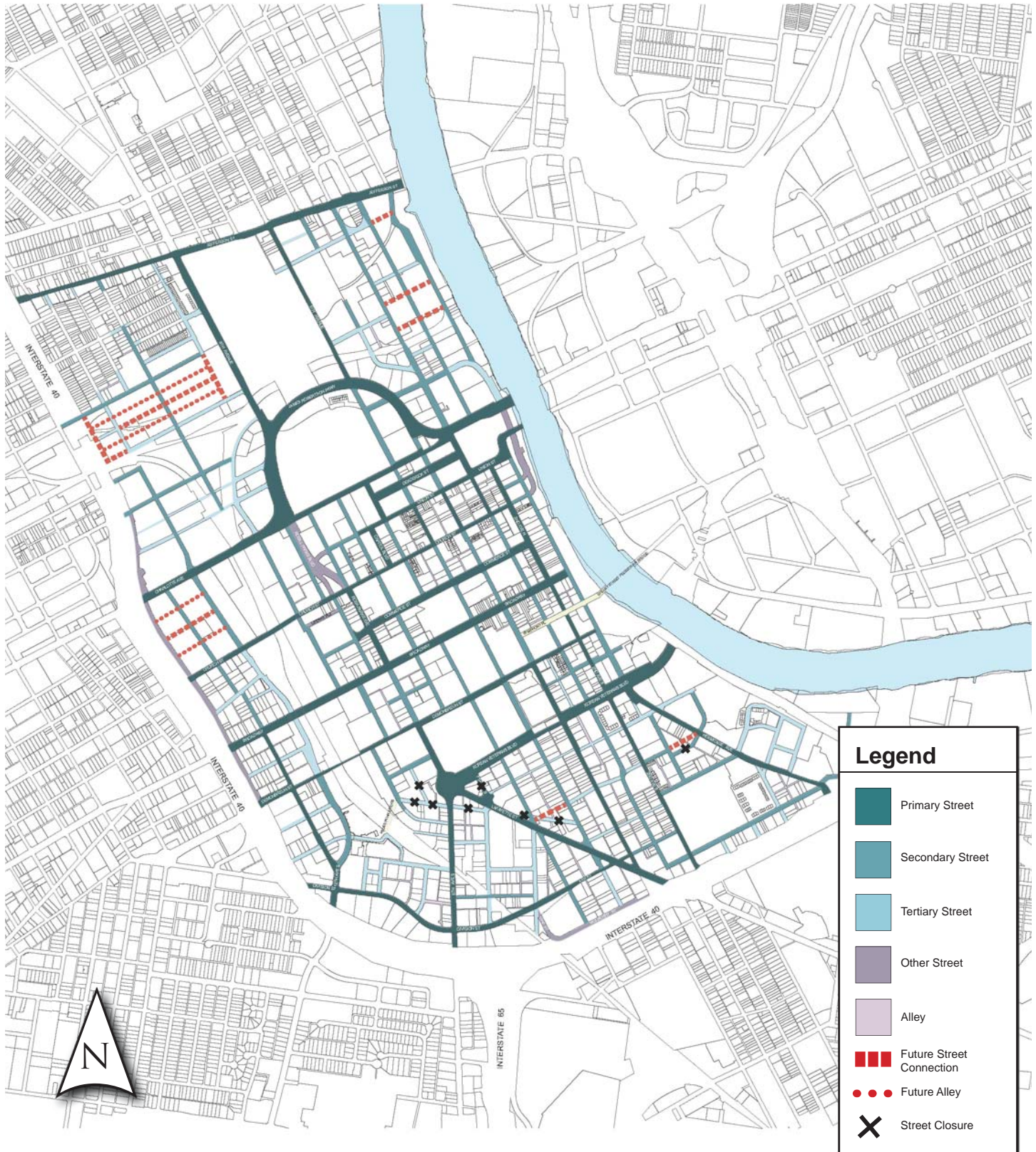
Future Streets

Downtown thrives on a connected system of streets which allow easy access within neighborhoods and to other parts of the city. There are, however, places for improvement. The Future Streets Plan show how streets could be realigned, connected and created in the future to improve mobility within Downtown.

Properties near an area highlighted for change on the Future Streets Plan shall consult with the Planning Department and the Department of Public Works to discuss the potential change.

Section IV: General Standards

Future Streets Plan



Section IV: General Standards

Lots and Frontages

Frontages

A Frontage is the specific way in which the building face addresses the street. It is the transition and interaction between the private and public realms. Building Frontages define the character and form of the public spaces within each neighborhood. The following standards shall apply to all development within the DTC.

- Buildings shall front a street (excluding alleys), open space, or a pedestrian passage.
- Principal and Minor Frontages
 - Every property shall establish one Principal Frontage along a street.
 - When a lot fronts more than one street the following priority shall be given when establishing the Principal Frontage: Primary Street, Secondary Street, Tertiary Street, Other Street.
 - Any other frontage(s) shall be treated as a Minor Frontage.
 - In the instance a property fronts multiple Primary streets, any may be chosen as the Principal Frontage or all may be treated as the Principal Frontages.
 - Along a Minor Frontage, the façade width may be reduced to the maximum depth of the building along the Principal Frontage. The remaining lot width shall be defined with a knee-wall according to the Walls and Fences section of the DTC.
 - Along a Minor Frontage, modifications may be granted for the reduction of ground level garage liners and/or glazing requirements.
- Façade Width
 - The minimum façade width is the minimum amount of the frontage that must be defined by a building and is designated as a percentage of the frontage.
 - If a single lot frontage is greater than two hundred feet, the façade width may be reduced to a minimum of one hundred and fifty feet in length.
- Open Space Frontages
 - When building facades front on open space the standards of the adjacent street type (excluding Tertiary) shall apply.
 - All buildings fronting open space shall have a minimum of one primary pedestrian entrance on the open space.
- Pedestrian Passage Frontages
 - When building facades front on a pedestrian passage the standards of the adjacent street type (excluding Tertiary) shall apply.
 - All buildings fronting a pedestrian passage shall have a minimum of one primary pedestrian entrance on the pedestrian passage.

Build-to Zone

- The Build-to Zone is the specified depth along a property's street frontage(s) in which the required minimum façade width must be located. The depth is Subdistrict and Street Type specific.
- Depending on site conditions the front of the Build-to Zone may begin at different locations.
 - When the existing sidewalk meets ~~with the Downtown Streetscape Design Guideline~~ Major and Collector Street Plan standards (including constrained right-of-way and local street dimensions as determined by Metro Planning) for sidewalk width, the Build-to Zone begins at the back of the sidewalk/property line.
 - When the existing sidewalk does not meet ~~with the Downtown Streetscape Design Guideline~~ the Major and Collector Street Plan standards (including constrained right-of-way and local street dimensions as determined by Metro Planning) for sidewalk width, the sidewalk ~~should~~ shall be widened on site and the Build-to Zone begins at the back of the new sidewalk.
 - When utility ~~or pedestrian~~ easements exist along the street frontage of a property the Build-to Zone shall begin at the back of the easement.
 - When buildings front an ~~Open~~ sSpace the Build-to Zone shall begin at the back of the ~~Open~~ sSpace.
- Attachments
 - Structures, including porches, stoops, and balconies shall not encroach beyond the front of the Build-to Zone.
 - Elements such as stairs, awnings, and landscaping may encroach beyond the front of the Build-to Zone. Any encroachments into the right-of-way must follow the Mandatory Referral process.

Section IV: General Standards

Lots and Frontages

- Entrances
 - All buildings shall have at least one pedestrian entrance on the Principal Building Frontage. This may be access to a lobby shared by individual tenants.
 - Corner entrances are appropriate on corner lots.
- Glazing
 - All street level exterior windows must have a minimum light transmission of 60 percent.
 - Modifications may be permitted insofar as it is determined that tinting does not substantially diminish the effect of the building wall or the pedestrian character of the street.
- Vehicular Access
 - When calculating the minimum façade width, access to structured parking shall be counted as part of the required façade width, and access to surface parking shall not be counted part of the required façade width. That is, access to surface parking is allowed in the “remaining” area, after the façade width requirement has been met. Surface parking is not allowed in the “remaining” area.

Active Use

- An active ground floor use requirement shall mean a habitable space occupied by retail, office, residential, institutional or recreational uses, specifically excluding parking and mechanical uses.
- An active use is required on the ground floor of all Primary streets, Secondary streets, Open Space and pedestrian passages. An active use is encouraged on Tertiary streets, particularly if the Tertiary street is the only street frontage, but is not required.
- Any references to the requirement of an active use on the ground floor on “streets” or “public streets” shall exclude Tertiary streets, Other streets, and alleys and shall include Open Space and pedestrian passages. **However, if a Tertiary street is a project’s only street frontage, all active use requirements shall apply on that frontage.**
- The term “active use” and “building liner” are synonymous.

Auto-oriented canopies and awnings

- Auto-oriented canopies and awnings, for uses such as drive-thrus and gas station pumps, may be attached to a building according to the following:
 - The building shall comply with all Frontage standards.
 - The canopy and/or awning shall be lower in height than the primary building.
 - The setback of the canopy and/or awning shall be a minimum of 15 feet from the back of the Build-to Zone.
- Exteriors
 - Any new roof or complete resurfacing of an existing roof must use a roofing material having an SRI of 29 or greater for roof slopes greater than 2:12 or SRI of 78 for slopes less than or equal to 2:12.

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Section IV: General Standards

Lots and Frontages: Specific to Storefront Frontage

The Storefront Frontage has a limited Build-to Zone that is close to the street, with building entrances accessible at sidewalk grade. The Storefront Frontage has substantial glazing on the facade at ground level, space for pedestrian-oriented signage, awnings, retail display, and other design features conducive with creating an active commercial streetscape.

The Storefront Frontage is commonly used for general commercial, office, retail, restaurant, lobby, etc.



Section IV: General Standards

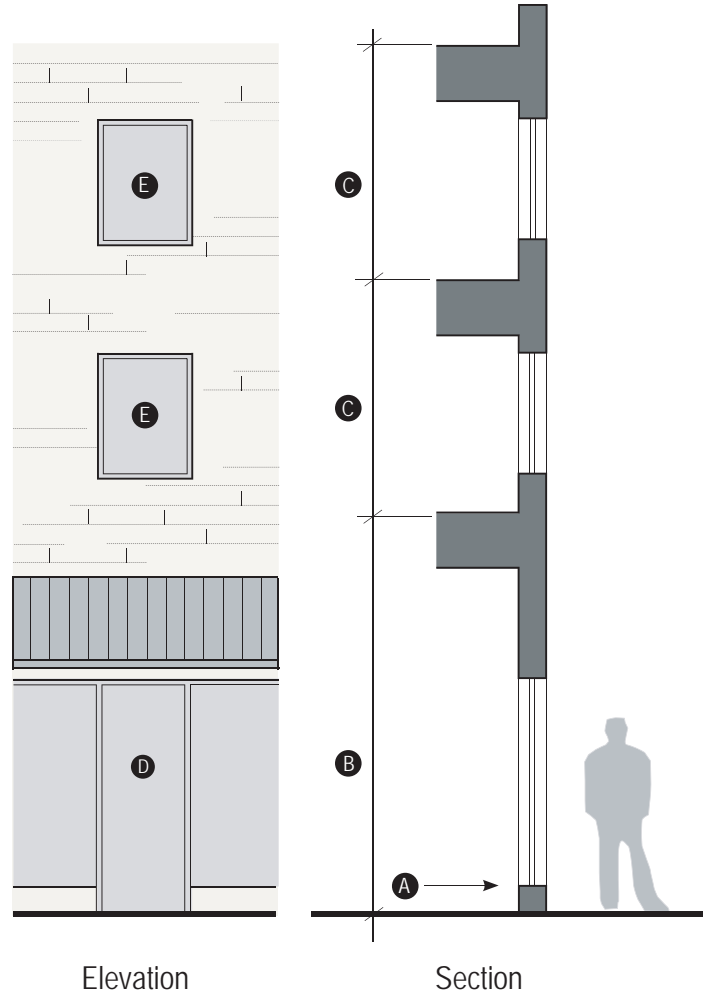
Lots and Frontages: Specific to Storefront Frontage

Storefront Frontage

| | |
|---------------------------------------|--------------------------|
| A Max. sill height | 3 ft |
| B Min. ground floor height | 14 ft from grade |
| C Min. upper floor(s) height | 10 ft floor to floor |
| D Min. ground floor glazing | |
| Principal Frontage | 40% from grade to 14 ft |
| Minor Frontage | 30 % from grade to 14 ft |
| E Min. upper floor(s) openings | 25% from floor to floor |

Notes

Where Storefront frontage is allowed, modifications may be given to allow for a Storefront arcade. All Storefront Frontage standards shall be met on the facade behind the arcade.



Section IV: General Standards

Lots and Frontages: Specific to Stoop Frontage

The Stoop Frontage has a limited to moderate Build-to Zone with the first floor elevated from the sidewalk grade. This frontage type utilizes a stoop - a small landing connecting a building entrance to the sidewalk by a stair or ramp - to transition from the public sidewalk or open space into the building.

Stoops are generally provided externally, but may be provided internally as necessitated for ADA compliance.

The Stoop Frontage is generally used for residential and live-work buildings, but may be appropriate for other uses.



Section IV: General Standards

Lots and Frontages: Specific to Stoop Frontage

Stoop Frontage

| | |
|---------------------------------------|-------------------------|
| A First floor elevation | |
| Min. | 18" from grade |
| Max. | 5 ft from grade |
| B Min. ground floor openings | 30% floor to floor |
| C Min. upper floor(s) openings | 25% from floor to floor |

Stoop

- D** Min. stoop width 5 ft
- E** Stoops may not extend beyond the front of the Build-to Zone.
- F** Steps may extend beyond Build-to Zone, but may not encroach into the public Right-of-Way.

Notes

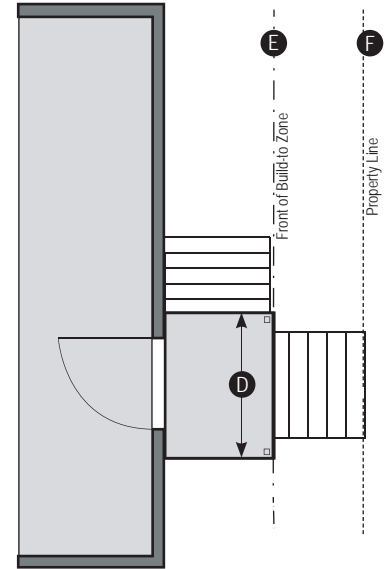
Greater first floor elevation allowed by modification for:

- Property with significant elevation change across the site at the street frontage.
- Development that incorporates below grade basement floors that are accessible from the exterior of the building.

Transition to first floor elevation may be accommodated on the interior of the building to allow for compliance with ADA accessibility requirements.

Entries shall not be recessed more than 4 feet from the facade of the building.

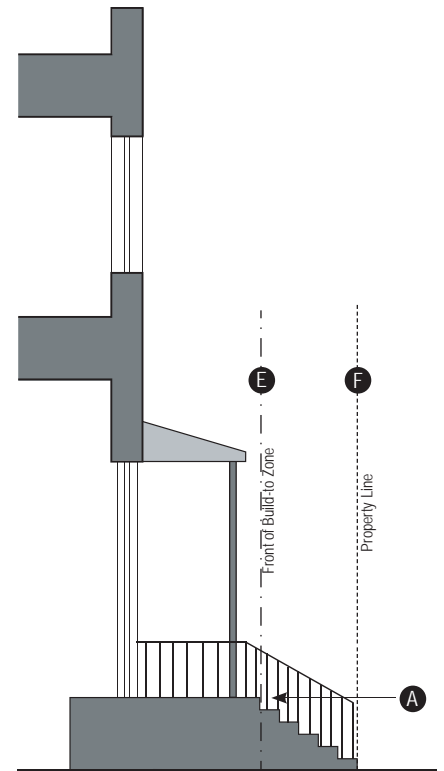
Doors shall face the street.



Plan



Elevation



Section

Section IV: General Standards

Lots and Frontages: Specific to Porch Frontage

The Porch Frontage has a moderate Build-to Zone with the first floor elevated from the sidewalk grade. The Porch Frontage utilizes a porch - an open air room appended to the mass of a building with floor and roof but no walls on at least two sides - to transition from the public sidewalk or open space into the building.

The Porch Frontage is primarily used for residential buildings.



Section IV: General Standards

Lots and Frontages: Specific to Porch Frontage

Porch Frontage

A First floor elevation

| | |
|------|-----------------|
| Min. | 18" from grade |
| Max. | 5 ft from grade |

B Min. ground floor openings

30% floor to floor

C Min. upper floor(s) openings

25% from floor to floor

Porch

- D Min. porch depth 5 ft
- E Stoops may not extend beyond the front of the Build-to Zone.
- F Steps may extend beyond Build-to Zone, but may not encroach into the public Right-of-Way.

Notes

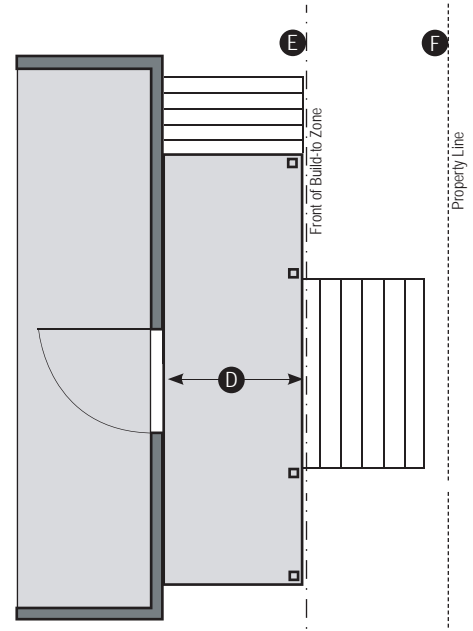
Greater first floor elevation allowed by modification for:

- Property with significant elevation change across the site at the street frontage.
- Development that incorporates below grade basement floors that are accessible from the exterior of the building.

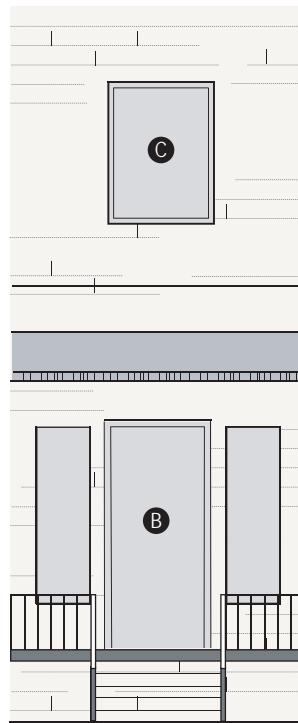
Transition to first floor elevation may be accommodated on the interior of the building to allow for compliance with ADA accessibility requirements.

Entries shall not be recessed more than 4 feet from the facade of the building.

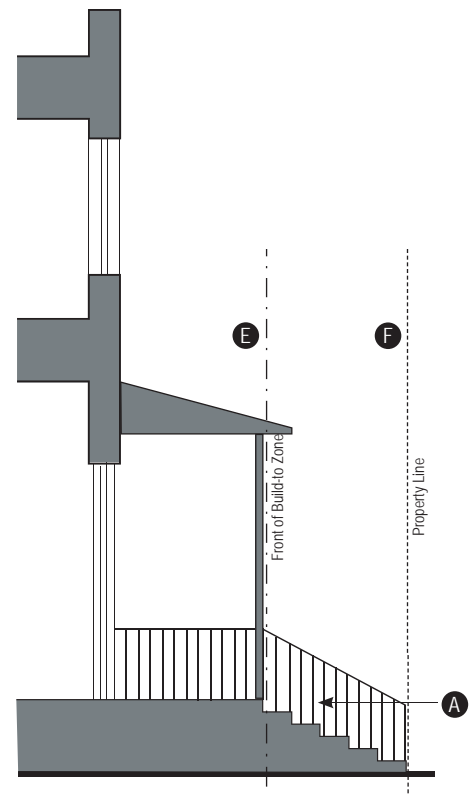
Doors shall face the street.



Plan



Elevation



Section

Section IV: General Standards

Lots and Frontages: Specific to Industrial Frontage

The Industrial Frontage shall be used to adapt existing buildings to the standards of the DTC and for new construction of buildings intended for industrial uses. The Industrial Frontage shall be allowed only in specified subdistricts on specified streets and shall be prohibited on Primary Streets.

The Industrial Frontage mitigates the negative impact of the “blank wall” on the street by requiring the Build-to Zone to be entirely landscaped with drought-resistant plantings. All landscaping shall be in a functioning bioswale, or irrigated using drip irrigation or sub-surface irrigation. If drought-tolerant species are used, no irrigation is required.

A primary pedestrian entrance is required on the Principle Facade.

Associated vehicular entrances shall comply with the Parking and Access and Mechanical, Screening and Loading standards.

Section IV: General Standards

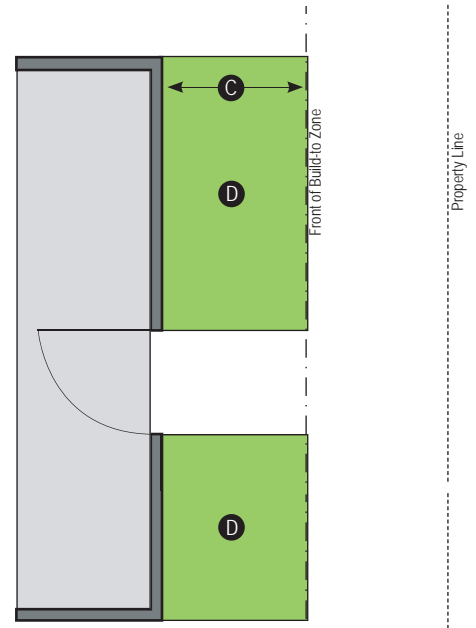
Lots and Frontages: Specific to Industrial Frontage

Industrial Frontage

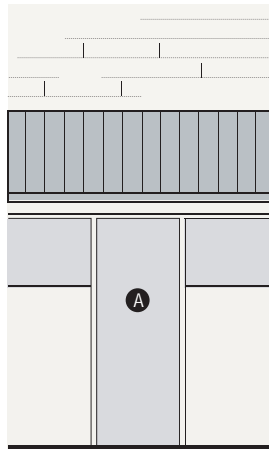
| | |
|----------------------------------|---|
| A Front door | Required on Principle Facade |
| B Min. Building Height | 25 ft from grade |
| C Build-to Zone | 5-10 ft |
| D Landscaping | Entire Build-to Zone shall be landscaped with drought resistant species; in a bioswale or irrigated 2'-6" |
| E Min. landscaping height | |

Notes

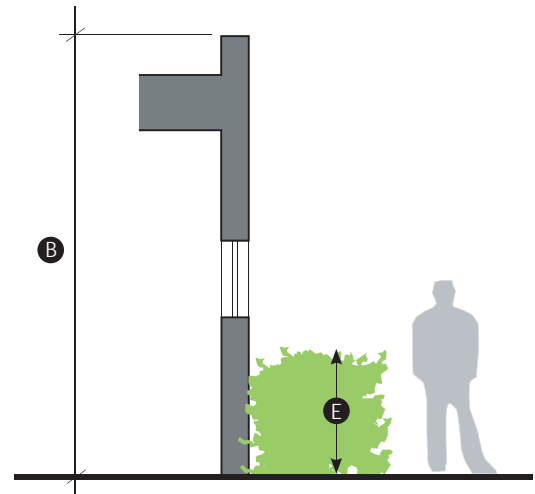
Industrial Frontage is prohibited on Primary streets. A building intended for an industrial use, with frontage on a Primary street shall comply with the standards of another frontage type on the Primary street.



Plan



Elevation



Section

Section IV: General Standards

Lots and Frontages: Specific to Civic Frontage

Civic buildings are designed and constructed for community use or benefit by governmental, cultural, educational, public welfare, or religious organizations. Civic buildings are inherently unique structures that present opportunities for unusual and iconic design within the urban fabric. Civic buildings should be designed with prominence and monumentality.

A Civic building shall be oriented to streets and public spaces and follow the intent of the particular subdistrict in which it is located with regard to pedestrian orientation, massing, and articulation.

Key architectural features should act as community focal points. Where possible, street axes should be terminated by the primary building form or architectural feature. Towers, spires, and other vertical forms are encouraged.

Civic buildings may include the following: community buildings, libraries, post offices, schools, religious institutions, publicly owned recreational facilities, museums, performing arts buildings, and municipal buildings.

Civic buildings shall be reviewed by modification.



Section IV: General Standards

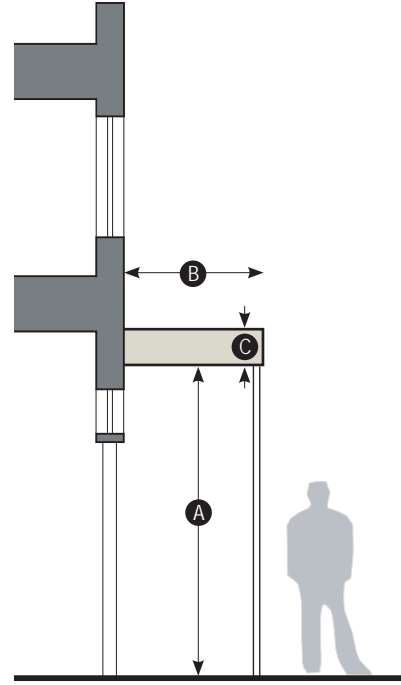
Canopies and Awnings

Canopies

| | |
|--------------------------------|-------------------|
| A Clearance | |
| Minimum from sidewalk | 8' |
| Minimum with ROW encroachment | 14' |
| Maximum | 25' |
| B Maximum projection | within 2' of curb |
| C Maximum canopy height | 4' |

Notes

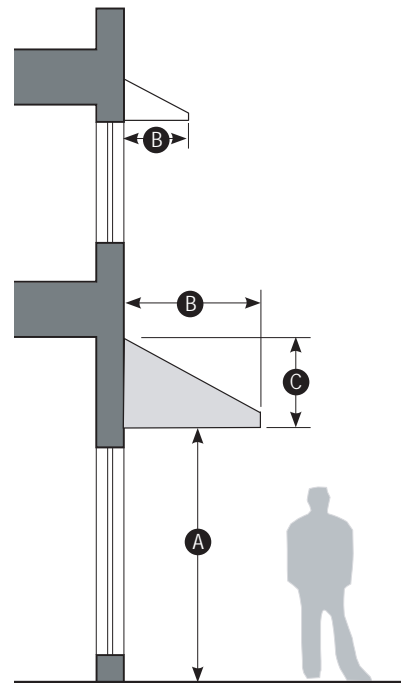
Canopies shall be permitted only over pedestrian and vehicular building entrances, and shall not be permitted above windows. Canopies shall be constructed as a roof-like structure. Fabrics and non-rigid plastic are prohibited.



Section

Canopies Awnings

| | |
|--------------------------------|----------------|
| A Clearance | |
| Minimum from sidewalk | 8' |
| Minimum with ROW encroachment | 14' |
| B Maximum projection | |
| First floor | 4' from facade |
| Upper floors | 2' from facade |
| C Maximum awning height | 5' |



Section

See the Lots and Frontages section for details on auto-oriented canopies and awnings.

Encroachments in the public right-of-way must meet Metropolitan Government's current clearance standards and be approved under the mandatory referral process prior to installation.

Section IV: General Standards

Parking and Access: General

Parking Requirements

- No parking is required within the boundary of the DTC.

Parking and Access General Standards

- In addition to the Parking and Access standards of the DTC the following shall apply:
 - 17.20.050 Handicapped parking, 17.20.060 Parking area design standards, and 17.20.130 Loading space requirements.
- When alleys are present, vehicular access from alleys is **preferred required**. Access from public streets shall be considered in the following order: Other Streets, Secondary Streets, Neighborhood Streets and then Primary Streets as approved by Metro departments. Reviewers shall consider the public safety, street character, and pedestrian experience.
- Vehicular / Pedestrian Conflict
 - Valet and “drop-off” areas shall be located within the right-of-way when space allows.
 - **If not provided within the right-of-way, valet and drop-off areas should be located internal to any constructed parking structures.**
 - Where driveways to parking facilities or drop-off areas cross the Sidewalk Corridor, **priority shall be given to the pedestrian realm and** the following design elements shall be required:
 - **The DTC and MCSP sidewalks and tree planting standards shall be maintained for any pedestrian islands created.**
 - **Curbcuts shall not exceed 24’ in width for two-way traffic, and shall not exceed 20’ in width for one-way traffic.**
 - Bollards or other protective device shall be used to separate pedestrian and vehicular areas.
 - Distinction between vehicular lane and pedestrian areas shall be indicated through changes in grade, color, texture and/or material.
- To reduce stormwater fees and impact, utilize Low Impact Development strategies published in Metro Water Services Stormwater BMPs for hardscaping, including parking and drive lanes.

Section IV: General Standards

Parking and Access: Specific to Structured Parking

Vehicular Access

- Vehicular openings to parking structures shall not exceed thirty-five feet in width.
- Vehicular openings shall have a minimum spacing of thirty-five feet.
- A parking structure shall be permitted no more than two vehicular access points.
- When alleys are present, at least one access point shall come from the alley.
 - Where extraordinary constraints can be demonstrated to exist that preclude access from an alley, vehicular access from a public street may be considered by modification.

Pedestrian Access

- All parking structures with parking available to the public shall have a clearly marked pedestrian entrance, separate from vehicular access, on street frontages. A publicly accessible building lobby may meet this requirement.

Location and Lining

- On the ground level, parking structures shall be located behind a liner building with an active use that is a minimum of fifteen feet deep.
- Upper level habitable liners are encouraged on all streets and are required on the south side of Division Street (within the Gulch South Subdistrict) and on James Robertson Parkway. See the Bonus Height Program for more information on incentives for upper level garage liners.
- Upper level facade treatments /cladding is required on all public street frontages, including any facades visible from the Interstates. Facade treatments shall integrate or complement the architectural characteristics of the habitable portion of the building and the surrounding built context.
- Underground parking that is visible from the street, shall not extend beyond the façade of the building. Underground parking that is completely below grade may extend beyond the façade of the building. Underground parking may not encroach into the right-of-way.

Section IV: General Standards

Parking and Access: Specific to Surface Parking

General Standards for Surface Parking

- Parking area screening and landscaping standards shall apply to all surface parking lots including, but not limited to, public and private parking facilities, driveways and access aisles, the outdoor display of automobiles and other vehicles that are for sale or lease.

Perimeter Screening Standards for Surface Parking

- Parking areas adjacent to public streets and open space shall be separated from the edge of the right-of-way and/or property line by a perimeter landscape strip a minimum of five feet in width which shall be landscaped per the standards of this section.
 - All perimeter landscape strips adjacent to public streets and open space shall include a fence or wall in accordance with the Fence and Wall Standards.
- Parking areas shall be separated from adjacent side lot lines by a perimeter landscape strip a minimum of 5 feet in width, which shall be landscaped per the standards of this section.
 - A two and one-half foot landscape strip may be provided if the required trees are to be planted in tree islands located adjacent to the property line.
 - Two adjacent properties may share equally in the establishment of a seven-foot (minimum) planting strip along the common property line. In instances where the common perimeter planting strip is part of a plan for shared access, each owner may count the respective area contributed toward that common planting strip toward the interior planting area requirements for the lot.
- Berms are not permitted in any landscape strips.

Interior Planting Requirements

- Parking areas shall be landscaped in accordance with the interior planting requirements of Title 17.24.160.
- Parking areas with less than twelve thousand square feet in total area shall be exempt from the interior and side lot line planting requirements.

Landscape Materials

- Perimeter landscape strips along public streets, open space and side lot lines.
 - Trees shall be installed at a rate of one tree for every thirty feet of frontage. Spacing may be adjusted with the approval of the Urban Forester based upon tree species, the presence of utilities, and the dimensions of the planting strip.
 - Evergreen shrubs and trees shall be installed at appropriate spacing to fully screen vehicles to a minimum height of two and one-half feet.
 - Plantings within fifteen feet of driveways or street intersections shall be maintained to a maximum height of two and one-half feet.
 - Plantings shall not obstruct views onto site as to impede the security of users.
- Tree and shrub species shall be chosen from the Urban Forestry Recommended and Prohibited Tree and Shrub List or an alternative species deemed appropriate by the Urban Forester.
- At planting, trees shall be a minimum of six feet in height and two caliper inches.
- All landscaping shall be in a functioning bioswale, or irrigated using drip irrigation or sub-surface irrigation. If drought-tolerant species are used, no irrigation is required.
- At planting, all landscaping shall meet the standards for size, form and quality set out in the American Standard for Nursery Stock (ANSI Z60.1, latest edition).
- All nursery stock shall be vigorous, healthy and free of diseases or infestation.

Section IV: General Standards

Mechanical, Service, and Loading

Applicability

The following elements shall be shielded from view from adjacent public streets, pedestrian corridors, and open spaces.

- Refuse collection, dumpsters, recycling bins, and refuse handling areas that accommodate a dumpster or five or more trash or recycling cans.
- Building or ground-mounted mechanical equipment, including, but not limited, to transformers, backflow preventors, telephone risers, equipment cabinets, generators, or similar devices.
- Mechanical equipment on roofs.
- Air conditioning or similar HVAC equipment.
- Loading docks, berths, or similar spaces including, but not limited, to service entrances and maintenance areas.
- Outdoor storage of materials, equipment, and vehicles.

Location and Access

- Applicable site elements shall be located along the alley, along an interior property line, or internal to the property.
- Service elements, such as loading docks and trash collection locations, should not be accessible from Primary streets, unless a Primary street is the only frontage.
- **Where access to loading areas and service elements cross the Sidewalk Corridor, priority shall be given to the pedestrian realm and the following design elements shall be required:**
 - **The DTC and MCSP sidewalks and tree planting standards shall be maintained for any pedestrian islands created.**
 - **Bollards or other protective device shall be used to separate pedestrian and vehicular areas.**
 - **Distinction between vehicular lane and pedestrian areas shall be indicated through changes in grade, color, texture and/or material.**

Screening Standards

- Applicable site elements shall be fully screened at all times, including immediately following planting if vegetative materials are to be used.
- Refuse collection and refuse handling areas shall be screened by a walled enclosure with gates in accordance with the Fence and Wall Standards of the DTC.

Screening Methods

- **Vegetative Materials:**
 - Vegetative materials shall be planted in two rows in staggered fashion.
 - All trees shall be evergreen with a minimum height at time of planting of at least six feet above the root ball.
 - All shrubs shall be evergreen with the minimum height and spacing necessary to fully screen the item intended for screening (but no less than thirty inches in height) at the time of planting.
 - Vegetative material shall be located immediately adjacent to the element being screened in a planting area a minimum of four feet wide.
- **Fencing and Walls**
 - Screening is permitted through the use of a fence or wall constructed in accordance with the Fences and Walls Standards of the DTC.
- **Parapet Walls**
 - Parapet walls or other techniques included as an integral part of the building design shall be used to totally screen any rooftop mechanical equipment from view from adjacent public rights-of-way or open space.
- **Integrated Building Elements or Features**
 - Building design or other structural features (e.g., knee walls, alcoves, wing walls, roof extensions, etc.) may also be used to fully or partially enclose site features required to be screened.
- **Alternative Screening Methods**
 - Alternative screening methods or materials that are not listed may be used following approval by the Planning Commission or its designee, provided that they are determined to be comparable to screening methods described in this subsection.

** In order to properly locate and screen mechanical equipment, approval may be required from applicable Metro departments and agencies.

Section IV: General Standards

Fences and Walls

Location

- Permitted Locations: Fences and walls constructed in accordance with the standards in this section may be constructed within:
 - The Build-to Zone.
 - A utility easement only through the express written consent from the utility or entity holding the easement.
 - A required landscape area, Tree Protection Zone, or open space.
- Prohibited Locations: No fence or wall shall be installed that:
 - Encroaches into a right-of-way (without approval through the Mandatory Referral process).
 - Blocks or diverts a natural drainage flow on to or off of any other land.
 - Compromises safety by blocking vision at street intersections or obstructs the visibility of vehicles entering or leaving driveways or alleys.
 - Blocks access to any above ground or pad-mounted electrical transformer, equipment vault, fire hydrant or similar device.

Appearance

- All fences shall be installed so that the finished side shall face outward; all bracing shall be on the inside of the fence.
- Fences and walls shall be constructed of any combination of brick, stone, masonry materials, treated wood posts and planks, rot-resistant wood, or metal. Chain link fencing shall be coated with dark green or black vinyl when visible from a public street or open space (excluding alleys).
- Chain-link fences are prohibited within the Build-to Zone.
- Razor wire is prohibited within the Build-to Zone.
- Fences and walls used to screen refuse areas shall be opaque and include gates that prohibit unauthorized users to access the area.

Standards by function and location

- Fences and walls within the Build-to Zone shall not exceed four feet in height.
 - Modifications may be made in order to properly secure playgrounds and parks.
 - The height of fences and walls along a sidewalk shall be measure from sidewalk grade.
- Fences and walls within the Build-to Zone that are greater than three feet high shall be a minimum of thirty percent transparent to allow visibility into the property.
- Fences and walls used to screen parking shall be a minimum of two and one-half feet above the grade of the parking lot.
 - When a fence or wall is combined with plantings the majority of the plantings shall be between the right-of-way and the fence or wall.
- Fences and walls used to screen mechanical, loading and refuse elements shall be a minimum of two feet taller than the element being screened.
- All other fences and walls shall have a maximum height of ten feet measured from grade.
- Fences surrounding athletic fields and courts may exceed the previous height limitations.

Section IV: General Standards

Open Space

The *Downtown Community Plan: 2007 Update* envisions accessible, enjoyable open spaces to help create vital and functioning neighborhoods within Downtown. To meet this goal, the DTC encourages many types of open spaces to serve the needs of both citizens and visitors for passive and active recreation. Public art, other amenities, and interactive features are encouraged in open spaces. Buildings conducive with the use of the Open Space and for public use may be approved by the Planning Commission and Parks Department.

It is a goal of the DTC to have open space within each ¼ mile radius neighborhood in Downtown. All public and private open spaces, greater than one-half acre have been mapped in order to determine the areas within the DTC that are in need of neighborhood open space. The one-quarter mile radius buffer area around existing open spaces is indicated in green on the Open Space Map. The areas that lack open space within one-quarter mile radius (deficiency areas) are shown in yellow. Public open space developed in any portion of a deficiency area will count toward fulfilling the open space need of that area. The development of the needed open space may be done by public or private entities.

The open space types and standards listed in the DTC shall be utilized by property owners in the development of public open space. The following standards shall apply to open space that is accessible to the public, including open space developed for credit under the Bonus Height Program. Private open space or amenity areas shall not be counted toward fulfilling the open space need and shall not be eligible for the Bonus Height Program.

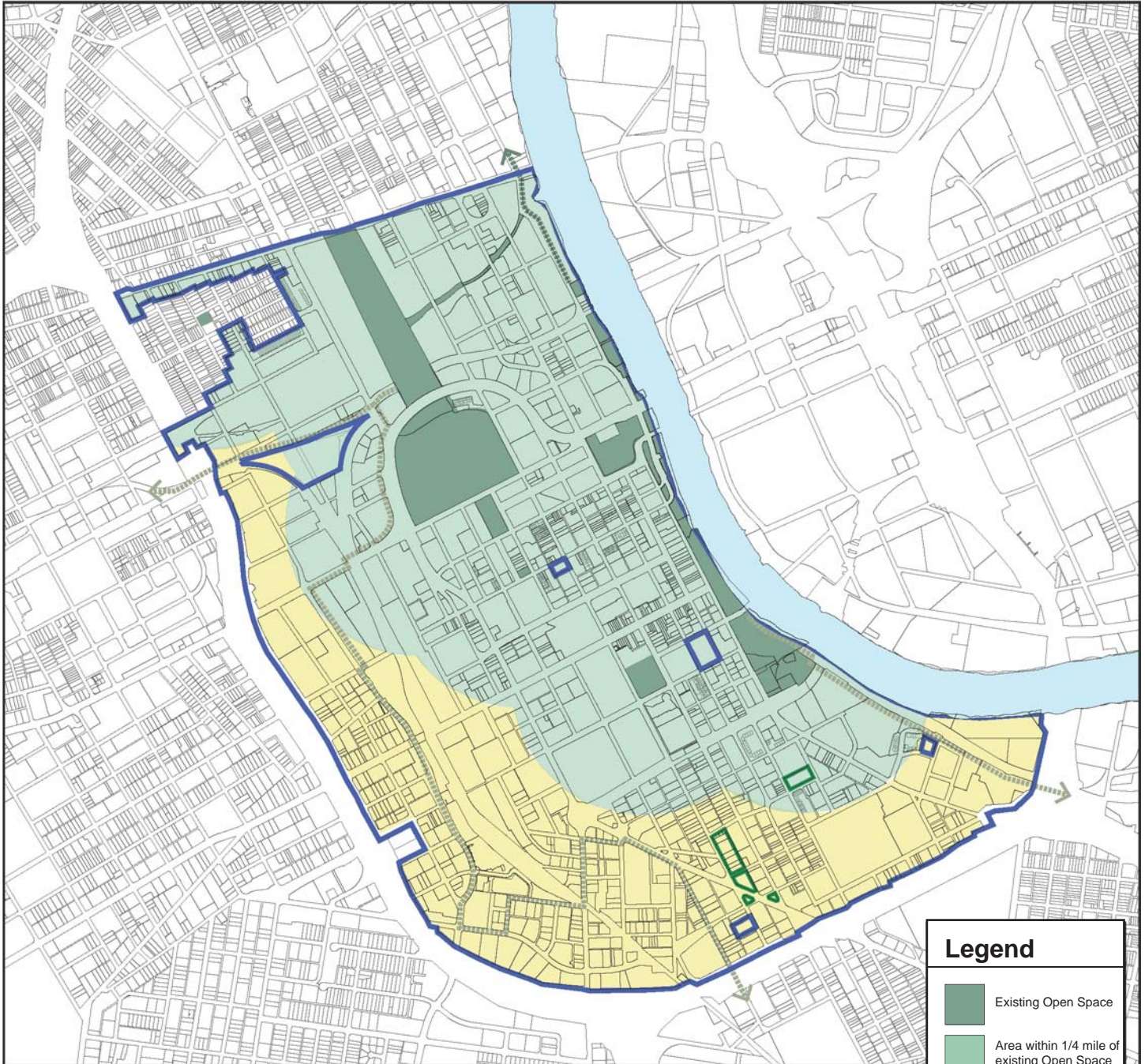
Open Space developed within the deficiency areas are eligible for greater bonuses through the Bonus Height Program than those developed in non-deficiency areas. See the Bonus Height Program section for more details.

The Open Space Plan also includes the existing and planned urban greenways.

Parks and greenways are publicly owned open space and shall follow the standards set out by the Parks Department.

Section IV: General Standards

Open Space Plan



Legend

- Existing Open Space
- Area within 1/4 mile of existing Open Space
- Area with an Open Space Deficiency
- Existing Urban Greenway
- Future Urban Greenway
- Potential parcels for Open Space
- DTC Boundary



Section IV: General Standards

Open Space: General Standards

Standards of Title 17 not varied by the following Open Space Standards shall apply within the DTC.

Calculation

- When calculating the open space square footage or acreage, the footprint of any building, whether public or private, shall be subtracted first. The remaining square footage shall be used for all calculations and percentages.

Access

- Every open space shall have a minimum of one primary pedestrian entrance along each street frontage and pedestrian frontage.
- All publicly accessible open space shall meet the appropriate standards of the American's with Disabilities Act.

Building frontage

- Buildings that are part of the same development as the open space and abut the open space shall have a minimum of one pedestrian entrance on the open space.

Seating

- Permitted types of seating include but are not limited to, moveable, fixed individual seats, fixed benches, seat walls, planter ledges and seating steps.

Paving Materials

- Asphalt may be approved by the Planning Commission or its designee for recreational jogging or bicycle paths only.

Landscaping

- Ground level green space shall consist of turf grass, shrubbery, perennial and annual beds, mulched areas and generally areas with "natural" material planted within six inches of grade.
 - Ground level green space does not include container plantings.
- Pervious surfaces include green space, porous concrete and modular pavers, areas with tree grates or areas that otherwise allow water to infiltrate into the soil.
- Trees shall be accommodated in planting areas with a minimum of 600 cubic feet of soil. When using structural soil, the planting area may be reduced to 300 cubic feet. The minimum opening at grade shall be 25 square feet.
- Planting areas shall not impede ingress/egress from buildings or pedestrian traffic.
- Tree Grates
 - When used, tree grates shall be modular and allow for removal as tree grows.
 - Tree grates shall be flush with grade to allow for unobstructed movement of pedestrian traffic.
- Tree and shrub species shall be chosen from the Urban Forestry Recommended and Prohibited Tree and Shrub List based on tree size and planting area provided or an alternative species deemed appropriate by the Urban Forester.

Section IV: General Standards

Open Space: Specific to Greens

Greens are larger, less formal Open Space consisting of a majority green space with laws, paths, and vegetation.

- A minimum of two sides shall be street frontages.
- Size ½ acres - 6 acres
- Greens shall maintain a minimum of 60 percent ground level green space and 70 percent pervious surface.
- Seating
 - A minimum of one linear foot of seating shall be required for each 900 square feet of gross open space.
 - Of the required seating, one linear foot for each 20 feet of street frontage shall be located within 15 feet of the property line.

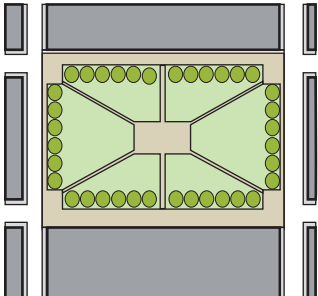


Section IV: General Standards

Open Space: Specific to Squares

Squares are Open Space used for unstructured recreational or civic uses. Landscaping consisting of lawns and trees is formally composed.

- A minimum of two sides shall be street frontages.
- Size: 1/2 acre – 5 acres
- Squares shall be required to maintain a minimum of 30 percent ground level green space and 50 percent pervious surface.
- Seating
 - A minimum of one linear foot of seating shall be required for each 700 square feet of gross open space.
 - Of the required seating, one linear foot for each 20 feet of street frontage shall be located within 15 feet of the property line.

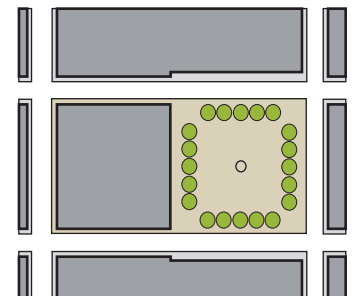


Section IV: General Standards

Open Space: Specific to Plazas

A Plaza is an Open Space used for unstructured civic and/or commercial purposes. A plaza is spatially defined by building frontages.

- Size: 2500 square feet – 20,000 square feet
- Plazas are required to maintain a minimum of 10 percent ground level green space and 40 percent pervious surface.
- Seating
 - A minimum of 40 linear feet of seating shall be required for Plazas, plus a minimum of one linear foot of seating for every 500 square feet of gross open space.
 - Of the required seating one linear foot for each 20 feet of street frontage shall be located within 15 feet of the property line.

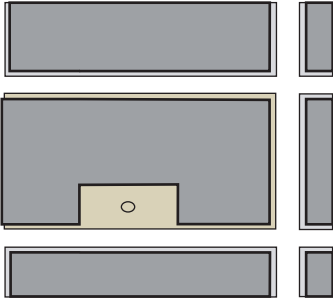


Section IV: General Standards

Open Space: Specific to Courts

A Court is Open Space accessible from the street and used for entry into a building. A Court is spatially defined by building frontages and is generally tucked back into the building.

- Size: 400 square feet – 2,500 square feet
- Seating
 - A minimum of 10 linear feet of seating shall be required for Courts, plus one linear foot of seating for each 300 square feet of open space minimum.
- Accessibility
 - Courts are permitted to be closed to the public by use of a gate. See the Fence and Wall Standards for details.

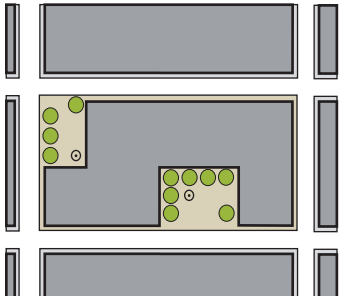


Section IV: General Standards

Open Space: Specific to Pocket Parks and Playgrounds

Pocket Parks and Playgrounds are Open Space that are accessible from the street and used for structured recreation, gardening or other community use.

- Size: 800 square feet – 1 acre
- Pocket Parks shall provide a community benefit such as a garden or playground.
- Pocket Parks are required to maintain a minimum of 20 percent ground level green space.
 - Additional ground area may be impervious provided space is structured for active recreation.
- Seating:
 - A minimum of one linear foot of seating shall be required for every 300 square feet of gross open space.
- Trees
 - The tree requirement may be waived for pocket parks that are designed for structured active recreation, such as basketball or tennis courts.
- Accessibility
 - Pocket Parks may be fenced for safety but shall remain open to the public during daylight hours.



Section IV: General Standards

Bonus Height Program

The Bonus Height Program (BHP) allows additional building height in Downtown in exchange for contribution to specified programs that provide benefits to the public. The Bonus Height shall be permitted if the proposed development contributes to specific public benefits in the amount and manner set forth herein.

Bonus Height shall be permitted in exchange for the following public benefit contributions: Leadership in Energy and Environmental Design (LEED) certification of individual buildings, LEED for Neighborhood Development, pervious surface, publicly-accessible Open Space, Workforce Housing, Civil Support Space, upper level garage liners, and underground parking.

Density bonuses for the preservation of historic buildings are given through the Transfer of Development Rights Program as outlined in Section 17.12.120 of the zoning code.

Bonus Height Standards

- Upon providing a binding commitment for the specified public benefit, the proposed development project shall be allowed to build within the restrictions of the Subdistrict, up to the Bonus Height Maximum as established within this section.
- Multiple height bonuses may be compounded insofar as the total additional height does not exceed the Bonus Height Maximum for the Subdistrict.
- Additional development rights achieved through the BHP may be transferred to another site within the DTC, provided the transferred height does not exceed the Bonus Height Maximum of the receiving site. By-right height may not be transferred; only bonus height received through the BHP may be transferred.
- Bonus height transfers shall be based on the square footage of the sending site, not the receiving site.
- No building permit shall be issued for bonus height until the Planning Commission has certified compliance with the provisions of this section, upon referral and assurance of compliance from applicable departments.

Section IV: General Standards

Bonus Height Program

LEED and LEED ND

The U.S. Green Building Council (USGBC) is a non-profit organization that oversees the Leadership in Energy and Environmental Design (LEED) Green Building Rating System.

LEED for Neighborhood Development integrates the principles of smart growth, urbanism and green building into the first national system for neighborhood design. LEED ND goes beyond the building to address sustainability on a neighborhood-wide basis.

The bonuses are specific to each Subdistrict. See the BHP Chart for details.

A different nationally-recognized, third-party system of overseeing green building and/or sustainable development practices may be substituted for LEED. Bonuses will be determined by the Planning Commission based on ratings equivalent to LEED silver, gold, and platinum.

Bonuses for individual buildings are given upon pre-certification of LEED silver, gold and platinum. Bonuses for neighborhoods are given upon pre-certification of LEED ND. Every property within the LEED ND neighborhood may utilize the bonus height. The bonuses are specific to each Subdistrict. See the BHP Chart for details.

The following shall apply to all new construction that utilizes the Bonus Height Program for LEED:

- Prior to issuance of a temporary certificate of occupancy for any use of the development, a report shall be provided for the review of the Department of Codes Administration and the Planning Commission by a LEED accredited professional. The report shall certify that all construction practices and building materials used in the construction are in compliance with the LEED certified plans and shall report on the likelihood of certification. If certification appears likely, temporary certificates of occupancy (as set forth below) may be issued. Monthly reports shall be provided as to the status of certification and the steps being taken to achieve certification. Once certification is achieved, the initial certificate of LEED compliance, as set forth herein, and a final certificate of occupancy (assuming all other applicable conditions are satisfied) shall be issued.
- To ensure that LEED certification is attained the Department of Codes Administration is authorized to issue a temporary certificate of occupancy once the building is otherwise completed for occupancy and prior to attainment of LEED certification. A temporary certificate of occupancy shall be for a period not to exceed three (3) months (with a maximum of two extensions) to allow necessary time to achieve final certification. Fees for the temporary certificate (and a maximum of two extensions) shall be \$100 or as may otherwise be set by the Metro Council. Once two extensions of the temporary certificate of occupancy are granted, any additional extensions shall be granted only in conjunction with a valid certificate of LEED noncompliance as set forth herein.
- If the property fails to achieve LEED certification, the Department of Codes Administration is authorized to issue a short-term certificate of LEED noncompliance. This certificate will allow the building to retain its certificate of occupancy pending attainment of LEED certification. A certificate of LEED noncompliance shall be for a period not to exceed three (3) months and may be renewed as necessary to achieve certification. The fee for noncompliance shall be issued every time the certificate is issued for up to ten years.
- The fee for a certificate of LEED noncompliance shall be based on the following formula: $F = [(CN-CE)/CN] \times CV \times 0.0075$, where:
 - F is the fee;
 - CN is the minimum number of credits to earn the level of LEED certification for which the project was pre-certified;
 - CE is the number of credits earned as documented by the report; and
 - CV is the Construction Value as set forth on the building permit for the structure.

Section IV: General Standards

Bonus Height Program

Pervious Surface

The integration of pervious surfaces into site design and building design benefits the individual development, the neighborhood and the city. Pervious surfaces can reduce stormwater runoff, flood risk, irrigation needs and the burden on infrastructure. Examples of pervious surfaces include impervious pavement, green roofs, bio-swales, landscaping, and green screens. As technology in this field advances, additional pervious surfaces may meet the intent of this standard.

- The number of square feet of Bonus Height shall be twice that of the number of square feet of Pervious Surface. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Green roofs that are utilized to meet LEED certification may not be “double counted” for both the LEED height bonus and the Pervious Surface height bonus. If the level of LEED certification would be met without the green roof, then the green roof may be counted for the Pervious Surface height bonus.

Publicly-Accessible Open Space

Accessible, enjoyable open spaces are essential for vital and functioning neighborhoods. Open space provides the community with opportunities to be in an outdoor setting, while encouraging social interaction. See the BHP Chart for details for a list of Subdistricts in which the Open Space bonus may be utilized.

Open Space must be designed to the open space standards of the DTC. To be eligible for the Height Bonus, open space must be a minimum of ¼ acre in area.

- Plazas are not eligible for the BHP.
- In Open Space deficiency areas (See the Open Space section of the General Standards), the number of square feet of Bonus Height shall be seven times that of the number of square feet in open space. Outside of Open Space deficiency areas, the number of square feet of Bonus Height shall be four times that of the number of square feet in open space. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Bonuses are available only for publicly accessible (whether publicly or privately owned) open space.

Section IV: General Standards

Bonus Height Program

Workforce Housing

Housing encompassing a range of sizes, costs and tenure (both rental and owner-occupied), to accommodate the diverse range of employees and their families, is key to Downtown's continued economic health and to sustainable development patterns for Nashville/Davidson County.

Metro Government shall require the developer to execute an agreement, restrictive covenant, or other binding restriction on land use that preserves affordability and establishes the manner in which the affordability will be monitored for the required period before final site plan review.

As listed below, the specified percentage of total units shall be reserved for ownership or rental by households with incomes below the specified percentage of the current Average Median Income (AMI) in Davidson County, as determined by MDHA.

Height bonuses are based on a percentage of the Maximum Height allowed on the property as dictated by the Subdistrict. In all cases, fifty percent (50%) of the additional stories shall be dedicated to Workforce Housing with twenty-five percent (25%) of the height bonus provided as housing for persons below one hundred percent (100%) of AMI and twenty-five percent (25%) of the height bonus provided as housing for persons below one hundred-twenty percent (120%) of AMI.

Workforce Housing Height Bonuses are:

- Level 1 - 10% increase in stories
- Level 2 - 20% increase in stories
- Level 3 - 30% increase in stories
- Level 4 - 40% increase in stories

Not all Levels are available in every Subdistrict. See the BHP Chart for details.

When percentage calculations result in a fraction of a story, the number of stories shall be rounded up.

The following shall apply to all construction that utilizes the BHP for Workforce Housing:

- Owner-occupied units shall remain affordable for 30 years.
- Renter-occupied units shall remain affordable for 30 years.
- Units that are converted from renter-occupied to owner-occupied shall remain affordable (as determined above) for 30 years beyond conversion.
- The size of all Workforce Housing units shall be at least 80% of the average size of market rate units.

Section IV: General Standards

Bonus Height Program

Civil Support Space

The dedication of Civil Support Space offers height bonus for the developer's contribution of space to a specific use or entity that serves to better the neighborhood or community. See the BHP Chart for details for a list of Subdistricts in which the Civil Support Space bonus may be utilized.

- Civil Support Space is typically on the ground level. Upper levels may be appropriate depending on the intended use.
- The number of square feet of Bonus Height shall be twice that of the number of square feet donated to Civil Support Space. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Civil Support Space shall be dedicated to the chosen use or uses for 15 years. Adherence to this standard shall be checked yearly by the Planning Commission or its designee.

The Planning Commission may require the developer to execute an agreement, restrictive covenant, or other binding restriction on land use that preserves the use of Civil Support Space for the required period before final site plan review.

The following uses are appropriate for Civil Support Spaces:

- Institutional Uses
 - Cultural center
 - Day care center
 - School day care
- Education
 - Community education
- Transportation Uses
 - Water taxi station
- Waste Management Uses
 - Recycling collection center
- Recreational and Entertainment Uses
 - Community playground
- Other Uses
 - Community garden

Other uses may be appropriate for Civil Support Space. The applicant may propose a different use for Civil Support Space to be approved by the Planning Commission.

Upper Level Garage Liner and Underground Parking

The public realm of the streetscape is improved by providing parking in underground structures and lining above ground parking structures with habitable space. See the BHP Chart for a list of Subdistricts in which the Garage Liner and Underground Parking bonuses may be utilized.

- Height bonuses are given for upper levels of habitable space, a minimum of 15' in depth, which masks a parking structure from view along public streets and open space. The minimum depth may be reduced by the Planning Commission provided the intent of an active streetscape is met.
- The number of square feet of Bonus Height shall be twice that of the number of square feet in Garage Liners. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- The number of square feet of Bonus Height shall be equal to the number of square feet in Underground Parking. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Height bonuses are not given for ground level liners, or upper level liners that are required by the DTC.

Public Parking

Parking accessible to the general public is important to the continued growth and vitality of Downtown. See the BHP Chart for a list of Subdistricts in which the Public Parking bonuses may be utilized.

- The number of square feet of Bonus Height shall be twice that of the number of square feet in Public Parking. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Public Parking shall be clearly marked as public, and shall be accessible to the public, at all hours that the garage is open, for the lifetime of the building.

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Section IV: General Standards

Bonus Height Chart

| Maximum height within the Subdistrict | LEED | LEED ND | Pervious Surface | Open Space | Workforce Housing | Civil Support Space | Upper Level Garage Liner & Underground Parking | Public Parking | Bonus Height Maximum | |
|---------------------------------------|--|---|------------------|------------|-------------------|--|--|--------------------------------|-----------------------------|--|
| Central | | | | | | | | | | |
| James Robertson | elevation of 560' | Any = 2 stories | 2 stories | 2 stories | 2 stories | All Levels to a maximum of elevation above sea level of 560' | 2 stories below 560' elevation | 2 stories below 560' elevation | 2 stories | elevation 560' |
| Core | | | | | | | | | | |
| Core | 30 stories | Silver = 4 stories; Gold = 8 stories Platinum = 12 stories | 2 stories | 2 stories | 8 stories | All Levels | 8 stories | 8 stories | 2 stories | Unlimited |
| Core-historic | | | | | | | | | | |
| Core-historic | 6 stories on the interior of blocks; 10 stories on the corners | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | 6 stories on the interior of blocks; 10 stories on the corners |
| Upper Broadway | | | | | | | | | | |
| Upper Broadway | 100' | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | 100' |
| Second and Broadway | | | | | | | | | | |
| Second | 8 stories within 105' | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | 8 stories within 105' |
| Broadway | 5 stories within 65' | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | 5 stories within 65' |
| SoBro | | | | | | | | | | |
| General | 30 stories | Silver = 4 stories; Gold = 8 stories Platinum = 12 stories | 2 stories | 2 stories | 8 stories | All Levels | 8 stories | 8 stories | 2 stories | Unlimited |
| west side of 1st and east side of 2nd | 15 stories within 220' | Silver = 4 stories; Gold or Platinum = 5 stories | 2 stories | 2 stories | 5 stories | Levels 1, 2, and 3 | 5 stories | 5 stories | 2 stories | 20 stories within 300' |
| Central River | | | | | | | | | | |
| Central River | 10 stories within 145' | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | 10 stories within 145' |
| South | | | | | | | | | | |
| Lafayette | | | | | | | | | | |
| General | 8 stories | Any = 2 stories | 2 stories | 2 stories | 2 stories | Levels 1, 2, and 3 | 2 stories | 2 stories | 2 stories | 11 stories |
| Lafayette Street | 12 stories | Any = 2 stories | 2 stories | 2 stories | 3 stories | Levels 1, 2, and 3 | 3 stories | 3 stories | 2 stories | 16 stories |
| Rutledge Hill | | | | | | | | | | |
| Primary and Secondary | 6 stories | Any = 1 story | 1 story | 1 story | 1 story | Levels 1, and 2 | 1 story | 1 story | 2 stories | 7 stories |
| Neighborhood Tertiary | 3 stories | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | 3 stories |
| Rolling Mill Hill | | | | | | | | | | |
| Rolling Mill Hill | 65 feet with an SEP of 1:1.5 | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | 65 feet with an SEP of 1:1.5 |
| Rutledge River | | | | | | | | | | |
| Rutledge River | 9 stories | Any = 2 stories | 2 stories | 2 stories | 2 stories | Levels 1, 2, 3 and 4 | 2 stories | 2 stories | 2 stories | 12 stories |
| West | | | | | | | | | | |
| Gulch North | | | | | | | | | | |
| General | 7 stories | Any = 2 stories | 2 stories | 2 stories | 2 stories | All Levels | 2 stories | 2 stories | 2 stories | 10 stories below 560' elevation |
| fronting Herman Street | 4 stories | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | 2 stories | 4 stories |
| Gulch South | | | | | | | | | | |
| General | 10 stories | Any = 2 stories | 2 stories | 2 stories | 2 stories | All Levels | 2 stories | 2 stories | 2 stories | 16 stories |
| fronting Church, Broadway, Demonbreun | 15 stories | Any = 2 stories | 2 stories | 2 stories | 3 stories | All Levels | 3 stories | 3 stories | 2 stories | 21 stories |
| at key intersections | 20 stories | Silver or Gold = 2 stories; Platinum = 3 stories | 2 stories | 2 stories | 4 stories | All Levels | 4 stories | 4 stories | 2 stories | 28 stories |
| North | | | | | | | | | | |
| Hope Gardens | | | | | | | | | | |
| Primary | 7 stories | Any = 1 story | 1 story | 1 story | 1 story | Level 1 | 1 story | 1 story | 1 story | 8 stories |
| Secondary | 4 stories | Any = 1 story | 1 story | 1 story | 1 story | Level 1 | 1 story | 1 story | 1 story | 5 stories |
| Neighborhood Tertiary | 3 stories | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | No bonus | 4-story No Bonus | 3 stories |
| Sulphur Dell | | | | | | | | | | |
| Primary | 7 stories | Any = 1 story | 1 story | 1 story | 1 story | Level 1 | Any = 1 story | 1 story | 1 story | 8 stories |
| Secondary | 5 stories | Any = 1 story | 1 story | 1 story | 1 story | Level 1 | Any = 1 story | 1 story | 1 story | 6 stories |
| Neighborhood Tertiary | 4 stories | Any = 1 story | 1 story | 1 story | 1 story | Level 1 | Any = 1 story | 1 story | 1 story | 5 stories |

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Section V

SIGN STANDARDS



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Section V: Sign Standards

Introductory Provisions

Intent

The purpose of these regulations is to set specific sign standards that accomplish the following:

- Establish reasonable and improved standards for Downtown business identification;
- Encourage creative and innovative approaches to regulating signs consistent with the principles of the Downtown Community Plan;
- Promote economic vitality in Downtown;
- Enhance the overall visual environment in Downtown by discouraging signs that contribute to the visual clutter of the streetscape;
- Ensure signs are designed for the purpose of identifying a business in an attractive and functional manner; and
- Ensure signs reinforce the existing and envisioned character and are complementary to the architectural design of Downtown.

Applicability

- These sign regulations apply to all properties zoned DTC and are not in an Historic Zoning Overlay. See map on Page 107.
- In addition to the standards set forth within this section, the following Sections of the Metro Zoning Code shall apply to all regulated signage within the DTC.
 - Section 17.04.06 - Definitions of general terms
 - Within Chapter 17.32 SIGN REGULATIONS:
 - Section 17.32.020 - General Provisions
 - Section 17.32.040 - Signs allowed without a permit
 - Section 17.32.050 - Prohibited signs
 - Section 17.32.060 - Permitted on-premises temporary signs
 - Section 17.32.145 - Landmark signs
 - Section 17.32.160 - Computations
 - Within Article XI. Sign Procedures
 - 17.40.490 - Permits and compliance tag
 - 17.40.510 - Unsafe, illegal, dilapidated and abandoned signs

Sign Permit Applications

Applications for sign permits shall be made with and reviewed by the Codes Department. All sign applications that do not involve Modifications shall only require Codes Department approval, regardless of whether the property is subject to additional design guidelines (e.g. MDHA redevelopment districts).

Applicants for sign permits shall submit the following information. Incomplete applications will not be accepted.

- Design and details of the signage depicting size and shape (including height, width and depth), anchoring, materials, lighting and other data necessary to determine compliance with the requirements of this section and with the requirements of the Metropolitan building code and the Metropolitan electrical code. Additional information may be required by Codes.
- Drawings and specifications, including building elevations or artist's rendering depicting the sign faces, and dimensions indicating sign placement on the building.
 - For ground signs and signs seeking a ROW encroachment, the distance of the signs from the corner of the building or property line should be included.
- A site plan, drawn to scale, depicting the location of the proposed signage and all relevant features of the site, including location and size of other regulated signs.
- The property address, applicant and sign designer's name and contact information.

Common Sign Plan

A common signage plan regulates signage for multiple businesses or tenants within one building or complex. A common sign plan is mandatory for all new developments and sign Modifications.

- A common signage plan shall provide for consistency among signs with regard to at least four of the following: materials; location of each sign on the building; sign proportions; color scheme; lighting; lettering or graphic style.
- The common signage plan shall establish an allowable area of signage for existing and future tenants with regard to all allowed signs types.
- The common signage plan shall indicate existing nonconforming signs as well as the amount and location of on-premises signage to be allocated to each tenant under the new plan.

Section V: Sign Standards

Modifications

Sign Permit Modifications

Requests for modifications to sign standards are reviewed through the process outlined on pages 14 and 15.

Modifications are reviewed by Planning staff and either the MDHA Design Review Committee or the DTC Design Review Committee.

The applicant may appeal a decision through the process described on pages 14 and 15.

All sign Modifications are Major Modifications. Two additional types of Modifications for signage related permits may be requested and are outlined below.

Modifications for Exceptional Design

Creative signage that does not fit the specific regulations of this section may be considered by the appropriate reviewing body (listed above), based on its merits, as they relate to all of the following design criteria:

- architecture
- the configuration or location of the building or property
- building scale
- legibility
- technical competence and quality in design, construction and durability

Applications for this type of Modification require submittal of a common sign plan for the property in question. Approval of any related structures (i.e. canopy) will require review by all applicable agencies. Exceptional design modifications shall not permit electronic changeable copy where it is otherwise not permitted.

Modifications for Tourist-oriented Businesses

Tourist-oriented businesses within DTC zoning may receive Modifications to allow greater sign area and use of digital technology. To qualify as a tourist-oriented business a business shall:

- have a minimum permanent fixed seating capacity of 500; and
- offer lawful activities or services to the general public of cultural, historical, recreational, educational, or entertainment purposes.

Applications for this type of Modification require submittal of a common sign plan for the property in question. Approval of any related structures (e.g. canopy) will require review by all applicable agencies. Tourist-oriented business Modifications shall be reviewed according to the design criteria listed under Modifications for Exceptional Design, above, in addition to the following:

- Large electronic or illuminated signs shall not adversely impact residential or hotel uses.
- All signs shall conform to the lighting standards of the DTC.
- See page 119 for additional information on changeable copy.

Right-of-way Encroachments

Where a sign is proposed to encroach into the public right-of-way an application shall be made with the Public Works Department in addition to the sign permit application with the Codes Department. Both applications shall include the requirements of the Public Works Department available at their website: <http://www.nashville.gov/pw/permits.asp>, in addition to the submittal requirements listed on the previous page.

Nonconforming Signs

Nonconforming Signs

Sections 17.40.660 and 17.40.690 of the Metro Zoning Code apply. This section further clarifies them.

Building Signs

A sign shall be brought into compliance with the provisions of the above mentioned sections if a sign permit is required to rebuild the sign. This does not include a panel change in a non-conforming cabinet sign, which shall be permitted. All new panels shall conform to all illumination standards herein.

Ground Signs

An existing ground sign may change the face or panel of a sign that does not meet the area or height standards within this section. However, in no instance shall there be an increase in the degree of nonconformity. All new panels shall conform to all illumination standards herein.

A sign shall be brought into compliance with the provisions of this title if at any time the sign is altered, repaired, restored or rebuilt to the extent that the cost exceeds fifty percent (50%) of the estimated replacement cost of the sign (in current dollar value). All permits within any six consecutive calendar months shall be aggregated for purposes of measuring the fifty percent standard.

Repair and Maintenance

If the alteration or repair is caused by involuntary damage or casualty, the sign may be altered or repaired to its pre-damaged condition.

A sign may be removed and taken off-site for repair and maintenance. The sign must be returned to the original location within 120 days of removal.

Section V: Sign Standards

General Standards

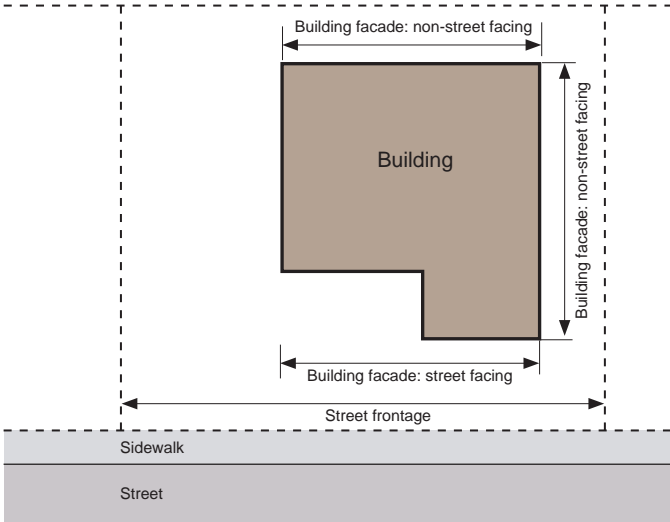
Materials

All permanent, on-premises signs shall be constructed of a rigid, weatherable material such as hard plastic, wood, MDO plywood, aluminum, steel, PVC, glass and/or Plexiglas. On-premises, permanent signs shall not be constructed of nonrigid materials including, but not limited to, vinyl, fabric, canvas, or corrugated plastic. The provisions of this subsection shall not apply to approved, permitted canopies, awnings, and porticoes.

Voluntary Removal of a Legally Non-conforming Ground Sign

Any property voluntarily removing a legally non-conforming ground sign shall be permitted to a bonus to one hundred fifty percent (150%) of the building signs allocated to the property. For example, if a property is allocated 200 sq ft of building signs, the property will be allowed 300 sq ft of buildings signs if a legally non-conforming ground sign is removed.

Building Facade and Street Frontage Measurement



Other Sign Types

Non Street-Facing Signs

Non street-facing building facades and alley frontages, not otherwise regulated, are allocated 1 square foot of sign area per 1 linear foot of building facade, to a maximum of the sign area permitted for the primary street frontage.

This includes lots adjoining open spaces, pedestrian walkways, or parking areas. Ground signs are not permitted on non street-facing building facades or alley frontages.

Temporary Signs

Temporary signs shall follow the standards of 17.32.060.

Murals

Only the company name, text relating directly to products or services sold on site, and logos shall count toward the sign area allocation. Otherwise, murals are exempt from this code.

Auto-oriented canopy/awning Signs

The allocation of signage for auto-oriented canopies and awnings shall be measured as walls signs and shall only be used on the canopy/awning. See the Auto-oriented Canopies and Awnings section of the Downtown Code for information on the design of canopies and awnings.

Parking Lot Signs

A pole-mounted projecting sign is allowed for surface parking lots with no associated building. One sign per street frontage is allowed. The maximum size shall be 36 square feet per sign. The side of the sign shall be attached to the pole, and the pole will be considered the "building façade". All projecting sign standards shall apply (Page 114); parking lot signs shall follow the standards of a 1-story building.

Section V: Sign Standards

Street Types

Pedestrian Streets

- Pedestrian streets are roadways with high pedestrian activity and slower moving vehicular traffic. Buildings along these streets are located at the back of the sidewalk creating a streetscape with active uses including retail, office and entertainment businesses.
- Pedestrian streets are generally located in the Downtown core, where more of the original street wall remains intact, and less opportunities exist for surface parking.
- Some streets outside of the Downtown core, such as Korean Veterans Boulevard (KVB), require Pedestrian designation. The MDHA and UDO standards for KVB require pedestrian-focused building design and streetscape. The standards for Pedestrian Street signage allow greater flexibility for such streets.

Gateway Streets

- Gateway streets are wider roadways that serve pedestrians, but focus primarily on automobile traffic and typically carry traffic at higher speeds than Pedestrian Streets.
- Generally located on the fringe of Downtown, Gateway streets serve as the key automobile entry points into Downtown.

Transitional Streets

- Transitional streets currently contain a mix of different building characters, transitioning from more auto oriented buildings and uses to a more mixed use pedestrian environment.
- As these street redevelop, buildings will be sited close to the sidewalk with a more pedestrian focus.

Interstate

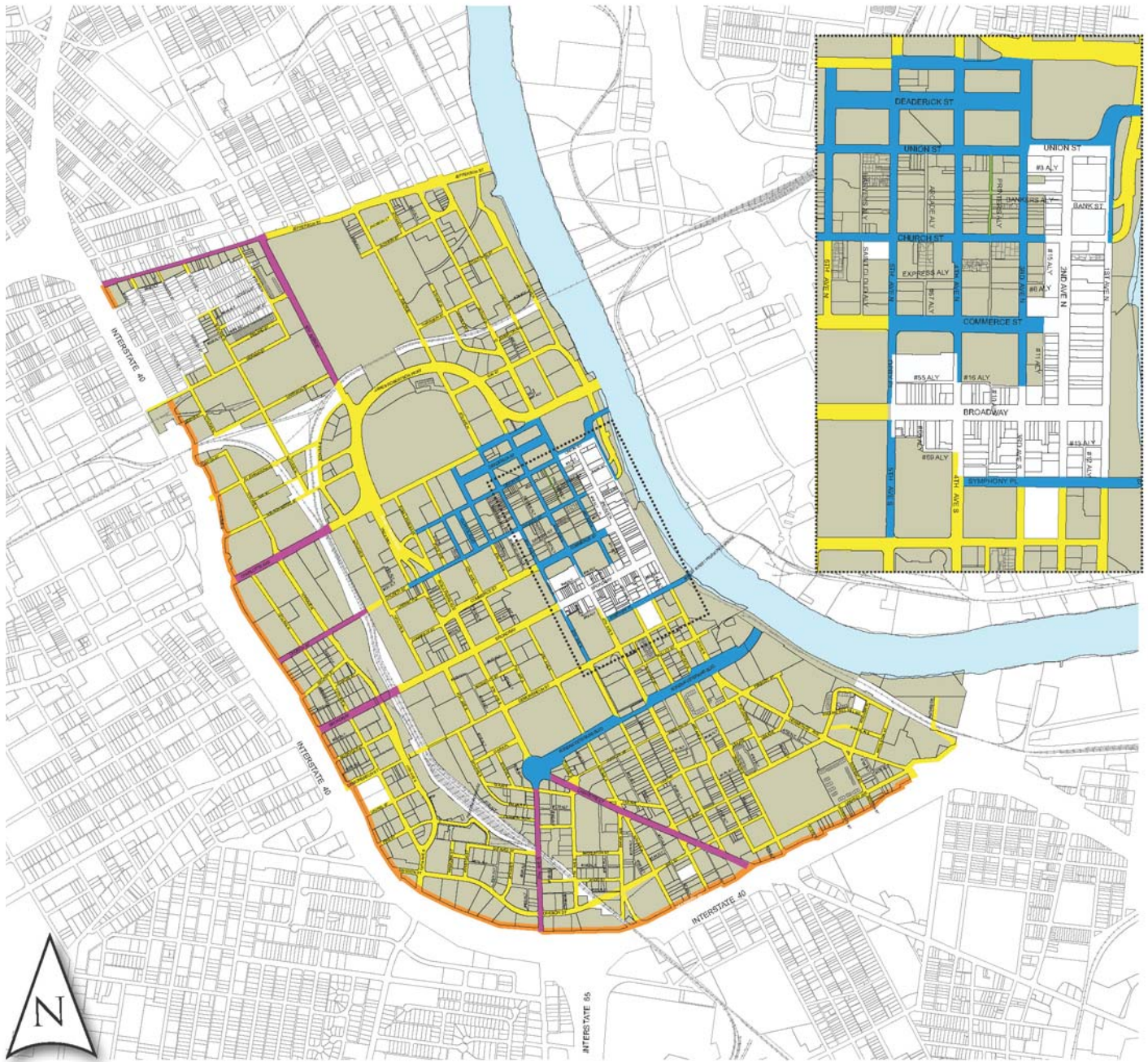
- Many properties are visible from the interstate loop which moves vehicles into and through Downtown.
- The opportunity to advertise to the interstate must be balanced with the safety of drivers and preservation of Nashville's iconic skyline.

Printers' Alley



- Printers' Alley is a unique, iconic alley in Downtown. As a public, pedestrian-only street with businesses on the ground floor and upper floors, unconventional standards apply to this street only.

Section V: Sign Standards

Signs: Map of Street Types for Signage Standards



Legend

| | | | |
|---|---------------------|---|-----------------------|
|  | Transitional Street |  | Interstate |
|  | Pedestrian Street |  | Printers' Alley |
|  | Gateway Street |  | Applicable properties |

- New streets that are not in existence as of the adoption of this ordinance shall be categorized as Transitional Streets.
- Properties within an Historic Zoning Overlay and/or with SP zoning are not subject to the sign standards of DTC zoning.

Section V: Sign Standards

Determining Sign Entitlements

Use this page as a guide to determine the sign entitlements of a property. You will need to know the length of the building on all street frontages, the length of alley frontages, and the length of all interior property lines.

STEP 1

What Street Types is the property on? Page 107

- Pedestrian
- Transitional
- Gateway
- Interstate
- Printers' Alley

STEP 2

How much square footage of signage is allowed? Pages 109-110

- Pedestrian
 - Building _____
 - Ground _____
 - Skyline _____
- Transitional
 - Building _____
 - Ground _____
 - Skyline _____
- Gateway
 - Building _____
 - Ground _____
 - Skyline _____
- Interstate
 - Building _____
 - Skyline _____
- Printers' Alley
 - Building _____
 - Skyline _____

STEP 3

What are the standards for the signs? Pages 111-117

STEP 4

What are the illumination standards for signs? Pages 118-119

STEP 5

Is changeable copy allowed? Page 119

STEP 6

- For a multi-tenant development: submit a Common Sign Plan to the Codes Department. Page 103
- For an individual tenant: submit a Sign Plan Proposal to the Codes Department. Page 103
- Additional types of signs may be allowed, depending on site conditions. Page 105

KEEP IN MIND

- There is no limit to the number of Building Signs per property.
- Sign entitlements are limited only by the total amount of square footage of signs allowed on the property, the maximum sizes of signs and the required placement of signs.
 - For example, if a building is allowed 100 sq ft of Building Signs, that can be used in one 100 sq ft sign or in five 20 sq ft signs. The only limit is the maximum dimensions of the sign type.
- Non street-facing facades are allowed signs. Page 105
- Contact the Codes Department with questions.

Section V: Sign Standards

Allocation of Sign Area by Street Type

The maximum sign area for each type of sign is determined by the Street Type and is established in the following tables. Specific requirements for each sign type are shown on the subsequent pages.

For each cell in the table below, there is a maximum allowed sign area that may be utilized with any combination and any number of signs associated with that cell, unless otherwise noted.

The measurements for "linear feet" shall be at grade.

| Pedestrian Street Type | |
|--|--|
| Building Signs | |
| Wall Sign | 1.5 square foot of sign area per 1 linear foot of building facade or 36 square feet, whichever is greater. When a Projecting Sign is used on the building, an additional 0.5 square feet of sign area per 1 linear foot of building facade shall be permitted, for a total of 2.0 square feet per 1 linear feet of building facade. |
| Awning Sign | |
| Canopy Sign | |
| Projecting Sign | |
| Shingle Sign | 9 square feet per sign |
| Ground Signs | |
| Monument Sign | 24 square feet |
| Skyline Signs - area determined by average height of building | |
| 75' to 100' | 480 square feet |
| 101' - 200' | 600 square feet |
| 201' and taller | 720 square feet |

| Gateway Street Type | |
|--|---|
| Building Signs | |
| Wall Sign | 1.5 square foot of sign area per 1 linear foot of building facade or 36 square feet, whichever is greater. Where no ground sign exists, an additional 0.5 square feet of sign area per 1 linear foot of building facade shall be permitted, for a total of 2.0 square feet per 1 linear feet of building facade. |
| Awning Sign | |
| Canopy Sign | |
| Projecting Sign | |
| Shingle Sign | 9 square feet per sign |
| Ground Signs | |
| Monument Sign | 64 square feet Properties with 300 or more feet of frontage are allowed one additional monument sign of an additional 64 square feet |
| Skyline Signs - area determined by average height of building | |
| 75' to 100' | 480 square feet |
| 101' - 200' | 600 square feet |
| 201' and taller | 720 square feet |

| Transitional Street Type | |
|--|--|
| Building Signs | |
| Wall Sign | 1.5 square foot of sign area per 1 linear foot of building facade or 36 square feet, whichever is greater. Where no ground sign exists, an additional 0.5 square feet of sign area per 1 linear foot of building facade shall be permitted for a total of 2.0 square feet per 1 linear feet of building facade. |
| Awning Sign | |
| Canopy Sign | |
| Projecting Sign | |
| Shingle Sign | 9 square feet per sign |
| Ground Signs | |
| Monument Sign | 32 square feet Properties with 300 or more feet of frontage are allowed one additional monument sign of an additional 32 square feet |
| Skyline Signs - area determined by average height of building | |
| 75' to 100' | 480 square feet |
| 101' - 200' | 600 square feet |
| 201' and taller | 720 square feet |

**** More Street Types on next page****

Section V: Sign Standards

Allocation of Sign Area by Street Type

The maximum sign area for each type of sign is determined by the Street Type and is established in the following tables. Specific requirements for each sign type are shown on the subsequent pages.

For each cell in the table below, there is a maximum allowed sign area that may be utilized with any combination and any number of signs associated with that cell, unless otherwise noted.

The measurements for "linear feet" shall be at grade.

| Interstate Street Type | |
|--|---|
| Building Signs | |
| Wall Sign | 1 square foot of sign area per 1 linear foot of street frontage |
| Awning Sign | |
| Canopy Sign | |
| Projecting Sign | |
| Shingle Sign | 9 square feet per sign |
| Ground Signs | Not allowed |
| Skyline Signs - area determined by average height of building | |
| 75' to 100' | 480 square feet |
| 101' - 200' | 600 square feet |
| 201' and taller | 720 square feet |

| Printers' Alley Street Type | |
|--|--|
| Building Signs | |
| Wall Sign | Ground floor: 2.0 square foot of sign area per 1 linear foot of street frontage Second floor: 1.5 square foot of sign area per 1 linear foot of street frontage Upper floors: 1 square foot of sign area per 1 linear foot of street frontage Signage allowed for each floor shall be used on that floor and shall not be redistributed to other parts of the building. |
| Awning Sign | |
| Canopy Sign | |
| Projecting Sign | |
| Shingle Sign | |
| Ground Signs | Not allowed |
| Skyline Signs - area determined by average height of building | |
| 75' to 100' | 480 square feet |
| 101' - 200' | 600 square feet |
| 201' and taller | 720 square feet |

Section V: Sign Standards

Building Sign: Wall Sign

Description

A wall sign is a building sign that is attached flat to, or mounted away from but parallel to, the building facade.

A wall sign may be painted on the building facade.

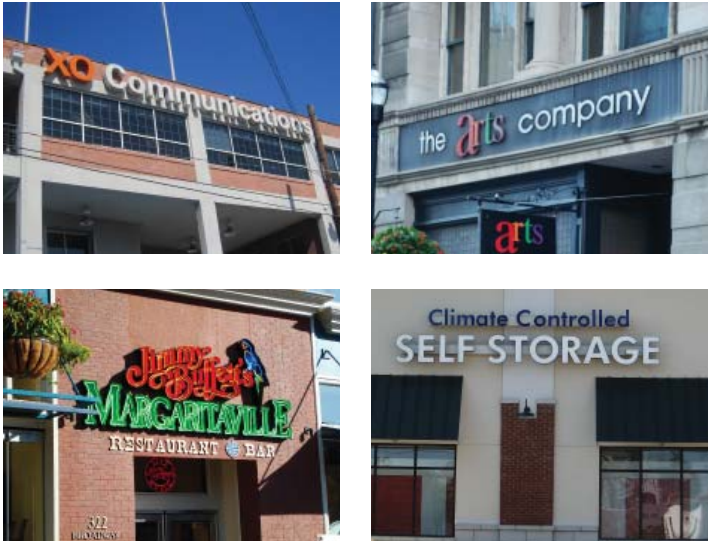
General Provisions

- A wall sign shall either be located lower than the window sills of the third story or at the top of the building, not to be placed below the windows of the highest floor.
- No portion of a wall sign may extend above the roof line or above a parapet wall of a building with a flat roof.
- No portion of a wall sign may extend above the lower eave line of a building with a pitched roof.
- A wall sign cannot cover windows or architectural details.
- An exposed raceway shall be finished to match the background wall or be integrated into the overall design of the sign.
- A wall sign can be externally or internally illuminated in accordance with pages 118.

Design Standards

- | | | |
|---|-------------------------------|--|
| A | Overall area allocation (max) | See pages 109-110 |
| B | Projection (max) | 13 inches |
| C | Exposed Raceway height (max) | 50% of the letter height, OR if the Raceway is used as the sign background, the Raceway may extend 3 inches beyond the largest part of the sign. |

Refer to the Illumination section (pages 118) for additional raceways standards and permitted locations.



Example of raceway incorporated as sign background

Section V: Sign Standards

Building Sign: Awning Sign

Description

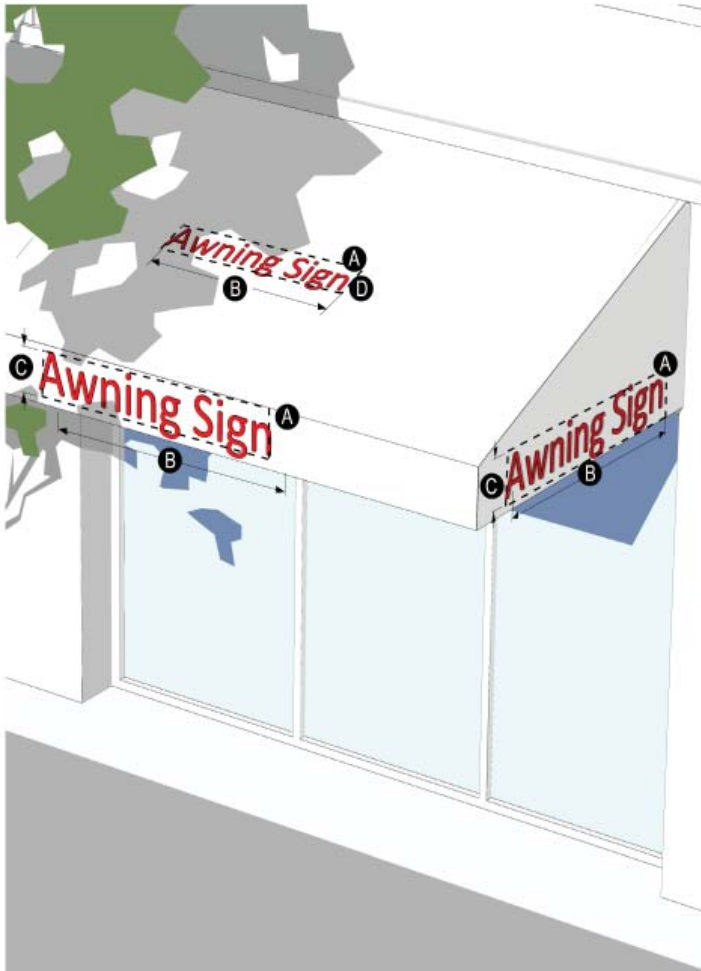
An awning sign is a type of building sign. Graphics and symbols are painted, sewn, or otherwise adhered to the awning material as an integrated part of the awning itself.

General Provisions

- Only awnings on first and second story windows may contain signs.
- A maximum of one sign is allowed per awning face.
- An awning sign may only be externally illuminated.
- See the Awnings section of the Downtown Code for additional information on the design of awnings.

Design Standards

| | |
|---|-------------------|
| Ⓐ Overall area allocation (max) | See pages 109-110 |
| Ⓑ Sign Width (max % of awning width/depth) | 75% |
| Ⓒ Height of text and graphics on valance (max) | 2 feet |
| Ⓓ Max area of sloping plane covered by sign | 50% |



Section V: Sign Standards

Building Sign: Canopy Sign

Description

A canopy sign is a type of building sign that is attached above, below or to the face of a canopy.

General Provisions

- A canopy sign cannot extend outside the overall length or width of the canopy. However, a canopy sign may extend above or below the canopy provided the sign meets all other design standards.
- Maximum of one sign per canopy face.
- Raceways are permitted for signs extending below or above the canopy.
- A canopy sign can be externally or internally illuminated in accordance with page 118.
- Cabinet signs are not permitted as canopy signs.

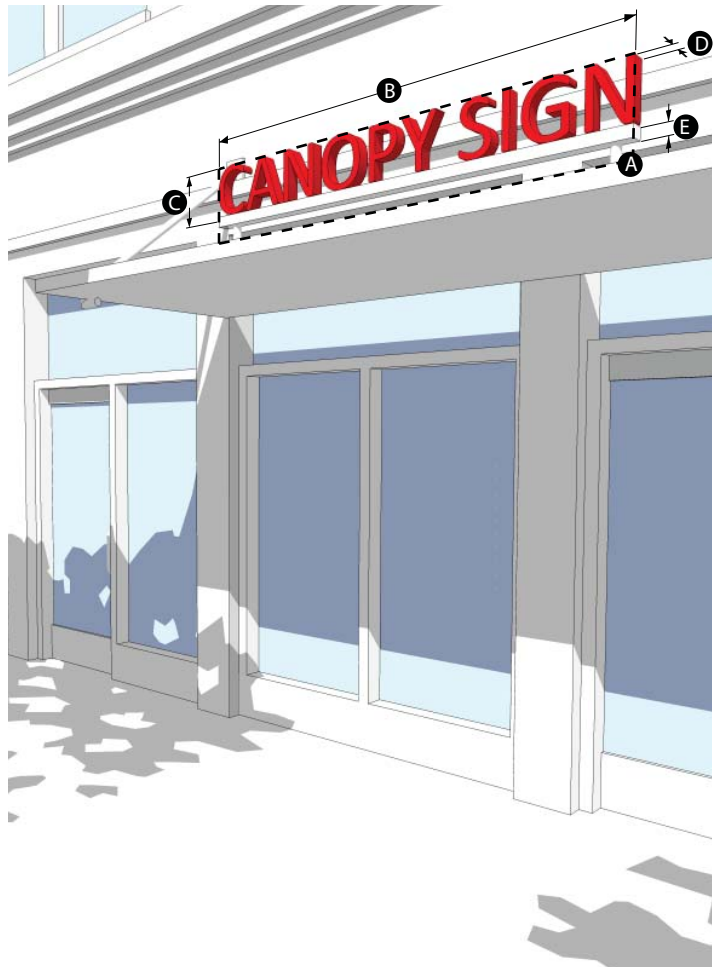
See the Canopies section of the Downtown Code for additional information on the design of canopies.

- This definition does not include freestanding canopies over fuel pumps. See the Auto-oriented canopy/awning Signs on page 105.

Design Standards

- | | |
|---|--|
| Ⓐ Overall area allocation (max) | See pages 109-110 |
| Ⓑ Sign Width (max % of canopy length or depth) | 75% |
| Ⓒ Height of text and graphics (max) | 2 feet |
| Ⓓ Depth (max) | 13 inches |
| Ⓔ Exposed Raceway height (max) | 50% of the letter height, OR if the Raceway is used as the sign background, the Raceway may extend 3 inches beyond the largest part of the sign. |

Refer to the Illumination section (pages 118) for additional raceways standards and permitted locations.



Section V: Sign Standards

Building Sign: Projecting Sign

Description

A projecting sign is a type of building sign that projects outward from the facade, typically at a ninety degree angle. Projecting signs are typically, but not always, vertically oriented and generally mounted above the first floor.

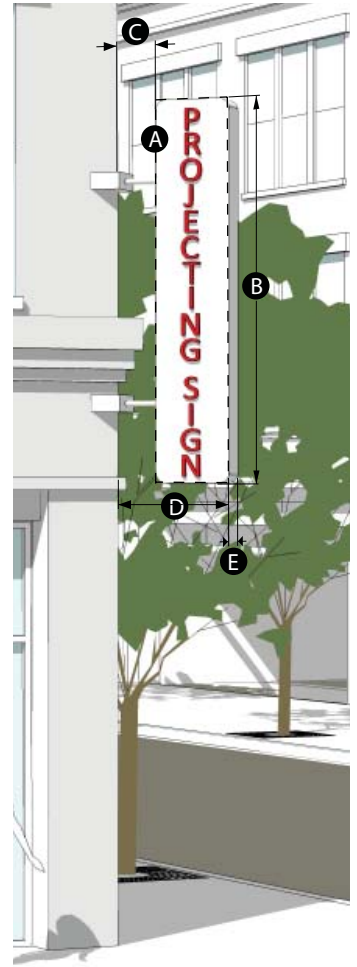
General Provisions

- A projecting sign must be located at least 25 feet from any other projecting sign. When building width prohibits adherence to this standard, flexibility shall be permitted though a Minor Modification to be reviewed by staff.
- A projecting sign may be erected on a building corner when the building corner adjoins the intersection of two streets. Allocation of sign area from both streets may be used, however, in no case shall the sign exceed the maximum dimensional standards below.
- A projecting sign shall be located below the window sills of the fourth story.
- The top of a projecting sign shall not extend above the building eave or top of parapet, except on one story buildings where the top of a projecting sign may have a maximum of 20% of the sign height above the top of the building.
- A projecting sign can be externally or internally illuminated in accordance with pages 118.



Design Standards

| | |
|--|-------------------|
| A Overall area allocation (max) | See pages 109-110 |
| B Height (max) | |
| 1 story buildings | 10 feet |
| 2 and 3 story buildings | 16 feet |
| 4 or more story buildings | 20 feet |
| C Spacing from facade (min) | 1 foot |
| D Projection Width (max) | 6 feet |
| E Depth of cabinet (max) | 18 inches |



Section V: Sign Standards

Building Sign: Shingle Sign

Description

A shingle sign is a smaller building sign that projects outward, typically at a ninety degree angle, and hangs from a bracket or support that is located over or near a building entrance.

General Provisions

- Signs shall be located within 8 feet of an active pedestrian building entrance. This does not include service entries or entries that primarily remain locked.
- An active pedestrian entrance at the corner of a building is allowed signs on both streets.
- A shingle sign shall be located below the window sills of the second story.
- A shingle sign shall not be internally illuminated.

Design Standards

| | |
|-----------------------------|---------------|
| Ⓐ Area (max) | 9 square feet |
| Ⓑ Height (max) | 3 feet |
| Ⓒ Spacing from facade (min) | 6 inches |
| Ⓓ Width (max) | 3 feet |
| Ⓔ Depth (max) | 6 inches |



Section V: Sign Standards

Ground Sign: Monument Sign

Description

A monument sign is a ground sign that is attached along its entire length to a continuous pedestal.

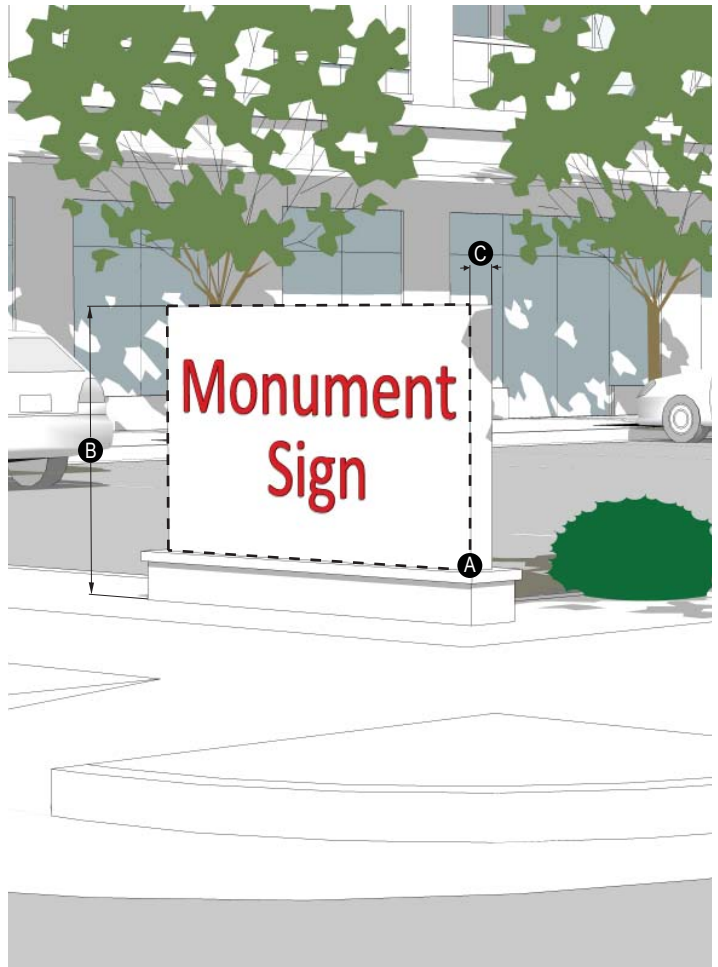
General Provisions

- Each property fronting on a Street Type which allows a monument sign is permitted one per street frontage. One additional monument sign is allowed for properties with 300 or more feet of street frontage on one street. Where more than one sign is permitted, signs along the same street frontage shall be spaced a minimum of 200 feet apart.
- A monument sign must be set back at least 5 feet from the front property line.
- A sign erected on the top of a retaining wall is required to meet the standards for a monument sign. The height of the wall shall be included in the overall height calculation. In this case, the 5 foot minimum setback is not required.
- A sign affixed to the face of a retaining wall or seat wall that is an integral part of a plaza or streetscape design may utilize the sign area allocated to wall signs. In this case, the 5 foot minimum setback is not required.
- A monument sign can be externally or internally illuminated in accordance with page 118.



Design Standards

| | |
|-----------------------------------|-------------------|
| A Sign area (max per sign) | See pages 109-110 |
| B Height (max) | |
| Pedestrian Street | 5 feet |
| Transitional Street | 10 feet |
| Gateway Street | 15 feet |
| C Depth (max) | 18 inches |



Section V: Sign Standards

Skyline Sign

Description

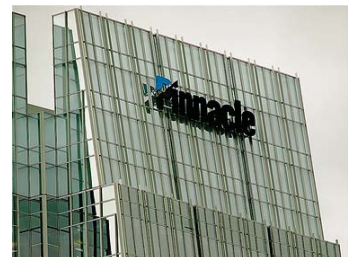
A building sign is attached flat to or mounted away from the building facade. Sign may be parallel to the building facade or vertical. Located on the upper band of a building.

General Standards

- A skyline sign is only allowed on buildings greater than 75 feet in height.
- A skyline sign must be located within the top third of the building.
- No portion of a skyline sign may extend above the roof line or above a parapet wall of a building with a flat roof.
- No more than one skyline sign per facade is allowed. However, additional skyline signs may be allowed as a Modification for Exceptional Design. See additional information on page 104.
- Raceways are not permitted on skyline signs.
- A skyline sign can be internally (but not externally) illuminated in accordance with page 118.

Design Standards

- | | |
|----------------------------------|-------------------|
| Ⓐ Area (max) | See pages 109-110 |
| Ⓑ Height (max) | 14 feet |
| Ⓒ Width (max % of facade length) | 60% |



Section V: Sign Standards

Illumination

Illumination of signs shall be in accordance with the following requirements:

External Illumination

- External light sources shall be placed close to, and directed onto, the sign and shielded to minimize glare into the street, sidewalks or onto adjacent properties.
- Projecting light fixtures used for externally illuminated signs shall be simple and unobtrusive in appearance. They should not obscure the sign.

Internal Illumination

- Channel letters may be internally lit or back-lit.
- For cabinet signs, the background must be opaque. Only graphics, text and logos may be illuminated, and a halo of one inch around graphics, text, and logos may be non-opaque.
- Exposed neon may be used for lettering or as an accent.

Prohibited Light Sources

The following light sources are prohibited on every Street Type except Printers' Alley:

- Blinking, flashing, chasing, and sequential lighting.
- Bare bulb illumination.

Raceways and Transformers

- Within Redevelopment Districts, raceways may only be permitted by Modification.
- Visible transformers are prohibited.



External light sources



Internally lit channel letters



Back lit channel letters



Internally lit cabinet signs with darker backgrounds

Section V: Sign Standards

Changeable Copy Signs (Manual or Electronic)

Changeable copy signs shall be in accordance with the following requirements:

Description

- Manual changeable copy sign. A sign or portion of a sign that has a readerboard for the display of text information in which each alphanumeric character, graphic or symbol is defined by objects, not consisting of an illumination device. Changeable copy is changed or re-arranged manually or mechanically without altering the face or the surface of the sign.
- Electronic changeable copy sign. A sign or portion of a sign that displays information in which each alphanumeric character, graphic, or symbol is defined by a small number of matrix elements using different combinations of light emitting diodes (LED's), fiber optics, light bulbs or other illumination devices within the sign.

Usage

- Manual changeable copy is allowed in conjunction with a wall sign, pylon sign or monument sign.
- Electronic Changeable copy is allowed in conjunction with a wall sign or a monument sign provided the changeable copy portion is no greater than 50% of the built sign area.
- An electronic changeable copy sign is not allowed in a Redevelopment District or on Interstate frontage.
- Tourist-oriented businesses within Downtown Nashville may receive Modifications to allow greater sign area and use of digital technology in accordance to the process on page 104.

Spacing

- The closest edge of an electronic changeable copy sign must be a minimum distance of 100 feet from any residential zoning district or historic district boundary.
- An electronic changeable copy sign must be separated from another electronic changeable copy sign by at least 50 feet.

Duration

- Any image or message or portion of the image or message must have a static display for minimum duration of eight seconds.
- Transition time must be immediate.
- No portion of the image or message may flash, scroll, twirl, change color or in any manner imitate movement.

Brightness

- The sign must not exceed a maximum illumination of 7,500 nits during daylight hours and a maximum illumination of 750 nits between dusk to dawn as measured from the sign's face at maximum brightness.
- Electronic changeable copy signs must have an automatic dimmer control to produce a distinct illumination change from a higher illumination level to a lower level.