

# Metropolitan Planning Commission



Staff Reports

**April 23, 2015**



## Metro Planning Commission Meeting of 04/23/2015

*Mission Statement: The Planning Commission is to guide the future growth and development for Nashville and Davidson County to evolve into a more socially, economically and environmentally sustainable community with a commitment to preservation of important assets, efficient use of public infrastructure, distinctive and diverse neighborhood character, free and open civic life, and choices in housing and transportation.*

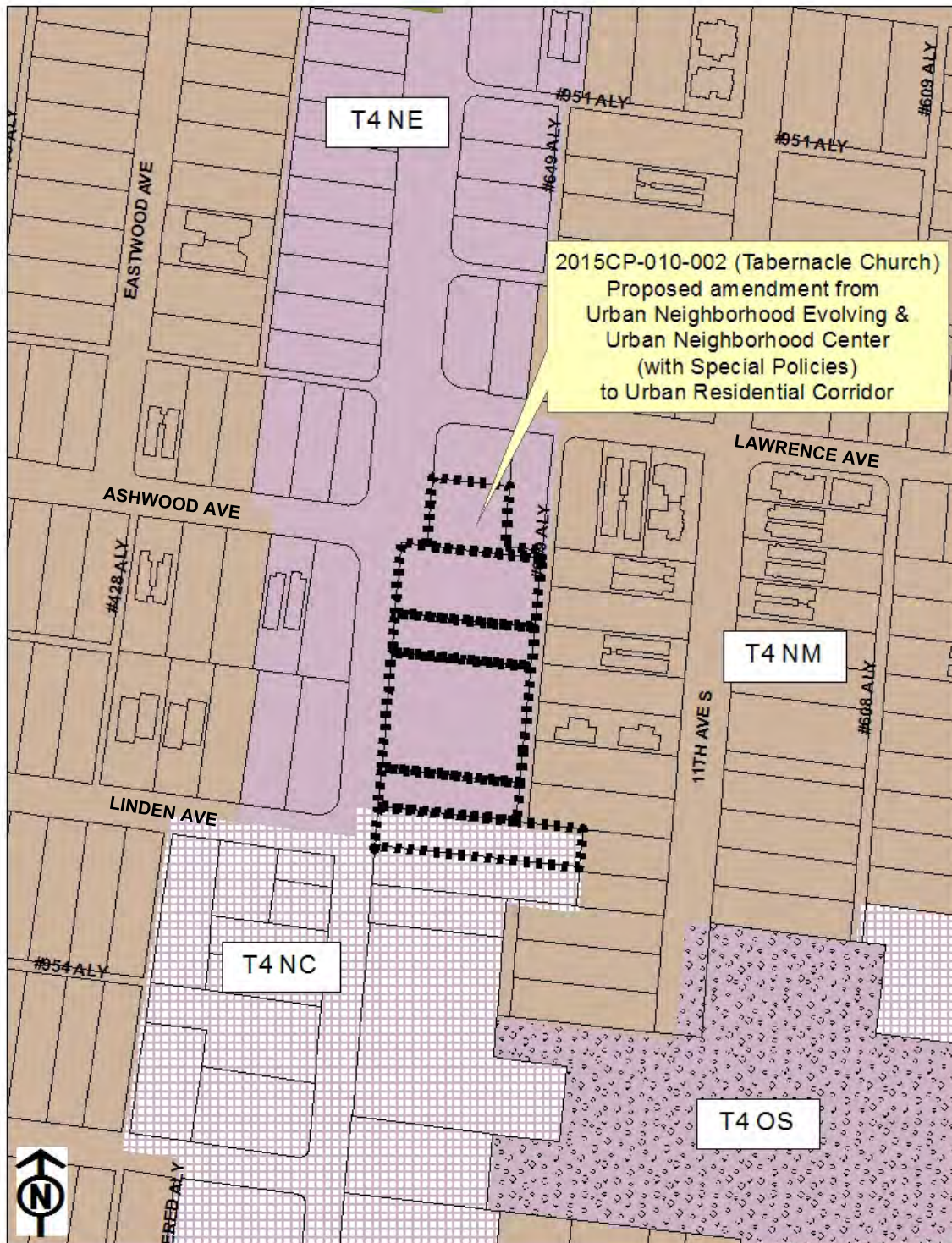


## **PREVIOUSLY DEFERRED ITEMS**

- **Community Plan Amendments**
- **Zoning Text Amendments**
- **Specific Plans**
- **Planned Unit Developments**
- **Subdivisions**



# Metro Planning Commission Meeting of 04/23/2015



**2015CP-010-002**  
**GREEN HILLS-MIDTOWN COMMUNITY PLAN AMENDMENT**  
Map 105-13, Parcels 198, 200-203, and 420  
Green Hills-Midtown  
17 – Sandra Moore



**Project No.  
Project Name**

**Minor Plan Amendment 2015CP-010-002  
Green Hills-Midtown Community Plan: 2005  
Update – Bristol 12 South**

**Associated Case  
Council District  
School District  
Requested by**

2015SP-010-001  
17 – Moore  
8 – Pierce  
Littlejohn Engineering Associates, applicant; Tabernacle Baptist Church, owner.

**Deferral**

This case was deferred from the February 12, 2015, February 26, 2015, and March 26, 2015, Planning Commission meetings.

**Staff Reviewer  
Staff Recommendation**

McCaig  
*Approve, including retaining and amending the special policy.*

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**APPLICANT REQUEST**

**Change the policy from Urban Neighborhood Evolving and Urban Neighborhood Center to Urban Residential Corridor from the 12<sup>th</sup> Avenue South Corridor Detailed Neighborhood Design Plan.**

Minor Plan Amendment

A request to amend the 12<sup>th</sup> Avenue South Corridor Detailed Neighborhood Design Plan element of the *Green Hills-Midtown Community Plan: 2005 Update* by changing the Community Character policy from Urban Neighborhood Evolving policy and Urban Neighborhood Center policy with Special Policies to Urban Residential Corridor policy for property located at 2206-2220 12<sup>th</sup> Avenue South, approximately 140 feet east of 12<sup>th</sup> Avenue South, (1.89 acres).

**CRITICAL PLANNING GOALS**

**Provides a Range of Housing Choices**

The Urban Residential Corridor policy supports various types of residential uses, including townhouses and flats, which add housing options to the surrounding 12South neighborhood, consisting of primarily single family housing.

**Creates Walkable Neighborhoods**

The Urban Residential Corridor policy allows for a variety of housing types that will help create a more walkable environment than exists today. The zoning districts used to implement the new policy place an emphasis on building and site design that support walkability, such as making strong connections between main entrances and sidewalks, orienting buildings toward the sidewalk, and minimizing the prominence of parking facilities.

**Supports Infill Development**

The Urban Residential Corridor policy will foster new development and redevelopment in an urban infill location where much of the needed infrastructure is already in place and additional infrastructure, such as sidewalks, can be made as zone changes occur to implement the new policy.



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### GREEN HILLS-MIDTOWN COMMUNITY PLAN

#### Current Policy

Urban Neighborhood Maintenance (T4 NM) policy is intended to preserve the general character of existing urban neighborhoods as characterized by their development pattern, building form, primarily residential land use and associated public realm. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and the public realm. Enhancements may be made to improve pedestrian, bicycle, and vehicular connectivity. This policy applies to the majority of the site.

Urban Neighborhood Center (T4 NC) policy is intended to preserve, enhance, and create urban neighborhood centers that fit in with the general character of urban neighborhoods. Infrastructure and transportation networks may be enhanced to improve pedestrian, bicycle, and vehicular connectivity. T4 NC areas are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, residential, and civic and public benefit uses. This policy applies to the southernmost property.

Special Policies: The *12<sup>th</sup> Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

- rezone properties to Specific Plan with the density of RM15;
- maintain and continue throughout the Subdistrict the current pattern of built setbacks and spacing that is found in the portion of this Subdistrict located on the west side of 12<sup>th</sup> Avenue South between #951 Alley (opposite South Douglas Avenue) and #666 Alley (alley north of Ashwood Avenue);
- limit heights in this Subdistrict to a maximum of three stories at 35 feet with additional height allowed for raised foundation when necessary for providing privacy;
- place buildings so that the primary pedestrian entrance is oriented to the street;
- access buildings from alleys; and,
- provide parking behind, beneath, or beside buildings.

#### DRAFT Preferred Future Policy

Urban Residential Corridor (T4 RC) policy is intended to preserve, enhance, and create urban residential corridors that support predominantly residential land uses; fit in with the general character of urban neighborhoods; and move vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

Urban Neighborhood Center (T4 NC) policy is intended to preserve, enhance, and create urban neighborhood centers that fit in with the general character of urban neighborhoods. Infrastructure and transportation networks may be enhanced to improve pedestrian, bicycle, and vehicular connectivity. T4 NC areas are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, residential, and civic and public benefit uses. This policy applies to the southernmost property.

Special Policies: The *12<sup>th</sup> Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing



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with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

- rezone properties to Specific Plan with the density of RM15;
- maintain and continue throughout the Subdistrict the current pattern of built setbacks and spacing that is found in the portion of this Subdistrict located on the west side of 12<sup>th</sup> Avenue South between #951 Alley (opposite South Douglas Avenue) and #666 Alley (alley north of Ashwood Avenue);
- limit heights in this Subdistrict to a maximum of three stories at 35 feet with additional height allowed for raised foundation when necessary for providing privacy;
- place buildings so that the primary pedestrian entrance is oriented to the street;
- access buildings from alleys; and,
- provide parking behind, beneath, or beside buildings.

### **Proposed Policy**

Urban Residential Corridor (T4 RC) policy is intended to preserve, enhance, and create urban residential corridors that support predominantly residential land uses; fit in with the general character of urban neighborhoods; and move vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

### **BACKGROUND**

The properties located at 2206-2220 12<sup>th</sup> Avenue South have been used as a religious institution and single family homes for years. Property records show that the church purchased its main property in 1966. The church acquired additional adjacent properties in 1982, 1985, 1998, and 2005, both for parking and for single family homes.

The Green-Hills Midtown Community Plan was last updated in 2005. However, a more specific planning study created the *12<sup>th</sup> Avenue South Corridor Detailed Neighborhood Design Plan* in 2008. The overarching goal of the design plan is to maintain the corridor as a livable and walkable community by providing a well-balanced mix of housing, neighborhood-scaled businesses, real transportation options, easily accessible open spaces, employment and social services, and civic and cultural opportunities. As one moves off the 12<sup>th</sup> Avenue corridor, the design plan highlights the importance of preserving the existing housing stock and single family residential character of the immediately adjacent residential area. The design plan accommodates additional housing types by allowing them along the corridor itself. This provides additional housing choices but also helps to preserve the existing single family housing.

### **COMMUNITY PARTICIPATION**

This is a minor plan amendment, and, as such, no community meeting is required. However, with two other major plan amendments currently active in the area, a community meeting was held on December 4, 2014, to discuss the three plan amendment requests and associated rezoning requests. Approximately 55 people attended the meeting, along with the applicants, and the area councilmember. For many attendees, the community meeting was the first time they were hearing about the plan amendment for the church property. Several attendees voiced concerns and left written comments regarding this proposal. Attendees were mainly concerned that the proposed amendment and rezoning, if approved, would:

- create a large-scale building that is out of character with the corridor due to its intensity;
- create too much density on a small piece of land;



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- create negative traffic impacts due to the property's location – at the point where 12<sup>th</sup> Avenue South narrows and enters the business area which is already congested; and
- create the need for a traffic light that in turn would create more cut-through traffic on neighborhood streets.

In January, Public Hearing Notices were mailed out to property owners within 600 feet prior to the MPC Public Hearing. Local neighborhood associations were also notified of both the community meeting and the public hearing. Copies of the notices were also placed on the Planning Department website.

### ANALYSIS

Currently, the six properties involved in the plan amendment contain a church, parking, and four single family homes. The properties are located along 12<sup>th</sup> Avenue South and have been designated by policy for additional housing opportunities. However, additional design guidance is provided through the detailed neighborhood design plan that discusses building height, access, density, and setbacks to provide a harmonious development with what exists along the corridor (see guidance under the Current Policy section above).

The applicant has requested Urban Residential Corridor policy which is applied to prominent urban corridors with adequate transportation capacity for higher intensity residential uses. Urban Residential Corridor policy allows buildings up to three stories in height. In deciding the character and form of what is appropriate in a specific location, locational factors – including the width of the street, depth of the lots, topography, existing character of the surrounding properties along the corridor, and how the development transitions to what is behind it – are considered. Appropriate density is secondary to the building's form and design. At this location along the 12<sup>th</sup> Avenue South corridor, staff recommends that the special policy be amended to remove the density limitation of RM15, but retain the other design features, including the limited height of 35 feet. The proposed special policy language is:

Special Policies: The *12<sup>th</sup> Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

- maintain and continue throughout the Subdistrict the current pattern of built setbacks and spacing that is found in the portion of this Subdistrict located on the west side of 12<sup>th</sup> Avenue South between #951 Alley (opposite South Douglas Avenue) and #666 Alley (alley north of Ashwood Avenue);
- limit heights in this Subdistrict to a maximum of three stories at 35 feet with additional height allowed for raised foundation when necessary for providing privacy;
- place buildings so that the primary pedestrian entrance is oriented to the street;
- access buildings from alleys; and,
- provide parking behind, beneath, or beside buildings.

The 12<sup>th</sup> Avenue South corridor is envisioned as a neighborhood-scaled center that includes opportunities for additional housing. Having this property redevelop as higher density residential uses is appropriate. The proposed development meets the policy intent of the proposed Urban Residential Corridor policy.





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### **STAFF RECOMMENDATION**

Staff recommends approval of the plan amendment, including retaining and amending or including the special policy, as it reflects the area's recommended policy change as part of NashvilleNext.



# Metro Planning Commission Meeting of 04/23/2015



**2015SP-010-001**

**BRISTOL 12 SOUTH**

Map 105-13, Parcel(s) 198, 200-203, 420

10, Green Hills - Midtown

17 (Sandra Moore)



**Project No.** 2015SP-010-001  
**Project Name** Bristol 12 South  
**Associated Case** 2015CP-010-002  
**Council District** 17 - Moore  
**School District** 8 - Pierce  
**Requested by** Littlejohn Engineering Associates, applicant; Tabernacle Baptist Church, owner.

**Deferrals** This case was deferred from the February 12, 2015, February 26, 2015, and March 26, 2015, Planning Commission meetings.

**Staff Reviewer** Milligan  
**Staff Recommendation** *Approve with conditions and disapprove without all conditions, subject to approval of the associated policy amendment.*

**APPLICANT REQUEST**

**Preliminary SP to permit a multi-family residential development.**

Preliminary SP

A request to rezone from Commercial Services (CS) and One and Two-Family Residential (R8) to Specific Plan-Residential (SP-R) for properties located at 2206, 2208, 2212, 2214, 2218, and 2220 12<sup>th</sup> Avenue South (1.89), to permit a multi-family residential development with up to 158 units.

**Existing Zoning**

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 5.79 dwelling units per acre including 25 percent duplex lots. *R8 would permit a maximum of 8 lots with 2 duplex lots for a total of 10 units.*

Commercial Service (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.

**Proposed Zoning**

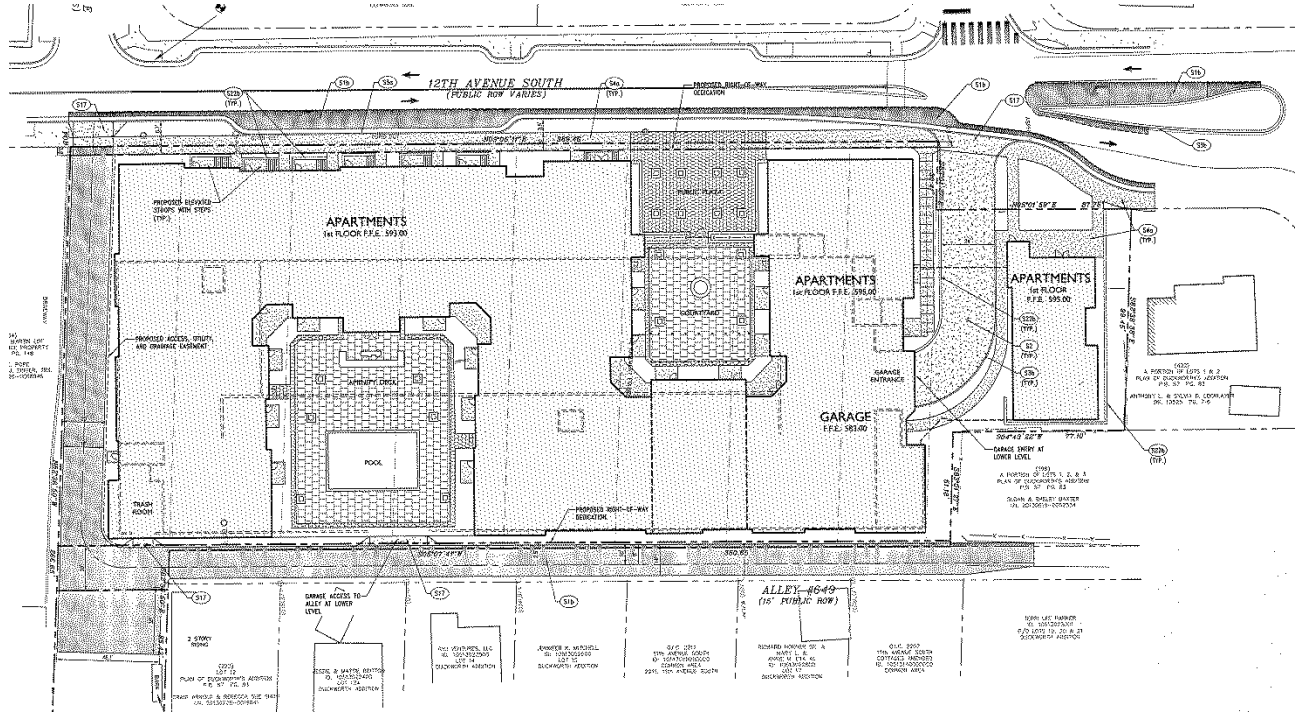
Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

**CRITICAL PLANNING GOALS**

- Supports a Variety of Transportation Choices
- Creates Walkable Neighborhoods
- Provides a Range of Housing Choices
- Supports Infill Development



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**Proposed Site Plan**



**Proposed 12<sup>th</sup> Avenue S Elevation**



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The proposed development supports several critical planning goals. The development is intensifying an underutilized urban site and providing for an additional housing option within an urban neighborhood. Providing for infill development on a site with existing infrastructure decreases the burden on Metro to provide for new infrastructure. The development provides sidewalks, increasing the walkability in an urban neighborhood. The additional residents will also be able to walk to nearby retail uses and restaurants, decreasing the dependency on automobile travel. The project is located on an existing bus line, providing for additional transportation choices for future residents.

### **GREEN HILLS MIDTOWN COMMUNITY PLAN**

#### **Current Policy**

T4 Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

T4 Urban Neighborhood Center (T4 NC) policy is intended to preserve, enhance, and create urban neighborhood centers that are compatible with the general character of urban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T4 Urban Neighborhood Centers are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, civic and public benefit land uses, with residential only present in mixed use buildings. T4 Urban Neighborhood Centers serve urban neighborhoods within a 5 minute walk.

Special Policies: The *12<sup>th</sup> Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

- rezone properties to Specific Plan with the density of RM15;
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- limit heights in this Subdistrict to a maximum of three stories at 35 feet with additional height allowed for raised foundation when necessary for providing privacy;
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- access buildings from alleys; and
- provide parking behind, beneath, or beside buildings.

#### **Growth and Preservation Concept Map**

The T4 Urban Neighborhood Center and Special Policies are not proposed to change. The T4 Urban Neighborhood Evolving area is proposed to change to T4 Urban Residential Corridor.



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T4 Urban Residential Corridor (T4 RC) policy is intended to preserve, enhance and create urban residential corridors that support predominately residential land uses; are compatible with the general character of urban neighborhoods as characterized by development pattern, building form, land use, and associated public realm; and that move vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

Special Policies: The *12<sup>th</sup> Avenue South Corridor Detailed Neighborhood Design Plan* provides additional design guidance in this area. These properties fall under Subdistrict 4 – Mixed Housing with a goal of enhancing the current developed condition of the corridor and providing a mixture of housing types to meet the diverse needs of the neighborhood. Objectives include to:

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- place buildings so that the primary pedestrian entrance is oriented to the street;
- access buildings from alleys; and
- provide parking behind, beneath, or beside buildings.

### **Proposed Policy**

T4 Urban Residential Corridor (T4 RC) policy is intended to preserve, enhance and create urban residential corridors that support predominately residential land uses; are compatible with the general character of urban neighborhoods as characterized by development pattern, building form, land use, and associated public realm; and that move vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

### Consistent with Policy?

The proposed development is consistent with the T4 Residential Corridor policy, including the height of the project. The development also meets the height restriction of the Special Policy for the area, providing for 3 stories within 35 feet, with some allowance for raised foundations. The development meets the intent of the T4 Residential Corridor policy and the Special Policies by providing for a development that is compatible with the general character of the existing urban neighborhood in both scale and design. Sidewalks and an improved pedestrian environment are being provided to enhance the existing neighborhood.

### **PLAN DETAILS**

The site is located at 2206, 2208, 2212, 2214, 2218, and 2220 12<sup>th</sup> Avenue South. The site is approximately 1.89 acres in size. The property is currently in use as a church, parking lot, and several residential dwelling units.

### Site Plan

The plan proposes a multi-family residential development with up to 158 residential dwelling units. The proposed building is 3 stories in 35 feet along 12<sup>th</sup> Avenue South, not including a raised foundation of no more than 36". Along the alley, the building has one garage level with three stories of residential above.



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Vehicular access is being proposed at two points along 12<sup>th</sup> Avenue South, one at the northern property line and one at the southern property line. The access on the southern property line will connect to the existing alley. The parking garage can be accessed from the alley. A Traffic Impact Study (TIS) was submitted by the applicant and reviewed and approved by the Department of Public Works.

Along 12<sup>th</sup> Avenue South, eight foot sidewalks are being proposed. The plan would meet the requirements of the Bike Parking Ordinance.

The plan proposes raised foundations along 12<sup>th</sup> Avenue South with the inclusion of stoops/porches for some of the ground floor units. The façade plane of the building will be interrupted every 50' to avoid a continuous, uninterrupted blank façade. Interruptions shall include a change in material, horizontal undulation, or a porch/stoop/balcony. Street facades shall also include a minimum of 15% glazing. A public plaza has been provided along 12<sup>th</sup> Avenue South. A landscape buffer is being provided along the northern boundary, adjacent to the existing residential properties.

### **ANALYSIS**

The proposed development meets the intent of the T4 Residential Corridor policy and the Special Policies for the area by complementing the existing form and intensity within the 12<sup>th</sup> Avenue South corridor. The height of the building along 12<sup>th</sup> Avenue South meets the general and special policies. The design of the building and the inclusion of stoops, porches, and the courtyard area along 12<sup>th</sup> Avenue South provides for a pedestrian friendly streetscape that is broken up and lessens the intensity along the street. Given the location of the building along the existing urban corridor and based on the design, staff recommends approval.

### **FIRE DEPARTMENT RECOMMENDATION**

#### **Approve with conditions**

- Fire Code issues for the structures will be addressed at permit application review.

### **STORMWATER RECOMMENDATION**

#### **Approved with conditions**

- Minimum storm pipe in ROW shall be 15" minimum

### **WATER SERVICES**

#### **Approved**

### **PUBLIC WORKS RECOMMENDATION**

#### **Approve with conditions**

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Record ROW dedication to the back of the proposed sidewalks, prior to building permit approval.

### **TRAFFIC AND PARKING RECOMMENDATION**

#### **Approved with conditions**



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In accordance with TIS findings, developer shall construct the following roadway improvements.  
 12th Avenue South and Ashwood Avenue/Site Access

- The proposed site access on 12th Avenue South shall be aligned with Ashwood Avenue and designed to include a minimum of one entering lane and one exiting lane.
- Developer shall construct a southbound left turn lane on 12th Avenue South at garage access. The left turn lane can be constructed by reconstructing the existing median. A minimum of 50 feet of storage and 50 feet of taper should be provided. The reconstruction of the median may require modification or relocation of the existing “12 South Neighborhood” sign within the median. As part of this requirement, storage shall be provided to accommodate the NBLT movement onto Ashwood in order to provide appropriate sight distance for vehicle and pedestrian movements.
- In order to improve intersection sight distance for traffic exiting the site, the driveway should be extended into the public right-of-way so that the sight lines are in front of the building face. On-street parking should not be provided on the east side of 12th Avenue South within 200 feet south of the site access in order to provide sufficient intersection sight distance for vehicles exiting the site access.
- A bulb-out should be constructed within the excess right-of-way north of the site access. A minimum of 16 feet of pavement should be provided between the median and the curb extension on the north side of the site access.
- Developer shall install a crosswalk for the west leg of Ashwood Avenue.

Maximum Uses in Existing Zoning District: **CS**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (814)	0.33	0.6 F	8,624 SF	407	15	43

Maximum Uses in Existing Zoning District: **R8**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (210)	1.56	5.4 D	10 U *	96	8	11

\*Based on two two-family lots.

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (220)	1.89	-	158 U	1082	82	105

Traffic changes between maximum: **CS** and **R8** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+579	+59	+51





## Metro Planning Commission Meeting of 04/23/2015

### **METRO SCHOOL BOARD REPORT**

**Projected student generation existing R8 district: 1 Elementary 0 Middle 1 High**

**Projected student generation proposed SP-R district: 7 Elementary 1 Middle 3 High**

The proposed SP-R zoning could generate 9 more students than what is typically generated under the existing R8 zoning. Students would attend Julia Green Elementary, J.T. Moore Middle School and Hillsboro High School. Julia Green and J.T. Moore Middle have been identified as over capacity and there is no capacity for elementary or middle school students within the cluster. This information is based upon data from the school board last updated October 2014.

The fiscal liability of 6 new elementary students is \$129,000 (6 X \$21,500 per student) and for 1 additional middle school student is \$26,000 (1 X \$26,000 per student). This is only for information purposes to show the potential impact of this proposal, it is not a staff condition of approval.

### **STAFF RECOMMENDATION**

Staff recommends approval with conditions and disapproval without all conditions subject to the approval of the associated policy change. If the associated policy change is not approved, staff recommends disapproval.

### **CONDITIONS**

1. Uses shall be limited to up to 158 multi-family residential dwelling units.
2. On the corrected set, add a note that the minimum raised foundation along 12<sup>th</sup> Avenue South shall be 18" and the maximum raised foundation along 12<sup>th</sup> Avenue South shall be 36".
3. The final site plan shall show an adjusted building location and public plaza in order to provide a minimum of 4 foot grass strip between the curb and the sidewalk and an 8 foot sidewalk up to the crosswalk on the south side of the entrance drive opposite Ashwood Avenue. The width of the frontage planting area shall remain as shown on the current plan.
4. With the final plat, dedicate right-of-way to the Major and Collector Street specifications. Any additional improvements, including sidewalks, shall be incorporated into a pedestrian easement on the final plat.
5. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM80-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
6. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
7. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
8. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



**NO SKETCH**



**Project No.  
Project Name**

**Text Amendment 2015Z-003TX-001  
DTC Housekeeping Text Amendment**

**Council Bill  
Council District  
School District  
Requested by**

BL2015-1053  
19 – Gilmore  
5 – Kim  
Councilmember Erica Gilmore and Metro Planning  
Department

**Deferral**

This case was deferred from the March 26, 2015, and  
April 9, 2015, Planning Commission meetings.

**Staff Reviewer  
Staff Recommendation**

Collins  
*Approve.*

**APPLICANT REQUEST**

**Amend the Zoning Code to update review processes, update maps, revise typographic errors, and refine the urban design standards of the Downtown Code.**

**TEXT AMENDMENT**

A request to amend Chapters 17.12, 17.37 and 17.40 of the Metropolitan Code to update the Downtown Code (DTC) standards.

**ANALYSIS**

This text amendment updates and refines the Downtown Code (DTC) design standards and review processes, to continue to encourage high quality development downtown. The amendments correct discrepancies and errors in the text and graphics; updates maps, as well as the review process for final site plan review, variances and modifications to DTC standards.

The general standards section of the DTC is also updated in order to continue providing opportunities for business to thrive, while enhancing the character of Downtown through thoughtful development, with care given to the public realm. For instance, the step-back provisions will be updated to allow for greater facade variation by-right, giving flexibility to project designers; the building step-back will be allowed to occur between a range of stories (so long as the minimum step-back depth is achieved by the upper story bound of the range), rather than the current mandate of requiring a building to fully step-back 15’ at a single story. Another update will require an active ground floor use on tertiary streets, if the tertiary street is a project’s only frontage. This will aid in further activating the public streetscape.

Direct references to the Major and Collector Street Plan are also added, strengthening the sidewalk corridors and streetscape language. Additional guidance is also provided on requiring parking structures to have architectural cladding or facade treatment to eliminate exposed parking decks downtown, if visible. Guidance is also provided on vehicular / pedestrian conflicts (such as loading docks and valet pull-offs), so that the pedestrian realm is adequately maintained at those points.

The Bonus Height Program is updated to include the preservation of historic buildings, and the enlargement of the upper level garage liner bonus depth from 15’ to 20’. Section 17.12.120 of the



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Zoning Code, Transfer of Development Rights, will be deleted with a follow-up council bill, as it's based on Floor Area Ratio, which does not exist within the DTC. Therefore, this program cannot and has not been implemented. In addition, the transfer of development rights process is already included within the DTC Bonus Height Program.

### **STAFF RECOMMENDATION**

Staff recommends approval.

**Please see Exhibit A at the end of this report for proposed edits to the DTC.**

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### **Ordinance No. BL2015-1053**

**An Ordinance amending Chapters 17.12, 17.37 and 17.40 of the Metropolitan Code to update the Downtown Code (DTC) standards. (Proposal No. 2015Z-003TX-001).**

WHEREAS, The Council of The Metropolitan Government of Nashville and Davidson County adopted the Downtown Code (DTC) on February 2, 2010; and

WHEREAS, The Metropolitan Government recognizes the importance of updating and refining the Downtown Code design standards and process to encourage high quality, sustainable urban development; and

WHEREAS, The Downtown Code strives to encourage active and interactive streetscapes; and

WHEREAS the DTC standards provide opportunities for businesses to thrive, while enhancing the character of Downtown through mixed-use development, housing choices, and commercial and entertainment opportunities; and

WHEREAS, the refinement of the DTC standards will continue to ensure a quality urban experience for the citizens and businesses of Metropolitan Nashville and Davidson County.

**NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:**

Section 1. That Section 17.12.060.F of the Metropolitan Code is hereby amended by inserting the following after the words "Urban Zoning Overlay District":

" , but not including the DTC district:"

Section 2. That Chapter 17.37 of the Metropolitan Code is hereby amended by replacing "Exhibit A" in its entirety with the attached Exhibit; which updates all maps with the most recently available property lines; revises any typographic and mapping errors; refines the modifications, variances and special exception process; updates the sidewalk requirements by referencing the Major and Collector Street Plan; and updates the General Standards by providing additional guidance on vehicular / pedestrian conflicts, parking structure facades, and street frontages.



## Metro Planning Commission Meeting of 04/23/2015

Section 3. That Section 17.40.170.B of the Metropolitan Code is hereby amended by deleting the words:

“DTC district,”

Section 4. That Section 17.40.170 of the Metropolitan Code is hereby amended by renumbering “C” as “D and inserting a new “C” as follows:

C. Final Approval by the Planning Department within the DTC district. Prior to the issuance of a building permit, the Planning Department shall review site plans in the DTC district to verify conformance with the provisions of Chapter 17.37 of this title. A site plan shall be submitted in form and content established by the Planning Department, and shall specifically describe the nature and scope of development to serve as the basis for determination of compliance with the DTC district standards.

Section 5. That this Ordinance shall take effect five (5) days from and after its passage and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.

Sponsored by: Councilmember Erica Gilmore

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### **Ordinance No. BL2015-XXXX**

**An Ordinance amending Chapters 17.12 of the Metropolitan Code to update the Downtown Code (DTC) standards. (Proposal No. 2015Z-003TX-001).**

WHEREAS, The Council of The Metropolitan Government of Nashville and Davidson County adopted the Downtown Code (DTC) on February 2, 2010; and

WHEREAS, The Metropolitan Government recognizes the importance of updating and refining the Downtown Code design standards and process to encourage high quality, sustainable urban development; and

WHEREAS, The Downtown Code strives to encourage active and interactive streetscapes; and

WHEREAS the DTC standards provide opportunities for businesses to thrive, while enhancing the character of Downtown through mixed-use development, housing choices, and commercial and entertainment opportunities; and

WHEREAS, the refinement of the DTC standards will continue to ensure a quality urban experience for the citizens and businesses of Metropolitan Nashville and Davidson County.



## **Metro Planning Commission Meeting of 04/23/2015**

NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:

Section 1. That Section 17.12.120 of the Metropolitan Code is hereby deleted in its entirety.

Section 2. That this Ordinance shall take effect five (5) days from and after its passage and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.

Sponsored by: Councilmember Erica Gilmore



**SEE NEXT PAGE**



# Metro Planning Commission Meeting of 04/23/2015



**2015SP-022-001**

6309 NOLENSVILLE PIKE

Map 173, Parcel(s) 150

12, Southeast

04 (Brady Banks)





**Project No.** 2015SP-022-001  
**Project Name** 6309 Nolensville Pike  
**Council District** 4 – Banks  
**School District** 2 – Brannon  
**Requested by** Anderson, Delk, Epps and Associates, applicant; Pence Leasing, LLC, owners.

**Deferrals** This case was deferred from the March 12, 2015, and April 9, 2015, Planning Commission meetings.

**Staff Reviewer** Sajid  
**Staff Recommendation** *Withdraw.*

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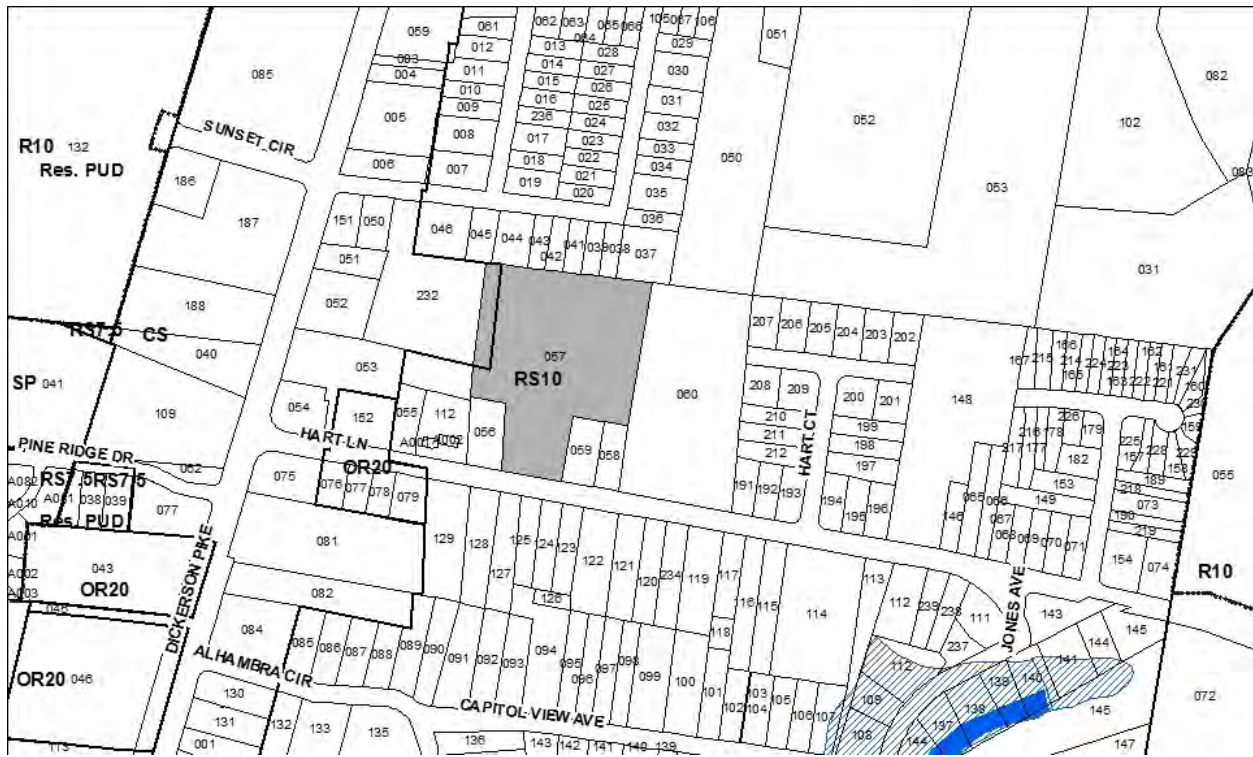
**APPLICANT REQUEST**  
**Preliminary SP to permit up to 108 residential units.**

Preliminary SP  
A request to rezone from Agricultural/Residential (AR2a) to Specific Plan-Residential (SP-R) zoning for property located at 6309 Nolensville Pike, approximately 1,530 feet north of Holt Road, (11.3 acres), to permit 108 residential units.

**STAFF RECOMMENDATION**  
Staff recommend withdrawal at the request of the applicant.



# Metro Planning Commission Meeting of 04/23/2015



**2015SP-029-001**  
**HART LANE COTTAGES**  
Map 060-12, Parcel(s) 057  
05, East Nashville  
08 (Karen Bennett)



**Project No.** 2015SP-029-001  
**Project Name** Hart Lane Cottages  
**Council District** 8 - Bennett  
**School District** 3 - Speering  
**Requested by** Dale & Associates, Inc., applicant; John Howard, owner.

**Deferrals** This case was deferred from the March 12, 2015, and March 26, 2015, Planning Commission meetings

**Staff Reviewer** Milligan  
**Staff Recommendation** *Approve with conditions and disapprove without all conditions.*

**APPLICANT REQUEST**  
**Preliminary SP to permit up to 26 residential units**

Preliminary SP  
 A request to rezone from Single-Family Residential (RS10) to Specific Plan-Residential (SP-R) zoning for properties located at 115 Hart Lane (4.59 acres), to permit up to 26 multi-family residential units.

**Existing Zoning**  
Single-Family Residential (RS10) requires a minimum of 10,000 square foot lot and is intended for single-family dwellings at a density of 3.7 dwelling units per acre. *RS10 would permit a maximum of 17 units.*

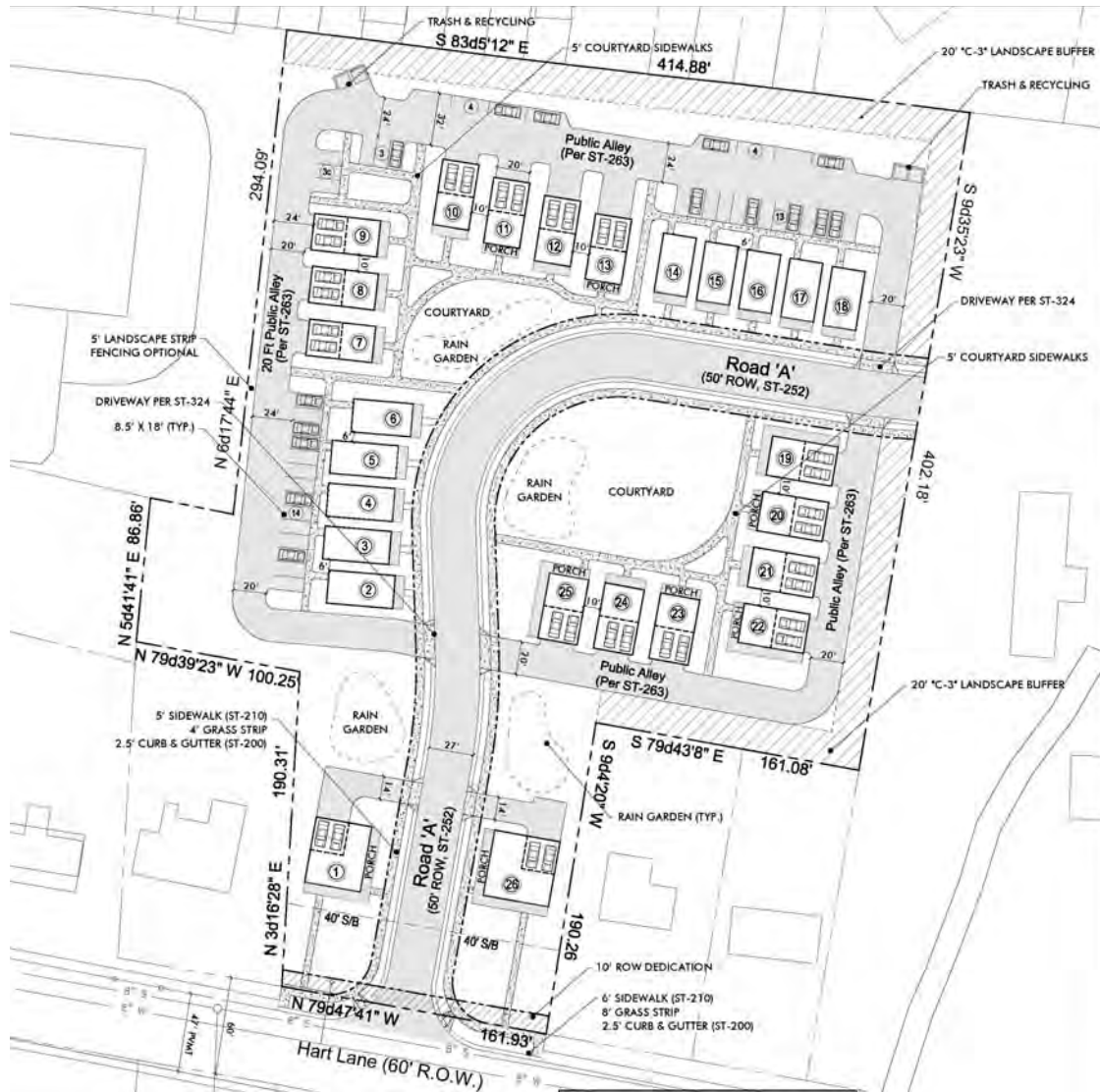
**Proposed Zoning**  
Specific Plan-Residential (SP-R) is a zoning District category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes one residential building type.

- CRITICAL PLANNING GOALS**
- Supports Infill Development
  - Creates Walkable Neighborhoods
  - Provides a Range of Housing Choices

This proposal meets several critical planning goals. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure such as roads, water and sewer, because it does not burden Metro with the cost of maintaining new infrastructure. The project will intensify development on an infill site and provide for a different housing type than currently exists in the immediate area. Sidewalks are being provided along Hart Lane and internal roads to provide for a more walkable neighborhood.



# Metro Planning Commission Meeting of 04/23/2015



**Proposed Site Plan**



## Metro Planning Commission Meeting of 04/23/2015

### **EAST NASHVILLE COMMUNITY PLAN**

#### **Existing Land Use Policy**

T4 Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

#### **DRAFT Preferred Future Land Use Policy**

No change proposed.

#### Consistent with Policy?

Yes. The proposed SP zoning is consistent with the proposed T4 NE policy. The proposed development is creating an additional housing option in this area while still being compatible with the general character of the area in regards to building placement. The units proposed along Hart Lane reflect the setbacks of the existing homes.

### **PLAN DETAILS**

The site is located at 115 Hart Lane, north of Hart Lane and east of Dickerson Pike. The site is approximately 4.59 acres in size. The current use of the property is 1 single-family detached unit.

#### Site Plan

The plan proposes up to 26 multi-family residential dwelling units. All of the units will be single-family detached units. There are two units proposed along Hart Lane. The Hart Lane units will have similar setbacks to the existing homes along Hart Lane.

The plan provides sidewalks along Hart Lane, along Conviser Drive and within the open space/courtyard area. Vehicular access to garages and parking is from a series of alleys, except for the Hart Lane units which will gain access from Conviser Drive. Street trees are proposed along Conviser Drive. Conviser Drive is stubbed out to the eastern property line, which will allow for a future connection to the existing Conviser Drive.

Architectural standards have been provided including specifications for raised foundations, window orientation, porches, materials, etc. Many units are planned with wraparound porches to provide for architectural interest.

### **ANALYSIS**

The plan is consistent with the draft preferred future land use policy and adds housing choice to an existing urban neighborhood. The plan meets several critical planning goals including creating a more pedestrian friendly, walkable streetscape and providing an infill development on an underutilized urban lot.



## Metro Planning Commission Meeting of 04/23/2015

### FIRE DEPARTMENT RECOMMENDATION

#### Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review

### STORMWATER RECOMMENDATION

#### Approved

### WATER SERVICES

#### Approved

- A revised availability study has been submitted, which matches the unit count in this Preliminary SP (28 units). Approved as a Preliminary SP only. Public construction plans must be submitted and approved prior to Final SP stage. The required capacity fees must be paid prior to Final SP approval.

### PUBLIC WORKS RECOMMENDATION

#### Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Indicate that the private alleys are to be dedicated into ROW prior to building permit approval

### TRAFFIC AND PARKING RECOMMENDATION

#### No exception taken

Maximum Uses in Existing Zoning District: **RS10**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	4.59	4.35 D	19 U	182	15	20

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (230)	4.59	-	28 U	213	19	22

Traffic changes between maximum: **RS10** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 9 U	+31	+4	+2



## **Metro Planning Commission Meeting of 04/23/2015**

### **METRO SCHOOL BOARD REPORT**

**Projected student generation existing RS10 district: 1 Elementary 1 Middle 1 High  
Projected student generation proposed SP-R district: 1 Elementary 1 Middle 1 High**

The proposed SP-R zoning district will generate no additional students that what could be generated under the existing RS10 zoning. Students would attend Tom Joy Elementary School, Jere Baxter Middle School and Maplewood High School. None of the schools have been identified as being over capacity. This information is based upon data from the school board last updated October 2015.

### **STAFF RECOMMENDATION**

Staff recommends approval with conditions and disapproval without all conditions. The plan is consistent with the draft preferred future policy and meets several critical planning goals.

### **CONDITIONS**

1. Permitted land uses shall be limited to up to 26 multi-family residential units.
2. Side façades of units that face a public street shall provide the following: a minimum of one principal entrance (doorway) and a minimum of 25% glazing. Elevations of side façades units facing a public street shall be submitted with the final site plan.
3. For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations and requirements of the RM6 zoning district as of the date of the application request or application.
4. A corrected copy of the preliminary SP plan incorporating the conditions of approval by the Metro Council shall be provided to the Planning Commission prior to or with the final site plan application.
5. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering, or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
6. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



# Metro Planning Commission Meeting of 04/23/2015



**2015SP-037-001**  
**SOUTHGATE STATION**  
Map 105-11, Parcel(s) 019-020, 309-310  
11, South Nashville  
17 (Sandra Moore)





**Project No.** 2015SP-037-001  
**Project Name** Southgate Station  
**Council District** 17 - Moore  
**School District** 5 - Kim  
**Requested by** Civil Site Design Group, PLLC, applicant; William Smallman and Alpha One, LLC, owners.

**Deferrals** This case was deferred from the March 26, 2015, and April 9, 2015, Planning Commission meetings.

**Staff Reviewer** Milligan  
**Staff Recommendation** *Defer to the May 28, 2015, Planning Commission meeting.*

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**APPLICANT REQUEST**  
**Preliminary SP to permit up to 41 residential units.**

Preliminary SP  
A request to rezone from One and Two-Family Residential (R6) to Specific Plan-Residential (SP-R) for properties located at 514, 518, and 520 Southgate Avenue and 1608 Pillow Street (1.67 acres), to permit a multi-family residential development with up to 41 units.

**STAFF RECOMMENDATION**  
Staff recommends deferral to the May 28, 2015, Planning Commission meeting at the request of the applicant.



# Metro Planning Commission Meeting of 04/23/2015



**2015SP-038-001**  
ETHEL & LESLIE  
Map 091-10, Parcel(s) 125-131  
07, West Nashville  
20 (Buddy Baker)



<b>Project No.</b>	<b>Zone Change 2015SP-038-001</b>
<b>Project Name</b>	<b>Ethel &amp; Leslie</b>
<b>Council District</b>	20 - Baker
<b>School District</b>	9 - Frogge
<b>Requested by</b>	Dale and Associates, applicant; Leslie Avenue, G.P., owner.

**Deferral** This case was deferred from the March 26, 2015, Planning Commission meeting.

**Staff Reviewer** Birkeland  
**Staff Recommendation** *Approve with conditions and disapprove without all conditions.*

**APPLICANT REQUEST**

**Zone change to permit 15 detached residential units.**

Preliminary SP

A request to rezone from One and Two-Family Residential (R8) to Specific Plan-Residential (SP-R) zoning for property located at 5825 and 5827 Leslie Avenue and five parcels of Leslie Avenue (unnumbered), on the south side of Leslie Avenue between Sterling Street and Ethel Street, (1.26 acres), to permit up to 15 detached residential units.

**Existing Zoning**

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 5.79 dwelling units per acre including 25 percent duplex lots. *There are currently 7 lots, none of which are eligible for duplexes, for a total of 7 units.*

**Proposed Zoning**

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes attached residential buildings.

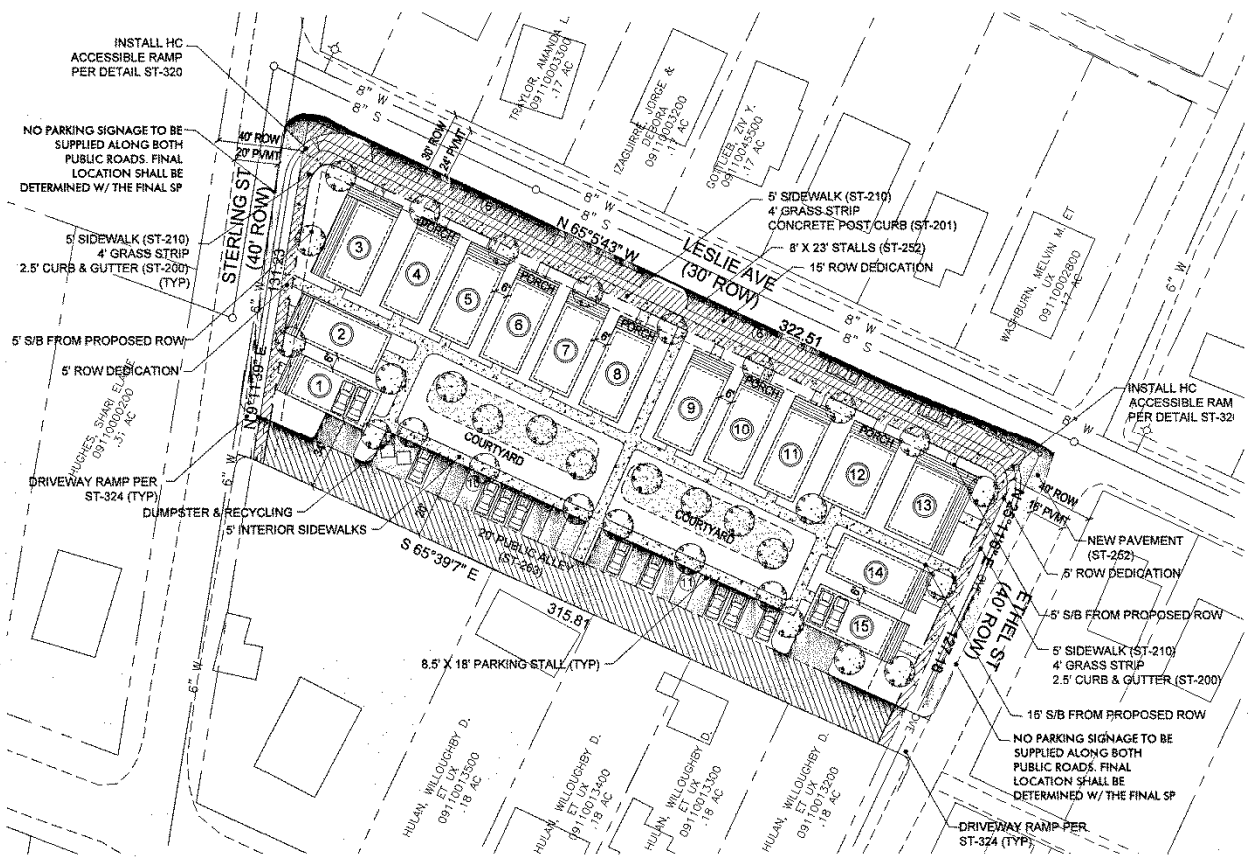
**CRITICAL PLANNING GOALS**

- Creates Walkable Neighborhoods
- Provides a Range of Housing Choices
- Supports infill development

The proposed SP creates an opportunity for infill housing in an area that is served by existing infrastructure. The request provides additional housing opportunities in the area. Housing options are important to serve a wide range of people with different housing needs. A new sidewalk along Ethel Street, Leslie Avenue and Sterling Street that will provide an improved pedestrian connection by providing pedestrians safe and access to other areas.



# Metro Planning Commission Meeting of 04/23/2015



Proposed Site Plan



## Metro Planning Commission Meeting of 04/23/2015

### WEST NASHVILLE COMMUNITY PLAN

#### Current Policy

T4 Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

#### Growth and Preservation Concept Map

No change proposed.

#### Consistent with Policy?

Yes. The policy supports the proposed detached residential units through form and interaction with the public realm. The SP will provide a mixture of housing types in a strategic location within West Nashville while providing improved pedestrian connectivity. The Neighborhood Evolving Policy also encourages improved alley access, which already exists along the rear of the property.

#### PLAN DETAILS

The site encompasses the entire block between Ethel and Sterling Streets, along Leslie Avenue in West Nashville. The site consists of seven existing parcels located at 5825 and 5827 Leslie Avenue and five parcels of Leslie Avenue (unnumbered). 5825 and 5827 Leslie Avenue have existing dwellings located on them; the other five parcels are vacant. The proposed SP includes 15 detached dwelling units; 11 units front Leslie Avenue, two units will front Ethel Street and two units will front Sterling Street.

The existing alley, between Sterling Street and Ethel Street, will be improved to public alley standards and will provide vehicular access from Ethel and Sterling Streets to the proposed dwelling units. Units 1 and 15 will have a two-car garage accessed from the alley. Additional surface parking for 21 vehicles has been provided on-site, off the alley. Right-of-way will be dedicated along Leslie Avenue that will allow for 12 additional parking stalls along the street. This SP has met and exceeded the Metro Zoning Code parking requirements. A courtyard will be established between the units and the on-site parking area to provide an active open space and landscaping. Additional landscaping will be placed throughout the SP.

Ethel, Leslie and Sterling Streets are identified as local streets in the Major and Collector Street Plan (MCSP). A new five foot sidewalk and a four foot planting strip will be installed along each street; each residential unit will have a connection to these sidewalks. Additionally, interior sidewalks, five feet in width, shall provide a pedestrian connection to the courtyard and the on-site parking area.

Architectural standards been included on the plan and shall be provided with the final site plan. The standards include that buildings 3 and 13 shall have wraparound porches on each street side, as well as the standard façade requirements. The proposed residential units shall have a maximum height limitation of three stories within 35 feet, measured to roofline.



# Metro Planning Commission Meeting of 04/23/2015

## ANALYSIS

The SP is consistent with the T4 Neighborhood Evolving policy and meets two critical planning goals. The 15 detached residential units will provide a well-designed development within West Nashville that needs additional housing.

## FIRE MARSHAL RECOMMENDATION

### Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review

## STORMWATER RECOMMENDATION

### Approved

## WATER SERVICES RECOMMENDATION

### Approved

- Approved as a Preliminary SP only. The required public sewer construction plans must be submitted and approved prior to Final SP stage. The required 30% capacity fees must be paid prior to Final SP approval.

## TRAFFIC AND PARKING RECOMMENDATION

### Approved with conditions

- Apply to Traffic and Parking to restrict on street parking along Ethel and Sterling Street frontage.

## PUBLIC WORKS RECOMMENDATION

### Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Prior to Final SP, revise the plans to indicate that all utility crossings within the ROW are at 90 degree angles with the centerline.
- Prior to building permit approval dedicate ROW to the back of the sidewalk along all public streets.

Maximum Uses in Existing Zoning District: **R8**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (210)	1.26	5.44 D	7 U *	67	6	8

\*Based on one two-family lot.



# Metro Planning Commission Meeting of 04/23/2015

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (210)	1.26	-	15 U	144	12	16

Traffic changes between maximum: **R8** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 8 U	+77	+6	+8

## METRO SCHOOL BOARD REPORT

**Projected student generation existing R8 district: 1 Elementary 1 Middle 1 High**

**Projected student generation proposed SP-R district: 3 Elementary 2 Middle 2 High**

The proposed SP-R zoning district could generate 4 more students than what is typically generated under the existing R8 zoning district. Students would attend Cockrill Elementary School, McKissack Middle School, and Pearl-Cohn High School. Cockrill Elementary School and Cane Pearl-Cohn High School have been identified as over capacity. There is capacity within the cluster for middle school and high school students. This information is based upon data from the school board last updated October 2014.

## STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions. The proposed SP is consistent with the T4 Neighborhood Evolving policy of the West Nashville Community Plan.

## CONDITIONS

1. Uses within this SP shall be limited to a maximum of 15 detached residential units.
2. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM15-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
3. The final site plan shall include architectural elevations showing raised foundations of 18-36" for residential buildings.
4. The dumpster and recycling area shall be screened by a six foot opaque fence.
5. No structure shall be more than three stories and shall be limited to a maximum height of 35 feet, measured to the roofline. Building elevations for all street and courtyard facing facades shall be provided with the final site plan. Buildings 3 and 13 shall have wraparound porches on each street side. The following standards shall be met:
  - a. Building facades fronting a street and courtyard shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
  - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except in dormers.
  - c. EIFS and vinyl siding shall be prohibited.
6. Ownership for units may be divided by a Horizontal Property Regime or a subdivision with a minimum lot size of 1,000 square feet.



## Metro Planning Commission Meeting of 04/23/2015

7. The preliminary SP plan approved by the metropolitan council is of such detail that the executive director of the planning department or his designee may waive the submittal of a final site plan.
8. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
9. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
10. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.

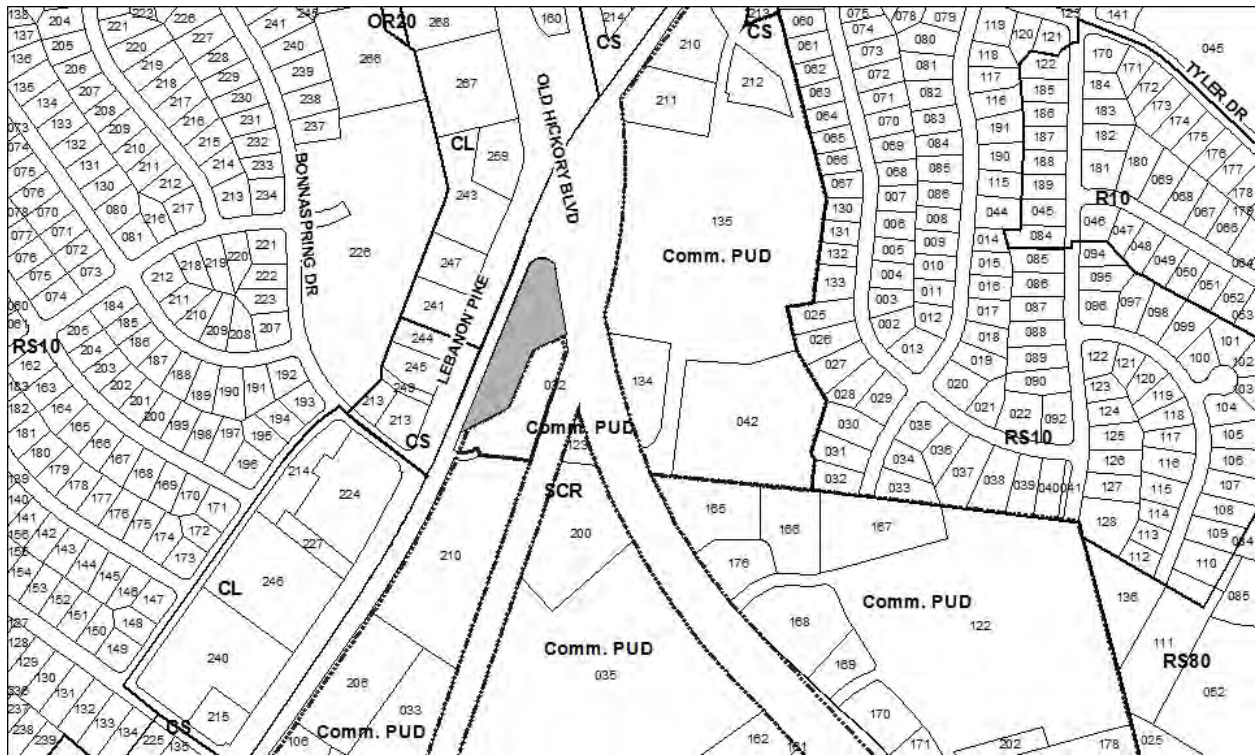




**SEE NEXT PAGE**



# Metro Planning Commission Meeting of 04/23/2015



**128-78G-001**

**HERMITAGE BUSINESS CENTER PUD (AMENDMENT)**

Map 075, Part of Parcel(s) 032

14, Donelson - Hermitage

14 (James Bruce Stanley)



<b>Project No.</b>	<b>Planned Unit Development 128-78G-001</b>
<b>Project Name</b>	<b>Hermitage Business Center (Amendment)</b>
<b>Council District</b>	14-Stanley
<b>School District</b>	4-Shepherd
<b>Requested by</b>	Civil Site Design Group, PLLC, applicant; Richard H. Watts Family Ltd. Partnership, owner.

**Deferrals** This case was deferred from the February 12, 2015, and March 12, 2015, Planning Commission meetings.

**Staff Reviewer** Milligan  
**Staff Recommendation** *Approve with conditions and disapprove without all conditions.*

**APPLICANT REQUEST**

**Amend PUD to add 1.2 acres to the existing commercial PUD.**

Amend PUD

A request to amend the Hermitage Business Center Commercial Planned Unit Development Overlay District for a portion of property located at 4001 Lebanon Pike, at the corner of Lebanon Pike and Old Hickory Boulevard, zoned Shopping Center Regional (SCR), to add 1.2 acres to the boundary of the PUD.

**Existing Zoning**

Shopping Center Regional (SCR) is intended for high intensity retail, office, and consumer service uses for a regional market area.

Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provision of an adequate roadway system or essential utilities and services. This PUD plan In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provision of essential utilities and streets.

**CRITICAL PLANNING GOALS**

- Supports Infill Development

The area is served by adequate infrastructure. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure, such as substandard roads, water and sewer, because it does not burden Metro with the cost of upgrading or building new infrastructure.





## Metro Planning Commission Meeting of 04/23/2015

### **DONELSON-HERMITAGE COMMUNITY PLAN**

#### **Current Policy**

Suburban Community Center (T3 CC) policy is intended to enhance suburban community centers encouraging their redevelopment as intense mixed use areas that are compatible with the general character of suburban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T3 Suburban Community Centers are pedestrian friendly areas, generally located at prominent intersections. T3 Suburban Community Centers serve suburban communities within a 10 to 20 minute drive.

#### **Growth and Preservation Concept Map**

No change is proposed.

#### Consistent with Policy?

Yes. The proposed plan is providing for additional commercial uses within an existing community center. Sidewalks are being provided to improve pedestrian connectivity and create a more walkable environment.

### **PLAN DETAILS**

The plan is proposing to add approximately 1.2 acres of property to the existing Hermitage Business Center Commercial Planned Unit Development. The Hermitage Business Center PUD was approved by the Metro Council for a total of 179,364 square feet of commercial development. The proposed 1.2 acre addition is adjacent to the area that was noted as Area 2 on the original PUD plan. Area 2 was indicated to have 16,431 square feet of commercial development.

#### Site Plan

The plan proposes a total of 17,515 square feet of commercial space for the existing Area 2 and the added 1.2 acres. The uses allowed are retail, restaurant, and automobile repair.

Access to the development will be from an existing cross access easement to the south of the property. No new vehicular access points are proposed along Lebanon Road or Old Hickory Boulevard. Sidewalks are being provided along Lebanon Road and Old Hickory Boulevard. The access drive will be relocated and aligned with Bonnaspring Drive with the development of the adjacent property located in the Juarez Drive Planned Unit Development.

The proposed plan limits signage as follows: on-premise ground signage will be limited to either 1 multi-tenant monument style sign with no more than 15 feet in height OR 1 monument style sign per building (no more than 3) of no more than 8 feet in height. All ground signs shall be oriented toward Lebanon Pike.

Landscaping is provided throughout the development in the parking areas. An additional landscape area is also proposed along Old Hickory Boulevard.

### **ANALYSIS**

The proposed amendment to the PUD is consistent with the existing land use policy and supports infill development, which is a critical planning goal.



## **Metro Planning Commission Meeting of 04/23/2015**

### **FIRE MARSHAL'S OFFICE**

N/A

### **PUBLIC WORKS RECOMMENDATION**

#### **No exception taken**

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.

### **TRAFFIC AND PARKING RECOMMENDATION**

#### **Conditions if approved**

- PUD plan shall identify cross access to all PUD parcels from the proposed Watts property driveway as described in the traffic analysis dated Feb.20/2015.
- This Watts property access drive shall be closed and PUD access relocated to the adjacent driveway opposite Bonnaspring Dr. upon construction of driveway by the adjacent PUD 208-67-G parcel.
- Watts property PUD 128-78G-001 shall contribute a pro rata share of the cost to signalize the proposed driveway on the adjacent parcel of PUD 208-67-G when a traffic signal is approved by Metro traffic engineer.

### **STORMWATER RECOMMENDATION**

**Approved**

### **WATER SERVICES**

**Approved**

- Approved as a PUD Amendment only. Public water and sewer construction plans must be submitted and approved before Final Site Plan stage. The required 30% capacity fees must be paid prior to Final Site Plan approval.

### **STAFF RECOMMENDATION**

Staff recommends approval with conditions and disapproval without all conditions as the request is consistent with the land use policy for the area.

### **CONDITIONS**

1. On the corrected plan, provide a new cover sheet with correct PUD reference and correct all PUD references throughout.
2. On-premise ground signage will be limited to either 1 multi-tenant monument style sign of no more than 15 feet in height OR 1 monument sign per building (no more than 3) of no more than 8 feet in height. All ground signs shall be oriented toward Lebanon Pike.
3. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
4. If the PUD final site plan or final plat indicates that there is less acreage than what is shown on the approved preliminary plan, the final site plan shall be appropriately adjusted to show the actual total acreage, which may require that the total number of dwelling units or total floor area be reduced.



## **Metro Planning Commission Meeting of 04/23/2015**

5. Prior to any additional development applications for this property, and prior to or with final PUD plan application, the applicant shall provide the Planning Department with a corrected copy of the preliminary PUD plan.



# Metro Planning Commission Meeting of 04/23/2015



**2015S-036-001**  
 HAYNIE'S CENTRAL PARK, RESUB LOT 86  
 Map 094-05, Parcel(s) 013-014  
 05, East Nashville  
 06 (Peter Westerholm)





**Project No.** 2015S-036-001  
**Project Name** Haynie’s Central Park, Resub Lot 86  
**Council District** 6 – Westerholm  
**School District** 5 – Kim  
**Requested by** Dale & Associates, applicant; Sherry Phillips Hopwood and Robert & Betty O’Malley, owners.

**Deferrals** This case was deferred from the March 26, 2015, Planning Commission meeting.

**Staff Reviewer** Sajid  
**Staff Recommendation** Approve with conditions.

**APPLICANT REQUEST**  
**Create 4 lots.**

Preliminary SP  
 A request for final plat approval to create four lots on properties located at 1100 and 1104 Glenview Drive, at the southeast corner of Glenview Drive and South 11th Street, zoned Single-Family Residential (RS5) (0.55 acres).

**Existing Zoning**  
Single-Family Residential (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre. *RS5 would permit a maximum of 4 units.*

**CRITICAL PLANNING GOALS**

- Supports Infill Development

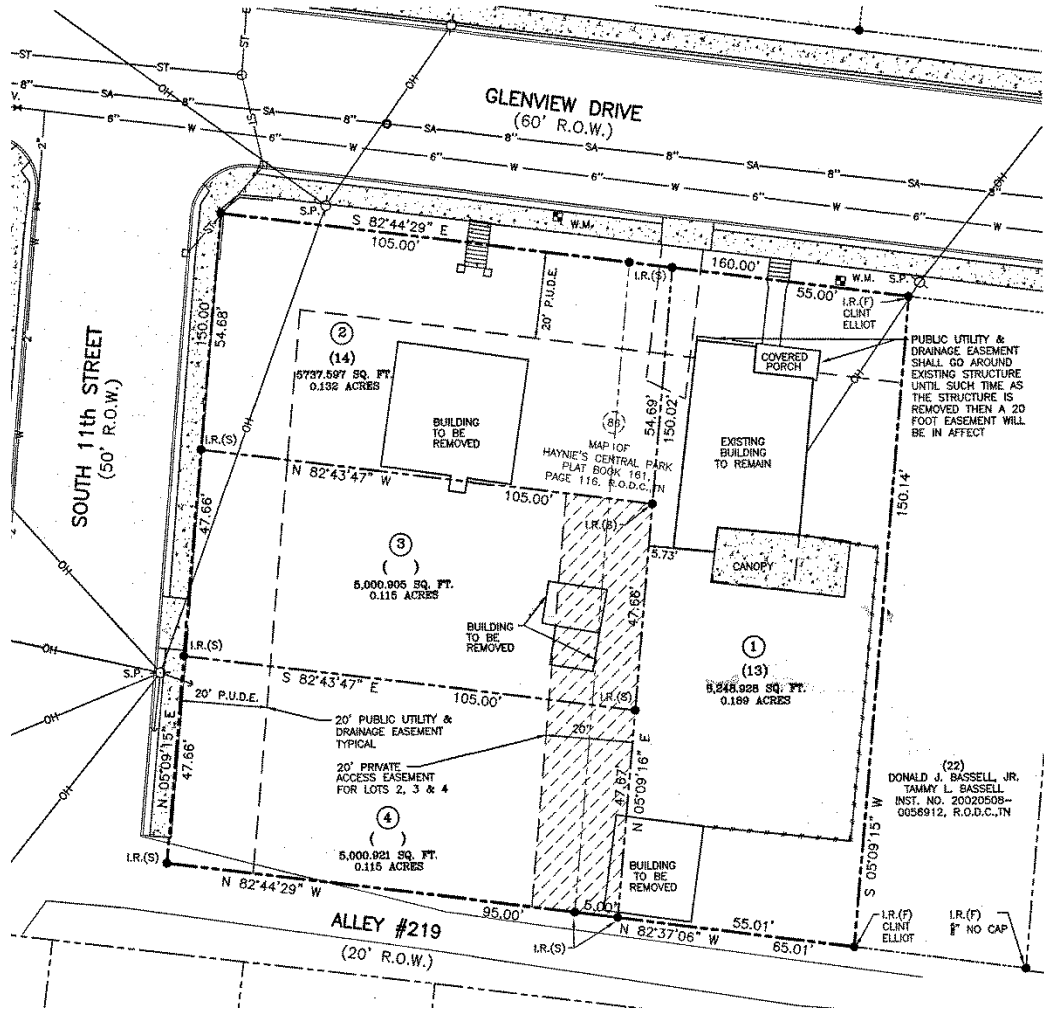
The proposed SP creates an opportunity for infill housing in an area that is served by existing infrastructure. Locating development in areas served by existing, adequate infrastructure does not burden Metro with the cost of upgrading or building new infrastructure.

**PLAN DETAILS**

The applicant requests final plat approval for a four lot subdivision of property located at the southeast corner of Glenview Drive and South 11<sup>th</sup> Street. While Lot 1 meets the infill compatibility analysis that is outlined in Section 3-5.2 of the Subdivision Regulations, there are no lots to which Lots 2-4 may be compared, requiring approval by the Planning Commission. The applicant requests approval under Section 3-5.2(f) of the Subdivision Regulations; under this section, the Planning Commission may grant approval of a subdivision that does not meet the compatibility criteria, if the subdivision can provide for harmonious development within the community.



# Metro Planning Commission Meeting of 04/23/2015



Proposed Subdivision



## Metro Planning Commission Meeting of 04/23/2015

[  
The existing lot is 23,988 SF acres is proposed to be subdivided into four lots with the following areas and street frontages:

- Lot 1: 8,248 Sq. Ft., (0.189 Acres), and 55 Ft. of frontage;
- Lot 2: 5,737 Sq. Ft., (0.132 Acres), and 54.68 Ft. of frontage.
- Lot 3: 5,000 Sq. Ft., (0.115 Acres), and 47.66 Ft. of frontage;
- Lot 4: 5,000 Sq. Ft., (0.115 Acres), and 47.66 Ft. of frontage.

The plan proposes one lot fronting Glenview Drive and three lots fronting South 11<sup>th</sup> Street. The existing house on Lot 1 is to remain while all other structures on the site shall be removed. The site is served by existing sidewalks. Alley access is available to the south of the site, and all proposed lots shall be limited to access either from the alley or from an access easement off of the alley.

### ANALYSIS

#### Lot Compatibility

Section 3-5.2 of the Subdivision Regulations outlines the criteria for reviewing infill subdivisions located within the Residential Medium policy area. Staff reviewed the final plat against the following criteria as required by the Subdivision Regulations:

#### Zoning Code

Both lots meet the minimum standards of the RS5 zoning district.

#### Street Frontage

All lots have frontage on a public street.

#### Density

Urban Neighborhood Maintenance land use policy supports density up to 20 dwelling units per acre. The proposed infill subdivision provides a density of 7.3 dwelling units per acres which falls within the range supported by policy.

#### Community Character

1. Lot frontage: The proposed lots must have frontage either equal to or greater than 70% of the average frontage of surrounding parcels or equal to or greater than the surrounding lot with the least amount of frontage, whichever is greater. In this case, Lot 1 must be equal to or greater than 35 ft., which is the smallest lot frontage of the surrounding lots. Lot 1 meets the lot frontage requirement. There are no available lots to which Lots 2-4 may be compared for frontage.

<b>Lot Frontage Analysis</b>	
Minimum Proposed	55'
70% of Average	29.4'
<b>Smallest Surrounding Parcel</b>	<b>35'</b>



## Metro Planning Commission Meeting of 04/23/2015

2. Lot size: The proposed lots must have lot area that is either equal to or greater than 70% of the lot size of the average size of surrounding parcels or equal to or larger than smallest surrounding lot, whichever is greater. In this case, the minimum lot area must be at least 5,256 square feet, which is the smallest lot area of the surrounding lots. Lot 1 meets the lot area requirement. There are no available lots to which Lots 2-4 may be compared for area.

<b>Lot Size Analysis</b>	
Minimum Proposed	8,248 SF
70% of Average	4,410 SF
<b>Smallest Surrounding Parcel</b>	<b>5,256 SF</b>

3. Street setback: The existing house on Lot 1 is setback about 17 feet from Glenview Drive. Per the Zoning Code, the minimum street setback for Lots 2-4 is proposed to be 20' since there are no adjacent houses to determine a contextual street setback. Staff recommends that Lot 2 include a platted side setback on Glenview Drive of 20 feet so that the side façade of the unit developed on that lot is no closer to the street than the existing house to remain on Lot 1.
4. Lot orientation: Lot 1 is oriented toward Glenview Drive, and Lots 2-4 are oriented toward South 11<sup>th</sup> Street. The proposed lot orientation is consistent with the surrounding area. While there are not existing lots on the same block face oriented toward South 11<sup>th</sup> Street, the proposed layout is similar to the existing lots across the street.

### Agency Review

All review agencies recommend approval.

### Harmony of Development

Lot 1 of the proposed subdivision meets the Community Character criteria. However, there are no lots to which Lots 2-4 may be compared. Therefore, the Planning Commission must determine if the subdivision provides for the harmonious development of the community. In this case, the applicant has proposed several conditions to attempt to meet this provision: limiting the access to alley access and limiting the building height to 2 stories in 35 ft.

Staff finds that the conditions proposed by the applicant overcome the incompatibility of the proposed lots with regard to lot frontage and lot area to provide for the harmonious development of the community with the condition that Lot 2 include a platted side setback on Glenview Drive of 20 feet so that the side façade of the unit developed on that lot is no closer to the street than the existing house to remain on Lot 1.

### **FIRE MARSHAL RECOMMENDATION**

**N/A**

### **PUBLIC WORKS RECOMMENDATION**

**No exception taken**

### **STORMWATER RECOMMENDATION**

**Approved**



## Metro Planning Commission Meeting of 04/23/2015

### **WATER SERVICES RECOMMENDATION**

#### **Approve with conditions**

- Approval is contingent on construction and completion of Metro Water Project # 15-SL-39. A bond value of \$17,000 has been assigned to 15-SL-39.

### **STAFF RECOMMENDATION**

Staff finds that the proposed subdivision can provide for the harmonious development of the community and recommends approval with conditions.

### **CONDITIONS (if approved)**

1. The final plat shall include the following standard on the plan: "Raised foundations of 18-36" are required for residential buildings."
2. Prior to recordation, the buildings to be removed shall be demolished and removed from the plat.
3. Prior to recordation, the existing curb cuts and driveways on Glenview Drive and South 11<sup>th</sup> Street shall be removed and replaced with sidewalk and curb.
4. Lot 2 shall include a platted side setback of 20 feet along the Glenview Drive frontage.
5. Prior to recordation, Metro Water Project #15-SL-39 shall either be bonded or completed and accepted.



**SEE NEXT PAGE**



## **RECOMMENDATIONS TO METRO COUNCIL**

- **Specific Plans**
- **Planned Unit Developments**



**NO SKETCH**





**Project No.**

**Text Amendment  
2014Z-006TX-002**

**Project Name**

**Posting of Public Hearing Signs and  
Newspaper Notice**

**Council Bill  
Council District  
School District  
Requested by**

BL2015-1100  
Countywide  
Countywide  
Codes Department

**Staff Reviewer  
Staff Recommendation**

Logan  
*Approve.*

**APPLICANT REQUEST**

**Modify public hearing sign posting requirements and delete newspaper notice requirements in the Zoning Code.**

**TEXT AMENDMENT**

A request to amend Section 17.40.730 of the Metropolitan Zoning Code to modify the general requirements of public notice signs and to delete Section 17.40.710 pertaining to notice by newspaper in its entirety.

**PURPOSE**

Prior to 2014, there was a requirement that the appropriate department of the Metropolitan Government post signs for public hearings required by the Zoning Code. The Codes Department and Planning Department posted all of the public hearing signs for Council public hearings and the Codes Department posted signs for Board of Zoning Appeal hearings. In 2014, the Planning Department initiated a text amendment to remove the requirement that Council public hearing signs to be posted by the appropriate department of the Metropolitan Government.

The removal of that requirement allowed a process similar to the Metro Planning Commission public hearing signs, which are posted by applicants and a certificate is returned to the Planning Department verifying that the signs have been posted. In 2014, the Codes Department opted to continue posting signs for Board of Zoning Appeal hearings.

This text amendment would revise the Board of Zoning Appeal hearing sign posting process to require applicants to post signs, with the exception of Councilmembers, who would have the choice of posting the signs themselves or returning the signs to the Codes Department.

This text amendment also removes the requirement for the Codes Department to advertise hearings in the newspaper. This method of advertisement is outdated and costs the Codes Department a significant amount of money. Metro posts agendas for all Board and Commissions and allows individuals to sign up for specific agendas to be emailed or texted to them.

**STAFF RECOMMENDATION**

Staff recommends approval.



## Metro Planning Commission Meeting of 04/23/2015

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### ORDINANCE NO. BL2015-1100

**An Ordinance to amend Section 17.40.730 of the Metropolitan Zoning Code to modify the general requirements of public notice signs and to delete Section 17.40.710 pertaining to notice by newspaper in its entirety. (Proposal No. 2014Z-006TX-002).**

NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:

Section 1. That Section 17.40.730 of the Code of The Metropolitan Government of Nashville and Davidson County is hereby amended by deleting subsection A in its entirety and substituting with the following new subsection A.

“General Requirements. Public notice signs shall be posted on any property subject to the public hearing provisions of this title. Public notice signs shall be installed by the owner or owner’s representative of the property for which the public hearing is required, unless the applicant is a member of the metropolitan council or a department of the metropolitan government. Where the applicant is a member of the metropolitan council or a department of the metropolitan government, then the applicant or the appropriate department of the metropolitan government will be responsible for posting applicable public notice signs.

Section 2. That Section 17.40.710 of the Code of The Metropolitan Government of Nashville and Davidson County is hereby amended by deleting this section in its entirety.

Section 3. This Ordinance shall take effect immediately after its passage and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.

Sponsored by: Walter Hunt



**SEE NEXT PAGE**



**NO SKETCH**



<b>Project No.</b>	<b>Text Amendment</b>
	<b>2015Z-007TX-001</b>
<b>Project Name</b>	<b>Sex Clubs</b>
<b>Council Bill</b>	BL2015-1099
<b>Council District</b>	Countywide
<b>School District</b>	Countywide
<b>Requested by</b>	Councilmember Karen Bennett Councilmember Steve Glover Councilmember Josh Stites
<b>Staff Reviewer</b>	Milligan
<b>Staff Recommendation</b>	<i>Approve substitute ordinance.</i>

**APPLICANT REQUEST**

**Define Sex Club and classify as a use permitted with conditions in certain districts.**

Text Amendment

An Ordinance amending Title 17 of the Metropolitan Code, Zoning Regulations, pertaining to sex clubs.

**ANALYSIS**

Currently, the Metropolitan Zoning Ordinance defines a club as follows:

“Club” means a facility which offers social, educational, cultural or other similar activities that are only available to members and their guests.

The definition of a club is a very broad definition that could include clubs of varying sizes and intensities. Clubs could range in size from having a dozen members to hundreds of members. Also, operating hours could vary widely depending on the type of club. The proposed text amendment would add a new definition for sex club and remove these types of clubs from the general club category. Additionally, the proposed text amendment would define zoning districts where allowed and distance requirements for the newly defined sex clubs.

Under the proposed text amendment, a sex club is defined as follows:

“sex club” means any establishment, business, or club that allows members to engage in specified sexual activities or the exposure of specified anatomical areas, and which meets one or more of the following criteria:

- a. The club does not provide membership to persons under twenty-one (21) years of age;
- b. The club’s members do not vote on the admittance of new members;
- c. The club accepts applications for membership without a current member’s written recommendation;
- d. The club grants membership on the same day a membership application is filed;
- e. The club contains rooms for couples and other adult-themed rooms for members.



## Metro Planning Commission Meeting of 04/23/2015

The draft text amendment would allow sex clubs as a use permitted with conditions in Industrial Warehousing/Distribution (IWD), Industrial Restrictive (IR), and Industrial General (IG). The proposed condition for sex clubs is as follows:

No sex club shall be located within one thousand feet (measured property line to property line of a residence, community education facility, day care, park, or religious institution.

### **PROPOSED SUBSTITUTE ORDINANCE**

The proposed substitute ordinance removes the operational standards from the definition of sex clubs. The use is allowed as a use permitted with conditions in Industrial Warehousing/Distribution (IWD), Industrial Restrictive (IR), and Industrial General (IG). In addition to the distance requirement in the original bill, operational criteria are added as conditions. These include specifications on age of member and membership admission policies.

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### **STAFF RECOMMENDATION**

Staff recommends approval of the substitute ordinance. The draft legislation allows for the location of sex clubs while protecting residential uses, schools, day cares, parks, and religious facilities from potential negative impacts associated with this type of adult use while also insuring proper membership admission standards.

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### **ORDINANCE NO. BL2015-1099**

**An ordinance amending Title 17 of the Metropolitan Code, Zoning Regulations, pertaining to sex clubs, all of which is more particularly described herein (Proposal No. 2015Z-007TX-001)**

WHEREAS, the Metropolitan Government has a legitimate governmental interest in protecting the health, safety, and welfare of Nashville's residents; and

WHEREAS, sex clubs have been shown to have a negative impact on the health, safety, and welfare of communities; and

WHEREAS, the Metropolitan Council deems it to be necessary for the protection of the public health, safety, and welfare that private sex clubs be restricted in the areas where they can locate and operate.

NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:

Section 1. That Section 17.04.060, Definitions of General Terms, is hereby amended by adding the following definition for "sex club":

"sex club" means any establishment, business, or club that allows members to engage in specified sexual activities or the exposure of specified anatomical areas, and which meets one or more of the



## Metro Planning Commission Meeting of 04/23/2015

following criteria:

- a. The club does not provide membership to persons under twenty-one (21) years of age;
- b. The club's members do not vote on the admittance of new members;
- c. The club accepts applications for membership without a current member's written recommendation;
- d. The club grants membership on the same day a membership application is filed;
- e. The club contains rooms for couples and other adult-themed rooms for members.

Section 2. That Section 17.08.030, District Land Use Tables, is hereby amended by adding "sex club" as a use permitted with conditions (PC) in the IWD, IR, and IG zoning districts.

Section 3. That Section 17.16.090, Industrial Uses, is hereby amended by adding the following new subsection G.:

G. Sex clubs. No sex club shall be located within one thousand feet (measured property line to property line) of a residence, community education facility, day care, park, or religious institution.

Section 4. Be it further enacted that this Ordinance shall take effect immediately after its passage and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.

Sponsored by: Karen Bennett, Steve Glover, Josh Stites

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### **SUBSTITUTE ORDINANCE NO. BL2015-1099**

**An Ordinance amending Title 17 of the Metropolitan Code, Zoning Regulations, pertaining to sex clubs, all of which is more particularly described herein (Proposal No. 2015Z-007TX-001).**

WHEREAS, the Metropolitan Government has a legitimate governmental interest in protecting the health, safety, and welfare of Nashville's residents; and

WHEREAS, sex clubs have been shown to have a negative impact on the health, safety, and welfare of communities; and

WHEREAS, the Metropolitan Council deems it to be necessary for the protection of the public health, safety, and welfare that sex clubs be restricted in the areas where they can locate and operate.

NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:

Section 1. That Section 17.04.060, Definitions of General Terms, is hereby amended by adding the following definition for "sex club":



## Metro Planning Commission Meeting of 04/23/2015

“Sex club” means any establishment, business, or club that allows members to engage in specified sexual activities or the exposure of specified anatomical areas, and which contains either rooms for couples or other adult-themed rooms or both for the use of members.

Section 2. That Section 17.08.030, District Land Use Tables, is hereby amended by adding “sex club” as a use permitted with conditions (PC) in the IWD, IR, and IG zoning districts.

Section 3. That Section 17.16.090, Industrial Uses, is hereby amended by adding the following new subsection G.:

### G. Sex clubs.

1. No sex club shall be located within one thousand feet (measured property line to property line) of a residence, community education facility, day care, park, or religious institution.
2. Sex clubs shall be prohibited from all of the following:
  - a. Admitting members that are younger than twenty-one (21) years of age;
  - b. Admitting any new member without the affirmative vote of a majority of the other members;
  - c. Accepting applications for membership without a current member’s written recommendation; and
  - d. Granting membership within twenty-four (24) hours from the time a membership application is filed.

Section 4. Be it further enacted that this Ordinance shall take effect immediately after its passage and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.





**SEE NEXT PAGE**



# Metro Planning Commission Meeting of 04/23/2015



**2015SP-024-001**  
620 S. 13TH STREET  
Map 094-01, Parcel(s) 075  
05, East Nashville  
06 (Peter Westerholm)



<b>Project No.</b>	<b>2015SP-024-001</b>
<b>Project Name</b>	<b>620 S. 13<sup>th</sup> Street</b>
<b>Council District</b>	6 – Westerholm
<b>School District</b>	5 – Kim
<b>Requested by</b>	James L. Smith, Jr., applicant and owner.
<b>Staff Reviewer</b>	Sajid
<b>Staff Recommendation</b>	<i>Approve with conditions and disapprove without all conditions.</i>

**APPLICANT REQUEST**

**Preliminary SP to permit up to 2 attached residential units.**

Preliminary SP

A request to rezone from Single-Family Residential (RS5) to Specific Plan-Residential (SP-R) for property located at 620 S. 13th Street, at the northwest corner of S. 13th Street and Sevier Street (0.18 acres), to permit an attached two-family structure.

**Existing Zoning**

Single-Family Residential (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre. *RS5 would permit a maximum of 1 unit.*

**Proposed Zoning**

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

**CRITICAL PLANNING GOALS**

- Supports Infill Development

The proposed SP creates an opportunity for infill housing in an area that is served by existing infrastructure. Locating development in areas served by existing, adequate infrastructure does not burden Metro with the cost of upgrading or building new infrastructure.

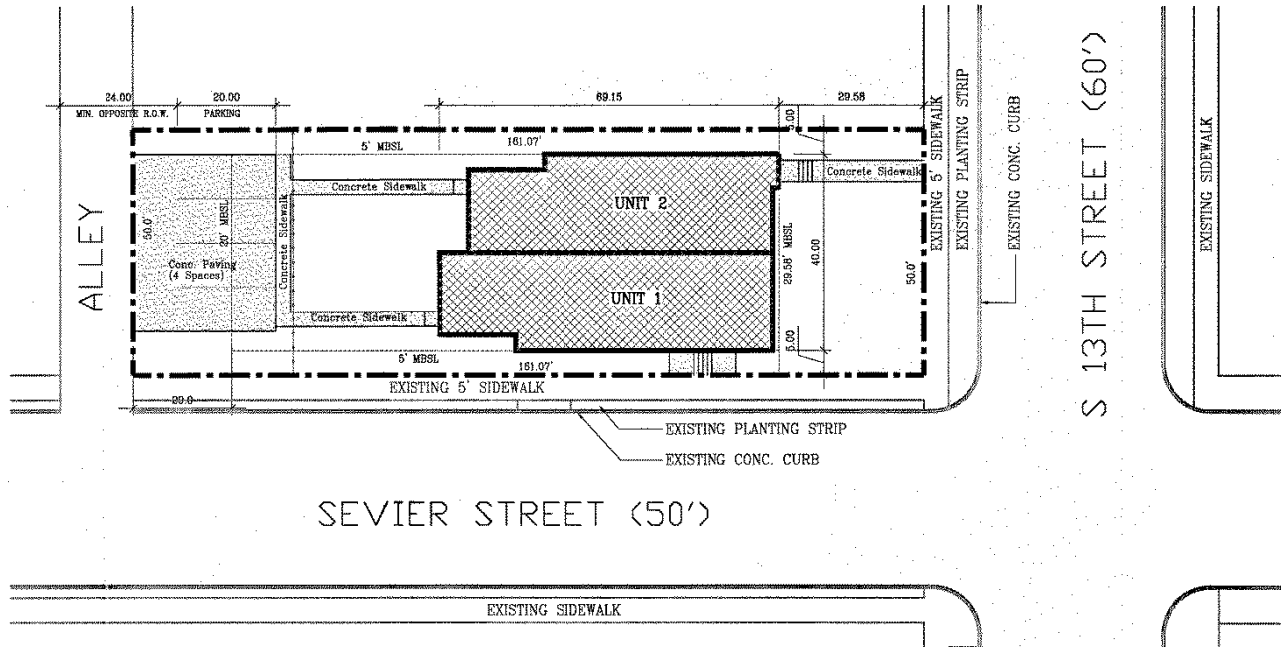
**EAST NASHVILLE COMMUNITY PLAN**

**Existing Policy**

Urban Neighborhood Maintenance (T4 NM) policy is intended to preserve the general character of urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use and the public realm. Where not present, enhancements are made to improve pedestrian, bicycle and vehicular connectivity.



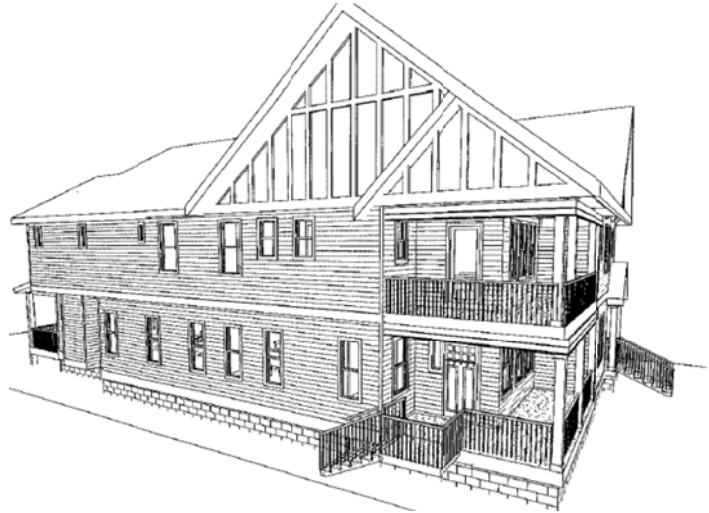
# Metro Planning Commission Meeting of 04/23/2015



## Proposed Site Plan



South 13<sup>th</sup> Street Elevation



Sevier Street Elevation



## Metro Planning Commission Meeting of 04/23/2015

### **Growth and Conservation Concept Map**

No change proposed.

#### Consistent with Policy?

Yes, the proposed SP is consistent with the Urban Neighborhood Maintenance policy which is intended to preserve the character of the existing neighborhood. The SP proposes an attached two-family structure in an area that is characterized by one and two family residential uses. Also, the rezoning request is a site plan based district that encourages flexibility in design so that the result is well suited to the subject property and the neighborhood.

### **PLAN DETAILS**

The site is located at the northwest corner of the intersection of South 13<sup>th</sup> Street and Sevier Street. Surrounding zoning includes RS5, and the area is characterized by a mixture of one and two family residential uses. Access to the property is from the existing improved alley that abuts the site to the west.

#### Site Plan

The plan proposes an attached duplex unit that incorporates front doors on both South 13<sup>th</sup> Street and Sevier Street, which creates a presence along both street frontages. The SP also maintains the front setback along South 13<sup>th</sup> Street. The maximum height for the duplex is 2 stories in 35' to the roofline. Architectural elevations have been included with the SP and show a two-story structure with lap siding as the primary material.

Access to the units is restricted to the existing improved alley located to the west of the site, and surface parking is located behind the structure. Planning staff recommends that the SP incorporate landscaping to screen the proposed parking from Sevier Street. Existing sidewalks are available along both street frontages.

### **ANALYSIS**

The proposed SP is consistent with Urban Neighborhood Maintenance land use policy and meets a critical planning goal. Therefore, staff recommends approval with conditions and disapproval without all conditions.

### **FIRE MARSHAL RECOMMENDATION**

#### **Approved with conditions**

- Water flow requirements for single-family homes that do not exceed 3600 sq. ft. is a minimum of 1000 gpm @ 20 psi. Fire Code issues for the structures will be addressed at permit application review.

### **STORMWATER RECOMMENDATION**

#### **Approve**

### **TRAFFIC & PARKING RECOMMENDATION**

#### **No exception taken**



# Metro Planning Commission Meeting of 04/23/2015

## WATER SERVICES RECOMMENDATION

### Approve

- Approved as a Preliminary SP only. The required capacity fees must be paid prior to Final SP approval.

## PUBLIC WORKS RECOMMENDATION

### No exception taken

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.

Maximum Uses in Existing Zoning District: **RS5**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.18	8.71 D	1 U	10	1	2

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (210)	0.18	-	2 U	20	2	3

Traffic changes between maximum: **RS5** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 1 U	+10	+1	+1

## METRO SCHOOL BOARD REPORT

Projected student generation existing RS5 district: 0 Elementary 0 Middle 0 High

Projected student generation proposed SP-R district: 0 Elementary 0 Middle 0 High

The proposed SP-R zoning district would generate one more students than what is typically generated under the existing RS5 zoning district. Students would attend Kirkpatrick Elementary School, Bailey Middle School, and Stratford High School. Kirkpatrick Elementary School has been identified as over capacity. There is capacity within the cluster for additional elementary school students. This information is based upon data from the school board last updated October 2014.

## STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.

## CONDITIONS

1. Uses within the SP shall be limited to 2 attached residential units.
2. The final site plan shall incorporate landscaping to screen parking visible from Sevier Street.

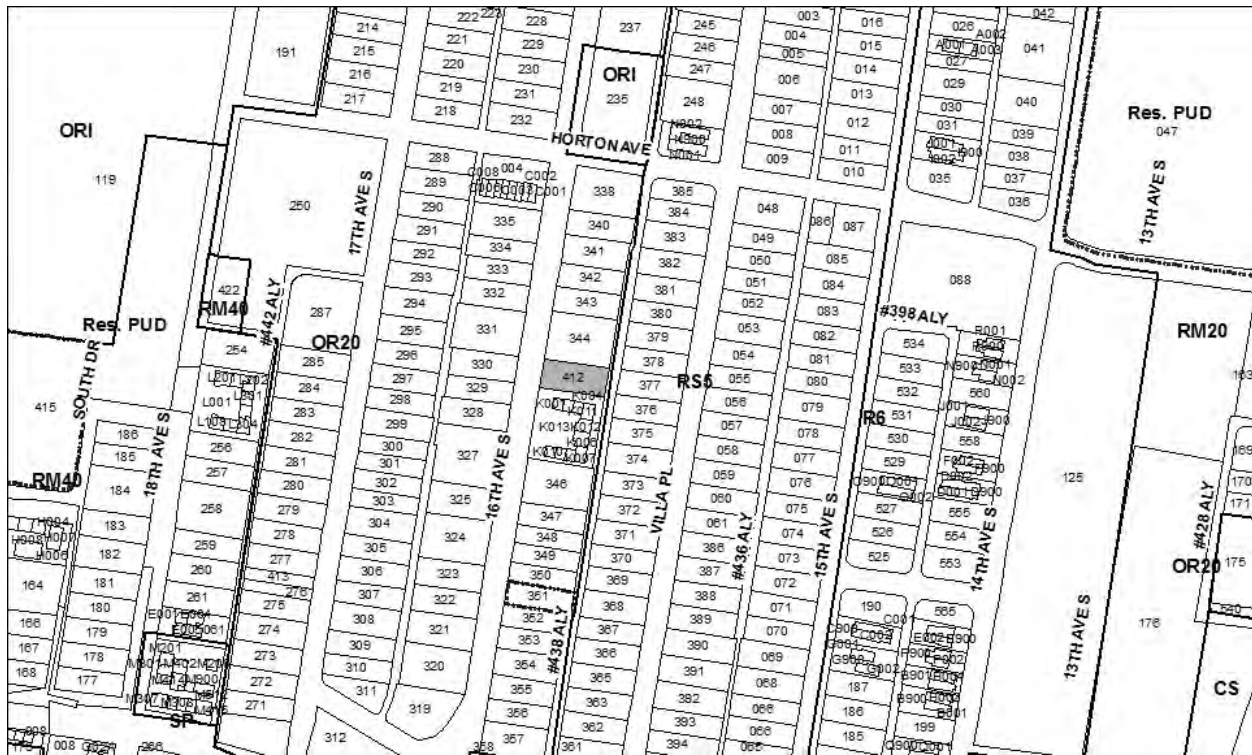


## Metro Planning Commission Meeting of 04/23/2015

3. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the R6 zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
4. The final site plan shall include detailed architectural elevations consistent with those include in the SP plan showing raised foundations of 18-36” for residential buildings.
5. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
6. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
7. The requirements of the Metro Fire Marshal’s Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



# Metro Planning Commission Meeting of 04/23/2015



**2015SP-035-001**  
TOWERY - 16<sup>TH</sup> AVENUE SOUTH SP  
Map 104-08, Parcel(s) 412  
10, Green Hills - Midtown  
17 (Sandra Moore)





**Project No.** **Zone Change 2015SP-035-001**  
**Project Name** **Towery-16<sup>th</sup> Avenue South SP**  
**Council District** 17 – Moore  
**School District** 5 – Kim  
**Requested by** Convent Place Partners, LLC, applicant and owner.

**Staff Reviewer** Swaggart  
**Staff Recommendation** *Defer or disapprove in accordance with Metro Planning Commission policy directive on Music Row rezoning.*

**APPLICANT REQUEST**

**Preliminary SP to permit an office building.**

Preliminary SP

A request to rezone from Office/Residential (OR20) to Specific Plan-Office (SP-O) zoning for property located at 1518 16th Avenue South, approximately 520 feet south of Horton Avenue, and within the South Music Row Neighborhood Conservation Overlay District, (0.22 acres), to permit an office building.

**Existing Zoning**

Office/Residential (OR20) is intended for office and/or multi-family residential units at up to 20 dwelling units per acre. *OR20 would permit a maximum of four residential units and up to 7,666 square feet of office space.*

**Proposed Zoning**

Specific Plan-Office (SP-O) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes office uses.

**CRITICAL PLANNING GOALS**

N/A

**GREENHILLS-MIDTOWN COMMUNITY PLAN**

District Office Concentration (D OC) policy is intended to preserve, enhance, and create Districts where office use is predominant and where opportunities for the addition of complementary uses are present. The development and redevelopment of such Districts occurs in a manner that is complementary of the varying character of surrounding communities as characterized by development patterns, building form, land use, and associated public realm.

**Growth and Preservation Concept Map**

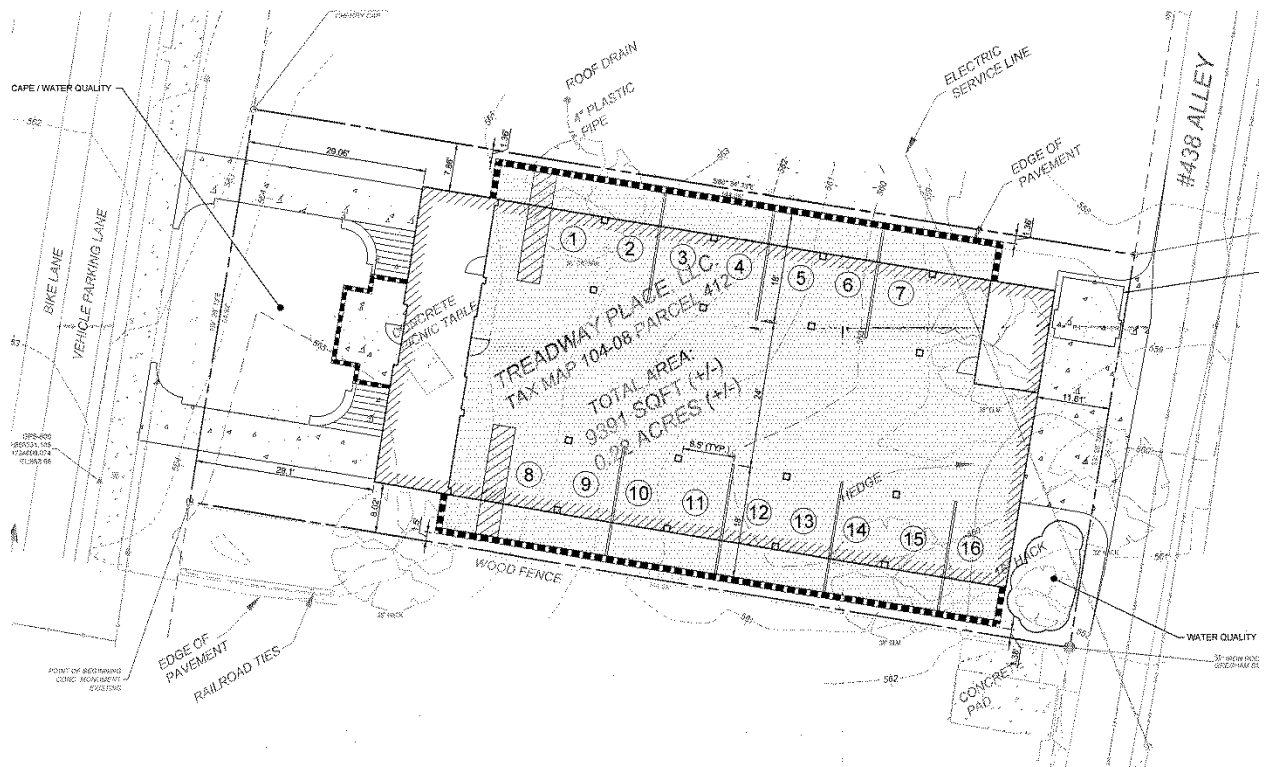
No change is proposed.

Consistent with Policy?

Yes. The proposed SP would permit an office use consistent with the policy. The plan also proposes sidewalks consistent with the Major and Collector Street Plan which fosters a more walkable street which is encouraged by the policy.



# Metro Planning Commission Meeting of 04/23/2015



**Proposed Site Plan**



**Elevation**



## Metro Planning Commission Meeting of 04/23/2015

### PLAN DETAILS

The site is approximately 0.22 acres in size and is located on the east side of 16<sup>th</sup> Avenue South in Music Row. The site is within the South Music Row Neighborhood Conservation Overlay. The property is currently vacant and aerial photography indicates that the property has been vacant since at least 2000.

The proposed SP is intended to permit slight variations from what is permitted under the current OR20 zoning district. The plan permits four deviations which are as follows:

1. Increased Floor Area. The proposed SP calls for 9,391 square feet of floor area which is approximately 1,725 more floor area than what is permitted under the current OR20 zoning district.
2. Increased Impervious Surfaces. The proposed SP permits an Impervious Surface Ratio (ISR) of 0.74. The current OR20 zoning district permits a maximum ISR of 0.70.
3. Decreased rear yard setback. The proposed SP would permit an approximately 11 foot setback at the rear of the property where the OR20 requires a minimum 20 foot rear yard setback. It is important to note that the Metro Historic Zoning Commission has the ability to permit adjustments to zoning setbacks, so the SP is not absolutely necessary for this deviation in standards.
4. Reduced perimeter parking landscape strip. The proposed SP does not require a perimeter landscape strip for the parking area. The existing zoning requires a minimum 2.5 foot strip.

### Site Plan

The plan calls for a 9,391 square foot, two-story building. The front setback along 16<sup>th</sup> Avenue South is approximately 30 feet. Vehicular parking is located at the rear of the building which is accessed by the alley along the rear property line. The plan calls for a 2.5 foot ROW dedication along the alley. The plan also calls for a 1.5 foot ROW dedication along 16<sup>th</sup> Avenue S. The plan calls for an eight foot wide sidewalk and four foot wide planting strip. ROW dedications would not be required under the existing OR20 district.

### ANALYSIS

Staff recommends disapproval or deferral based on the Planning Commission's February 12, 2015, policy directive to defer or disapprove any rezoning request on Music Row pending the development of an overall design plan for the future of Music Row after NashvilleNext. Any deferral would need to be until the study requested by the Commission has been completed so that staff and the Commission can evaluate the proposal under the study.

While staff is recommending disapproval, staff finds that this request could be differentiated from previous rezoning request in Music Row which led to the Commission's February 12, 2015, policy directive to defer or disapprove any rezoning in Music Row. The request is different in several ways:

1. The property is currently vacant and has been vacant for numerous years. Previous proposals were on developed sites, which required the demolition of structures considered important to the essence of Music Row. No structure will be demolished with this proposal.
2. The request includes only one lot and the scale of the proposal is similar to the scale of surrounding development. Previous proposals included numerous properties and called for



## Metro Planning Commission Meeting of 04/23/2015

structures that were not necessarily consistent with the surrounding development pattern in terms of scale.

3. The current zoning would permit development on the property. The proposed SP is intended to only permit minor deviations from the current OR20 zoning district. It is also important to note that if the property were to develop under OR20, the Metro Historic Zoning Commission could authorize different setbacks than what is required by zoning.
4. The site is within the South Music Row Conservation Overlay District and the design has been approved by the Metro Historic Zoning Commission (MHZC). MHZC staff is also recommending approval of the proposed SP. Previous rezoning requests have not been within an overlay and did not require the design to be approved by the MHZC.

### **FIRE MARSHAL'S OFFICE**

N/A

### **METRO HISTORIC ZONING COMMISSION**

#### **Approved**

The MHZC approved the plan including elevations on March 18, 2015.

### **PUBLIC WORKS RECOMMENDATION**

#### **Approve with conditions**

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Indicate widening alley to ½ MPW standard ST-263 and dedicate 2.5' of ROW along the alley.
- A TIS may be required prior to final SP.

### **STORMWATER RECOMMENDATION**

#### **Ignore**

### **WATER SERVICES**

#### **Approved**

### **STAFF RECOMMENDATION**

Staff recommends that the proposed SP be deferred or disapproved. If approved, then staff recommends that it be approved with conditions and disapproved without all conditions.

### **CONDITIONS (if approved)**

1. Uses shall be limited to those uses permitted under the OR20 zoning district with the exception that no residential uses shall be permitted.
2. A Traffic Impact Study (TIS) may be required prior to the approval of any final site plan.
3. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the OR20-A zoning district as of the date of the applicable request or application.



## Metro Planning Commission Meeting of 04/23/2015

4. No vehicular access shall be permitted from 16<sup>th</sup> Avenue.
5. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
6. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon approval from the Metro Historic Zoning Commission or its designee. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
7. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



# Metro Planning Commission Meeting of 04/23/2015



**2015SP-041-001**  
MAXON COTTAGES  
Map 091-10, Parcel(s) 250, 461  
07, West Nashville  
20 (Buddy Baker)



<b>Project No.</b>	<b>2015SP-041-001</b>
<b>Project Name</b>	<b>Maxon Cottages</b>
<b>Council District</b>	20 – Baker
<b>School District</b>	9 – Frogge
<b>Requested by</b>	Dale and Associates, applicant; GMAT Holdings, G.P., owner.
<b>Staff Reviewer</b>	Sajid
<b>Staff Recommendation</b>	<i>Approve with conditions and disapprove without all conditions.</i>

**APPLICANT REQUEST**

**Preliminary SP to permit up to 4 detached residential units.**

Preliminary SP

A request to rezone from One and Two-Family Residential (R8) to Specific Plan-Residential (SP-R) zoning for property located at 511 Snyder Avenue and 5800 Maxon Avenue, at the northwest corner of Maxon Avenue and Snyder Avenue, (0.34 acres), to permit up to four detached residential units.

**Existing Zoning**

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 5.79 dwelling units per acre including 25 percent duplex lots. *R8 would permit a maximum of 1 lot with 1 duplex lot for a total of 2 units.*

**Proposed Zoning**

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

**CRITICAL PLANNING GOALS**

- Supports Infill Development

The proposed SP creates an opportunity for infill housing in an area that is served by existing infrastructure. Locating development in areas served by existing, adequate infrastructure does not burden Metro with the cost of upgrading or building new infrastructure.

**WEST NASHVILLE COMMUNITY PLAN**

**Existing Policy**

Urban Neighborhood Maintenance (T4 NM) policy is intended to preserve the general character of urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use and the public realm. Where not present, enhancements are made to improve pedestrian, bicycle and vehicular connectivity.







## Metro Planning Commission Meeting of 04/23/2015

### Growth and Conservation Concept Map

No change proposed.

#### Consistent with Policy?

Yes. The proposed SP is consistent with the Urban Neighborhood Maintenance policy, which is intended to preserve the character of the existing neighborhood. The SP proposes detached units, which reflects the predominant development pattern in the area. Also, the rezoning request is a site plan based district that encourages flexibility in design so that the result is well suited to the subject property and the neighborhood.

### PLAN DETAILS

The site is located at the northwest corner of the intersection of Maxon Avenue and Snyder Avenue. Surrounding zoning includes R8, and the area is characterized by a mixture of one and two family residential uses. Access to the property is from the existing improved alley that abuts the site to the west.

#### Site Plan

The plan proposes four detached residential units with two units fronting Maxon Avenue and two units fronting Snyder Avenue. Unit 3 includes a side façade oriented toward Maxon Avenue and incorporates a wraparound porch so that the building addresses both street frontages. Architectural images have not been included with the preliminary SP. The SP, however, includes notes that address design considerations for the SP. The design conditions address doorway placement, glazing, window orientation and porches and will be subject to review for appropriateness during the final site plan review. Also, EIFS and vinyl siding are not permitted as building materials. The maximum height for all units is 2 stories in 35' to the roofline. The plan incorporates a Type B-5 landscape buffer yard between the site and the existing single-family residential to the north that includes an opaque fence.

Parking for the units is provided in garages and surface parking and includes several spaces for guest parking. The SP proposes to dedicate right-of-way along Maxon Avenue and the alley and to install sidewalks along both Maxon Avenue and Snyder Avenue.

### ANALYSIS

The proposed SP is consistent with Urban Neighborhood Maintenance land use policy and meets a critical planning goal. Therefore, staff recommends approval with conditions and disapproval without all conditions.

### FIRE MARSHAL RECOMMENDATION

#### **Approved with conditions**

- Fire Code issues for the structures will be addressed at permit application review.

### STORMWATER RECOMMENDATION

#### **Conditional if approved**

- Storm improvements may be required on Snyder and Maxon (to be determined during construction drawing review).



## Metro Planning Commission Meeting of 04/23/2015

### TRAFFIC & PARKING RECOMMENDATION

**No exception taken**

### WATER SERVICES RECOMMENDATION

**Approve**

- Approved as a Preliminary SP only. Public sewer construction plans must be submitted and approved prior to Final SP stage. The required capacity fees must be paid prior to Final SP approval.

### PUBLIC WORKS RECOMMENDATION

**Approve with conditions**

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Prior to building permit approval ROW dedications must be recorded.

Maximum Uses in Existing Zoning District: **R8**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (210)	0.36	5.44 D	2 U *	20	2	3

\*Based on one two-family lot.

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (210)	0.36	-	4 U	39	3	5

Traffic changes between maximum: **R8** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 2 U	+19	+1	+2

### METRO SCHOOL BOARD REPORT

**Projected student generation existing R8 district: 0 Elementary 0 Middle 0 High**

**Projected student generation proposed SP-R district: 1 Elementary 0 Middle 0 High**

The proposed SP-R zoning district would generate one more students than what is typically generated under the existing R6 zoning district. Students would attend Cockrill Elementary School, McKissack Middle School, and Pearl-Cohn High School. Cockrill Elementary School and Pearl-Cohn High School have been identified as over capacity. There is capacity within the cluster for additional elementary school students. There is capacity for high school students in an adjacent cluster. This information is based upon data from the school board last updated October 2014.



## Metro Planning Commission Meeting of 04/23/2015

### STAFF RECOMMENDATION

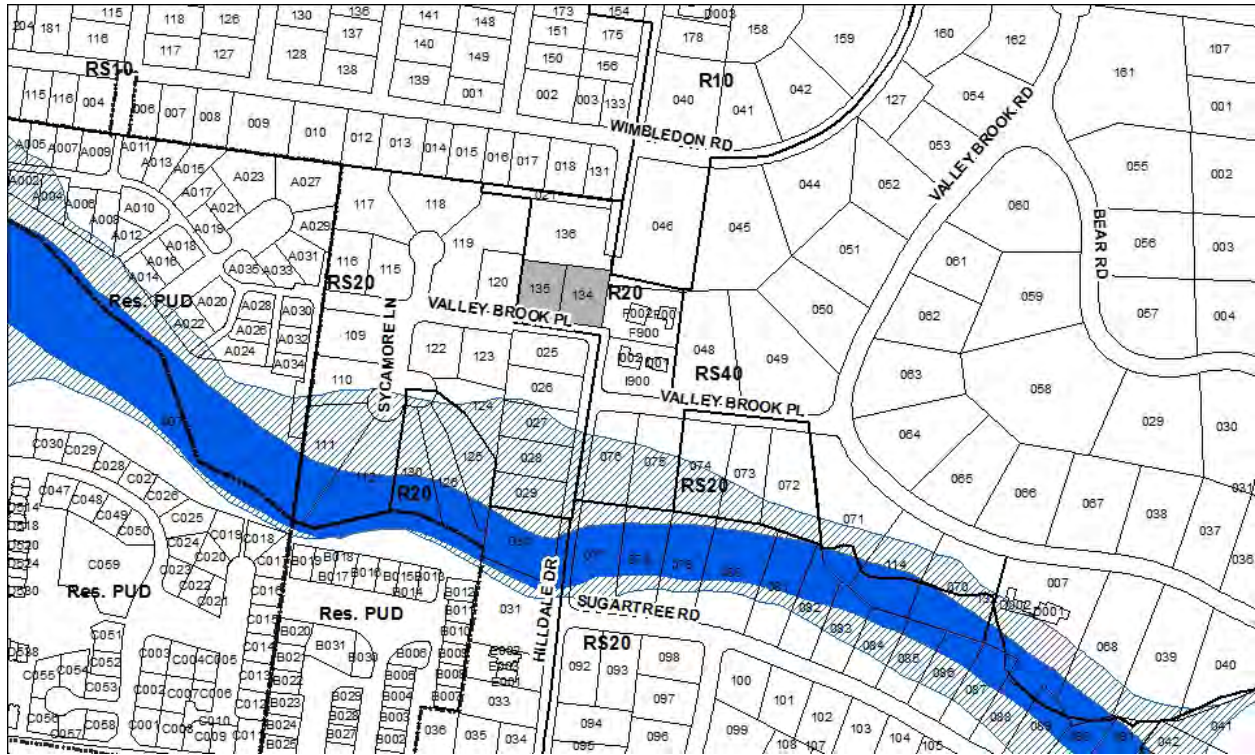
Staff recommends approval with conditions and disapproval without all conditions.

### CONDITIONS

1. Uses within the SP shall be limited to four detached residential units.
2. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM15-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
3. The final site plan shall include architectural elevations that incorporate the required design considerations and show raised foundations of 18-36" for residential buildings. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
4. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
5. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



# Metro Planning Commission Meeting of 04/23/2015



**2015SP-042-001**

**VALLEY BROOK PLACE**

Map 117-09, Parcel(s) 134-135

10, Green Hills - Midtown

25 (Sean McGuire)



<b>Project No.</b>	<b>2015SP-042-001</b>
<b>Project Name</b>	<b>Valley Brook Place</b>
<b>Council District</b>	25 - McGuire
<b>School District</b>	8 - Pierce
<b>Requested by</b>	Dale and Associates, Inc., applicant; Dakota Avenue Partners, LLC, owner.
<b>Staff Reviewer</b>	Thomas
<b>Staff Recommendation</b>	<i>Approve with conditions and disapprove without all conditions.</i>

**APPLICANT REQUEST**

**Preliminary SP to permit three single-family lots.**

Preliminary SP

A request to rezone from One and Two-Family Residential (R20) to Specific Plan-Residential (SP-R) zoning and for preliminary site plan approval for property located at 2800 and 2804 Valley Brook Place, approximately 950 feet south of Woodmont Boulevard (0.92 acres), to permit three single-family lots.

**Existing Zoning**

One and Two-Family Residential (R20) requires a minimum 20,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 2.31 dwelling units per acre including 25% duplex lots. *R20 would permit a maximum of two duplex lots with a total of four units.*

**Proposed Zoning**

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

**CRITICAL PLANNING GOALS**

N/A

**GREEN HILLS – MIDTOWN COMMUNITY PLAN**

**Current Policy**

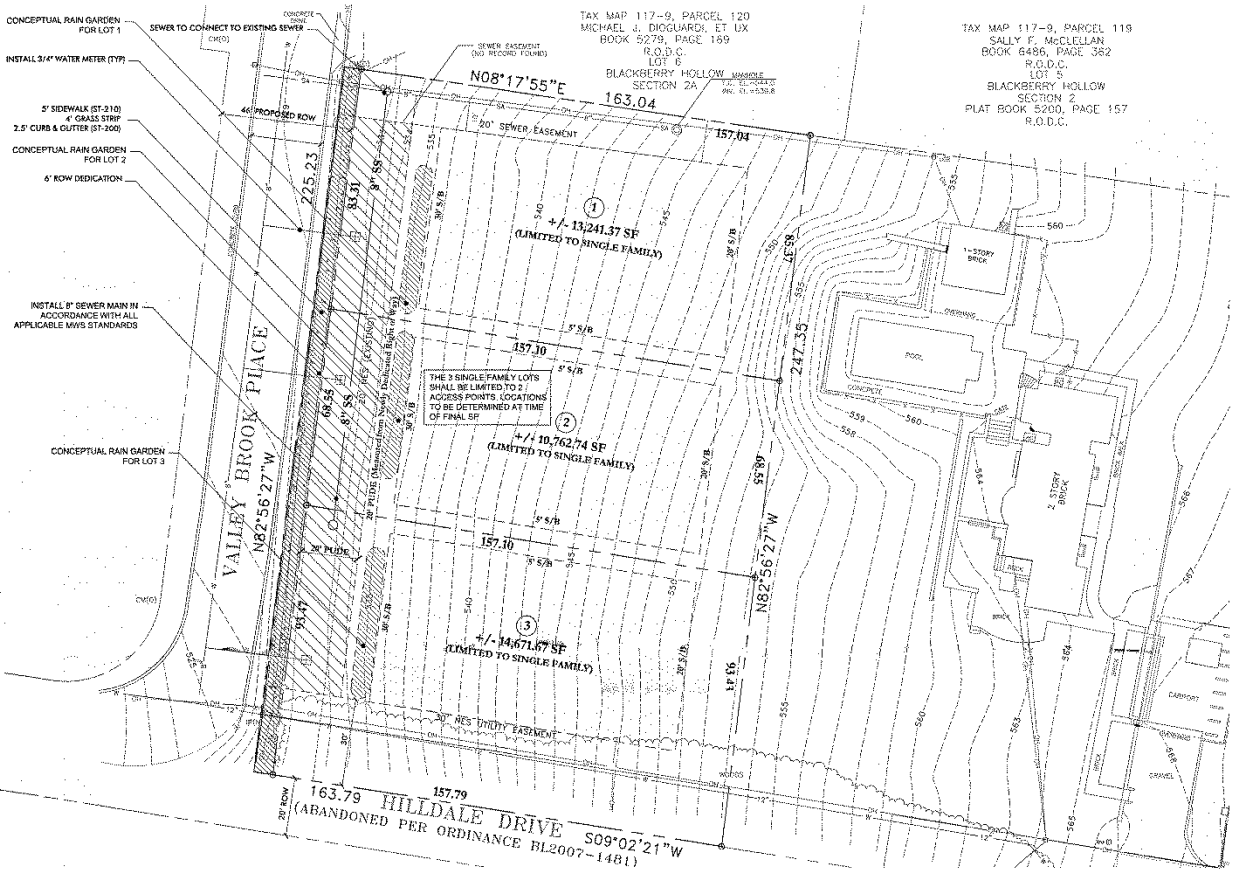
T3 Suburban Neighborhood Maintenance (T3 NM) is intended to preserve the general character of suburban neighborhoods as characterized by their development pattern, building form, land use and associated public realm. T3 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and the public realm. Where not present, enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

**Growth and Preservation Concept Map**

No changes are proposed.



# Metro Planning Commission Meeting of 04/23/2015



Proposed Site Plan



## Metro Planning Commission Meeting of 04/23/2015

### Consistent with Policy?

Yes. The proposed SP zoning is consistent with the T3 NM policy. The plan ensures moderate lot coverage, buildings are oriented to the street with consistent setbacks, and the five foot sidewalk along Valley Brook Place facilitates pedestrian connectivity.

### **PLAN DETAILS**

The site is located at 2800 and 2804 Valley Brook Place, north of the intersection of Valley Brook Place and Hilldale Drive. An unbuilt portion of Hilldale Drive right of way immediately east of the site was abandoned in 2007, which prohibits a connection to Wimbledon Road to the north.

The two lots, approximately 0.92 acres in size, are each currently eligible for duplexes, allowing a total of four units.

### Site Plan

The plan proposes three single-family lots facing Valley Brook Place. The plan provides sidewalks along Valley Brook Place. A note is included on the plan indicating the three lots will be limited to two access points, the locations of which will be determined at time of the final SP. An additional note on the plan restricts garage access to side and/or rear load entry only; front loaded garages are prohibited. Parking in the street setback is also prohibited as noted on the plan.

Building elevations will be provided with the final SP. Notes on the plan prohibit EIFS and vinyl siding and limit the maximum height of the units to 40 feet in three stories. Additional notes limit the maximum building coverage to 35% of each lot, establish the street setback at 30 feet, require porches to be a minimum six feet in depth, and limit retaining walls to a maximum of four feet in height unless otherwise specified with the final SP.

### **SUBDIVISION REGULATIONS**

The three proposed lots share a block face with only one other lot and do not meet the infill compatibility analysis outlined in Section 3-5.2 of the Subdivision Regulations. An exception to the compatibility criteria may be granted by the Planning Commission for a Specific Plan for a subdivision by approval of the rezoning.

Staff finds that the three single-family residential lots, with the conditions proposed by the applicant on the SP, overcome the incompatibility of the proposed lots with regard to frontage and lot area. In addition, staff finds the three single-family residential lots are more in keeping with the character than the existing zoning which permits four duplex units, maximum 45 feet in three stories, front load garages, and parking in the front setback.

### **ANALYSIS**

The proposed SP is consistent with the T3 NM land use policy, provides shared driveways and provides a five foot sidewalk along Valley Brook Place. Staff recommends approval of the SP with conditions and disapproval without all conditions.

### **FIRE MARSHAL RECOMMENDATION**

#### **Approved with conditions**

- Fire Code issues for the structures will be addressed at permit application review.



# Metro Planning Commission Meeting of 04/23/2015

## STORMWATER RECOMMENDATION

Approved

## WATER SERVICES RECOMMENDATION

Approved

- Approved as a Preliminary SP only. The required public sewer construction plans must be submitted and approved before the Final SP can be approved. Also, the required 30% capacity fees must be submitted before the Final SP can be approved.

## PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Prior to Final SP, indicate location of access point(s). All driveways must have adequate sight distance. Submit site distance evaluation for each driveway prior to Final SP. Location and number of driveways shall comply with Metro Zoning Code.

## TRAFFIC & PARKING RECOMMENDATION

No exception taken

Maximum Uses in Existing Zoning District: **R20**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (0)	0.92	2.17 D	4 U *	39	3	5

\*Based on two two-family lots.

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (0)	0.92	-	3 U	29	3	4

Traffic changes between maximum: **R20** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	- 1 U	-10	-	-1

## METRO SCHOOL BOARD REPORT

Projected student generation existing R20 district: 0 Elementary 0 Middle 0 High

Projected student generation proposed SP-R district: 0 Elementary 0 Middle 0 High

Because the SP would permit one less unit than the current zoning, it is not anticipated to generate more students than what is typically generated under the existing R20 district. Students would attend Julia Green Elementary School, J.T. Moore Middle School, and Hillsboro High School. Julia





## **Metro Planning Commission Meeting of 04/23/2015**

Green and J.T. Moore have been identified as being over capacity by the Metro School Board; however, there is capacity within an adjacent cluster for both. This information is based upon data from the school board last updated October 2014.

### **STAFF RECOMMENDATION**

Staff recommends approval with conditions and disapproval without all conditions.

### **CONDITIONS**

1. Uses within the SP shall be limited to a maximum of three single-family residential lots.
2. For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations and requirements of the RS10 zoning district as of the date of the applicable request or application.
3. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
4. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
5. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



# Metro Planning Commission Meeting of 04/23/2015



**2015Z-016PR-001**

Map 072-13, Parcel(s) 312, 445

05, East Nashville

05 (Scott Davis)



<b>Project No.</b>	<b>Zone Change 2015Z-016PR-001</b>
<b>Project Name</b>	<b>902 &amp; 904 Douglas Avenue</b>
<b>Council District</b>	5 – Davis
<b>School District</b>	5 – Kim
<b>Requested by</b>	Councilman Scott Davis, applicant; Michael W. O’Neill, owner.
<b>Staff Reviewer</b>	Sajid
<b>Staff Recommendation</b>	<i>Approve.</i>

**APPLICANT REQUEST**

**Zone change from Single-Family Residential to One and Two-Family Residential.**

Zone Change

A request to rezone from Single-Family Residential (RS5) to One and Two-Family Residential (R6) for properties located at 902 and 904 Douglas Avenue, on the south side of Douglas approximately 400 feet west of Emmett Avenue (0.34 acres).

**Existing Zoning**

Single-Family Residential (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre. *RS5 would permit a maximum of 2 units.*

**Proposed Zoning**

One and Two-Family Residential (R6) requires a minimum 6,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 7.71 dwelling units per acre including 25 percent duplex lots. *R6 would permit a maximum of 2 lots with 2 duplex lots for a total of 4 units.*

**CRITICAL PLANNING GOALS**

- Supports Infill Development
- Supports a Range of Housing Choices

The proposed R6 zoning district supports development that expands housing options in the neighborhood and creates opportunities for infill housing in an area that is already served by existing infrastructure. Locating development in areas served by existing, adequate infrastructure does not burden Metro with the cost of upgrading or building new infrastructure.

**EAST NASHVILLE COMMUNITY PLAN**

**Existing Structure Plan Policy**

Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.



## Metro Planning Commission Meeting of 04/23/2015

### Greenwood Detailed Neighborhood Design Plan (DNNDP)

Mixed Housing (MH) is intended for single family and multi-family housing that varies on the size of the lot and the placement of the building on the lot. Housing units may be attached or detached, but are not encouraged to be randomly placed. Generally, the character should be compatible to the existing character of the majority of the street.

### Growth and Conservation Concept Map

No change proposed for structure plan policy. Special policy will not be carried forward with the Growth and Conservation Concept Map.

### Consistent with Policy?

Yes, the proposed R6 zoning district is consistent with the existing structure plan policy and the detailed policy of the DNNDP which both encourage a mixture of housing types. R6 permits one and two family residential whereas the existing RS5 zoning limits the site to only one housing type – single-family detached.

### PUBLIC WORKS RECOMMENDATION

N/A

Maximum Uses in Existing Zoning District: **RS5**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.34	8.71 D	2 U	20	2	3

Maximum Uses in Proposed Zoning District: **R6**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (210)	0.34	7.26 D	4 U *	39	3	5

\*Based on two two-family lots.

Traffic changes between maximum: **RS5** and **R6**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 2 U	+19	+1	+2

### METRO SCHOOL BOARD REPORT

**Projected student generation existing RS5 district: 0 Elementary 0 Middle 0 High**

**Projected student generation proposed R6 district: 0 Elementary 0 Middle 0 High**

The proposed R6 district would not generate any more students than what is typically generated under the existing RS5 zoning district. Students would attend Hattie Cotton Elementary School, Gra-Mar Middle School, and Maplewood High School. Hattie Cotton Elementary School has been



## **Metro Planning Commission Meeting of 04/23/2015**

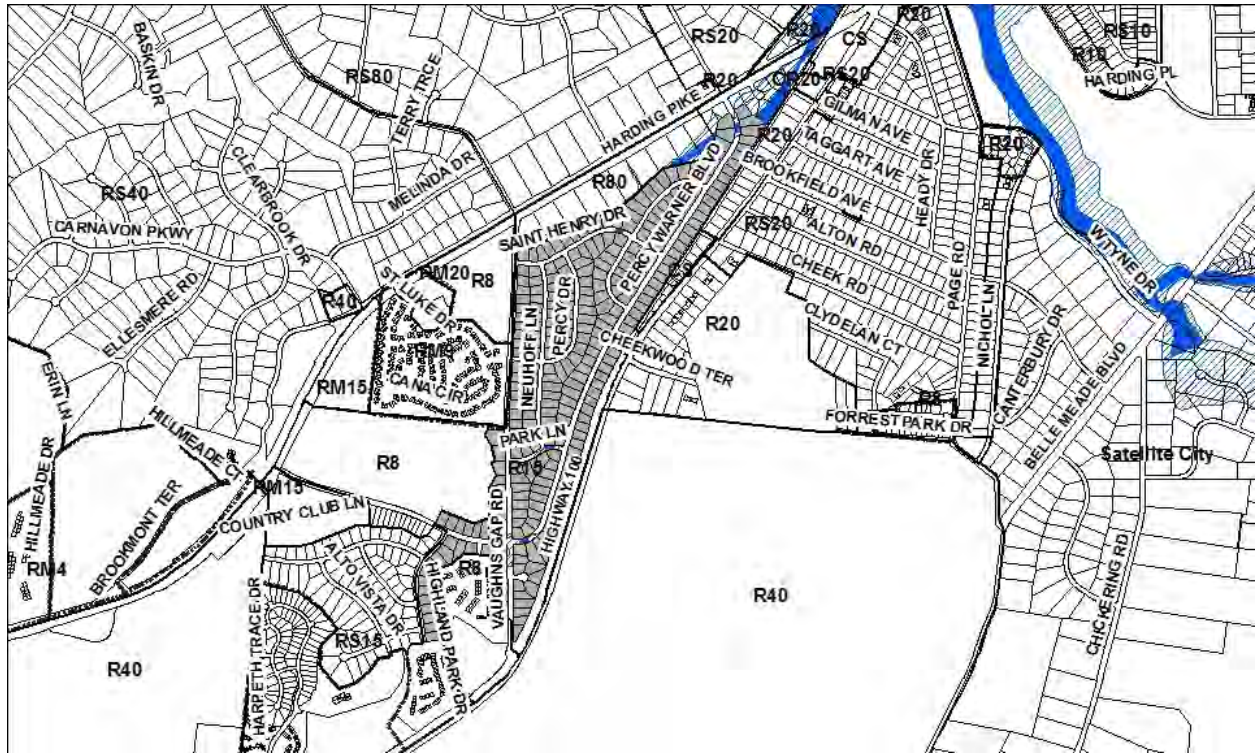
identified as over capacity. There is capacity within the cluster for additional elementary school students. This information is based upon data from the school board last updated October 2014.

### **STAFF RECOMMENDATION**

Staff recommends approval of the zone change as the request is consistent with both the land use policy and special policy and supports two critical planning goals.



## Metro Planning Commission Meeting of 04/23/2015



### 2015Z-017PR-001

- Map 129-08, Parcel(s) 022-072
- Map 129-11, Parcel(s) 019-077
- Map 129-12, Parcel(s) 001-085
- Map 129-15, Parcel(s) 001-075, 082-092
- Map 129-16, Parcel(s) 001-006
- Map 143-03, Parcel(s) 001-013
- 06, Bellevue; 07, West Nashville
- 23 (Emily Evans)



<b>Project No.</b>	<b>Zone Change 2015Z-017PR-001</b>
<b>Council Bill</b>	BL2015-1083
<b>Council District</b>	23-Evans
<b>School District</b>	6-Pierce
<b>Requested by</b>	Councilmember Emily Evans, applicant; various property owners.
<b>Staff Reviewer</b>	Milligan
<b>Staff Recommendation</b>	<i>Approve with a substitute or amendment to remove certain lots at key intersections to allow for a variety of housing units in the area and disapprove without a substitute or amendment.</i>

**APPLICANT REQUEST**  
**Zone change from R15 to RS15.**

Zone Change

A request to rezone from One and Two-Family Residential (R15) to Single-Family Residential (RS15) zoning for various properties located along Percy Warner Boulevard, Edwin Warner Drive, Saint Henry Drive, Vaughns Gap Road, Vaughns Gap Court, Percy Drive, Park Lane, Highland Park Drive, and Neuhoff Lane, west of Highway 100 (approximately 117 acres).

**Existing Zoning**

One and Two-Family Residential (R15) requires a minimum 15,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 3.09 dwelling units per acre.

**Proposed Zoning**

Single-Family Residential (RS15) requires a minimum 15,000 square foot lot and is intended for single-family dwellings at a density of 2.47 dwelling units per acre.

**CRITICAL PLANNING GOALS**

N/A

**WEST NASHVILLE COMMUNITY PLAN; BELLEVUE COMMUNITY PLAN**

**Current Policy**

Suburban Neighborhood Maintenance (T3 NM) is intended to preserve the general character of suburban neighborhoods as characterized by their development pattern, building form, land use and associated public realm. T3 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and the public realm. Where not present, enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Conservation (CO) is intended to preserve and enhance environmentally sensitive land within all Transect Categories except T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils.



## Metro Planning Commission Meeting of 04/23/2015

### Growth and Preservation Concept Map

No changes proposed.

#### Consistent with Policy?

The policy supports a variety of housing types, including single-family as well as non-single-family. Non single-family residential uses include two family and multi-family uses. The policy promotes development that is primarily consistent with the existing development pattern. The proposed rezoning area contains only single-family detached dwelling units. There are attached and multi-family units in adjacent areas. The rezoning would limit the potential for a variety of housing types within the area by removing the option of two-family dwellings.

#### **ANALYSIS**

The proposed RS15 zoning district would limit development in the subject area to only single-family uses. The proposed rezoning area contains only single-family detached dwelling units at the current time. Given that the policy states that efforts should be made to retain the existing character of the neighborhood, in terms of development pattern, building form, and land use, it is reasonable to rezone the property to RS15. This is in keeping with the existing character of the area as well as the policy. There are currently existing duplexes in the general vicinity of this neighborhood as well as some small pockets of multi-family. In order to allow for the potential for more variety of housing in the area at a future time, Staff is recommending that some lots located at key intersections, outside of any flood area, remain as R15.

The below map indicates the lots that Staff is recommending be removed from the request and remain as R15. The lots are shown with a diagonal hatch. The lots include the following addresses:

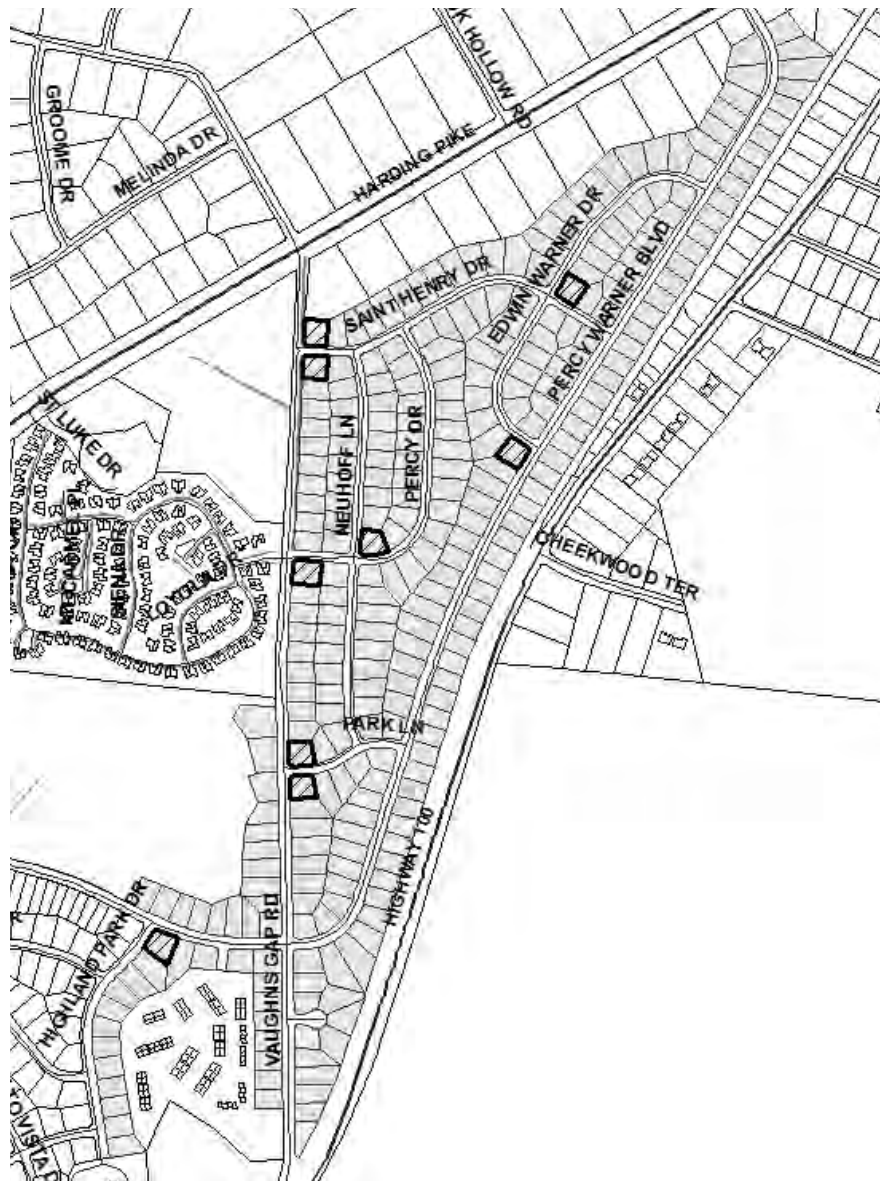
Parcel	Address	Street
12912004600	1022	EDWIN WARNER DR
12911004900	920	NEUHOFF LN
12915008800	834	PERCY WARNER BLVD
12912003400	957	PERCY WARNER BLVD
12915001600	54	VAUGHNS GAP RD
12915000400	56	VAUGHNS GAP RD
12911005800	74	VAUGHNS GAP RD
12911002400	94	VAUGHNS GAP RD
12911001900	96	VAUGHNS GAP RD

The lots to remain should be located at strategic locations, meet the minimum standards of the zoning ordinance for the construction of a two-family unit, and not be located within a flood zone.





## Metro Planning Commission Meeting of 04/23/2015

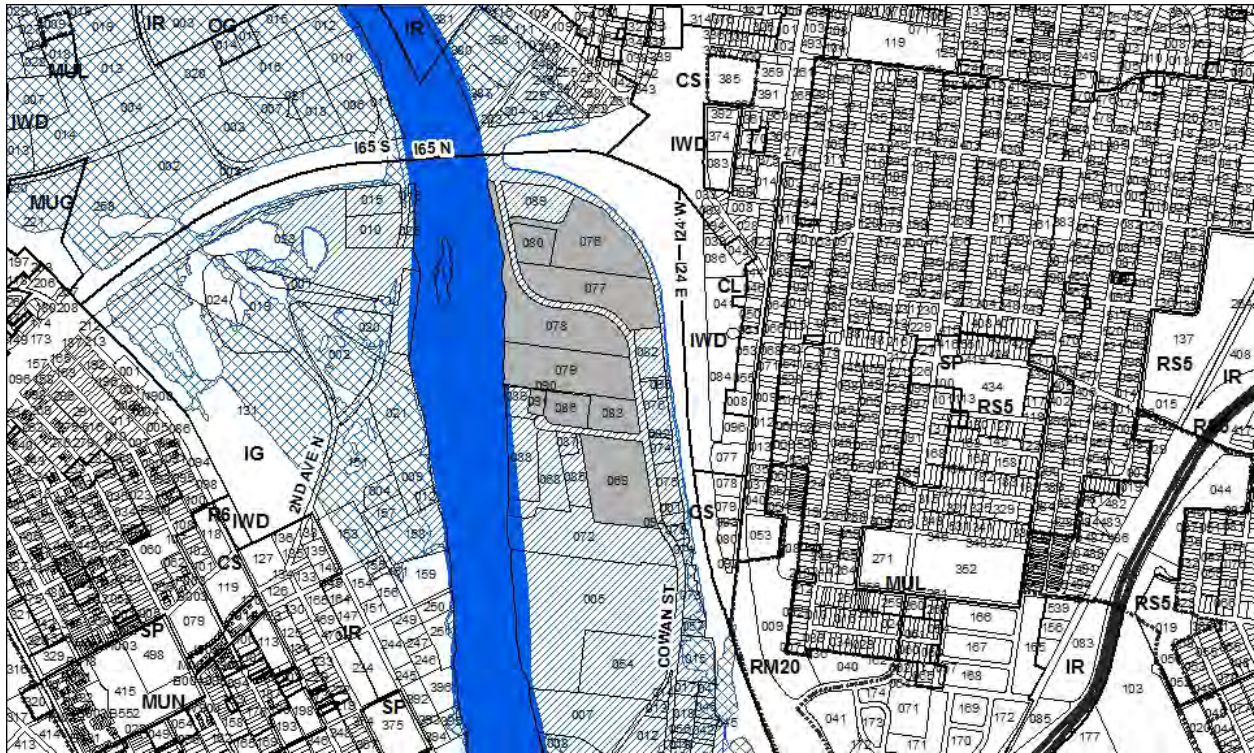


### **STAFF RECOMMENDATION**

Staff recommends approval with a substitute or amendment to remove certain lots at key intersections to allow for a variety of housing units in the area and disapproval without a substitute or amendment.



## Metro Planning Commission Meeting of 04/23/2015



### 2015Z-018PR-001

Map 082-02, Parcel(s) 076-080

Map 082-06, Parcel(s) 069, 083, 086, 091

05, East Nashville

05 (Scott Davis)



<b>Project No.</b>	<b>Zone Change 2015Z-018PR-001</b>
<b>Council District</b>	5-S. Davis
<b>School District</b>	5-Kim
<b>Requested by</b>	Civil Site Design Group, PLLC, applicant; and various owners.
<b>Staff Reviewer</b>	Thomas
<b>Staff Recommendation</b>	<i>Approve.</i>

**APPLICANT REQUEST**

**Zone change from IG to MUI-A**

Zone Change

A request to rezone from Industrial General (IG) to Mixed Use Intensive-Alternative (MUI-A) zoning for properties located at 700, 740, 750, 800, 815, 901, 905 Cowan Street and 1420 and 1432 Cowan Court, approximately 2,000 feet north of Jefferson Street (59.13 acres).

**Existing Zoning**

Industrial General (IG) is intended for a wide range of light manufacturing uses at moderate intensities within enclosed structures.

**Proposed Zoning**

Mixed Use Intensive-A (MUI-A) is intended for a high intensity mixture of residential, retail, and office uses and is designed to create walkable neighborhoods through the use of appropriate building placement and bulk standards.

**CRITICAL PLANNING GOALS**

- Supports Infill Development

The rezoning to MUI-A allows for a traditionally industrial area to redevelop with residential, retail, and office uses. The development fills in gaps and promotes a more compact design than exists with industrial uses. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure such as roads, water, and sewer, because it does not burden Metro with the cost of maintaining new infrastructure.

**EAST NASHVILLE COMMUNITY PLAN**

**Current Policy**

Conservation (CO) is intended to preserve and enhance environmentally sensitive land within all Transect Categories except T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils.

East Nashville Community Plan Special Policy Area 4 The alternate policy for this Potential Open Space area is T4 Urban Mixed Use Neighborhood. T4 MU is intended to preserve, enhance, and create urban, mixed use neighborhoods characterized by a development pattern that contains a diverse mix of residential and nonresidential land uses, and that are envisioned to remain or develop in a mixed use pattern. T4 MU areas are areas intended to be mixed use in nature with the presence



## Metro Planning Commission Meeting of 04/23/2015

of commercial and even light industrial uses, but also a significant amount of moderate to high density residential development.

### Growth and Preservation Concept Map

T5 Center Mixed Use Neighborhood (T5 MU) is intended to preserve and enhance urban mixed use neighborhoods that are characterized by a development pattern that contains a diverse mix of residential and nonresidential land uses, and that are envisioned to remain or develop in a mixed use pattern. T5 MU areas are intended to be among the most intense areas in Davidson County. T5 MU areas include the County's major employment centers, representing several sectors of the economy including health care, finance, retail, the music industry, and lodging.

### Consistent with Policy?

Yes. The rezoning is consistent with the T5 Center Mixed Use Neighborhood policy. The rezoning to MUI-A will allow for urban residential development that is mixed with civic and public benefit, and high intensity commercial and office land uses.

### FIRE MARSHAL'S OFFICE RECOMMENDATION

N/A

### PUBLIC WORKS RECOMMENDATION

N/A

### TRAFFIC AND PARKING RECOMMENDATION

#### Conditions if approved

- Traffic study may be required at time of development.

Maximum Uses in Existing Zoning District: **IG**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Warehousing (150)	59.13	0.6 F	1,545,421 SF	5501	464	495

Maximum Uses in Proposed Zoning District: **MUI-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Office (710)	59.13	5.0 F	12,878,514 SF	56205	9143	14503

Traffic changes between maximum: **IG** and **MUI-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+50,704	+8,679	+14,008



## **Metro Planning Commission Meeting of 04/23/2015**

### **METRO SCHOOL BOARD REPORT**

**Projected student generation existing IG district: 0 Elementary 0 Middle 0 High**

**Projected student generation proposed MUI-A district: 27 Elementary 19 Middle 44 High**

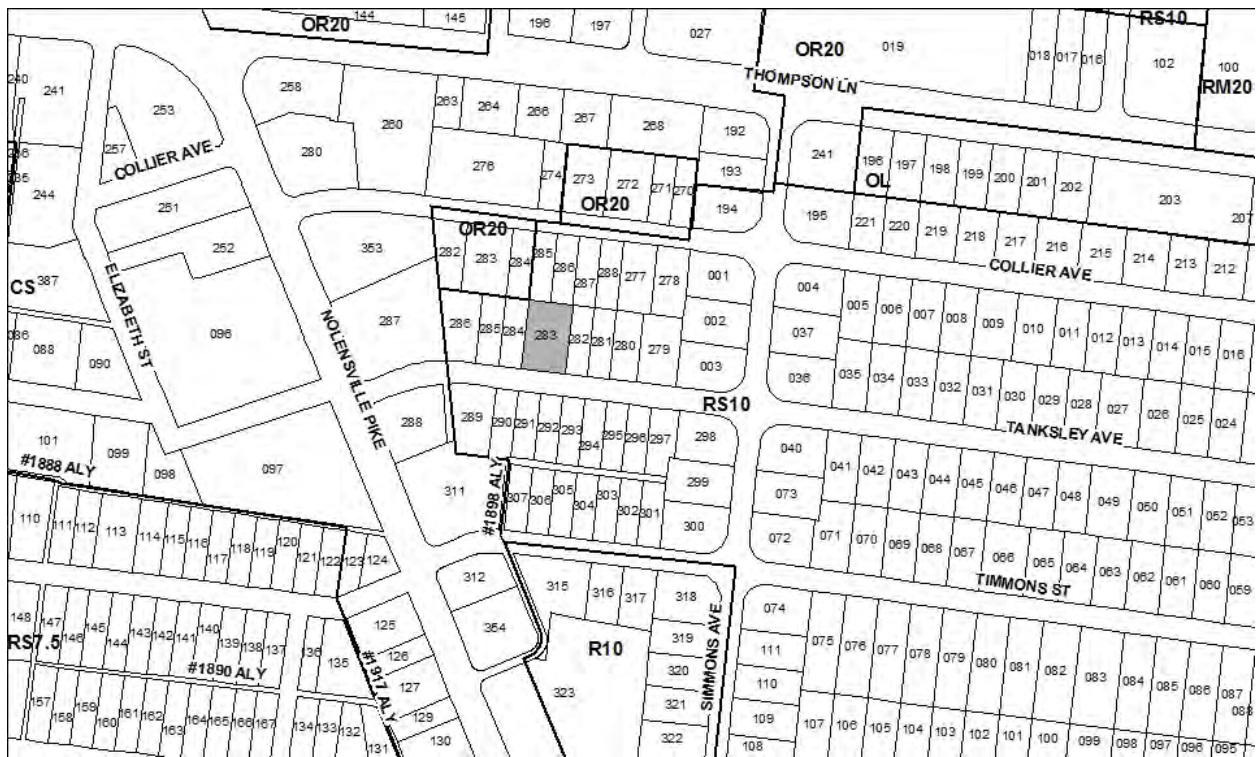
The proposed student generation numbers were calculated using the urban infill factor and assumed a 1,000 square foot residential unit with 40% of the site being used for non-residential floor area. Students would attend Glenn Elementary School, Jere Baxter Middle School, and Maplewood High School. None of the schools have been identified as being over capacity by the Metro School Board. This information is based upon data from the school board last updated October 2014.

### **STAFF RECOMMENDATION**

The requested rezoning to MUI-A is consistent with the T5 Center Mixed Use Neighborhood policy. The rezoning will allow for a high intensity mixture of residential, retail, and office uses and ensures walkable neighborhoods through bulk standards and building placement. Staff recommends approval.



# Metro Planning Commission Meeting of 04/23/2015



**2015Z-019PR-001**  
308 TANKSLEY AVE  
Map 119-13, Parcel(s) 283  
11, South Nashville  
16 (Tony Tenpenny)



<b>Project No.</b>	<b>Zone Change 2015Z-019PR-001</b>
<b>Council District</b>	16-Tenpenny
<b>School District</b>	7-Pinkston
<b>Requested by</b>	Larissa Lentile, applicant and owner.
<b>Staff Reviewer</b>	Milligan
<b>Staff Recommendation</b>	<i>Approve.</i>

**APPLICANT REQUEST**

**Zone change from RS10 to RS7.5**

Zone Change

A request to rezone from Single-Family Residential (RS10) to Single-Family Residential (RS7.5) zoning for property located at 308 Tanksley Avenue, approximately 440 feet east of Nolensville Pike (0.36 acres).

**Existing Zoning**

Single-Family Residential (RS10) requires a minimum of 10,000 square feet and is intended for single-family dwellings at a density of 3.7 dwelling units per acre. *RS10 would permit a maximum of 1 unit.*

**Proposed Zoning**

Single-Family Residential (RS7.5) requires a minimum 7,500 square foot lot and is intended for single-family dwellings at a density of 4.94 dwelling units per acre. *RS7.5 would permit a maximum of 2 units.*

**CRITICAL PLANNING GOALS**

- Supports Infill Development

The rezoning to RS7.5 allows for the creation of an additional residential building lot in an existing urban neighborhood where infrastructure exists. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure such as roads, water, and sewer, because it does not burden Metro with the cost of maintaining new infrastructure.

**SOUTH NASHVILLE COMMUNITY PLAN**

**Current Policy**

T4 Urban Neighborhood Evolving (T4 NE) is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

**Growth and Preservation Concept Map**

No change proposed.



## Metro Planning Commission Meeting of 04/23/2015

### Consistent with Policy?

Yes. The rezoning is consistent with the T4 Urban Neighborhood Evolving policy. The rezoning to RS7.5 will allow for the lot to be subdivided into 2 lots and provide for an additional infill housing unit in an existing urban neighborhood.

### **ANALYSIS**

The requested rezoning to RS7.5 is consistent with the T4 Urban Neighborhood Evolving policy. The rezoning will allow for the lot to be subdivided and provide for an additional infill dwelling unit in an existing urban neighborhood. The existing lots on the street are mostly under 10,000 square feet. The RS7.5 zoning will be more in line with the existing lot sizes along the street. Under the current Subdivision Regulations, staff would support a subdivision of this lot into two lots under the proposed zoning.

### **FIRE MARSHAL’S OFFICE RECOMMENDATION**

N/A

### **PUBLIC WORKS RECOMMENDATION**

N/A

### **TRAFFIC AND PARKING RECOMMENDATION**

**No exception taken**

Maximum Uses in Existing Zoning District: **RS10**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.36	4.35 D	1 U	10	1	2

Maximum Uses in Proposed Zoning District: **RS7.5**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.36	5.80 D	2 U	20	2	3

Traffic changes between maximum: **RS10** and **RS7.5**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 1 U	+10	+1	+1

### **STAFF RECOMMENDATION**

Staff recommends approval.

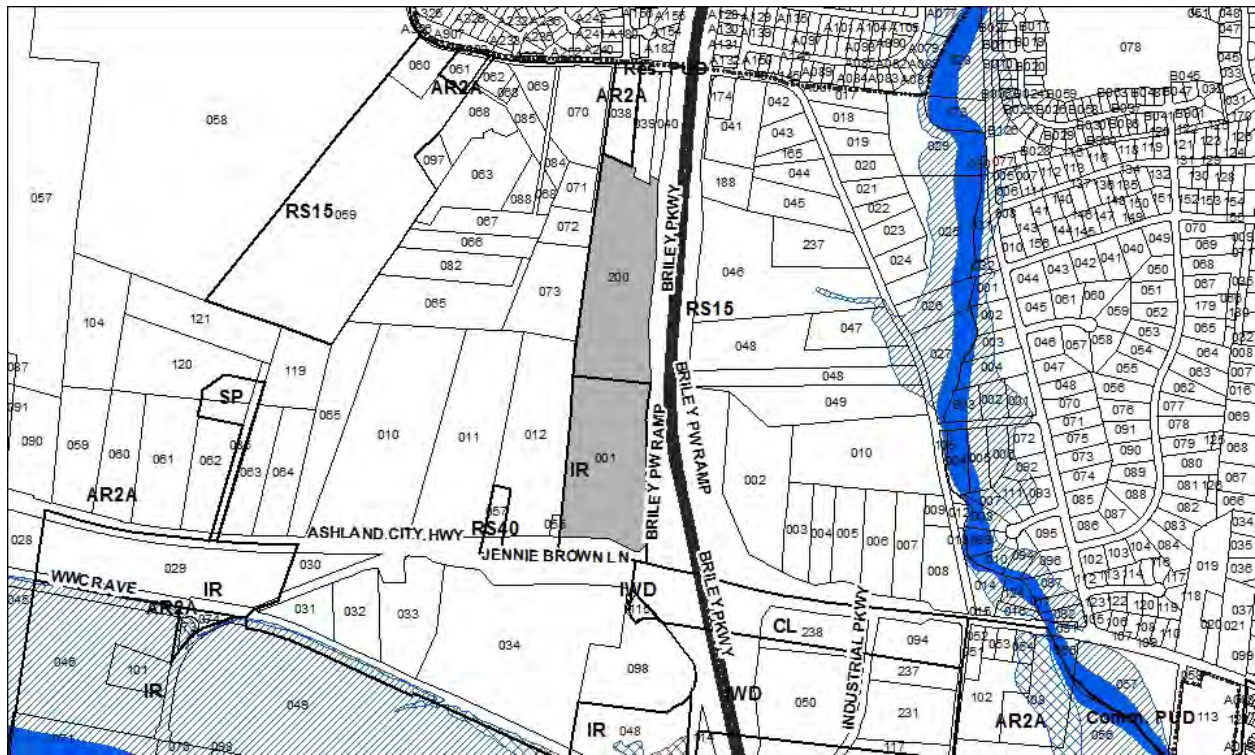




**SEE NEXT PAGE**



# Metro Planning Commission Meeting of 04/23/2015



## 2015Z-020PR-001

Map 058, Parcel(s) 200

Map 069, Parcel(s) 001

03, Bordeaux - Whites Creek

01 (Lonnell Matthews, Jr.)



<b>Project No.</b>	<b>Zone Change 2015Z-020PR--001</b>
<b>Council District</b>	1-Matthews
<b>School District</b>	1-Gentry
<b>Requested by</b>	Dale & Associates, Inc., applicant; Alton Ross, Jr., owner.
<b>Staff Reviewer</b>	Milligan
<b>Staff Recommendation</b>	<i>Disapprove.</i>

**APPLICANT REQUEST**

**Zone change from RS15 and IR to IWD.**

Zone Change

A request to rezone from Single-Family Residential (RS15) and Industrial Restrictive (IR) to Industrial Warehousing (IWD) zoning for properties located at Ashland City Highway (unnumbered) and Cato Road (unnumbered), approximately 1,100 feet east of Jennie Brown Lane (28.4 acres).

**Existing Zoning**

Single-Family Residential (RS15) requires a minimum 15,000 square foot lot and is intended for single-family dwellings at a density of 2.47 dwelling units per acre. *RS15 would permit a maximum of 35 units.*

Industrial Restrictive (IR) is intended for a wide range of light manufacturing uses at moderate intensities within enclosed structures.

**Proposed Zoning**

Industrial Warehousing (IWD) is intended for a wide range of warehousing, wholesaling, and bulk distribution uses.

**CRITICAL PLANNING GOALS**

N/A

**BOURDEAUX-WHITES CREEK COMMUNITY PLAN**

**Current Policy**

D Industrial (D IN) is intended to preserve, enhance, and create Industrial Districts in appropriate locations. The policy creates and enhances areas that are dominated by one or more industrial activities, so that they are strategically located and thoughtfully designed to serve the overall community or region, but not at the expense of the immediate neighbors. Types of uses in D IN areas include non-hazardous manufacturing, distribution centers and mixed business parks containing compatible industrial and non-industrial uses. Uses that support the main activity and contribute to the vitality of the D IN are also found.

T2 Rural Neighborhood Maintenance (T2 NM) is intended to preserve the general character of rural neighborhoods as characterized by their development pattern, building form, land use, and associated public realm. T2 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and



## Metro Planning Commission Meeting of 04/23/2015

public realm. Where not present, enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Conservation (CO) is intended to preserve and enhance environmentally sensitive land within all Transect Categories except T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils.

### Growth and Preservation Concept Map

No change proposed.

### Consistent with Policy?

The rezoning of the front property from IR to IWD is consistent with the District Industrial policy. The rezoning of the back property from RS15 to IWD is not consistent with the T2 Rural Neighborhood Maintenance Policy. The T2 Rural Neighborhood Maintenance Policy is a residential only policy and does not support non-residential zoning, especially an intense zoning such as IWD.

### ANALYSIS

The requested IWD is inconsistent with the T2 Rural Neighborhood Maintenance policy. The District Industrial policy is a reflection of the existing IR zoning, which was approved in 1996. There is a lack of utilities in this area to serve any new development. The front parcel has access to public water but it does not have access to public sewer. A public sewer main extension would have to be constructed through several parcels to serve this property, and potentially have to cross Briley Parkway. The rear parcel does not have access to public water. A public water main extension would have to be constructed to serve this property and it would have to cross through another property. The availability of public sewer to the rear lot has not yet been determined. Staff does not support the rezoning of rural property that is not served by adequate infrastructure and does not support the extension of utilities into this rural area. The extension of utilities would promote higher intensity development in an environmentally sensitive area and a rural area that is not appropriate for higher intensities.

### FIRE MARSHAL'S OFFICE RECOMMENDATION

N/A

### PUBLIC WORKS RECOMMENDATION

N/A

### TRAFFIC AND PARKING RECOMMENDATION

#### Conditions if approved

1. Traffic study may be required at time of development

Maximum Uses in Existing Zoning District: **RS15**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	14.2	2.90 D	41 U	458	39	49



## Metro Planning Commission Meeting of 04/23/2015

Maximum Uses in Existing Zoning District: **IR**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Warehousing (150)	14.2	0.6 F	371,131 SF	1322	112	119

Maximum Uses in Proposed Zoning District: **IWD**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Warehousing (150)	28.4	0.8 F	989,683 SF	3524	297	317

Traffic changes between maximum: **RS15 , IR and IWD**

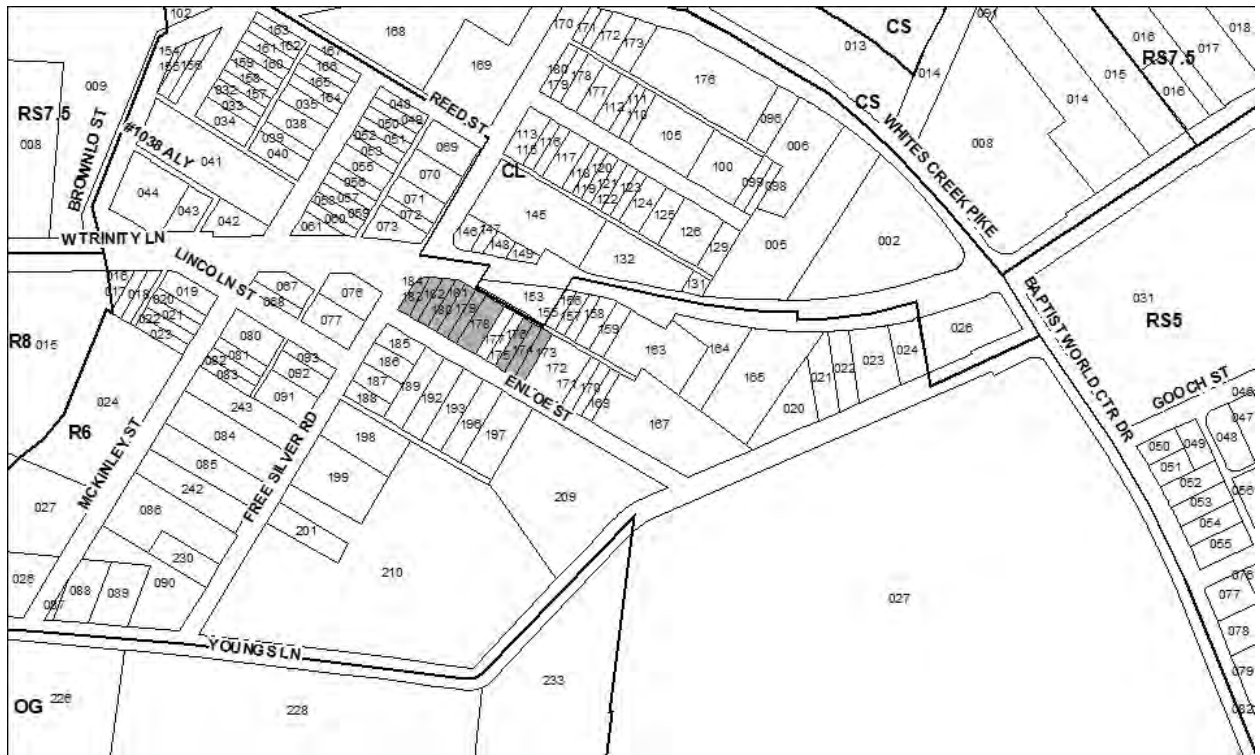
Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+1,744	+146	+149

### STAFF RECOMMENDATION

Given the lack of utilities and the inconsistency of the rear property with the policy for the area, staff recommends disapproval.



# Metro Planning Commission Meeting of 04/23/2015



**2015Z-021PR-001**

Map 070-08, Parcel(s) 173-175, 178-184

03, Bordeaux - Whites Creek

02 (Frank R. Harrison)



<b>Project No.</b>	<b>Zone Change 2015Z-021PR-001</b>
<b>Council District</b>	2 – Harrison
<b>School District</b>	1 – Gentry
<b>Requested by</b>	Forrest Henderson, applicant and owner.
<b>Staff Reviewer</b>	Swaggart
<b>Staff Recommendation</b>	<i>Disapprove. Approve RM9-A.</i>

**APPLICANT REQUEST**  
**Zone change from R6 to RM9.**

Zone Change

A request to rezone from One and Two-Family Residential (R6) to Multi-Family Residential (RM9) zoning for properties located at 701 Enloe Street and Enloe Street (unnumbered), at the southeast corner of W. Trinity Lane and Free Silver Road (0.64 acres).

**Existing Zoning**

One and Two-Family Residential (R6) requires a minimum 6,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 7.71 dwelling units per acre including 25 percent duplex lots. *R6 would permit a maximum of four lots with one duplex lot for a total of five units.*

**Proposed Zoning**

Multi-Family Residential (RM9) is intended for single-family, duplex, and multi-family dwellings at a density of nine dwelling units per acre. *RM9 would permit a maximum of 5 units.*

**CRITICAL PLANNING GOALS**

N/A

**EAST NASHVILLE COMMUNITY PLAN**

**Current Policy**

Suburban Community Center (T3 CC) policy is intended to enhance suburban community centers encouraging their redevelopment as intense mixed use areas that are compatible with the general character of suburban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T3 Suburban Community Centers are pedestrian friendly areas, generally located at prominent intersections. T3 Suburban Community Centers serve suburban communities within a 10 to 20 minute drive.

**Growth and Preservation Concept Map**

No change is proposed.

Consistent with Policy?

The policy would support multi-family, but it does not support RM9. A design based zoning such as a SP, an overlay, or an A district is necessary to ensure that development confirms to the intent of the policy. Since this is a unique property with multiple street frontages and not much lot depth, a design based zoning would help to ensure an appropriate design for this site.



## **Metro Planning Commission Meeting of 04/23/2015**

### **FIRE MARSHALL'S OFFICE RECOMMENDATION**

N/A

### **PUBLIC WORKS RECOMMENDATION**

N/A

### **TRAFFIC AND PARKING RECOMMENDATION**

#### **Conditions if approved**

Traffic study may be required at time of development.

### **STORMWATER RECOMMENDATION**

N/A

### **WATER SERVICES RECOMMENDATION**

N/A

### **SCHOOL REPORT**

The proposed RM9 zoning district does not permit any additional residential units than what is currently permitted with the current R6 district. Since the request does not permit additional density, then it will not generate any additional students from what would be generated under the current R6 district.

### **STAFF RECOMMENDATION**

Staff recommends disapproval of the proposed RM9 zoning district, but approval of RM9-A.

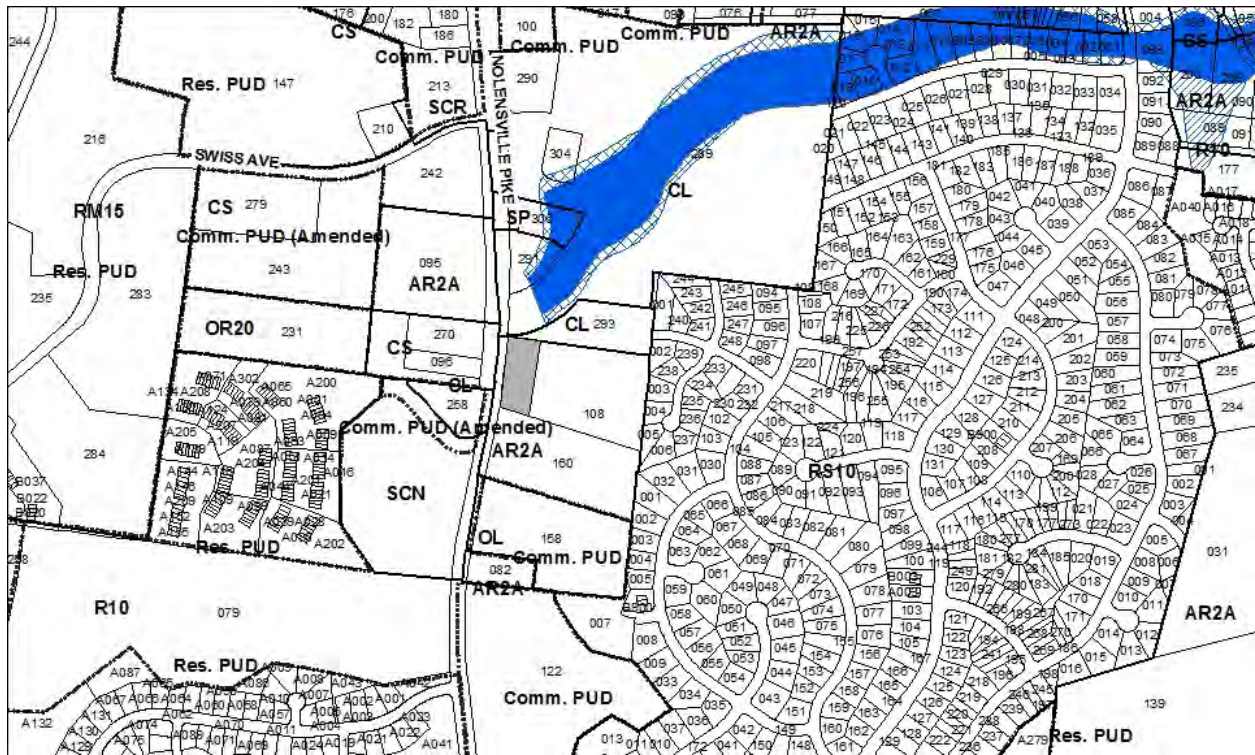




**SEE NEXT PAGE**



# Metro Planning Commission Meeting of 04/23/2015



**2015Z-022PR-001**

Map 161, Part of Parcel(s) 108

12, Southeast

31 (Fabian Bedne)



<b>Project No.</b>	<b>Zone Change 2015Z-022PR-001</b>
<b>Council District</b>	31 – Bedne
<b>School District</b>	2 - Brannon
<b>Requested by</b>	Lucy Ann Hardy, applicant and owner.
<b>Staff Reviewer</b>	Birkeland
<b>Staff Recommendation</b>	<i>Approve.</i>

**APPLICANT REQUEST**  
**Zone change from AR2A to CL**

Zone Change

A request to rezone from Agricultural/Residential (AR2a) to Commercial Limited (CL) zoning for a portion of property located at Nolensville Pike (unnumbered), approximately 950 feet south of Swiss Avenue (1 acre).

**Existing Zoning**

Agricultural/Residential (AR2a) requires a minimum lot size of two acres and intended for uses that generally occur in rural areas, including single-family, two-family, and mobile homes at a density of one dwelling unit per two acres. The AR2a District is intended to implement the natural conservation or rural land use policies of the general plan. *AR2a would permit a maximum of 2 lots with 2 duplex lots for a total of 4 units.*

**Proposed Zoning**

Commercial Limited (CL) is intended for retail, consumer service, financial, restaurant, and office uses.

**CRITICAL PLANNING GOALS**

N/A

**SOUTHEAST COMMUNITY PLAN**

**Current Policy**

T3 Suburban Community Center (T3 CC) policy is intended to enhance suburban community centers encouraging their redevelopment as intense mixed use areas that are compatible with the general character of suburban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T3 Suburban Community Centers are pedestrian friendly areas, generally located at prominent intersections. T3 Suburban Community Centers serve suburban communities within a 10 to 20 minute drive.

Conservation (CO) is intended to preserve and enhance environmentally sensitive land within all Transect Categories except T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils.



## Metro Planning Commission Meeting of 04/23/2015

### Growth and Preservation Concept Map

No change is proposed.

#### Consistent with Policy?

Yes. The proposed CL district allows uses that are consistent with the T3 Suburban Community Center land use policy.

### FIRE MARSHAL RECOMMENDATION

N/A

### PUBLIC WORKS RECOMMENDATION

N/A

### TRAFFIC AND PARKING RECOMMENDATION

#### Conditions of Approval

- Traffic study may be required at the time of development.

### WATER SERVICES RECOMMENDATION

N/A

### STORMWATER RECOMMENDATION

No agency review required

Maximum Uses in Existing Zoning District: **AR2a**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	1.0	0.5 D	1 U	10	1	2

Maximum Uses in Proposed Zoning District: **CL**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (814)	1.0	0.6 F	26,136 SF	1156	29	85

Traffic changes between maximum: **AR2a** and **CL**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+1,146	+28	+83



## **Metro Planning Commission Meeting of 04/23/2015**

### **METRO SCHOOL BOARD REPORT**

No school report was prepared because this request is not likely to generate additional students.

### **STAFF RECOMMENDATION**

The parcel located at Nolensville Pike (unnumbered), across the street from 5837 Nolensville Pike, is approximately 5 acres. The proposed zoning change from AR2a to CL will only affect one acre along the western portion of the lot, abutting Nolensville Pike. A zone change from AR2a to CL is consistent with the T3 Suburban Community Center land use policy and surrounding uses, therefore, staff recommends approval.



**SEE NEXT PAGE**

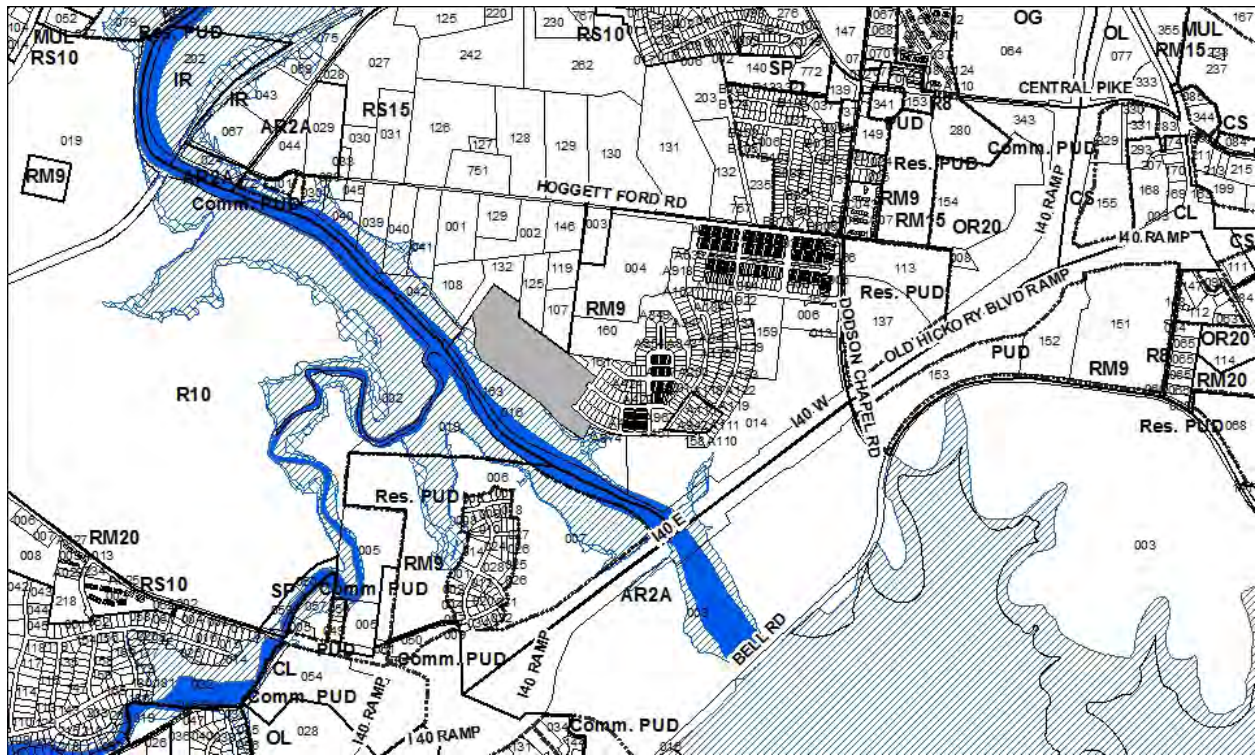


## **PLANNING COMMISSION ACTIONS**

- **Planned Unit Developments**



# Metro Planning Commission Meeting of 04/23/2015



**2004UD-002-007**

VILLAGES OF RIVERWOOD, PHASE 6

Map 097, Parcel(s) 161, 163

14, Donelson - Hermitage

14 (James Bruce Stanley)





<b>Project No.</b>	<b>2004UD-002-007</b>
<b>Project Name</b>	<b>Villages of Riverwood, Ph 6</b>
<b>Council District</b>	20 – Baker
<b>School District</b>	9 - Frogge
<b>Requested by</b>	Ragan-Smith Associates Inc., applicant; Beazer Homes Corp., owner.
<b>Staff Reviewer</b>	Birkeland
<b>Staff Recommendation</b>	<i>Approve with conditions.</i>

**APPLICANT REQUEST**

**Revise a portion of the Urban Design Overlay and for final site plan for Phase 6.**

Revise Preliminary Plan and Final Site Plan

A request for revision to preliminary UDO and final site plan approval for Phase 6 only for a portion of the Villages of Riverwood Urban Design Overlay located at Hoggett Ford Road (unnumbered), Dodson Chapel Road (unnumbered), and Hoggett Ford Road (unnumbered), approximately 1,250 feet south of Hoggett Ford Road (36.03 acres), zoned Multi-Family Residential (RM9), to permit 93 dwelling units.

**Existing Zoning**

Multi-Family Residential (RM9) is intended for single-family, duplex, and multi-family dwellings at a density of nine dwelling units per acre.

**VILLAGES OF RIVERWOOD UDO**

In 2004, the preliminary Villages of Riverwood Urban Design Overlay (UDO) was approved by Metro Council. The plan included a total of 1,978 dwelling units and 65,000 square feet of mixed-use development, including the possibility of office and retail, and a future assisted-living facility.

The residential portion of the plan is comprised of single-family detached units, townhouse units, and stacked flats. The southernmost portion of the site, adjacent to the Stones River, is planned to include a 776-unit assisted living facility. Final site plan approvals have been granted for other phases of this UDO.

**APPLICATIONS**

October 2014

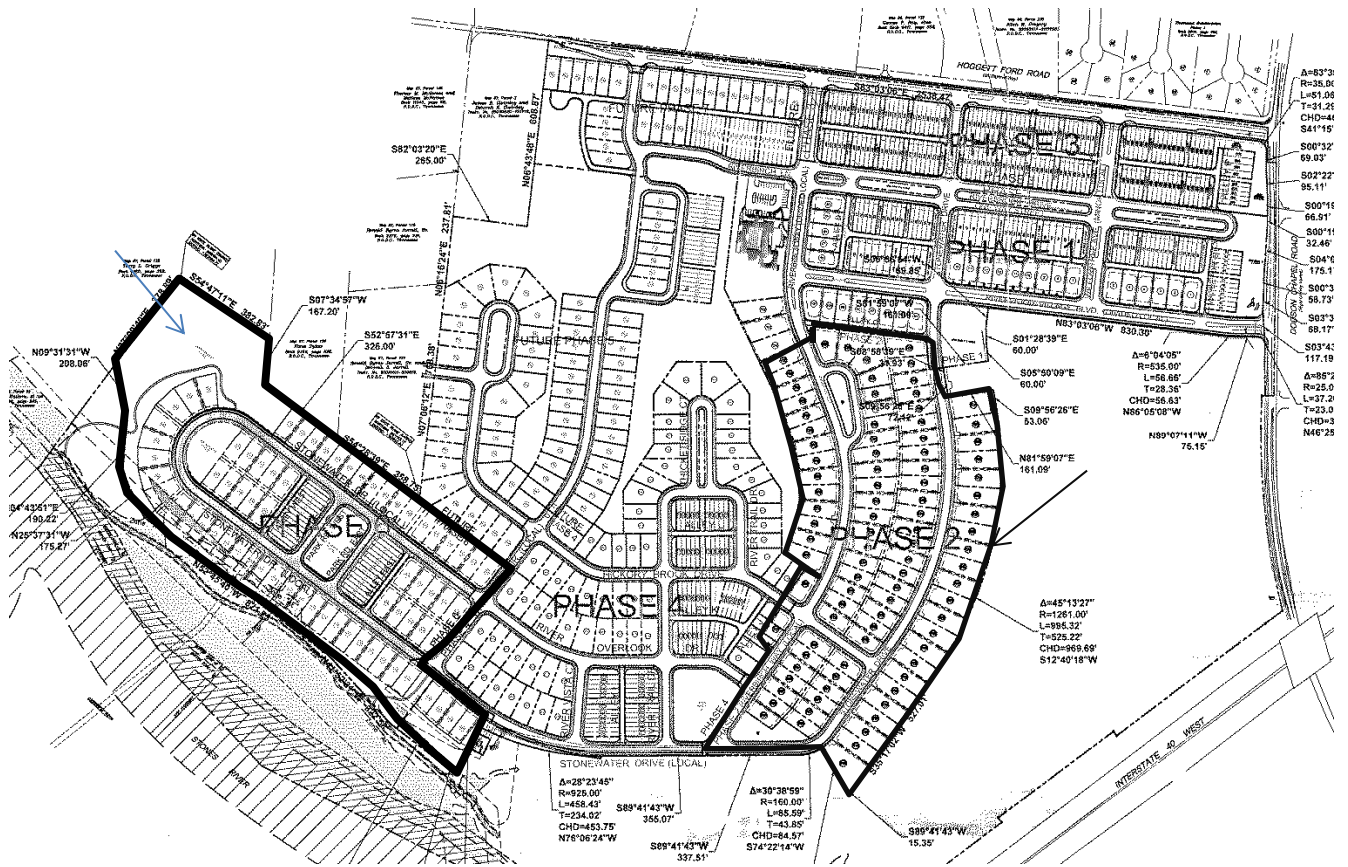
The Metro Planning Commission approved a revision to Phases 5, 6 and 7 and for final site plan for Phase 6 to the Villages of Riverwood Urban Design Overlay. The revision and final site plan proposed slight changes to layout and an overall reduction in the amount of proposed dwelling units. Additionally, the revision and final site plan for Phase 6 included conditions regarding the greenway along the Stones River, to the southwest of Phase 6.

April 2015

The proposed plan for the Villages of Riverwood Urban Design Overlay District is a request for a revision and final site plan for Phase 6 only. Phase 6 is located along the southwestern edge of the



# Metro Planning Commission Meeting of 04/23/2015



Proposed UDO Plan



## Metro Planning Commission Meeting of 04/23/2015

UDO boundary. Access to this area is provided by Whitebirch Drive that connects to Hoggett Ford Road, along the north side of the other phases.

The revised plans show a shift of two lots within the development from Phase 2 to Phase 6. Two unrecorded lots had been reflected incorrectly within Phase 2 and included in the overall lot count for the entire development. These two lots have been removed from Phase 2 and are now depicted as lots 558 and 559 within Phase 6. Because these lots were already accounted for in the overall lot count for the development, the total number of dwelling units is unchanged with this request.

The following table illustrates the proposed changes for the unit counts within the Villages of Riverwood - Phases 2 and 6.

		Phase 2	Phase 6	Total Housing Type
2014	Single Family	94	67	161
	Townhome		24	24
	<b>2014 Total</b>	<b>94</b>	<b>91</b>	<b>185</b>
2015	Single Family – General	92	69	161
	Townhome		24	24
	<b>2015 Total</b>	<b>92</b>	<b>93</b>	<b>185</b>
	<b>Total Change</b>	<b>-2</b>	<b>+2</b>	<b>- 0 -</b>

The previous plan in October required easements for the greenway trail to be dedicated prior to recordation of the final plat for Phase 6. With this revision and final site plan for Phase 6, easements for the greenway trail shall be dedicated and the developer will build a greenway trail prior to the recordation of the final plat for Phase 5 or 7, whichever comes first. The greenway trail will still provide connections into Phase 6. On the final site plan for Phases 5 and 7, all proposed greenway easements and open spaces will be identified to provide interconnectivity between the phases.

These changes are minor and allow the same access points within the UDO to remain the same, providing interconnectivity between the phases.

### **FIRE MARSHAL RECOMMENDATION**

#### **Approved with Conditions**

- Fire Code issues for the structures will be addressed at permit application review. Water flow requirements for single-family homes that do not exceed 3600 sq. ft. is a minimum of 1000 gpm @ 20 psi. Provide this data to pre-approve the future homes.

### **PARKS RECOMMENDATION**

#### **Approved with Conditions**

- A greenway/conservation easement acceptable to Metro Parks shall be recorded prior to the recordation of the final plat for Phase 5 or 7, whichever comes first. The greenway/conservation easement shall include all of the floodway and floodway buffer area and include the greenway trail.



## **Metro Planning Commission Meeting of 04/23/2015**

### **STORMWATER RECOMMENDATION**

**Approved**

### **WATER RECOMMENDATION**

**Approved**

- Approved for Phase 6 only. This approval does not apply to any private water and sewer utilities, as plans for these must be submitted through a separate review process with Metro Water Permits.

### **PUBLIC WORKS RECOMMENDATION**

**Approved with Conditions**

- Complete roadway repairs on Riverwood Village Blvd prior to recording final plats in Phase 6.

### **TRAFFIC & PARKING**

**No exception taken**

### **STAFF RECOMMENDATION**

Staff recommends approval with conditions. The proposed modification is consistent with the intent of the UDO.

### **CONDITIONS**

1. Prior to the recordation of the final plat for Phase 5 or 7, whichever comes first, the application shall provide confirmation that the “Dedicated Conversation Greenway Public Access Trail Easement Area, “ associated with Phase 6, has been recorded.
2. A “Dedicated Conservation Greenway Public Access Trail Easement Area” easement shall include all of the floodway and floodway buffer area, including the greenway trail.
3. Provide access easements for all greenway connection points to public rights-of-way.
4. If the greenway associated with Phase 6 is not constructed prior to the approval of a final plat for phase 5 or 7, whichever comes first, the greenway in phase 6 shall be bonded with phase 5 or 7, whichever comes first.
5. Prior to the construction of the greenway, construction plans for the greenway shall be reviewed and approved by Metro Parks.
6. This approval does not include any signs. Signs in planned unit developments must be approved by the Metro Department of Codes Administration except in specific instances when the Metro Council directs the Metro Planning Commission to review such signs.
7. The requirements of the Metro Fire Marshal’s Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
8. Authorization for the issuance of permit applications will not be forwarded to the Department of Codes Administration until four additional copies of the approved plans have been submitted to the Metro Planning Commission.
9. The UDO final site plan as approved by the Planning Commission will be used by the Department of Codes Administration to determine compliance, both in the issuance of permits for construction and field inspection. Significant deviation from these plans may require reapproval by the Planning Commission and/or Metro Council.
10. Prior to any additional development applications for this property, and in no event later than 120 days after the effective date of the enacting ordinance, the applicant shall provide the

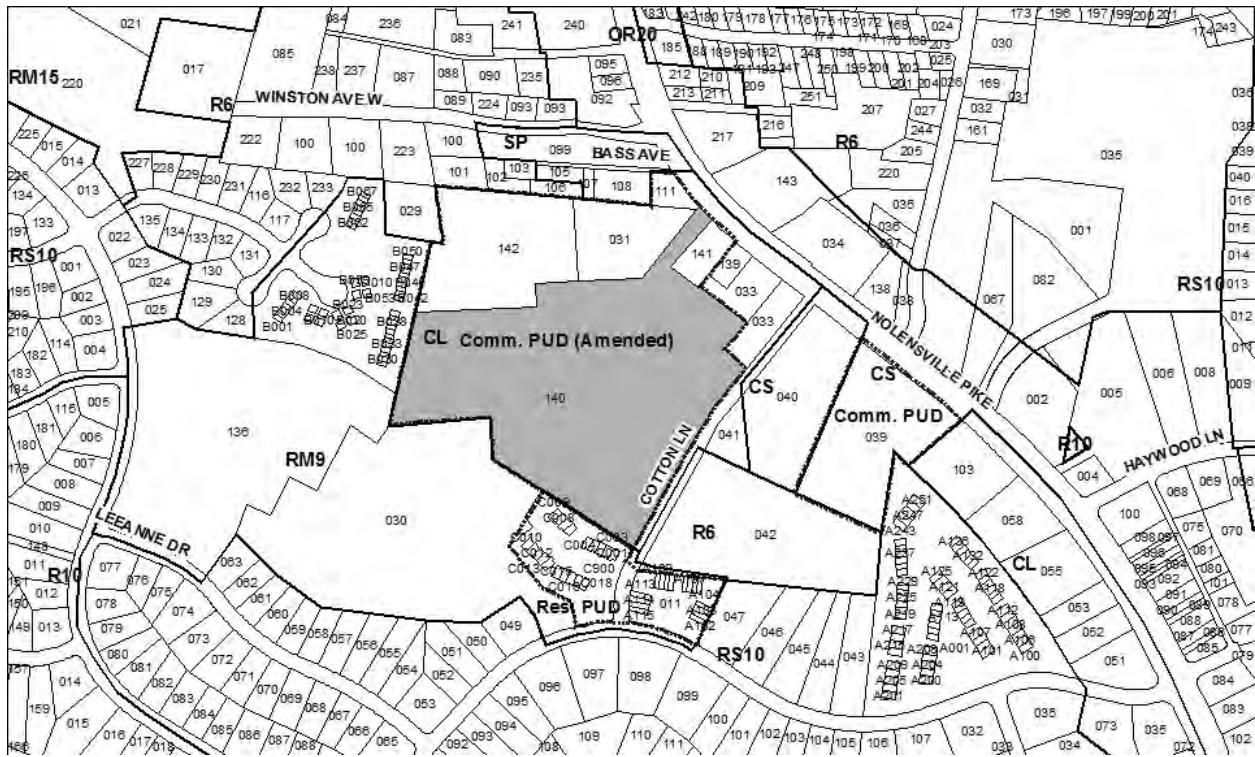


## **Metro Planning Commission Meeting of 04/23/2015**

Planning Department with a corrected copy of the preliminary UDO plan. If a corrected copy of the preliminary UDO plan incorporating the conditions of approval therein is not provided to the Planning Department within 120 days of the effective date of the enacting ordinance, then the corrected copy of the preliminary UDO plan shall be presented to the Metro Council as an amendment to this UDO ordinance prior to approval of any grading, clearing, grubbing, final site plan, or any other development application for the property.



# Metro Planning Commission Meeting of 04/23/2015



**91P-006-002**

THOMPSON STATION (METRO SCHOOLS)

Map 147-11, Parcel(s) 140

12, Southeast

27 (Davette Blalock)



<b>Project No.</b>	<b>Planned Unit Development 91P-006-002</b>
<b>Project Name</b>	<b>Thompson Station (Metro Schools)</b>
<b>Council District</b>	27 – Blalock
<b>School District</b>	2 – Brannon
<b>Requested by</b>	Metro Nashville Public Schools, applicant, JC Sun Investment, Inc., owner.
<b>Staff Reviewer</b>	Swaggart
<b>Staff Recommendation</b>	<i>Approve with conditions.</i>

**APPLICANT REQUEST**

**Revise PUD to permit a Metro School.**

Revise PUD & Final Site Plan

A request to revise the preliminary plan for a portion of a commercial PUD and for final site plan approval for property located at 4529 Nolensville Pike, approximately 475 feet north of Cotton Lane (15.09 acres), zoned Commercial Limited (CL), to convert an existing building to Community Education and add green space.

**Existing Zoning**

Commercial Limited (CL) is intended for retail, consumer service, financial, restaurant, and office uses.

Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provision of an adequate roadway system or essential utilities and services. This PUD plan In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provision of essential utilities and streets.

**CRITICAL PLANNING GOALS**

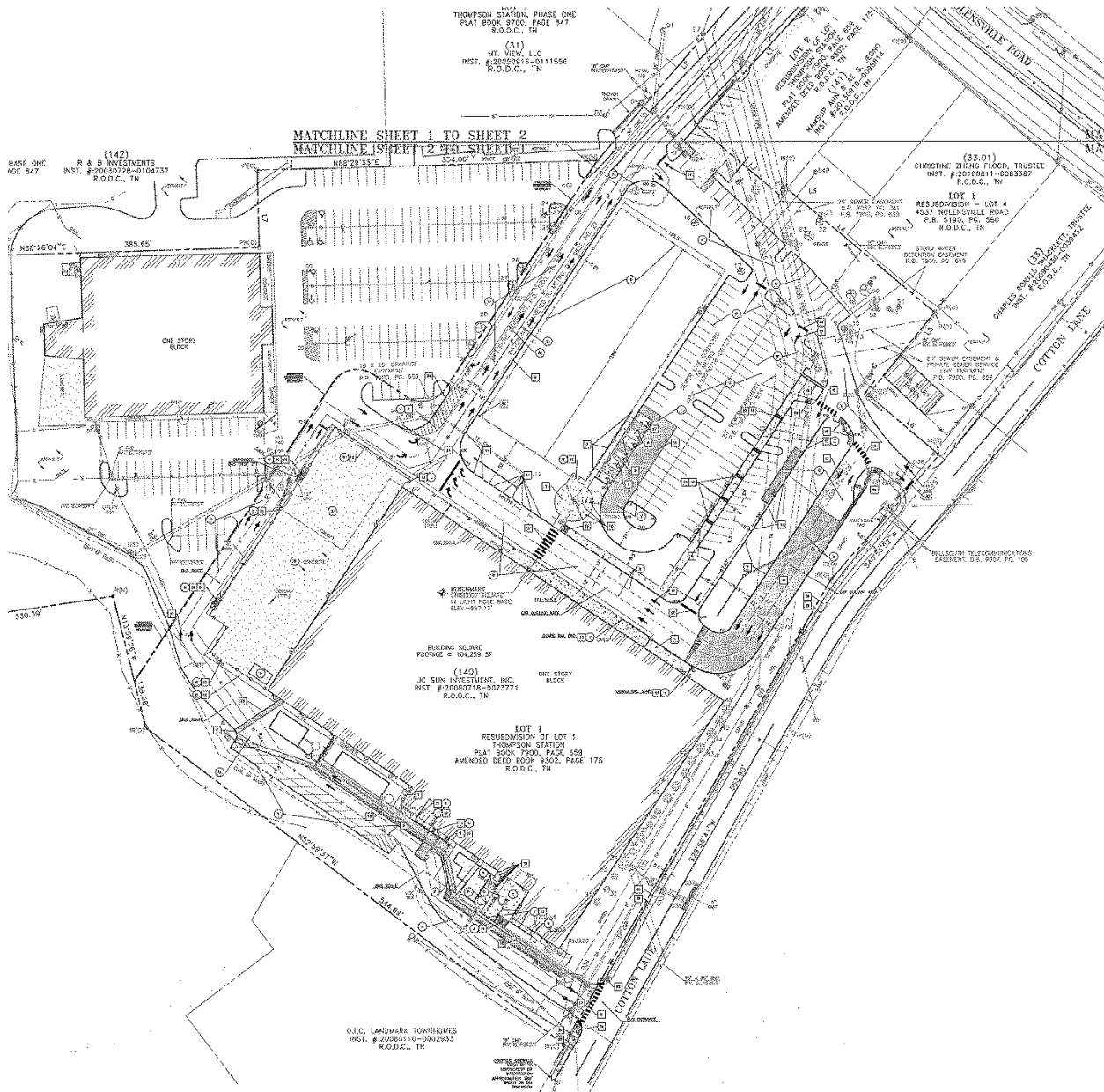
N/A

**PLAN DETAILS**

The subject PUD is located on the west side of Nolensville Pike boarding Cotton Lane to the south. The PUD was originally approved in 1991 for various commercial uses. The PUD has been revised several times in the past. The original PUD boundary was reduced in size in 2002 when Council approved the cancelation of three properties within the overlay, located to the north along Bass Avenue. The PUD is approved for 201,540 square feet of various uses. The PUD is currently developed and includes approximately 171,621 square feet of floor space consisting of various commercial uses. A revision to the PUD was recently approved for another property in the PUD and included an addition that would increase the floor area to approximately 191,194 square feet (when constructed).



# Metro Planning Commission Meeting of 04/23/2015



**Proposed Site Plan**





## Metro Planning Commission Meeting of 04/23/2015

### Site Plan

The plan calls for the retail building, formerly Lowes, to be converted to a Metro School. The request does not propose any expansion of the building footprint. The primary changes include the redesign of the existing parking lot. As proposed the parking area will be redesigned to include student drop-off/pick-up, parking and green space.

### **ANALYSIS**

Staff finds that the proposed revision is consistent with the Council approved PUD plan. The proposal does not call for a use that is not permitted today, increase the floor area over what is permitted or make changes to the layout that significantly deviate from the Council approved PUD plan. Since the request does not propose any major changes to the Council approved PUD plan, then staff finds the request can be approved as a minor modification not requiring Council approval.

Section 17.40.120.G permits the Planning Commission to approve “minor modifications” under certain conditions. Staff finds that the request is consistent with all the requirements of Section 17.40.120.G, which is provided below for review.

G. Status of Earlier Planned Unit Developments (PUDs). The following provisions shall apply to a planned unit development (PUD) approved under the authority of a previous zoning code and remaining a part of the official zoning map upon the enactment of this title.

1. The planned unit development (PUD) shall be recognized by this title according to the master development plan and its associated conditions specified in the PUD ordinance last approved by the metropolitan council prior to the effective date of the ordinance codified in this title.
2. The planning commission may consider and approve minor modifications to a previously approved planned unit development subject to the following limitations. All other modifications shall be considered by the planning commission as an amendment to the previously approved planned unit development and shall be referred back to the council for approval according to the procedures of Section 17.40.120(A)(5). That portion of a planned unit development master plan being amended by the council shall adhere to all provisions of this code:
  - a. In the judgment of the commission, the change does not alter the basic development concept of the PUD;
  - b. The boundary of the planned unit development overlay district is not expanded;
  - c. There is no change in general PUD classification (e.g. residential to any classification of commercial or industrial PUD; any change in general classification of a commercial PUD; or any change in general classification of an industrial PUD);
  - d. There is no deviation from special performance criteria, design standards, or other specific requirements made part of the enacting ordinance by the council;
  - e. There is no introduction of a new vehicular access point to an existing street, road or thoroughfare not previously designated for access;
  - f. There is no increase in the total number of residential dwelling units originally authorized by the enacting ordinance;
  - g. There is no change from a PUD approved exclusively for single-family units to another residential structure type;
  - h. The total floor area of a commercial or industrial classification of PUD shall not be increased more than ten percent beyond the total floor area last approved by the council;



## Metro Planning Commission Meeting of 04/23/2015

- i. If originally limited to office activities, the range of permitted uses in a commercial PUD shall not be expanded to broader classifications of retail, commercial or industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- j. If originally limited to office, retail and other general commercial activities, the range of permitted uses in a commercial PUD shall not be expanded to include industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- k. If originally limited to commercial activities, the range of permitted uses in a commercial PUD shall not be expanded to broader classifications of retail, commercial or industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- l. In the determination of the commission, the nature of the change will have no greater adverse impact on those environmentally sensitive features identified in Chapter 17.28 of this code than would have occurred had the development proceeded in conformance with the previous approval.
- m. In the judgment of the commission, the planned unit development or portion thereof to be modified does not meet the criteria for inactivity of Section 17.40.120.H.4.a.

### **FIRE MARSHAL'S OFFICE**

#### **Approve with Conditions**

- Fire Code issues for the structures will be addressed at permit application review.

### **PUBLIC WORKS RECOMMENDATION**

#### **No Exceptions Taken**

### **TRAFFIC & PARKING RECOMMENDATION**

#### **Approve with Conditions**

- Arrival and dismissal plan and signage, striping and signal plans shall be required with construction documents.

### **STORMWATER RECOMMENDATION**

#### **Approve with conditions**

Pending final grading plans approval.

### **WATER SERVICES**

#### **Approve**

### **STAFF RECOMMENDATION**

Staff recommends approval with conditions.



## Metro Planning Commission Meeting of 04/23/2015

### CONDITIONS

1. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Stormwater Management division of Water Services.
2. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Traffic Engineering Sections of the Metro Department of Public Works for all improvements within public rights of way.
3. This approval does not include any signs. Signs in planned unit developments must be approved by the Metro Department of Codes Administration except in specific instances when the Metro Council directs the Metro Planning Commission to review such signs.
4. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
5. If the PUD final site plan or final plat indicates that there is less acreage than what is shown on the approved preliminary plan, the final site plan shall be appropriately adjusted to show the actual total acreage, which may require that the total number of dwelling units or total floor area be reduced.



**SEE NEXT PAGE**



# **EXHIBIT A**

# Nashville

## DOWNTOWN CODE

### **PROPOSED TEXT AMENDMENT TO DTC - EXHIBIT**

2015Z-003TX-001

**PROPOSED CHANGES ARE SHOWN IN RED**

Amended by Ordinance No. BL2013-376  
as adopted on April 02, 2013

Amended by Ordinance No. BL2012-142  
as adopted on May 21, 2012

Amended by Ordinance No. BL2011-896  
as adopted on May 26, 2011


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Attachment to Ordinance No. BL2009-586  
as adopted on February 02, 2010

## Document Contact Information

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Planning Department staff provides consultations for developing within the Downtown Code. Call (615) 862-7190 or email [planningstaff@nashville.gov](mailto:planningstaff@nashville.gov) to schedule a meeting.

 The Planning Department does not discriminate on the basis of age, race, sex, color, national origin, religion or disability in access to, or operation of, its programs, services, and activities, or in its hiring or employment practices. For ADA inquiries, contact Josie Bass, ADA Compliance Coordinator, at 862-7150 or e-mail her at [josie.bass@nashville.gov](mailto:josie.bass@nashville.gov). For Title VI inquiries contact Shirley Sims-Saldana or Denise Hopgood of Human Relations at 880-3370. For all employment-related inquiries call 862-6640.

# Section I: Introduction

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# Section I: Introduction

# Section I: Introduction

## The Downtown Code and the Downtown Plan

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The Downtown Code (DTC) implements the community vision set forth in the Downtown Community Plan: 2007 Update. Through the community planning process, stakeholders reached a common vision for the future of Downtown. The Plan envisions multiple distinct neighborhoods within Downtown, each with its own character and scale, which contribute to the vitality of Downtown as the center of the city and the region.

The DTC regulates the physical form of buildings to ensure each makes a positive contribution to a complete urban environment. By ensuring a specific and predictable urban form, the DTC ensures that all new construction makes a positive contribution to the public realm – streets and open space – and that all investments are held to the same standard.

While the DTC is a regulating document, the Downtown Plan contains placemaking tools and guidelines and should be used as a companion document to the DTC.

The Guiding Principles of the Downtown Plan have directed the standards in the DTC and are as follows:

### **Ensure that Downtown remains the civic, commercial and entertainment center for Nashville, Middle Tennessee and the Southeast.**

- The regulations of the DTC clarify the vision for each neighborhood in Downtown. Under the DTC, certain areas are allowed to have high-rise towers, while others are zoned for neighborhood-scale development, while still others are allowed to be more transitional with support uses necessary to sustain urban business.
- In recent years, nearly all projects in Downtown have sought rezoning or variances to existing zoning. The DTC alleviates this need by allowing significantly greater development rights, in forms based on construction norms and urban design objectives, than existing zoning. The DTC also allows the modification of standards for site-specific issues.

### **Provide opportunities for continued growth while preserving and enhancing the character that inspires residents and businesses to move Downtown.**

- The Downtown Plan called for areas of increased height and density within Downtown. The Plan extended the boundaries of the Core – the tallest and most intense neighborhood – and provided guidance regarding additional height in other neighborhoods. The DTC codifies these opportunities in the subdistrict standards for these neighborhoods.
- Similarly, the Downtown Plan acknowledged the difficulty of developing within the sky exposure plane in some neighborhoods. The DTC modified the allowable form of build-

- ings by allowing additional height at the street in exchange for an overall height-cap. The result is that properties, and thus neighborhoods, will have the same intensity as in CF zoning but in a form that is aligned with typical construction methods and creates a more predictable urban environment.
- The form and shape of development under CC and CF zoning is unpredictable. The DTC provides clear direction on minimum development and maximum development. For instance, the Gulch is zoned for a specific character, while Sulphur Dell is zoned for something different. This ensures certainty about the character and scale of development in each neighborhood.
- In an urban environment, the street level design and function of a building is of the utmost importance. The interaction of the building with the street should enliven the street, making it comfortable, safe and interesting for pedestrians. The DTC is based on frontage design – storefront, stoop, porch, industrial, and civic – and includes standards on glazing, vehicular access, landscaping, and active uses on the ground level. Correctly designed, these attributes will contribute to safe and interesting streets to result in vibrant neighborhoods and a healthy Downtown.
- A safe and interesting urban environment attracts people. People who feel comfortable in Downtown – enjoying available activities, prospering from the businesses and services, and lingering in the spaces and places – want to be Downtown. The DTC fosters this desirable urban environment through the attention to pedestrian-oriented design, appropriate scale and massing, and neighborhood creation.

# Section I: Introduction

## The Downtown Code and the Downtown Plan

### Create strategic mixed use to facilitate Downtown's transformation into a 24/7 community.

- The Plan calls for a “thoughtful mixture of uses including residential, retail and office, to ensure that Downtown doesn't close at 5 p.m. or even after the concert ends or the restaurants close, but is instead a welcoming home for a diverse residential population.” Multiple stories encourage multiple uses within each building, increasing the likelihood that one of the uses will be active at any given hour. Compare this to one story buildings which lack activity during the off-hours of the single use.
- To be viable, Downtown must accommodate numerous services and functions. By promoting form over use, the DTC allows for a mixture of uses. Because various functions occur at different times of the day – working, shopping and recreating during the day, entertainment and home-life at night – mixed-use, 24/7 neighborhoods use existing infrastructure more efficiently and function in a more sustainable way.

### Create and nurture urban neighborhoods.

- As stated in the Plan, “While residential *development* has flourished in recent years, the creation and enhancement of urban *neighborhoods* is still a goal. Residential living will thrive in Downtown when residents feel that they are part of a neighborhood, supported by shared public spaces and the services and amenities needed for daily life.” The DTC directly addresses the development characteristics of neighborhood design: building mass and scale, frontage design, and open space design.
- To create these distinctive urban neighborhoods, the DTC aligns the regulations of each subdistrict with the intended character of the neighborhood. For instance, the South Gulch is envisioned to continue as a high-rise and mid-rise, mixed-use neighborhood. The DTC codifies mid-rise height in the general subdistrict and allows high-rise buildings on key intersections and along important streets. In contrast, the North Gulch is envisioned to be a low-rise neighborhood – to preserve Capitol views and transition into the Hope Gardens and John Henry Hale neighborhoods. The DTC codifies this vision by capping the overall

height, allowing for less intense development such as two story houses and townhouses, and encouraging porch and stoop frontages. These are two examples of how the DTC aligns the zoning of neighborhoods with the vision cast during the community planning process.

- “Since 2000, Downtown has experienced an unprecedented residential boom. In 2000, there were approximately 1500 dwelling units in Downtown. As of 2006, over 2600 new residential units were under construction, planned or proposed... Since 2000 there has been over \$500 million in capital investments in residential construction, with another \$400 million planned or proposed.” In addition, “since 2000, Downtown office building development has grown modestly with \$140 million of development completely or under construction.” These strong numbers increase the viability of Downtown. To continue this good momentum, the emphasis must be on place-making as well as development. The DTC encourages the creation of mixed-use, sustainable neighborhoods that have flexibility to address the needs of citizens over time.

### Create active, attractive streets and streetscapes.

- “...the Downtown Plan encourages walking as a primary mode of transportation in Downtown. Walking is encouraged in the Downtown Plan by making the walk safe, interesting, and comfortable...” Streets are the most plentiful open space in Downtown and should prioritize the pedestrian experience while appropriately accommodating vehicular traffic. The DTC emphasizes frontage design, requires active ground level uses, and sets standards for vehicular activity.
- The DTC prioritizes the location of vehicular access points per street type – Primary, Secondary, Tertiary, Other, and Alley. By prioritizing, instead of regulating, the DTC allows flexibility for site-specific solutions to be reached in collaboration between the developer and Metro departments.
- While the DTC sets standards on the development of private property, active and attractive streets must be created by a collaborative process with all Metro Departments. As development proposals are offered, Metro will retain a commitment to creating a strong pedestrian-oriented urban

# Section I: Introduction

## The Downtown Code and the Downtown Plan

environment. “As Downtown becomes home to many of its workers, and mass transit options into Downtown improve, providing welcoming routes for pedestrians will provide benefits in terms of improved mobility and reduced traffic congestion.”

- The DTC references the *Downtown Streetscape Design Guidelines*, and the *Major and Collector Street Plan including constrained right-of-ways and local streets*, created by Metro Public Works and Metro Planning, and **encourages requires** their use.

### Protect and reuse historic structures and districts.

- The DTC has a subdistrict for 2<sup>nd</sup> Avenue and Lower Broadway that reinforces the historic zoning overlay for these streets. To encourage the adaptive reuse of the historic structures, this subdistrict is eligible for transfer of development rights through the Bonus Height Program.
- The creation of the Core Historic subdistrict encourages the preservation of the existing historic buildings between 3<sup>rd</sup> and 5<sup>th</sup> Avenues North, while allowing for appropriately scaled and appropriately detailed infill.
- The creation of the Rutledge Hill subdistrict encourages the preservation of the existing historic buildings in this historic residential area, while allowing for appropriately scaled and appropriately detailed infill.
- During the community planning process, the preservation of views to the Capitol building was identified as important to the development of neighborhoods north of Capitol Hill. The maximum building heights in subdistricts north of Capitol Hill are limited to the elevation of the base of the Capitol building to ensure this that this important civic view is preserved.

### Create environmentally sustainable and energy efficient development.

- In order to meet the sustainability goals of the Downtown Plan and to achieve Metro Government’s vision of Nashville as the greenest city in the US, the DTC encourages urban infill and energy efficient development.
- By location alone, urban infill is more energy efficient than green-field development because it utilizes existing streets and infrastructure. By emphasizing mixed-use, walkable neighborhoods within Downtown, the DTC reinforces Metro Government’s commitment to sustainability and

responsible use of resources. Part of being sustainable is using existing under-utilized infrastructure – water lines, sewer lines, electricity, and streets – in lieu of creating new infrastructure and continuing green-field consumption.

- Location, however, does not ensure sustainability. The creation of the DTC emphasizes the need for a denser Downtown to provide citizens with all daily needs within walking distance, the need for buildings that can be adapted for new uses over time, the preservation and adaptive reuse of existing buildings, and the addition of street trees and open spaces.
- In addition to encouraging sustainable urbanity, the DTC encourages the measurement of energy efficient development through the US Green Building Council’s LEED program. (LEED may be substituted for a different nationally-recognized, third-party system of overseeing green building and/or sustainable development practices.)
- The Bonus Height Program of the DTC provides height bonuses for new construction that meets the standards of LEED silver, gold or platinum. LEED takes credits a project for its urban environment, but also for the sustainability of the building itself. The public benefit associated with LEED certification is significant. The lighter the building treads on the infrastructure of the city, the lighter the burden on the city to maintain the infrastructure. This helps the city function better as a whole. The bonuses are appropriately scaled for each subdistrict and are offered in exchange for the level of contribution of this important public benefit.

### Create “great spaces” throughout Downtown for the enjoyment of citizens and visitors.

- In some areas of Downtown, open space is appropriately scaled and designed for the envisioned intensity of the neighborhood. In most areas, however, open space is dramatically lacking. The DTC identifies ¼ mile radius neighborhoods (about a five minute walk from edge to center) within Downtown to show the open space deficiencies. For every quarter mile neighborhood, there should be at least a quarter acre of well-designed public open space. This open space may be provided by public or private initiatives.
- The Downtown Plan recommended that Downtown have unique types of open space available to meet the needs

# Section I: Introduction

## The Downtown Code and the Downtown Plan

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of citizens. The DTC provides standards for the creation of these open spaces: greens, squares, plazas, courts and pocket parks/playgrounds. The open spaces will serve as important “great spaces” to help create the vital and functioning neighborhoods envisioned by the Downtown Plan.

- The Bonus Height Program of the DTC provides height bonuses for the development of public open spaces. To be eligible for the height bonus, the development must follow the standards for open space design. The bonuses are appropriately scaled for each subdistrict and are offered in exchange for this important public amenity.

### **Provide for improved mobility in and through Downtown to support other principles for healthy growth in Downtown.**

- “The Downtown Plan encourages walking as a primary mode of transportation in Downtown.” The DTC standards focus on the interaction between the building and street – the frontage of the building – to make the pedestrian realm safe, comfortable and interesting. This goal will be reached when public and private entities remain committed to creating a pedestrian-oriented Downtown.
- The Downtown Plan notes that “as Downtown becomes home to many of its workers, and mass transit options into Downtown improve, providing welcoming routes for

pedestrians will provide benefits in terms of improved mobility and reduced traffic congestion.” While the DTC sets standards on the development of private property, improved transportation options must be created by a collaborative process between the public and private sectors.

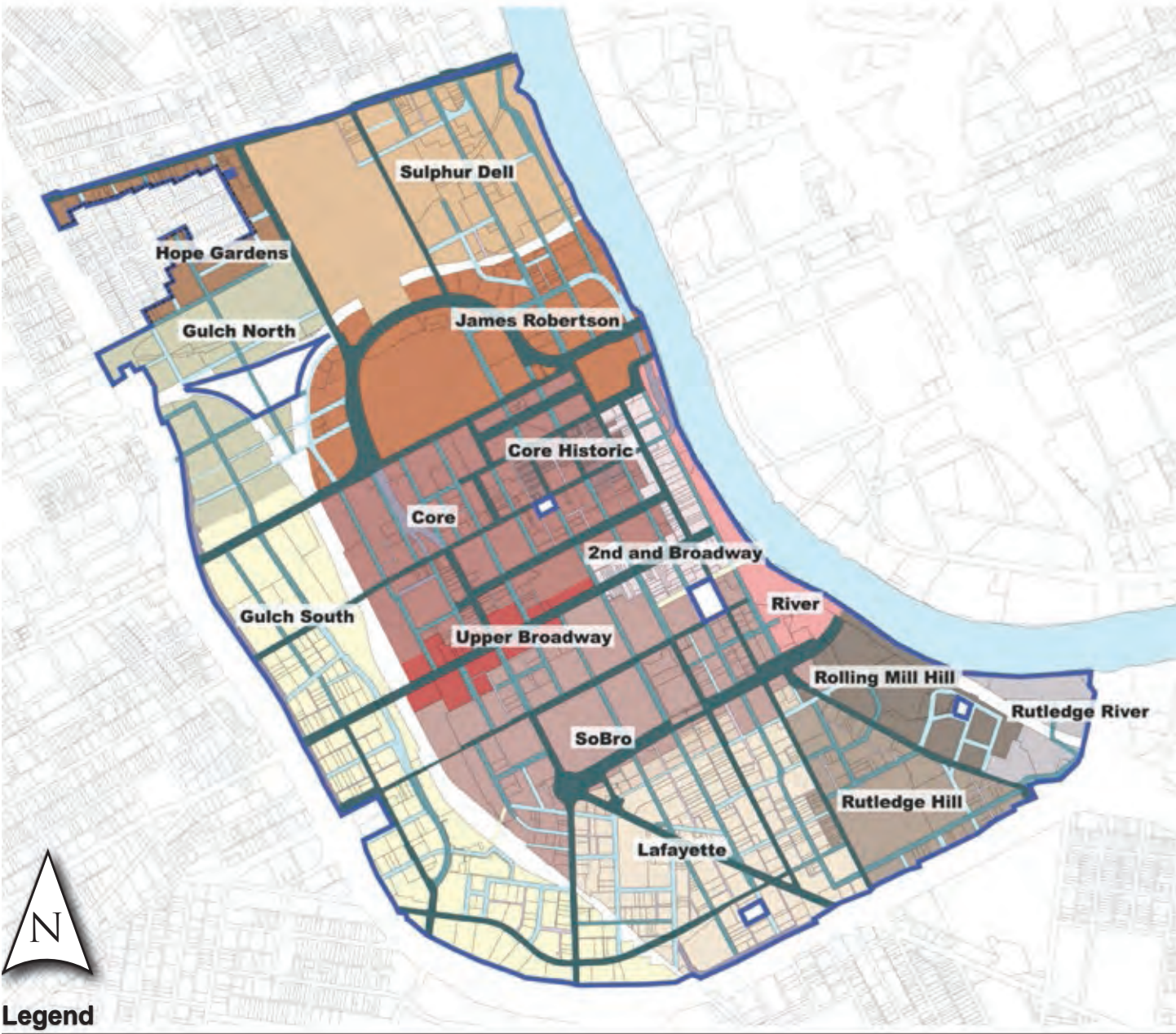
The Downtown Plan sets forth the common vision for the future of Downtown, and acknowledges that “the creation of the Downtown envisioned by the community can only be achieved through cooperative efforts of the public and private sectors and through the informed involvement of residents, businesses and investors in Downtown. Adherence to these guiding principles in the development actions of both the public and private sector will create the Downtown Nashville that the community has envisioned – an expanding, vibrant Downtown with opportunities for growth and development that embody the urban experience of a great city.” The DTC is one of several tools to strengthen Downtown through public and private investments.

By focusing on the creation of distinctive neighborhoods, pedestrian-oriented development, the DTC reaches toward the goal of an economically healthy, socially vibrant, and sustainable Downtown.

















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# Section I: Introduction

## DTC Regulating Plan: Subdistrict Boundaries



### Legend

 James Robertson Subdistrict - page 18	 Upper Broadway Subdistrict - page 26	 Lafayette Subdistrict - page 38	 Gulch North Subdistrict - page 46
 Core Subdistrict - page 20	 SoBro Subdistrict - page 34	 Rutledge Hill Subdistrict - page 40	 Hope Gardens Subdistrict - page 50
 Core Historic Subdistrict - page 22	 River Subdistrict - page 32	 Rutledge River Subdistrict - page 44	 Sulphur Dell Subdistrict - page 52
 2nd and Broadway Subdistrict - page 28	 Rolling Mill Hill Subdistrict - page 42	 Gulch South Subdistrict - page 48	 DTC Boundary



# Section I: Introduction

## Application of the DTC

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### General Provisions

If necessary, to adhere to the laws and regulations of Federal, State, or local departments or agencies, the regulations in this chapter may be modified. Such modifications may be approved by the Planning Commission, the DTC Design Review Committee or Planning Staff, in accordance with the Modifications section of this Chapter.

To the extent that the provisions of the Downtown Code is inconsistent or in conflict with the provisions of the Gateway Urban Design Overlay District that is also zoned DTC, the provisions of the DTC zoning shall be controlling; however, any provisions of the Gateway UDO may be used provided that the standards of the DTC zoning are met.

### Applicable Chapters and Sections of the Zoning Code

In addition to the standards set forth within this document, the following Chapters and Sections of the Metro Zoning Code shall apply to properties with DTC zoning.

- All of Chapter 17.04 GENERAL PROVISIONS AND DEFINITIONS
- Within Chapter 17.08 ZONING DISTRICTS AND LAND USE TABLES
  - Section - 17.08.010 Zoning districts established.
  - Section - 17.08.020 Zoning districts described.
  - Section - 17.12.120 Transfer of development rights.
- All of Chapter 17.16 LAND USE DEVELOPMENT STANDARDS
- Within Chapter 17.20 PARKING, LOADING AND ACCESS
  - Section - 17.20.050 Handicapped parking.
  - Section - 17.20.060 Parking area design standards.
  - Section - 17.20.070 Queuing requirements for drive-through facilities.
  - Section - 17.20.130 Loading space requirements.
- Within Chapter 17.24 LANDSCAPING, BUFFERING AND TREE REPLACEMENT
  - Section - 17.24.010 Purpose and intent.
  - Section - 17.24.020 Landscape plan required.
  - Section - 17.24.030 Standards for form and quality of plants.
  - Section - 17.24.040 Spacing standards.
- Article II. Tree Protection and Replacement
  - Section - 17.24.090 Removal of protected trees.
  - Section - 17.24.100 Replacement of trees.
  - Section - 17.24.110 Protection of trees during development activities.
  - Section - 17.24.120 Less desirable trees.
  - Section - 17.24.160 Interior planting requirements.
  - Section - 17.24.170 Nonconforming parking areas.
- All of Chapter 17.28 ENVIRONMENTAL AND OPERATIONAL PERFORMANCE STANDARDS
- All of Chapter 17.32 SIGN REGULATIONS
- All of Chapter 17.36 OVERLAY DISTRICTS, except Article XII, Urban Zoning Overlay (UZO) District.
- All of Chapter 17.40 ADMINISTRATION AND PROCEDURES, except as otherwise provided for within this document.

# Section I: Introduction

## Application of the DTC: How to Use this Document

### How to Use this Document

The Downtown Code is organized by Subdistricts and Street Types, as identified on the Regulating Plan.

To determine the standards which apply to a particular property:

- On the Regulating Plan, identify the Subdistrict in which the property is located and on what type of street(s) it fronts.
- Consult the Building Regulations for development standards relevant to the Subdistrict.
- Consult the Use Table for uses allowed in each Area.
- Consult the General Standards section for guidance on development standards for all Subdistricts.

### Subdistricts and Areas

- Downtown Nashville consists of numerous neighborhoods with unique character. The development standards for the DTC are organized by Subdistricts, which establish

the zoning of each neighborhood to create or maintain the envisioned character.

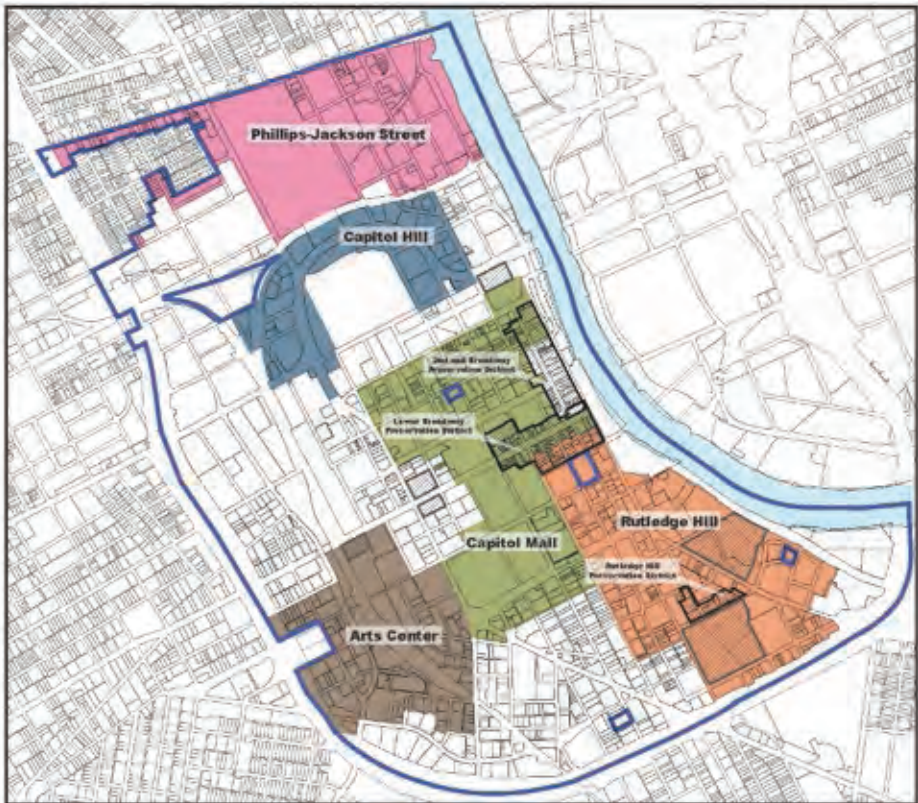
- The DTC is divided into 4 Areas (North, South, West and Central), which establish the allowed uses.
- The DTC includes General Standards that apply to all Subdistricts.

### Regulating Plan

- The Regulating Plan is the official zoning map of the DTC. The Subdistrict boundaries are shown on the series of maps which comprise the Regulating Plan.

### Overlapping Plans

Within the area governed by the DTC, there exist other regulations and design guidelines intended to work in conjunction with the DTC. The DTC does not exempt development from complying with the regulations of other Federal, State, and Local departments and agencies. These departments and agencies should be contacted during the development process to address their rules, regulations and policies.



*MDHA redevelopment districts and Historic overlays at the time of adoption of the DTC.*

# Section I: Introduction

## Application of the DTC: Modifications and Design Review

### Modifications to the Standards

Based on site-specific issues, an applicant may seek modifications to the standards of this document. Any standard within the DTC may be modified, insofar as the intent of the standard is being met, the modification results in better urban design for the neighborhood as a whole, and the modification does not impede or burden existing or future development of adjacent properties.

The DTC, the Downtown Plan and any other policies and regulations from governing agencies shall be consulted when considering modifications. Any standards that shall not be modified are explicitly noted in this document.

Modifications may be approved by Planning staff, the Downtown Code Design Review Committee (DTC DRC) or the Planning Commission.

- Minor modifications – deviations of 20 percent or less – may be approved by Planning Staff.
- Any determination made by the Planning Staff may be appealed to the DTC DRC.
- Major modifications – deviations of more than 20 percent – and modifications of standards without numbers may be approved by the DTC DRC. Within MDHA redevelopment districts, the MDHA DRC shall act as the DTC DRC.
- Any determination made by the DTC DRC or the MDHA DRC regarding standards of the DTC may be appealed to the Planning Commission.
- **No modification shall be granted for overall building height.**

### Variances and special exceptions

Variances and special exceptions that are not specifically for standards of the DTC zoning district shall follow the procedures of Chapter 17.40, Articles VII and VIII of the zoning code.

Additionally, ~~variances and special exceptions~~ to the DTC standards on ~~height at the street and~~ overall height shall follow the procedures of Chapter 17.40, Articles VII and VIII. The DTC DRC shall provide a recommendation to the Board of Zoning Appeals (BZA) on such cases.

Standards specific to the DTC zoning district may be modified based on the modifications section of this document.

### Design Review Committee

Applications that meet all applicable standards of the DTC shall be reviewed by staff before building permits are granted. Such “by-right” applications will not be reviewed by the DTC DRC. The DTC DRC will review applications seeking modifications.

The DTC DRC is subject to the rules and procedures adopted by the Planning Commission. The DTC DRC will consist of eight (8) voting members.

One member shall be nominated by each of the following with confirmation by the Planning Commission:

- Chamber of Commerce
- Civic Design Center
- Downtown Partnership
- Urban Residents Association

One member shall be appointed by the following:

- Mayor
- Vice-Mayor, on behalf of the Metro Council
- Historic Commission
- Planning Commission

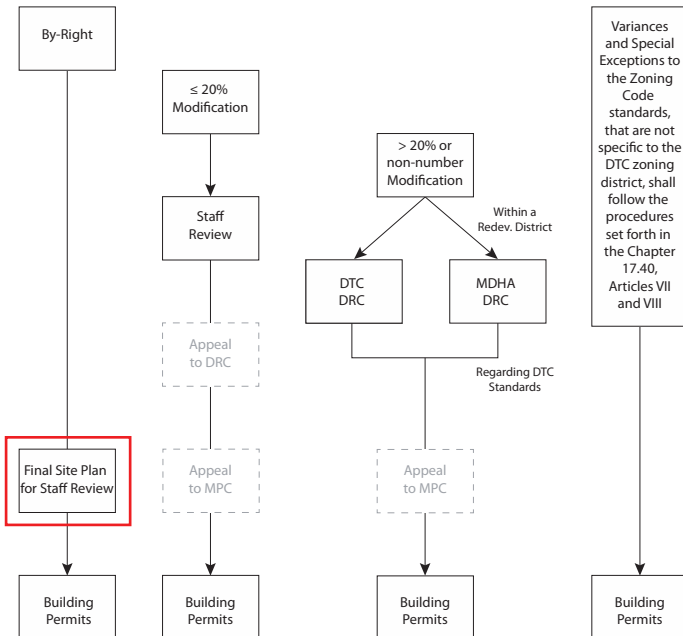
Each member shall be a design professional, with a degree or several years of experience in architecture, landscape architecture, planning or urban design. The members nominated by the Nashville Area Chamber of Commerce and the Nashville Downtown Partnership shall be exempt from this requirement.

Each term shall be four years, with the appointees of the Mayor, Vice-Mayor and Planning Commission serving an initial two-year term upon adoption of the DTC.

**Any vacancy occurring during the unexpired term of any members shall be filled in the manner prescribed herein for the original selection of the members of the DTC DRC.**

# Section I: Introduction

## Application of the DTC: Compliance



### Compliance with the DTC shall be required according to the following:

- **Level 1:** An addition of twenty-five percent or less of the square footage of the existing building
  - To the extent practicable, any addition shall be in compliance with applicable standards of the DTC.
- **Level 2:** An addition of more than twenty-five percent of the square footage of the existing building or 1000 square feet, whichever is greater
  - Improvements to the sidewalk **corridor** according to the *Downtown Streetscape Design Guidelines Major and Collector Street Plan* (including constrained right-of-way and local street dimensions as determined by Metro Planning)
  - Where possible, the addition of street trees
  - Parking lot landscaping standards
  - To the extent practicable, any addition shall be in compliance with applicable standards of the DTC.
- **Level 3:** Redevelopment after the demolition or destruction of more than five percent and less than fifty percent of the existing building
  - Improvements to the sidewalk **corridor** according to the *Downtown Streetscape Design Guidelines Major and Collector Street Plan* (including constrained right-of-way and local street dimensions as determined by Metro Planning)
  - Where possible, the addition of street trees
  - Parking lot landscaping standards
  - To the extent practicable, new construction shall be in compliance with applicable standards of the DTC.
- **Level 4:** Redevelopment after the demolition or destruction of more than fifty percent of the existing building
  - All standards of the DTC
  - Improvements to the sidewalk **corridor** according to the *Major and Collector Street Plan* (including constrained right-of-way and local street dimensions as determined by Metro Planning)
- **Level 5:** New construction of buildings, parking, open space, etc
  - All standards of the DTC
  - Improvements to the sidewalk **corridor** according to the *Major and Collector Street Plan* (including constrained right-of-way and local street dimensions as determined by Metro Planning)

### Signage Compliance

- For those properties within Historic Zoning Overlays, the sign standards of the CF zoning district shall apply, along with historic zoning district design guidelines.
- For those properties not within Historic Zoning Overlays, the sign standards of the DTC zoning district shall apply.
- No new billboards are allowed within the DTC boundaries.

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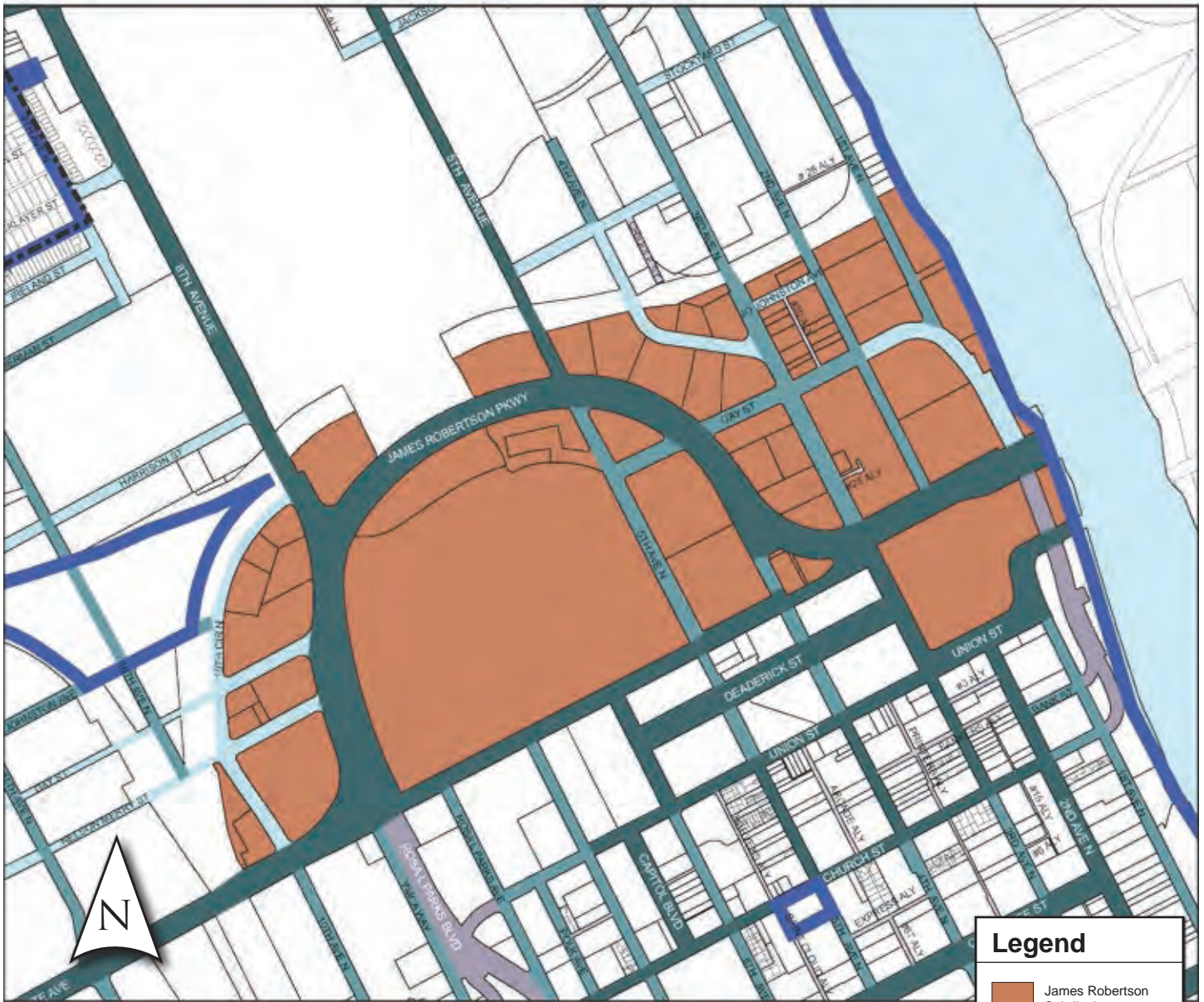
# Section II: Subdistrict Standards

Amended by Ordinance No. BL2012-142  
as adopted on May 21, 2012

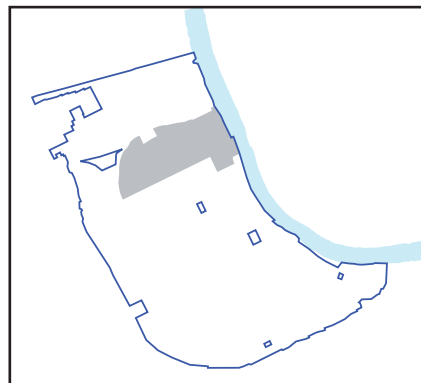
Amended by Ordinance No. BL2011-896  
as adopted on May 26, 2011

# Section II: Subdistrict Standards

## James Robertson Subdistrict: Regulating Plan



The James Robertson Subdistrict is the primarily civic area surrounding Capitol Hill. The Downtown Plan emphasizes “preserving the James Robertson neighborhood’s treasured civic and open space resources, while encouraging redevelopment to offer a mixture of uses...while recognizing that the area’s uses will continue to be dominated by government activities.” The importance of the State Capitol as a Nashville and Tennessee landmark also warrants maintaining views of the Capitol from all vantage points to the west, north and east. The DTC allows a maximum height of 560’ above sea level (the elevation of the base of the capitol building) to preserve these views.



Legend	
	James Robertson Subdistrict
	Primary Street
	Secondary Street
	Tertiary Street
	Other Street
	Alley

# Section II: Subdistrict Standards

## James Robertson Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

##### Primary Street

- Storefront Frontage
  - James Robertson Boulevard
    - West of 3rd Ave 20'-30'
    - East of 3rd Ave 0'-10'
  - Charlotte Avenue 0'-10'
- Stoop Frontage
  - James Robertson Boulevard
    - West of 3rd Ave 20'-30'
    - East of 3rd Ave 5'-10'
  - Charlotte Avenue 5'-10'

##### Secondary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'

##### Tertiary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'

#### B Facade width

- Primary Street 80% of lot frontage min.
  - Secondary Street 80% of lot frontage min.
  - Tertiary Street 60% of lot frontage min.
- Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

#### C Min. building depth 15' from building facade

A building liner is required surrounding parking structures on the all floors facing James Robertson Blvd.

### Height

#### D Max. elevation of 560'

#### Step-back \*

Step-back required on all public streets and Open Space

#### E Step-back after between 4th and 8th 7 stories

#### F Min. step-back depth 15'

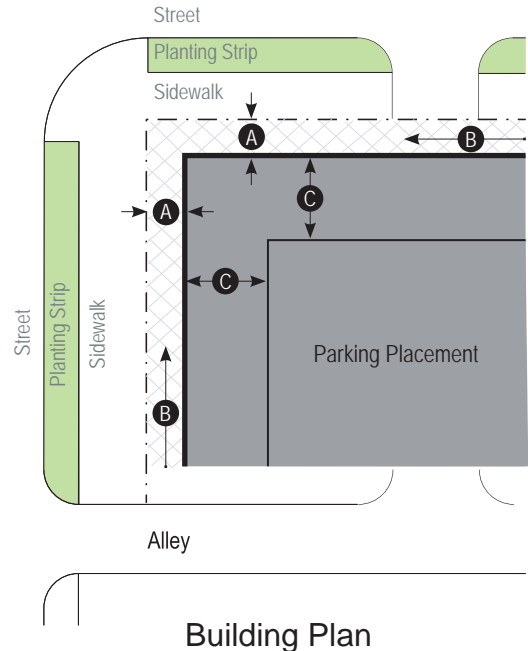
\* see page 61 for full description

### Sidewalk & Planting

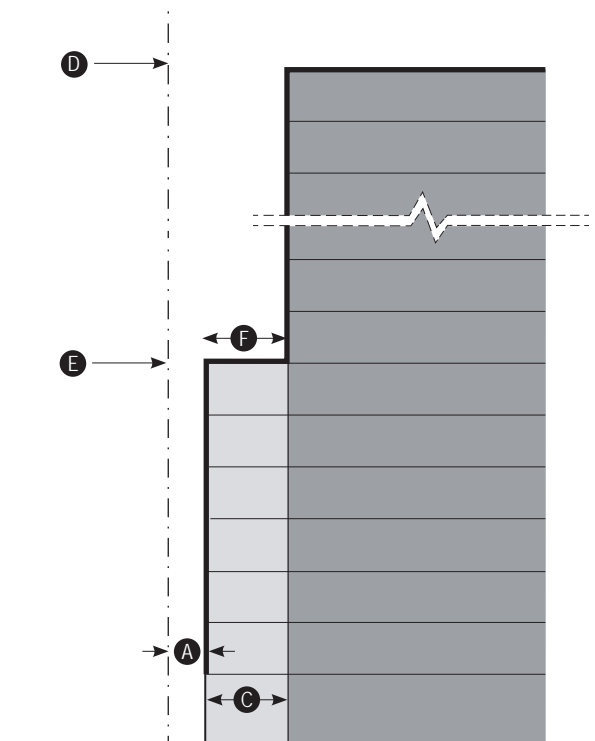
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



Building Plan

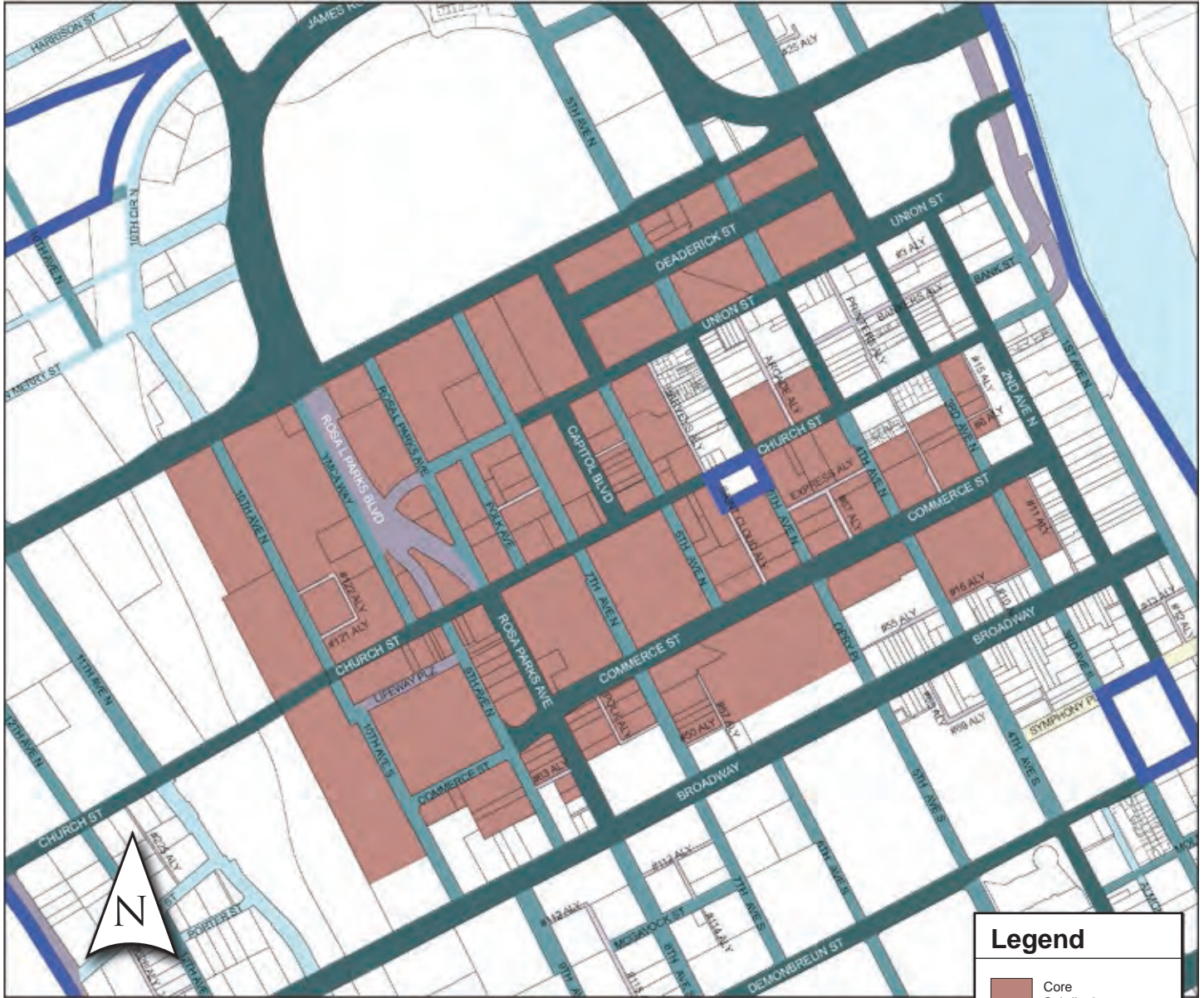


Building Section

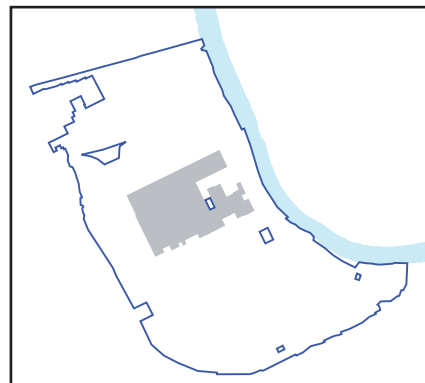


# Section II: Subdistrict Standards

## Core Subdistrict: Regulating Plan



The Core is the heart of the Downtown business district, the economic engine of the Middle Tennessee region, and a significant economic force in the Southeast. It is the densest neighborhood in Downtown and has the greatest height allowances. Pedestrian comfort and safety should be prioritized with an interesting sidewalk realm, activity on the ground level of buildings, and controlled vehicular access.



# Section II: Subdistrict Standards

## Core Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'

#### B Facade width

Primary Street	80% of lot frontage min.
Secondary Street	80% of lot frontage min.
Tertiary Street	60% of lot frontage min.
Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.	

#### C Min. building depth 15' from building facade

### Height

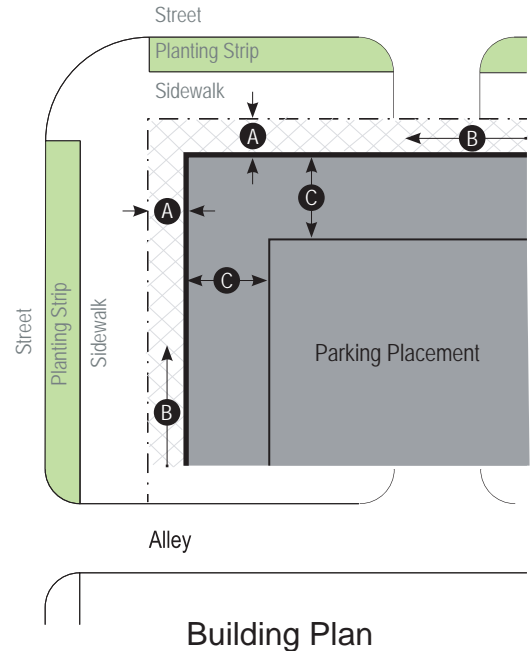
#### D Max. 30 stories Additional height available through the Bonus Height Program

### Sidewalk & Planting

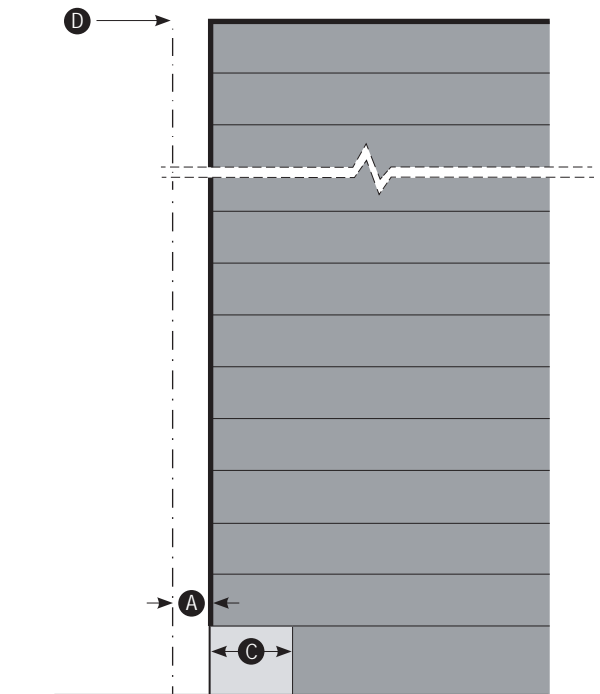
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



Building Plan



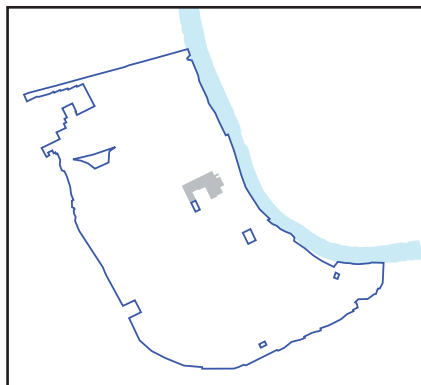
Building Section








# Section II: Subdistrict Standards

## Core Historic Subdistrict: Regulating Plan



The Core Historic neighborhood has two historic urban spaces – the Arcade and Printers’ Alley. This neighborhood is comprised several historic buildings, many of which have been recently renovated. The height maximums for this subdistrict reflect historic urban design features – lower buildings mid-block and taller buildings to “book-end” the blocks at the corners. The adaptive reuse of historic buildings is encouraged and new construction should be of appropriate scale and detailing, maintaining the existing storefront rhythm. Pedestrian comfort and safety should be prioritized with an interesting sidewalk realm, activity on the ground level of buildings, and controlled vehicular access.



Legend	
	Core Historic Subdistrict
	Primary Street
	Secondary Street
	Tertiary Street
	Other Street
	Alley
	Pedestrian Frontage

# Section II: Subdistrict Standards

## Core Historic Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'
• Stoop Frontage	5'-10'

#### B Facade width

Primary Street	95% of lot frontage min.
Secondary Street	95% of lot frontage min.

#### C Min. building depth 15' from building facade

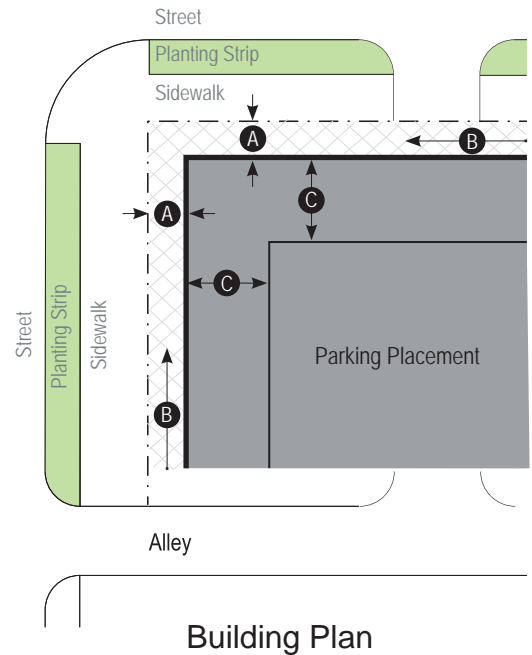
A building liner is required surrounding parking structures on all floors facing public streets and Open Space.

### Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59

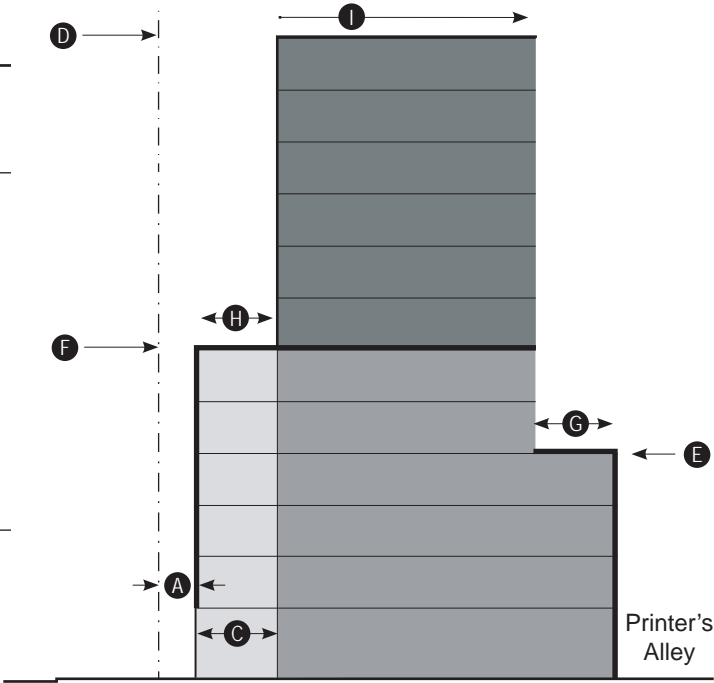


# Section II: Subdistrict Standards

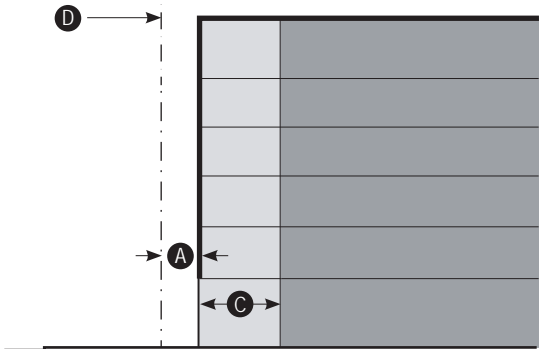
## Core Historic Subdistrict: Building Regulations

**Height**

<b>D Max.</b>		
On Corners		10 stories
Mid-Block		6 stories
<b>Step-back</b>		
Step-back after		
<b>E</b>	• On Printer's Alley	4 stories
<b>F</b>	• All Others	6 stories
Depth		
<b>G</b>	• On Printer's Alley	10' min.
<b>H</b>	• All Others	10' min. and max.
Step-back not required for buildings fronting Church Street.		
Buildings 6 stories or less shall not step-back and all stories shall occupy the Build-to Zone.		
<b>I Max. tower dimensions</b>		90' x 90'



Block Corner Section



Mid-Block Section

# Section II: Subdistrict Standards

## Core Historic Subdistrict: Building design and Facade articulation

### Building design and Facade articulation

- |                                       |  |
|---------------------------------------|--|
| <b>L</b> Primary building divisions   | 20'-50' wide   |
| <b>M</b> Secondary building divisions | 5'-25' wide<br>Secondary building divisions are defined by solid vertical elements that consist of changes in materials or planes within the facade. |
| <b>N</b> First Floor height           | 16' min.   |
- Windows**
- O** Ground floor - 60% glazing required from 2 feet above grade to the finished floor of the 2nd story
  - P** Window sill height 18"-24"
- Q** Upper Floors - Windows shall be vertically oriented at a ratio of 2:1 or greater.

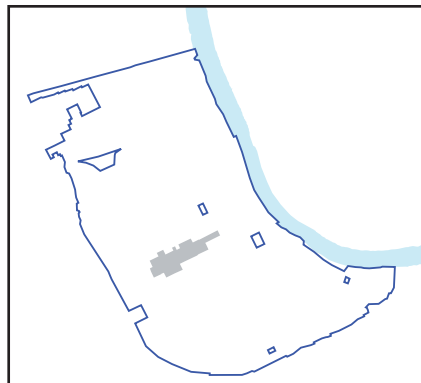


# Section II: Subdistrict Standards

## Upper Broadway Subdistrict: Regulating Plan



The Upper Broadway area is one of the most important gateways into Downtown. Several civic and cultural buildings front this urban corridor. Maintenance and adaptive reuse of historic buildings is encouraged, and the height and scale of new buildings should be in-keeping with the existing urban pattern. When properties front more than one street, Broadway should be the Principal frontage.



### Legend

- Upper Broadway Subdistrict
- Primary Street
- Secondary Street
- Other Street
- Alley

# Section II: Subdistrict Standards

## Upper Broadway Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

- |                       |       |
|-----------------------|-------|
| Primary Street        |       |
| • Storefront Frontage | 0-5'  |
| Secondary Street      |       |
| • Storefront Frontage | 0'-5' |

\*\* Civic Frontages are encouraged in this subdistrict.

#### B Facade width

- |                  |                          |
|------------------|--------------------------|
| Primary Street   | 80% of lot frontage min. |
| Secondary Street | 80% of lot frontage min. |
- Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

#### C Min. building depth 15' from building facade

A building liner is required surrounding parking structures on all floors facing Broadway and Open Space.

### Height

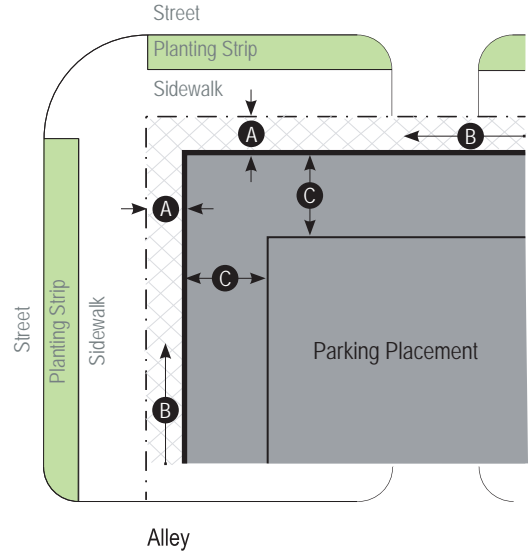
- |        |      |
|--------|------|
| D Max. | 100' |
|--------|------|

### Sidewalk & Planting

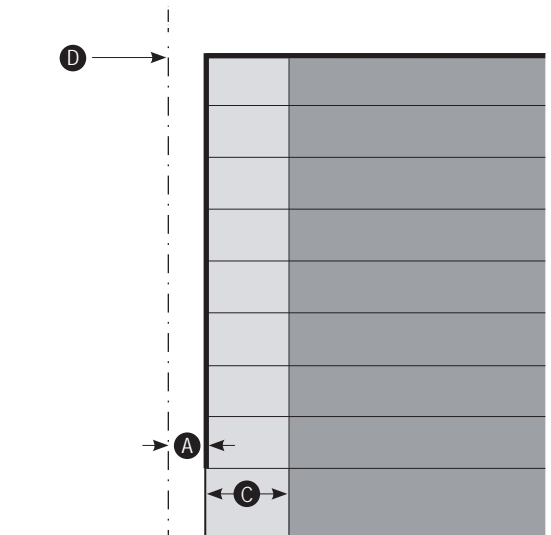
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



Building Plan

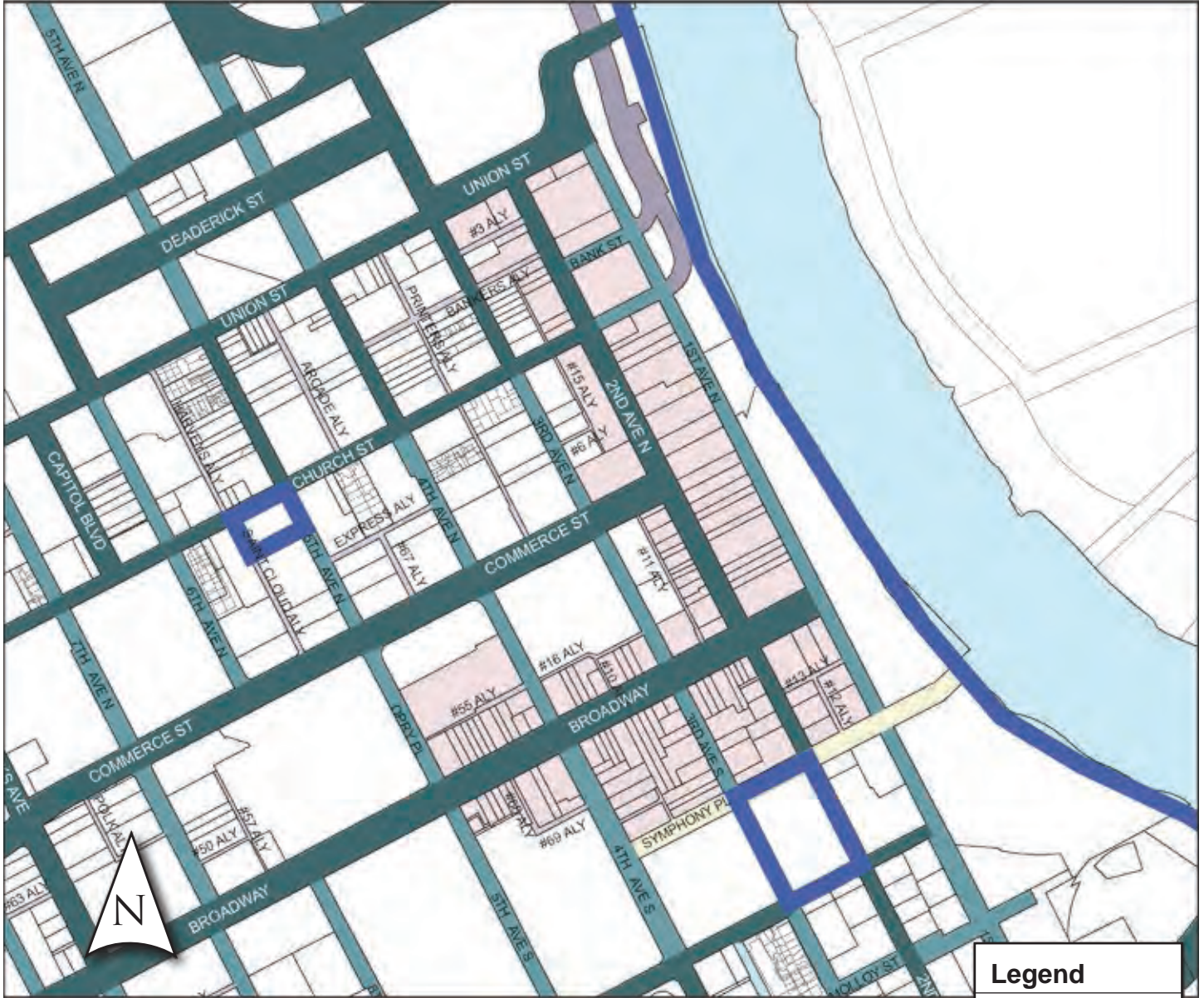


Building Section

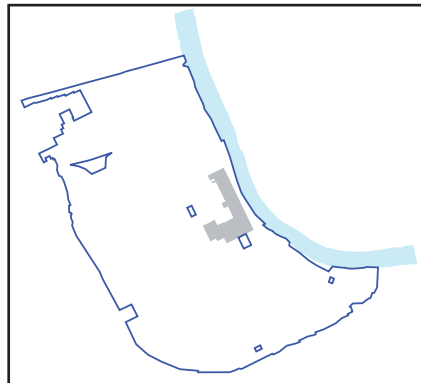


# Section II: Subdistrict Standards

## 2nd and Broadway Subdistrict: Regulating Plan



Second and Broadway is the heart of Downtown; where the main street of the city meets the Cumberland River. The Downtown Plan encourages maintaining “the low-scale, pedestrian-friendly historic character” and adaptively reusing existing historic structures “in order to respect, maintain, and enhance not only individual structures, but the existing character of the Second and Broadway neighborhood as a whole.” With the exception of the Stahlman building on the northeast corner of 3rd and Union, this neighborhood is overseen by two historic zoning overlays: *The Second Avenue Historic Zoning Overlay* and *The Broadway Historic Zoning Overlay*. Property owners must contact the Metro Historic Commission for additional details.



**Legend**

- 2nd and Broadway Subdistrict
- Primary Street
- Secondary Street
- Other Street
- Alley
- Pedestrian Frontage

# Section II: Subdistrict Standards

## 2nd and Broadway Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'
Secondary Street	
• Storefront Frontage	0'

#### B Facade width

Primary Street	100% of lot frontage min.
Secondary Street	100% of lot frontage min.

#### C Min. building depth 15' from building facade

### Height

#### D Min. 40'

#### E Max. at the street

• On Broadway	5 stories to a max. height of 65'
• On 2nd Ave	8 stories to a max. height of 105'
• On Union St	12 stories to a max. height of 180'
• All other streets	8 Stories to a max. height of 105'

#### F Min. step-back depth

• On Broadway	30'
• On all other streets	20'

#### G Max. height 1 additional story

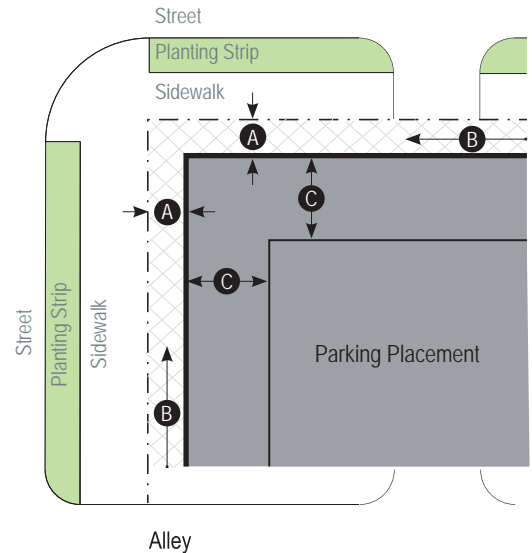
- Within 150' of the right-of-way of Broadway, height shall not exceed 6 stories or 80'
- Between 150' and 200' of the right-of-way of Broadway, height shall not exceed 7 stories or 90'.

### Sidewalk & Planting

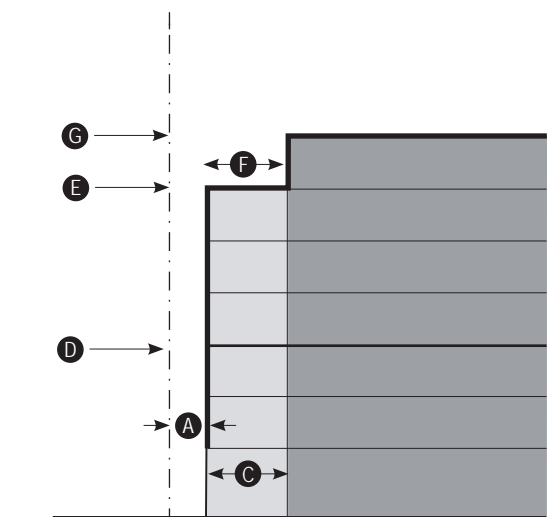
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



Building Plan



Building Section

# Section II: Subdistrict Standards

## 2nd and Broadway Subdistrict: Building design and Facade articulation

### Building design and Facade articulation

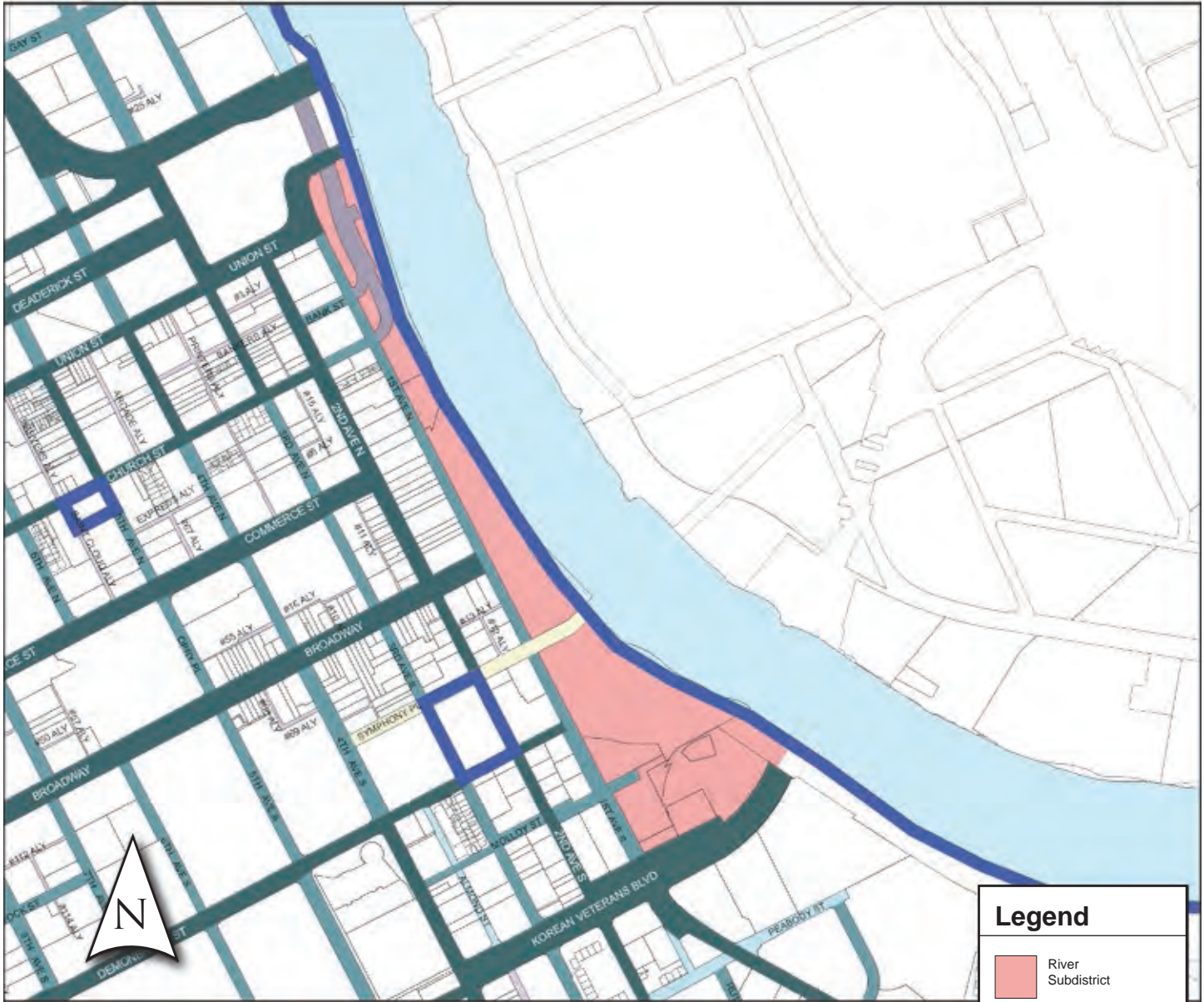
- |                                       |  |
|---------------------------------------|--|
| <b>L</b> Primary building divisions   | 20'-50' wide   |
| <b>M</b> Secondary building divisions | 5'-25' wide<br>Secondary building divisions are defined by solid vertical elements that consist of changes in materials or planes within the facade. |
| <b>N</b> First Floor height           | 16' min.   |
- Windows**
- O** Ground floor - 60% glazing required from 2 feet above grade to the finished floor of the 2nd story
  - P** Window sill height 18"-24"
- Upper Floors** - Windows shall be vertically oriented at a ratio of 2:1 or greater.



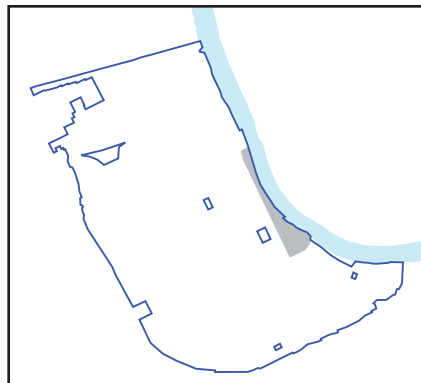
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# Section II: Subdistrict Standards

## River Subdistrict: Regulating Plan



The River subdistrict is a unique area between 1<sup>st</sup> Avenue South and the Cumberland River. This area is owned by the city and is envisioned to be an amenity for all residents and visitors. New development should be done in conjunction with the Parks Department's *Riverfront Redevelopment Plan* and should treat the river as an amenity while keeping a strong urban edge along 1<sup>st</sup> Avenue South and the boulevard. The maximum height is low in order to maintain views of the river from many vantage points in Downtown.



Legend	
	River Subdistrict
	Primary Street
	Secondary Street
	tertiary Street
	Other Street
	Alley
	Pedestrian Frontage

# Section II: Subdistrict Standards

## River Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-15'

#### B Facade width

Primary Street	80% of lot frontage min.
Secondary Street	80% of lot frontage min.
1st Avenue	70% of lot frontage min.
Tertiary Street	60% of lot frontage min.
Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.	

#### C Min. building depth 15' from building facade

### Height

#### D Max. 10 stories ~~to a max. height of 145'~~

#### Step-back \*

Step-back required on 1st Avenue frontage

#### E Step-back ~~after between~~ 4th and 8th stories ~~within a max. height of 105'~~

#### F Min. step-back depth 15'

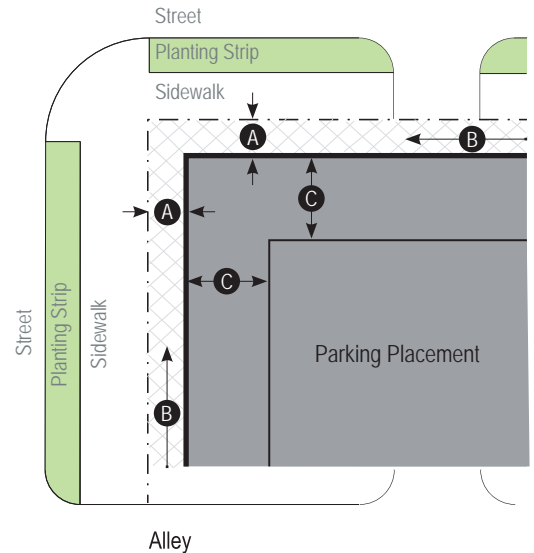
\* see page 61 for full description

### Sidewalk & Planting

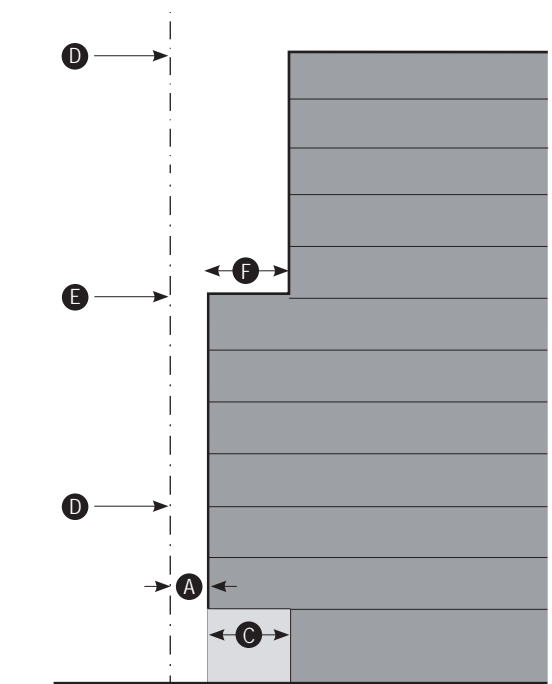
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



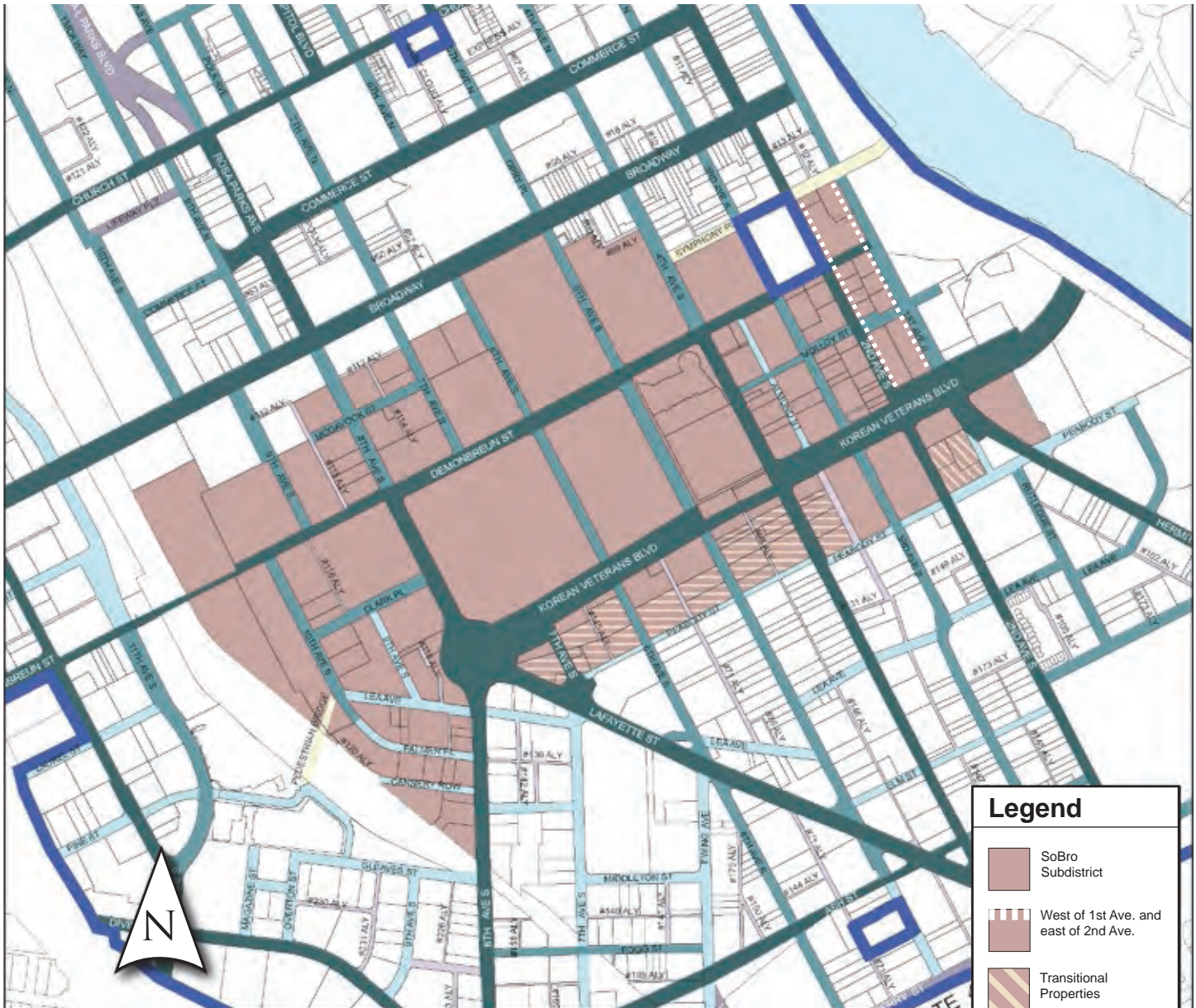
Building Plan



Building Section

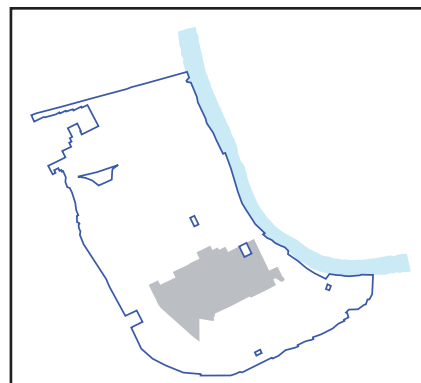
# Section II: Subdistrict Standards

## SoBro Subdistrict: Regulating Plan



The SoBro neighborhood is intended to be a high-intensity, mixed-use neighborhood emphasizing cultural and entertainment uses with a mix of residential and office uses. SoBro is an extension of the Core in height and intensity, but it is also a transition to the lower, mid-rise Lafayette neighborhood.

Properties with frontage on the boulevard, and Transitional Properties that consolidate to have frontage on the boulevard, shall be part of the SoBro Subdistrict. Properties south of the boulevard – Transitional Properties – without frontage on the boulevard, shall be part of the Lafayette Subdistrict.



# Section II: Subdistrict Standards

## SoBro Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-15'

#### B Facade width

Primary Street	80% of lot frontage min.
Secondary Street	80% of lot frontage min.
Tertiary Street	60% of lot frontage min.
Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.	

#### C Min. building depth 15' from building facade

### Height

#### D Min.

- On ~~the Korean Veterans Boulevard & the roundabout~~ 3 stories or 35'

#### E Max.

- West side of 1st Ave and east side of 2nd Ave unless fronting the Boulevard 15 stories ~~to a max. height of 220'~~
  - 8th Avenue Frontage, south of the roundabout 8 stories within 100' of 8th Ave; 30 stories beyond 100' of 8th Ave stories
  - Subdistrict general 30 stories
- Additional height available through the Bonus Height Program

#### F Step-back \*

- Step-back ~~after~~ between
- Buildings taller than 150' 4th and 8th stories ~~105' feet~~
  - ~~Properties on 1st Ave, 2nd Ave, and the Boulevard~~ 8 stories within 105'

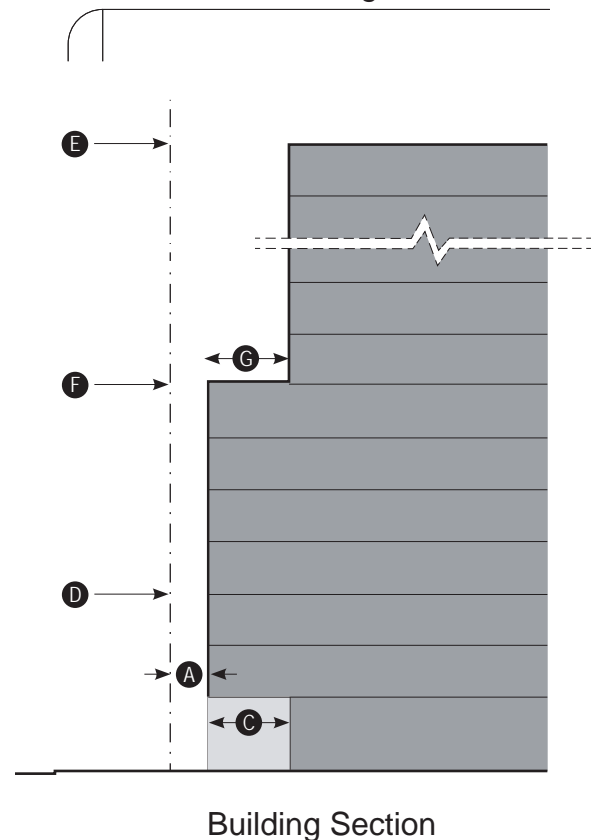
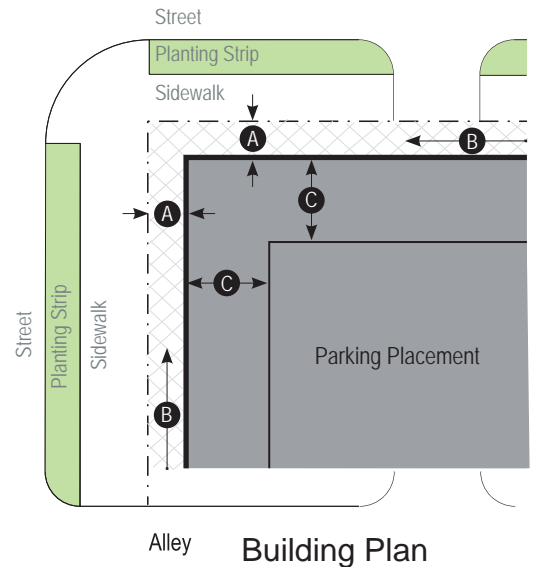
#### G Min. step-back depth 15'

- \* see page 61 for full description

### Notes

All standards of the Gateway UDO shall apply to development along the boulevard except maximum height at the street, step-back depth, overall height, and floor area ratio. The review process for the DTC and the UDO will be consolidated.

Uses: page 55; General Standards: page 59



### Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).



## Building Regulations - Fronting Roundabout



- Buildings fronting the future roundabout shall respond to the circle with a facade perpendicular to the radius of the roundabout or a curved facade concentric with the roundabout.
- Curvilinear architectural elements such as arcades are encouraged.
- Only Storefront Frontage shall be permitted fronting the roundabout.

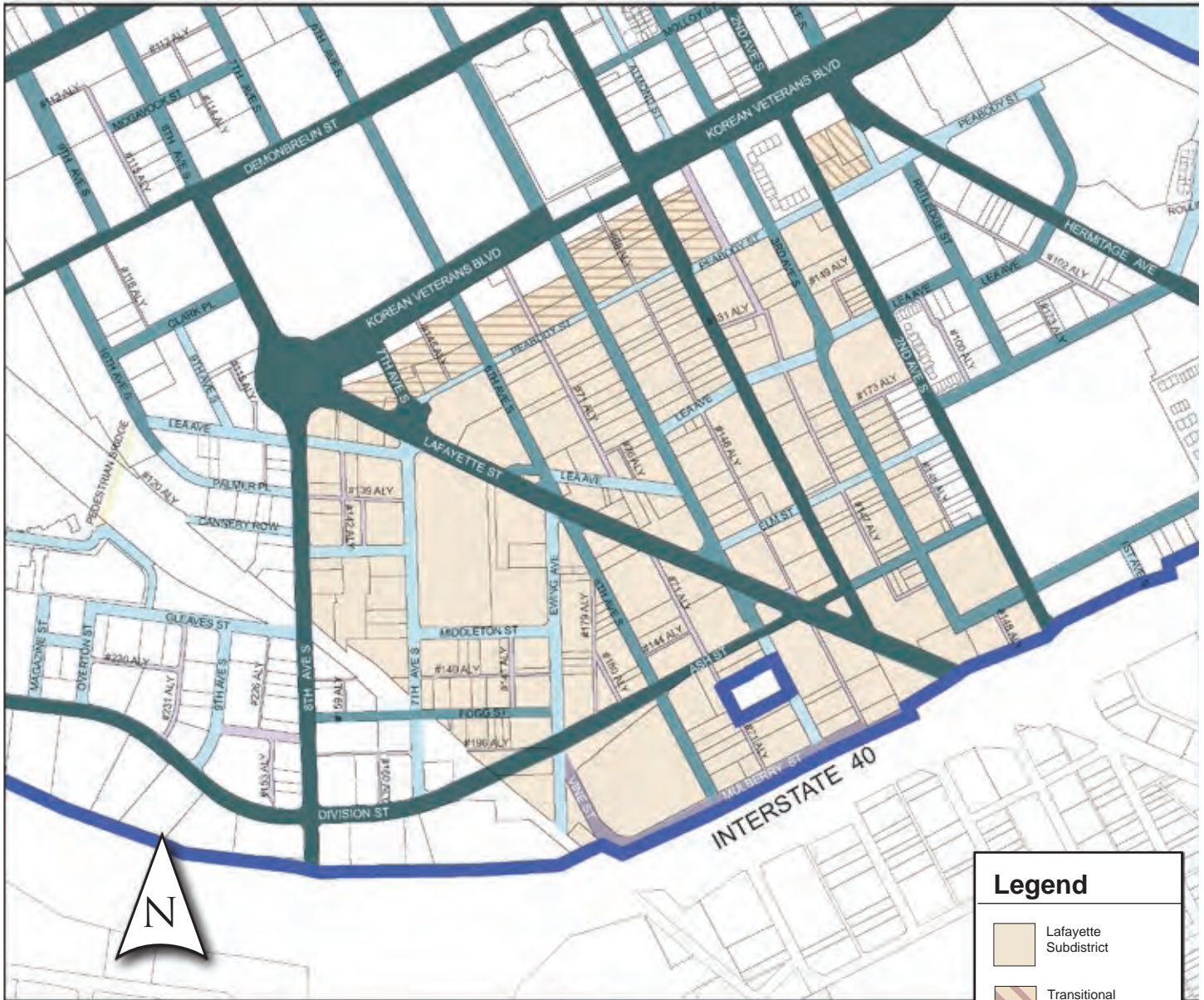
## Legend

	Primary Street
	Secondary Street
	tertiary Street
	Other Street
	Alley

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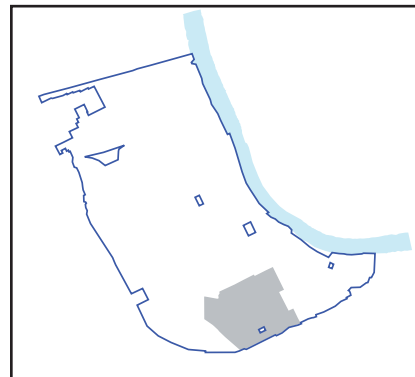
# Section II: Subdistrict Standards

## Lafayette Subdistrict: Regulating Plan



The Lafayette neighborhood is currently a primarily industrial and business services environment with strong transportation connections to the Gulch, SoBro, Midtown, and South Nashville neighborhoods. This neighborhood will likely retain many of the industrial and business service uses while accommodating a greater variety of uses. The neighborhood is primarily low-rise and should act as a transition from the height of the Core and SoBro to the single-family neighborhoods to the South.

Transitional properties that consolidate to have frontage on the boulevard shall be part of the SoBro Subdistrict. Transitional properties that do not consolidate to have frontage on the boulevard shall be part of the Lafayette Subdistrict.



# Section II: Subdistrict Standards

## Lafayette Subdistrict: Building Regulations

### Frontage

**A Allowed Frontage Types with Required Build-to Zone**

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'
Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'
Industrial Frontage is allowed in this Subdistrict.	

**B Facade width**

Primary Street	80% of lot frontage min.
Secondary Street	80% of lot frontage min.
Tertiary Street	60% of lot frontage min.
Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.	

**C Min. building depth** 15' from building facade

### Height

**D Min.**  
• On the roundabout 3 stories or 35'

**E Max.**  
• On Lafayette St 12 stories  
• Subdistrict general 8 stories  
Additional height available through the Bonus Height Program

**Step-back \***  
Step-back required for all buildings fronting public streets

**F Step-back after between** 6 4th and 7th stories  
**G Min. step-back depth** 15'

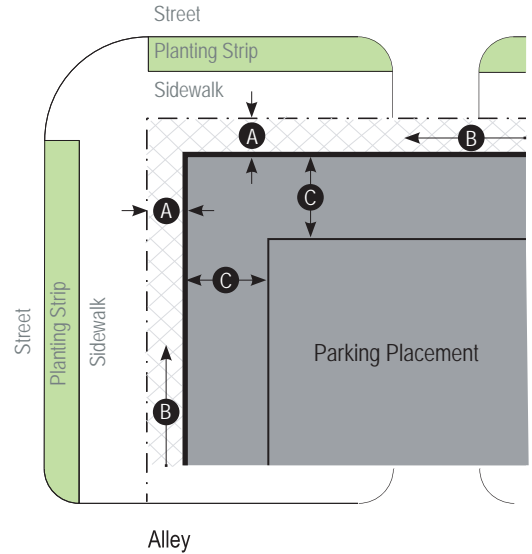
\* see page 61 for full description

### Sidewalk & Planting

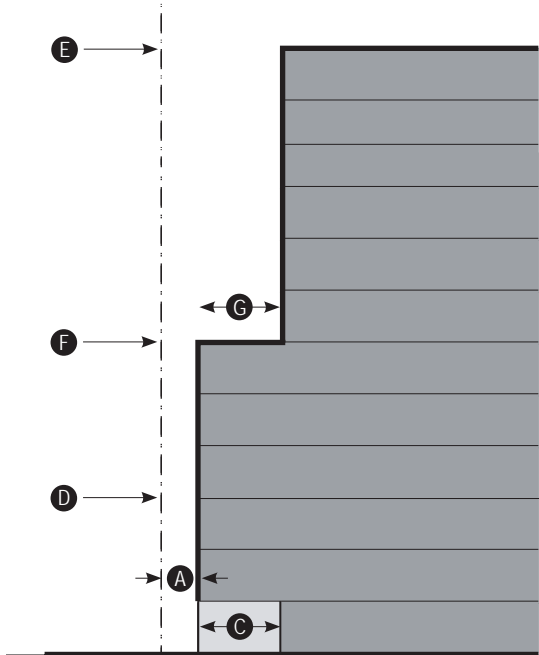
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



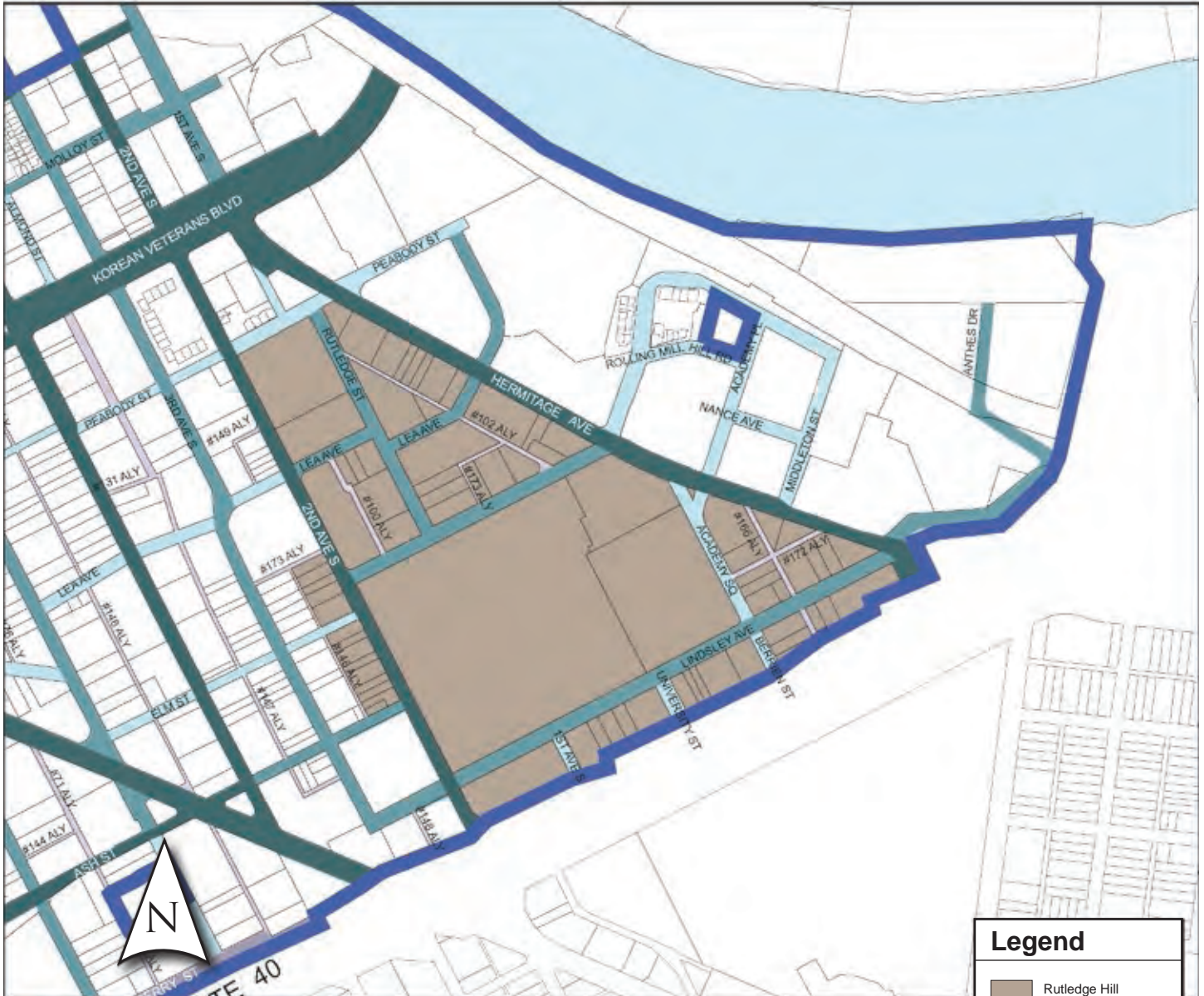
Building Plan



Building Section

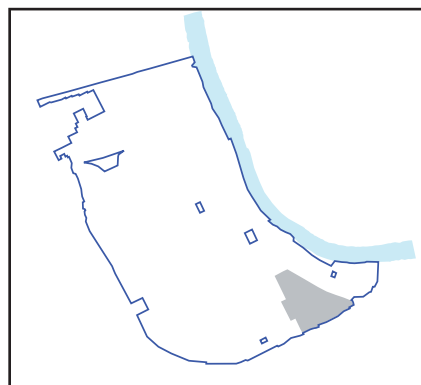
# Section II: Subdistrict Standards

## Rutledge Hill Subdistrict: Regulating Plan



The Rutledge Hill neighborhood includes a variety residential and civic historic buildings. This area is largely intact and new buildings of complimentary height, scale and massing.

Some of these properties are governed by a National Register District that regulates development, restoration and demolition. Property owners must contact the Metro Historic Commission for additional details.



# Section II: Subdistrict Standards

## Rutledge Hill Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'
Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'

#### B Facade width

Primary Street	80% of lot frontage min.
Secondary Street	60% of lot frontage min.
Tertiary Street	60% of lot frontage min.
Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.	

#### C Min. building depth 15' from building facade

### Height

#### D Max.

• Primary and Secondary streets	6 stories
• Tertiary streets	4 stories
Additional height available through the Bonus Height Program	

#### Step-back

Step-back required for all buildings fronting public streets

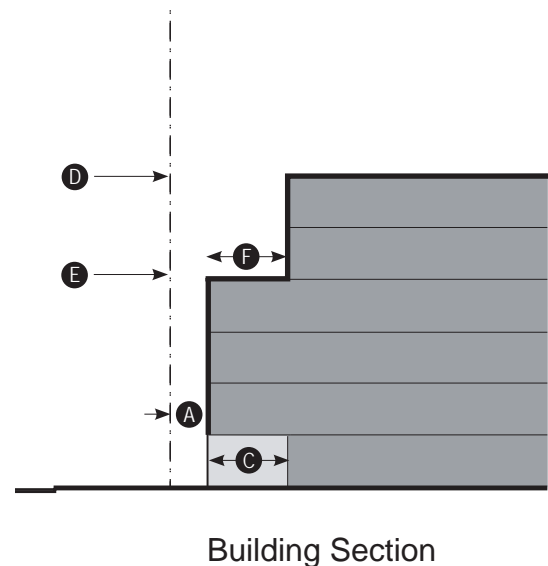
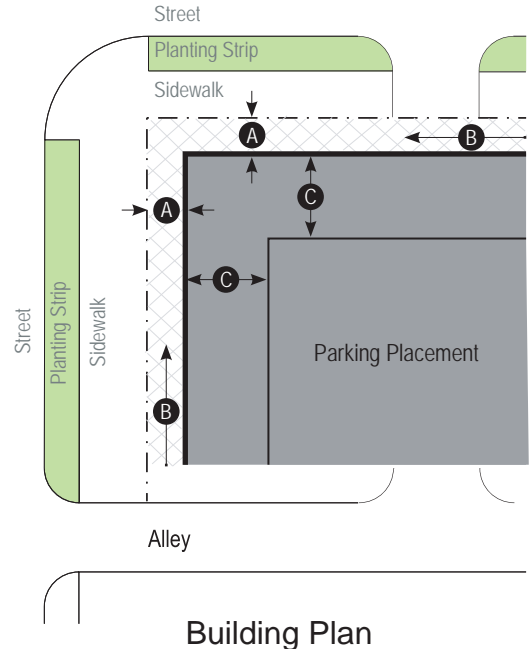
E Step-back after	4 stories
F Min. step-back depth	15'

### Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

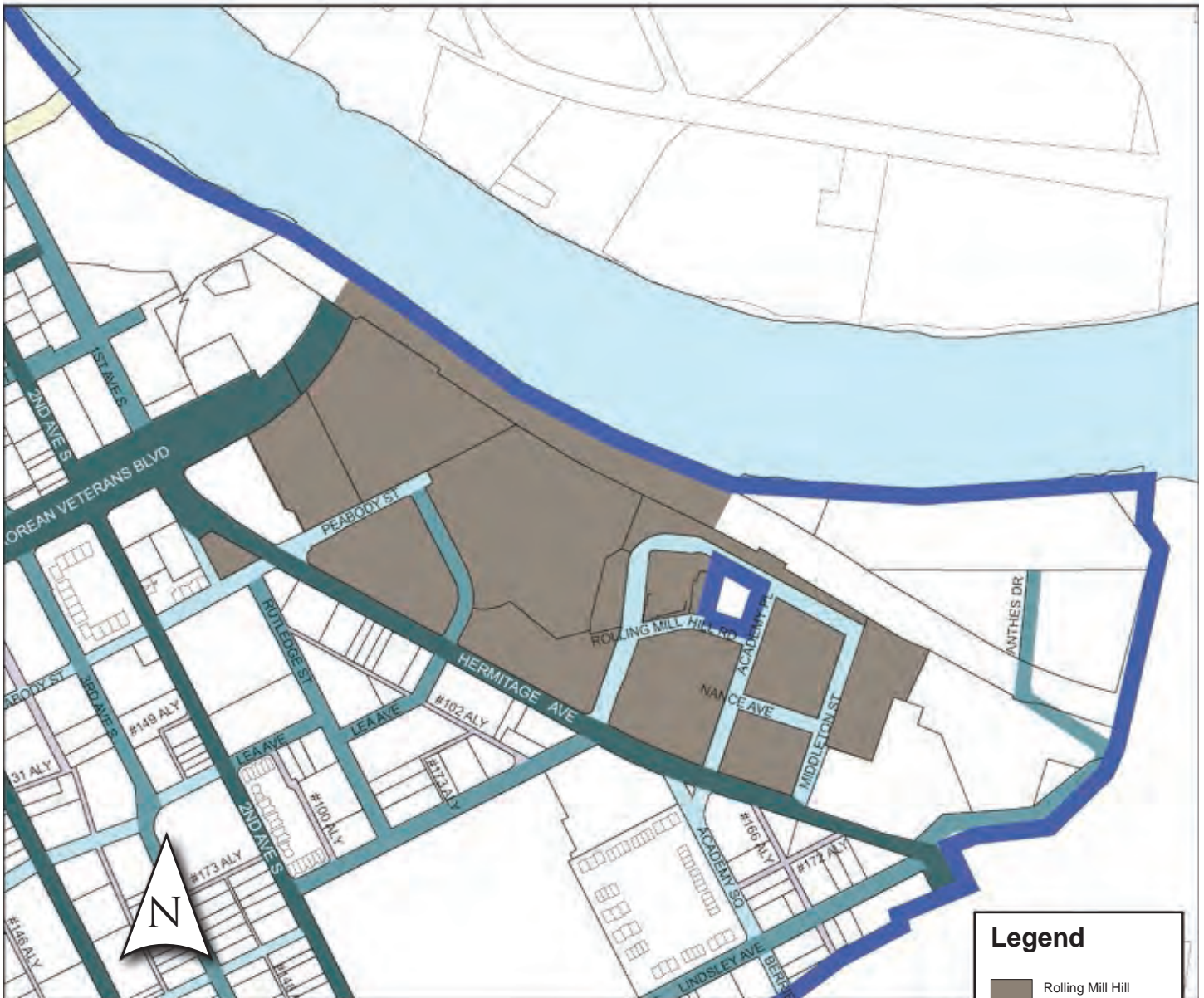
### Notes

Uses: page 55; General Standards: page 59

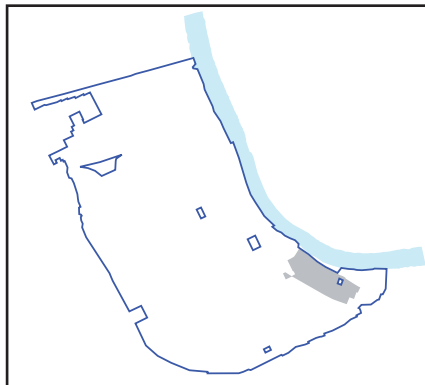


# Section II: Subdistrict Standards

## Rolling Mill Hill Subdistrict: Regulating Plan



The redevelopment of the Rolling Mill Hill neighborhood is overseen by the Metropolitan Development and Housing Agency (MDHA) through the *Master Plan and Development Guidelines for Rolling Mill Hill*.



Legend	
	Rolling Mill Hill Subdistrict
	Rolling Mill Hill Historic Subdistrict
	Primary Street
	Secondary Street
	Tertiary Street
	Alley

# Section II: Subdistrict Standards

## Rolling Mill Hill Subdistrict: Building Regulations

### Frontage

**A Allowed Frontage Types with Required Build-to Zone**

All streets 0-20'

**B Facade width**

Minimum 25% of the lot frontage or 25', which ever is greater.  
Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

**C Min. building depth** 15' from building facade

A building liner is required surrounding parking structures on the ground floor facing public streets and Open Space.

### Height

**D Max. height at the street** 65'

**E Height Control Plane**

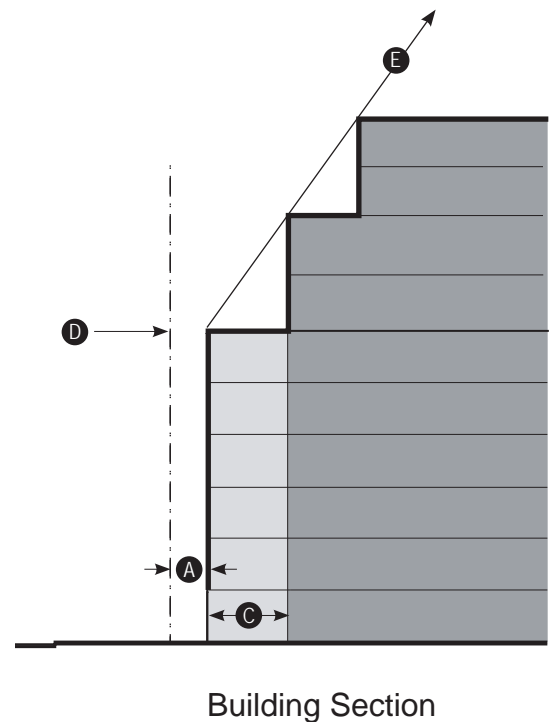
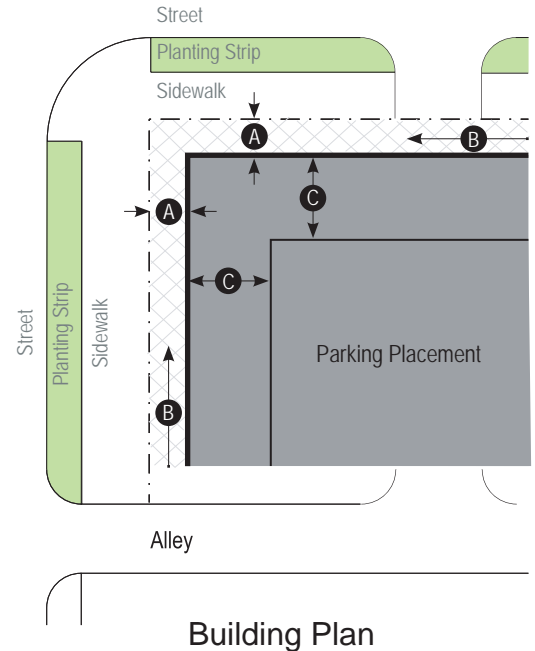
Applies from all streets and Open Space.  
1 foot horizontal to 1.5 feet vertical

### Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

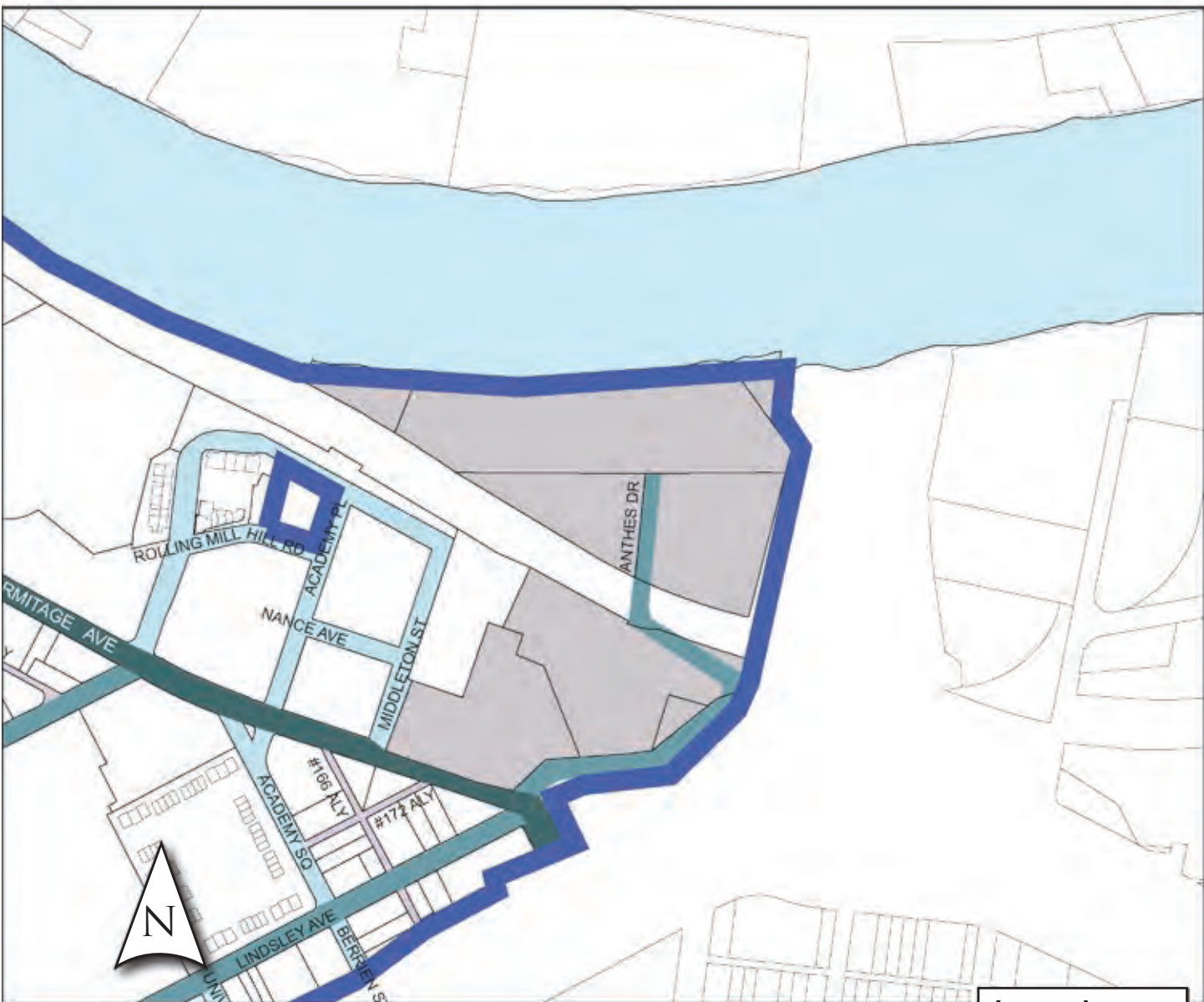
Uses: page 55; General Standards: page 59





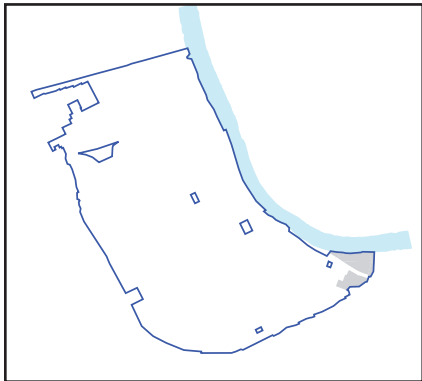
# Section II: Subdistrict Standards

## Rutledge River Subdistrict: Regulating Plan



The Rutledge River neighborhood is situated in a somewhat isolated area along the Cumberland River. Redevelopment of this area should make the most of the riverside location and the adjacent commuter rail line, while preserving the historic building on Hermitage Avenue.

New streets within the Rutledge River Subdistrict shall be Secondary or Tertiary Streets.



Legend	
	Rutledge River Subdistrict
	Primary Street
	Secondary Street
	Tertiary Street
	Alley

# Section II: Subdistrict Standards

## Rutledge River Subdistrict: Building Regulations

### Frontage

**A Allowed Frontage Types with Required Build-to Zone**

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'
Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'

Industrial Frontage is allowed in this Subdistrict.

**B Facade width**

Primary Street	80% of lot frontage min.
Secondary Street	80% of lot frontage min.
Tertiary Street	60% of lot frontage min.

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

**C Min. building depth** 15' from building facade

### Height

<b>D Max.</b>	9 stories
---------------	-----------

Additional height available through the Bonus Height Program

**Step-back \***

Step-back required for all buildings fronting public streets

<b>E</b> Step-back <b>after</b>	<b>between</b>	<b>6 4th and 7th stories</b>
<b>F</b> Min. step-back depth		15'

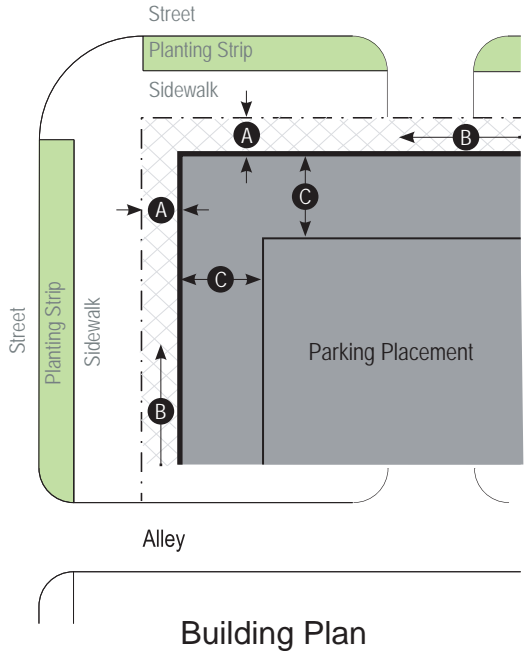
\* see page 61 for full description

### Sidewalk & Planting

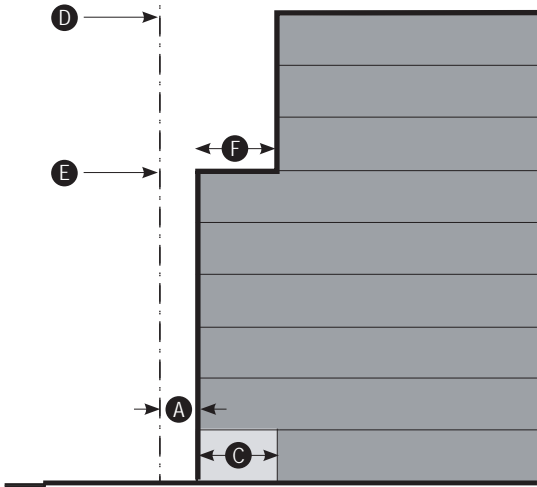
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



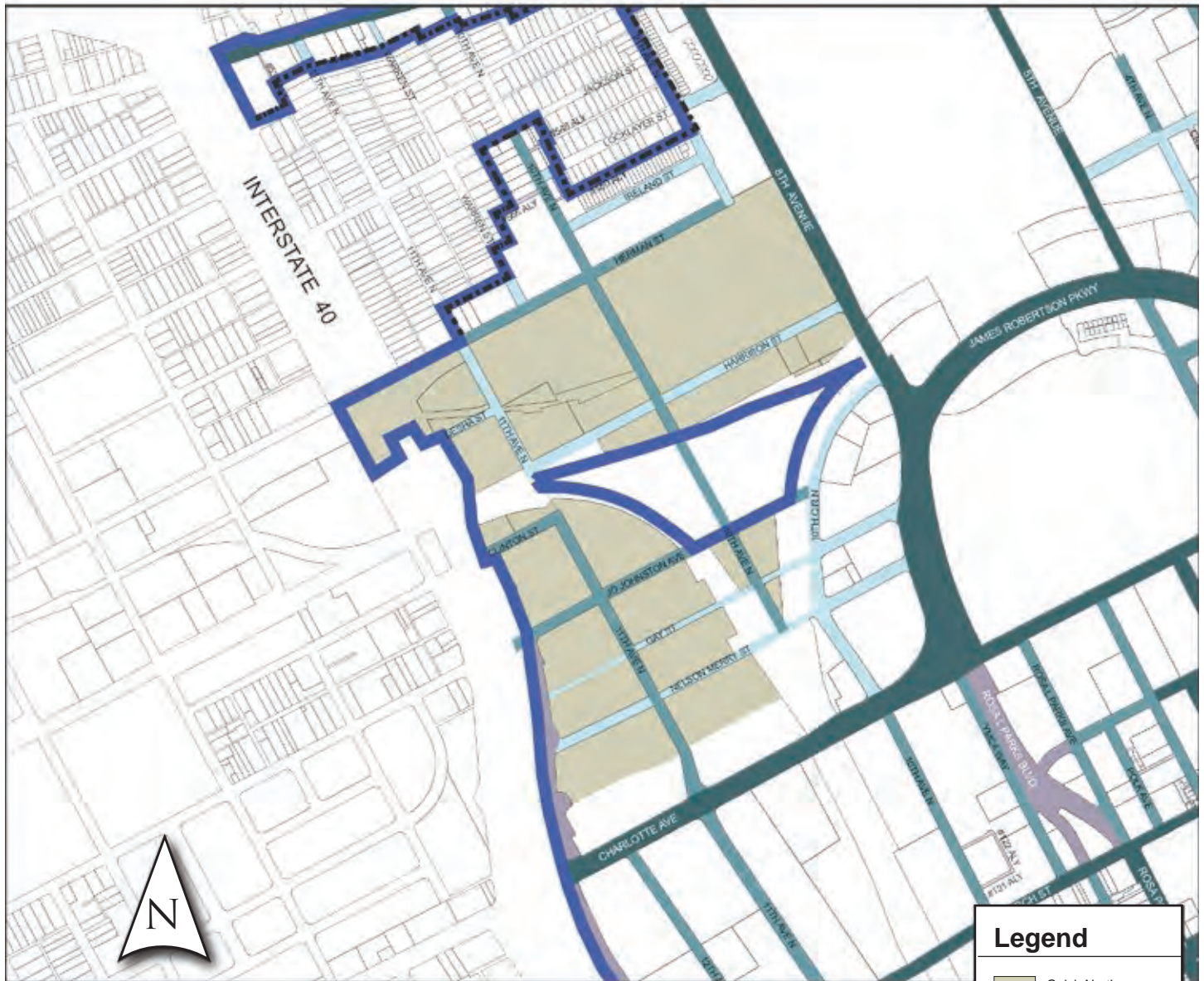
Building Plan



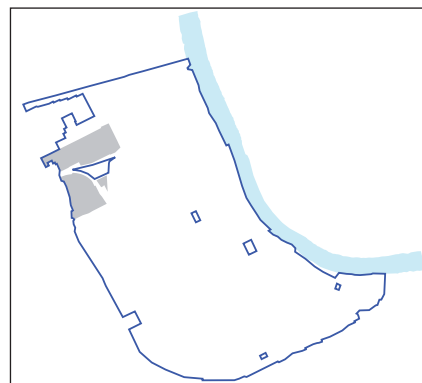
Building Section

# Section II: Subdistrict Standards

## Gulch North: Regulating Plan



The North Gulch neighborhood is an area of transition within Downtown. Bordered on the north and west by single-family residential neighborhoods, on the east by prominent state government landmarks, and on the south by industrial, The North Gulch is envisioned to be a unique area that integrates and harmonizes these diverse uses and building types. Redevelopment of this area should make the most of the existing industrial buildings, interstate access and the internal railroad lines.



Legend	
	Gulch North Subdistrict
	Primary Street
	Secondary Street
	Tertiary Street
	Other Street
	Alley

# Section II: Subdistrict Standards

## Gulch North: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

##### Primary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'

##### Secondary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'
- Porch Frontage 10'-15'

##### Tertiary Street

- Storefront Frontage 0'-10'
- Stoop Frontage 5'-10'
- Porch Frontage 10'-15'

Industrial Frontage is allowed in this Subdistrict on streets north of Harrison Street, including Harrison Street.

#### B Facade width

- Primary Street 80% of lot frontage min.
- Secondary Street 60% of lot frontage min.
- Tertiary Street 60% of lot frontage min.

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

#### C Min. building depth 15' from building facade

### Height

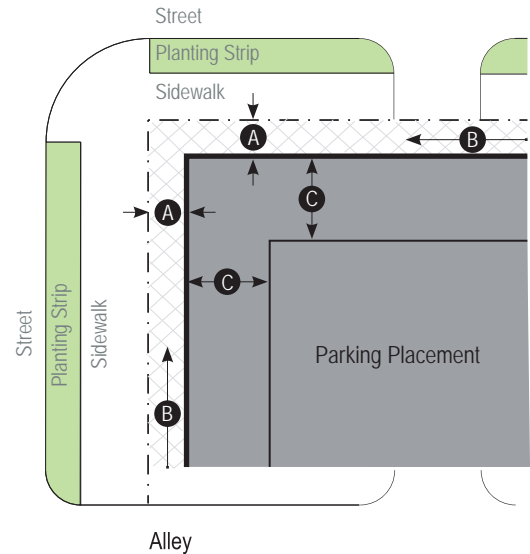
- D Max.** 7 stories
- Buildings fronting Herman St 4 stories
- Additional height available through the Bonus Height Program

### Sidewalk & Planting

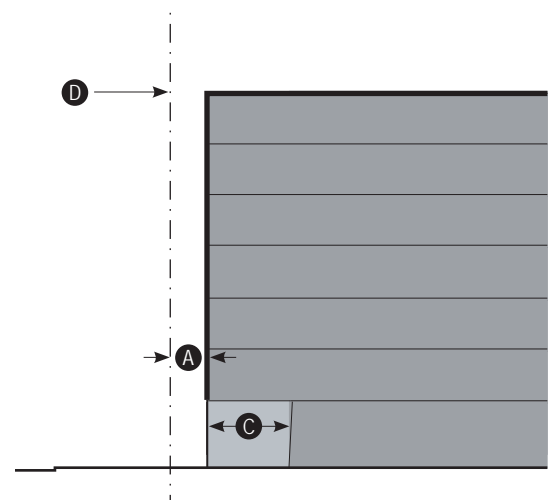
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



Building Plan



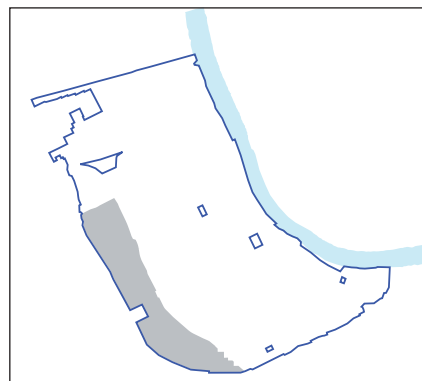
Building Section

# Section II: Subdistrict Standards

## Gulch South: Regulating Plan



The Gulch South neighborhood is an eclectic neighborhood of business services, restaurant, and retail with many new residential buildings. The area is a link between Downtown and Midtown and is envisioned to be mixed-use mid-rise buildings with opportunities for additional height at key intersections and along important streets. Connectivity – vehicular, rail, bicycle and pedestrian – should be preserved and additional connectivity is strongly encouraged.



Legend	
	Gulch South Subdistrict
	Primary Street
	Secondary Street
	Tertiary Street
	Other Street
	Alley

# Section II: Subdistrict Standards

## Gulch South: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'
Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'

#### B Facade width

Primary Street	80% of lot frontage min.
Secondary Street	80% of lot frontage min.
Tertiary Street	60% of lot frontage min.

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

#### C Min. building depth 15' from building facade

### Height

#### D Max.

• On Church St, Broadway, Demonbreun	15 stories
• At the intersection of 12th Ave and Demonbreun, 12th Ave and Division, 8th Ave and Division	20 stories
• Subdistrict general	10 stories

Additional height at intersections applies to frontage within 150 feet of the intersection

Additional height available through the Bonus Height Program

#### Step-back \*

Step-back required along all frontages in Subdistrict general and along Broadway.

E Step-back after between	7 4th and 8th stories
F Min. step-back depth	15'

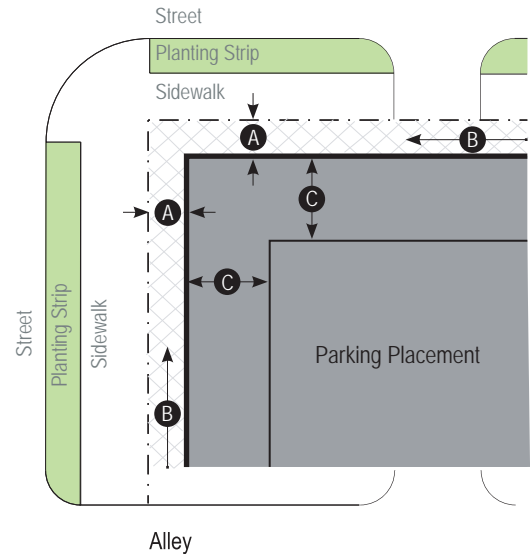
\* see page 61 for full description

### Sidewalk & Planting

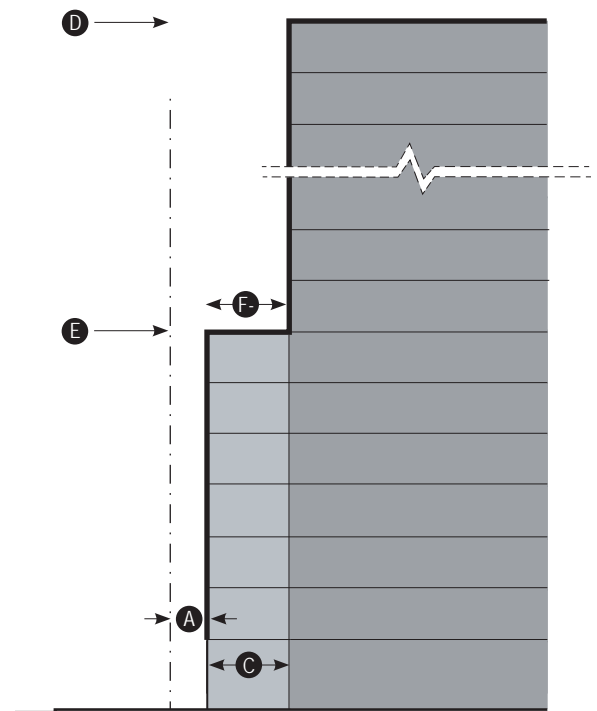
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



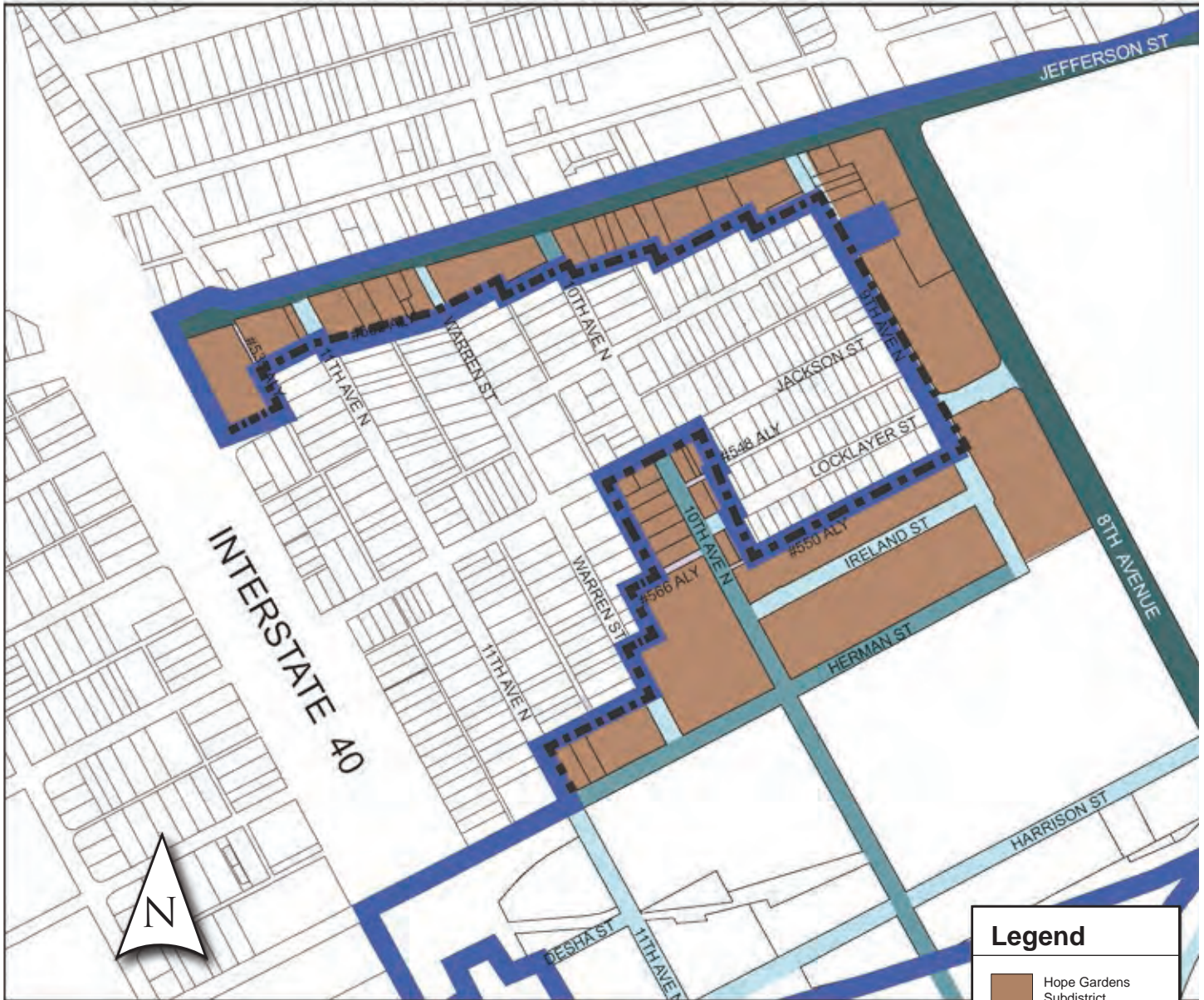
Building Plan



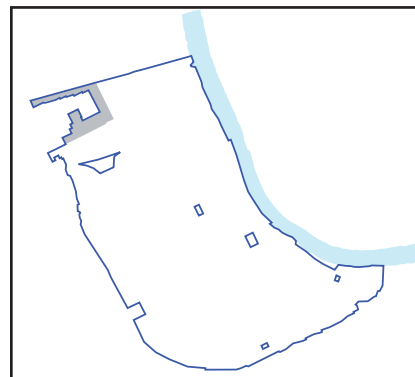
Building Section







# Section II: Subdistrict Standards

## Hope Gardens Subdistrict: Regulating Plan



The Hope Gardens subdistrict includes the commercial and multi-family areas surrounding the single-family residential at the center of the Hope Gardens neighborhood. Development along the major streets – Jefferson Street and Rosa Parks Boulevard – should be low-rise and should transition in height and mass near the single-family areas. The existing commercial, residential and industrial uses are all important factors within this area and can be maintained while providing opportunities for mixed-use. The harmonization of these many uses – through the regulation of the building forms – will ensure the vitality of this mixed-use neighborhood.



Legend	
	Hope Gardens Subdistrict
	Property lines adjacent to single family neighborhood
	Primary Street
	Secondary Street
	Tertiary Street
	Alley

# Section II: Subdistrict Standards

## Hope Gardens Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'
Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'

Industrial Frontage is allowed in this Subdistrict along Herman Street only.

#### B Facade width

Primary Street	80% of lot frontage min.
Secondary Street	60% of lot frontage min.
Tertiary Street	60% of lot frontage min.

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

#### C Min. building depth 15' from building facade

A building liner is required surrounding parking structures on the ground floor facing public streets and Open Space.

### Height

#### D Max.

• Primary Street	7 stories
• Secondary Street	4 stories
• Tertiary Street	3 stories

Additional height available through the Bonus Height Program

#### Step-back

Step-back required for properties abutting the single family neighborhood. Step-back shall be measured from the abutting property line.

E Step-back required after	3 stories
F Minimum step-back depth	30'

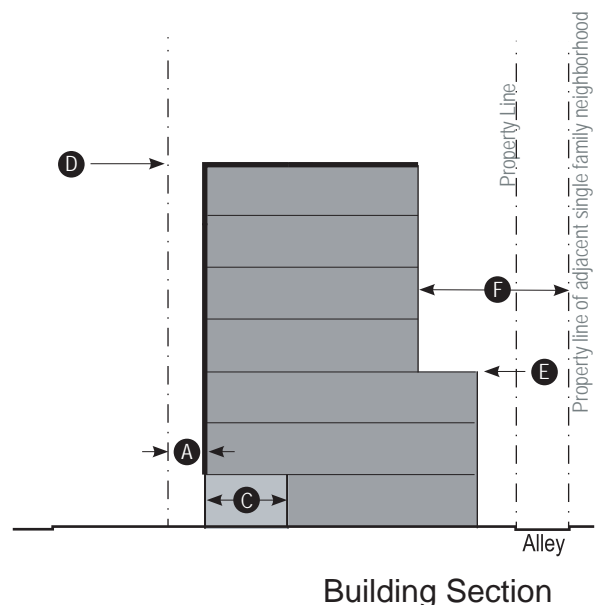
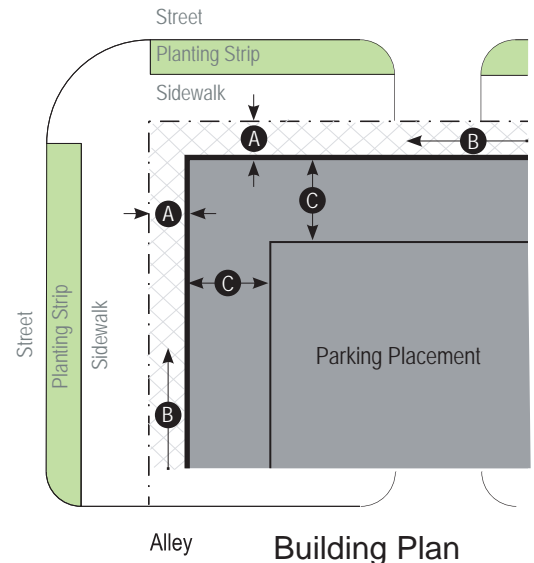
### Buffer

#### Landscape buffer

A landscaped buffer in accordance with 17.24.240 B-5 shall be required along any property line directly abutting a single or two-family zone district.

### Notes

Uses: page 55; General Standards: page 59



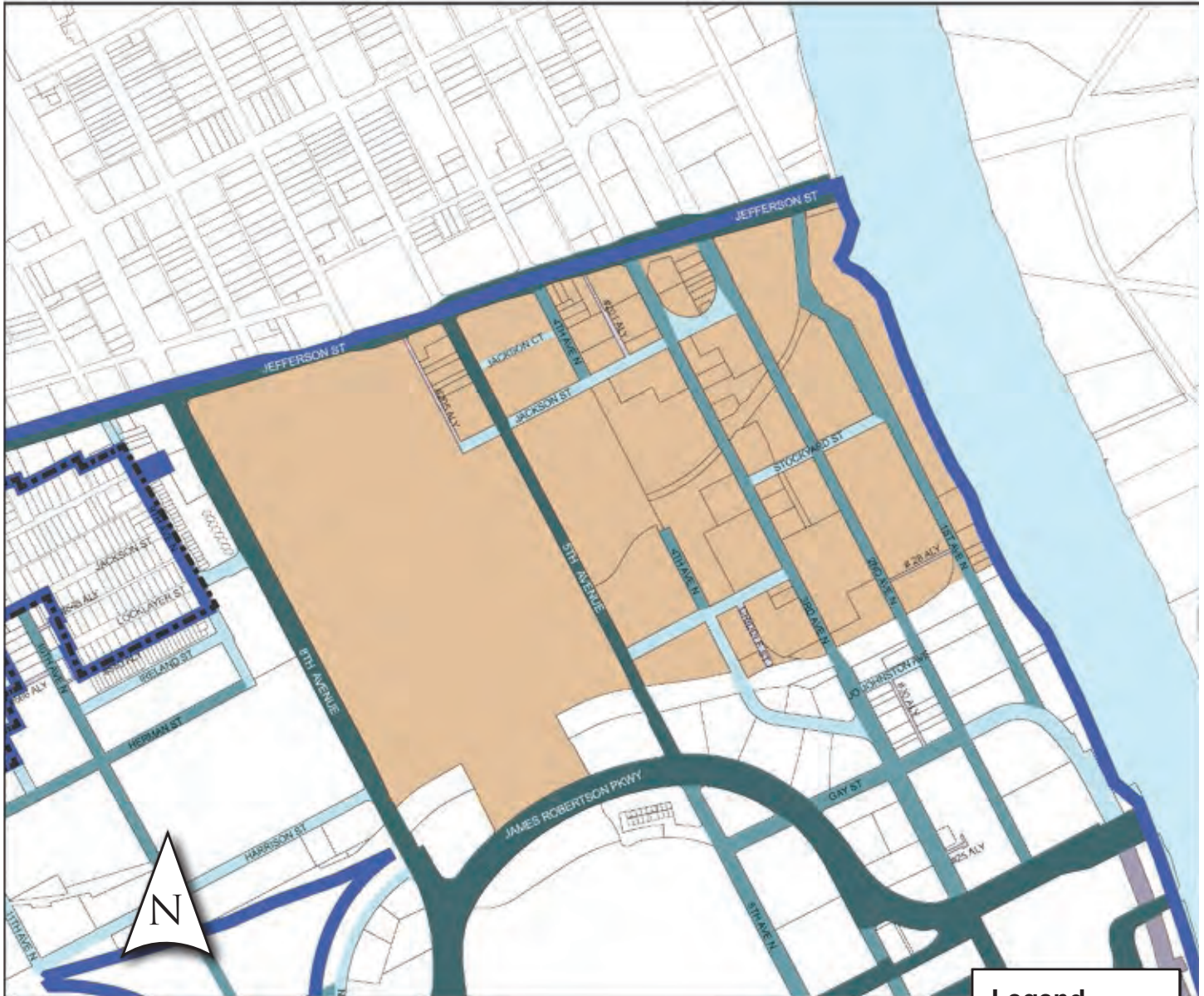
### Sidewalk & Planting

Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

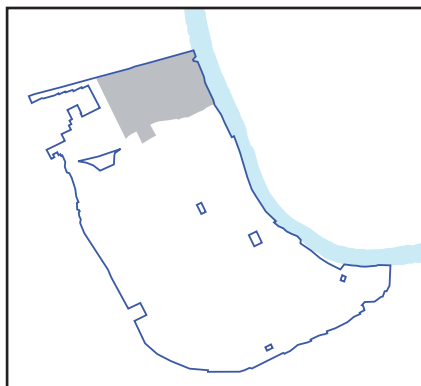







# Section II: Subdistrict Standards

## Sulphur Dell: Regulating Plan



The Sulphur Dell neighborhood is a mixed-use neighborhood surrounding the Bicentennial Mall on the north side of Downtown. This area includes many state-owned properties and is envisioned to be a cultural and civic destination within the State. Mixed-use and residential buildings will diversify the neighborhood and provide a transition in height and use into neighborhoods to the north.



Legend	
	Sulphur Dell Subdistrict
	Primary Street
	Secondary Street
	Tertiary Street
	Alley

# Section II: Subdistrict Standards

## Sulphur Dell Subdistrict: Building Regulations

### Frontage

#### A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'
Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'

#### B Facade width

Primary Street	80% of lot frontage min.
Secondary Street	80% of lot frontage min.
Tertiary Street	60% of lot frontage min.
Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.	

#### C Min. building depth 15' from building facade

### Height

#### D Max.

• Primary Street	7 stories
• Secondary Street	5 stories
• Tertiary Street	4 stories

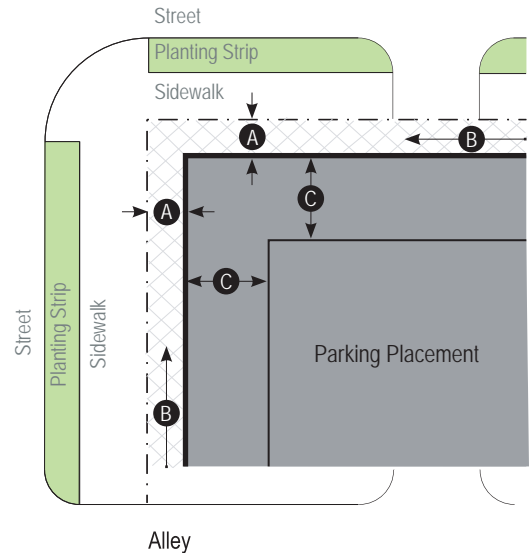
Additional height available through the Bonus Height Program

### Sidewalk & Planting

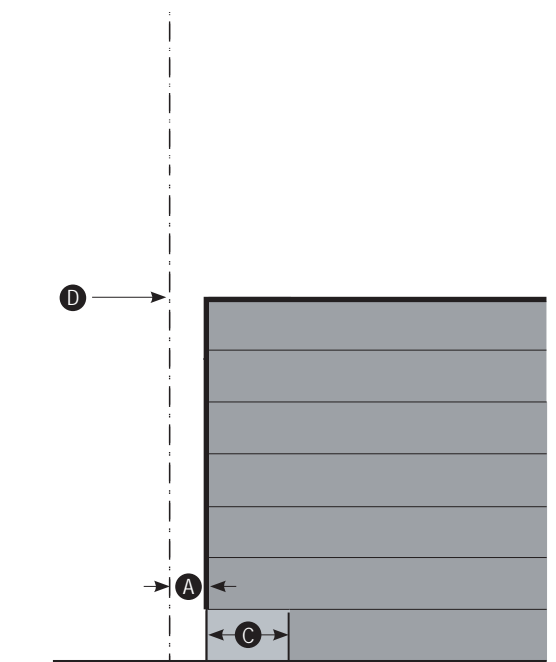
Improvements to the sidewalk corridor according to the General Standards and the Major and Collector Street Plan (including constrained right-of-way and local street dimensions as determined by Metro Planning).

### Notes

Uses: page 55; General Standards: page 59



Building Plan



Building Section

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# Section III: Uses

Amended by Ordinance No. BL2012-142  
as adopted on May 21, 2012

Amended by Ordinance No. BL2011-896  
as adopted on May 26, 2011

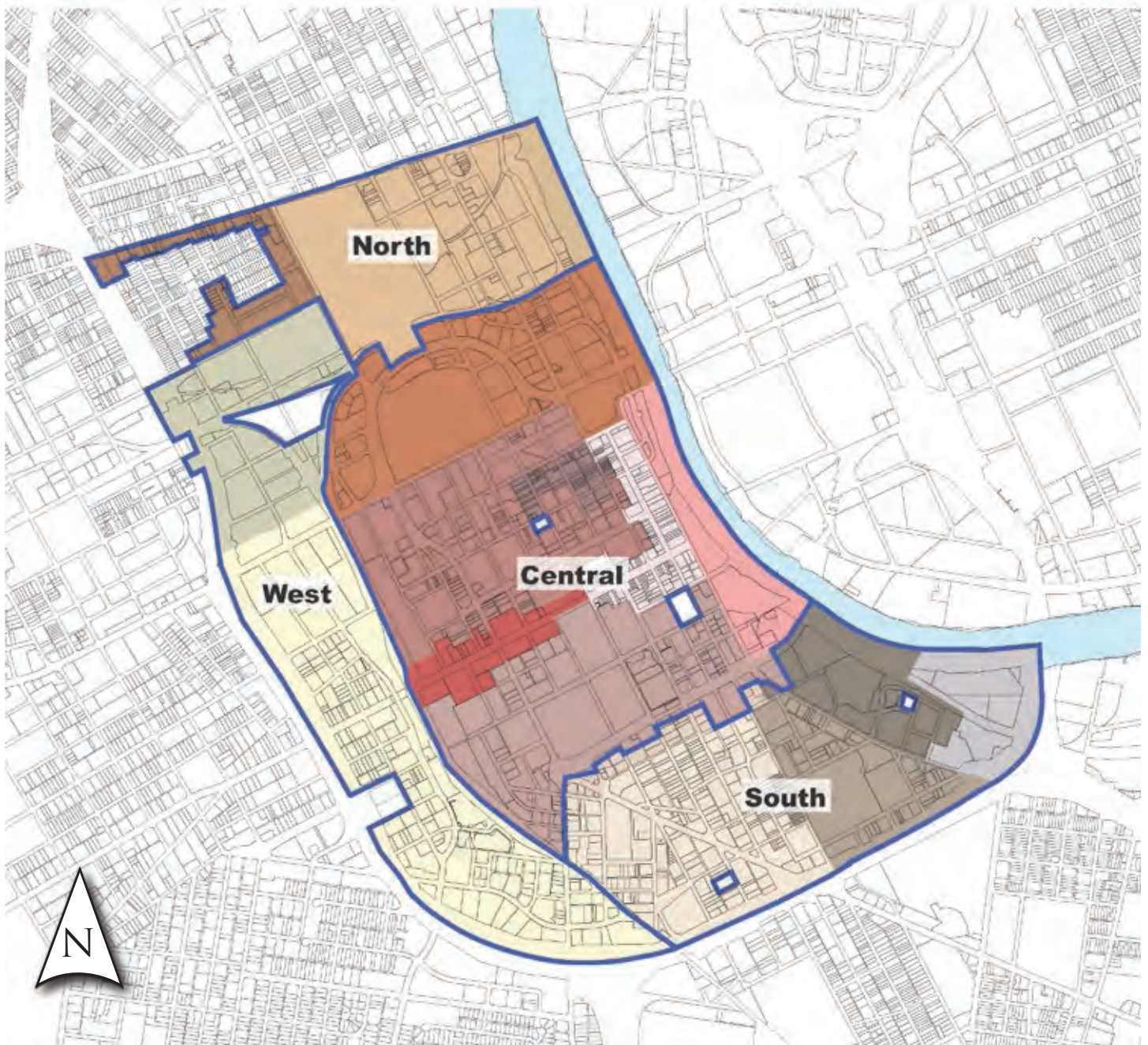
# Section III: Uses

## Use Areas

Land uses within the DTC are determined by Area – Central, South, West, and North. To create a sustainable and mixed-used Downtown, the form-based zoning of each subdistrict regulates the shape, scale, and placement of the buildings, and allows a variety of uses.

Uses Area boundaries are the same as Subdistrict boundaries. To determine the allowed land uses, locate the property on the Area Plan, and refer to the Area column on the Land Use Chart for the allowed uses.

Uses Permitted with Conditions or Permitted by Special Exceptions or Accessory shall follow the standards of Chapter 17.16. If standards within Chapter 17.16 and the DTC conflict, the stricter shall apply.



# Section III: Uses

## Use Tables

P Permitted by right PC Permitted with conditions SE Special Exception A Accessory O Overlay District	North	South	West	Central
<b>Residential Uses:</b>				
Single-family	P	P	P	P
Two-family	P	P	P	P
Multi-Family	P	P	P	P
Mobile home dwelling				
Accessory apartment	P	P	P	P
Boarding house	P	P	P	P
Consignment sale	P	P	P	P
Garage sale	A	A	A	A
Historic bed and breakfast homestay	P	P	P	P
Historic home events	P	P	P	P
Home occupation	P	P	P	P
Rural bed and breakfast homestay				
Security residence				
<b>Institutional Uses:</b>				
Correctional facility	P	P	P	P
Cultural center	P	P	P	P
Day care center (up to 75)	P	P	P	P
Day care center (over 75)	P	P	P	P
Day care home	P	P	P	P
Day care--parent's day out	A	A	A	A
School day care	P	P	P	P
Monastery or convent	P	P	P	P
Orphanage	P	P	P	P
Religious institution	P	P	P	P
<b>Educational Uses:</b>				
Business school	P	P	P	P
College or university	P	P	P	P
Community education	P	P	P	P
Dormitory	P	P	P	P
Fraternity/sorority house	P	P	P	P
Personal instruction	P	P	P	P
Vocational school	P	P	P	P
<b>Office Uses:</b>				
Financial institution	P	P	P	P
General office	P	P	P	P
Leasing/sales office	P	P	P	P
<b>Medical Uses:</b>				
Animal hospital				
Assisted-care living	P	P	P	P
Hospice	P	P	P	P
Hospital	P	P	P	P
Medical appliance sales	P	P	P	P
Medical office	P	P	P	P

P Permitted by right PC Permitted with conditions SE Special Exception A Accessory O Overlay District	North	South	West	Central
Medical or scientific lab	P	P	P	P
Nonresidential drug treatment facility	P	P	P	P
Nursing home	P	P	P	P
Outpatient clinic	P	P	P	P
Rehabilitation services	P	P	P	P
Residence for handicapped (8 or more)	P	P	P	P
Veterinarian	P	P	P	P
<b>Commercial Uses:</b>				
After-hours establishment	PC	PC	PC	PC
Animal boarding facility	P	P	P	P
ATM	P	P	P	P
Automobile convenience	PC	PC	PC	PC
Automobile parking	P	P	P	P
Automobile repair		P		
Automobile sales, new		P	P	P
Automobile sales, used				
Automobile service	P	P	P	P
Bar or nightclub	P	P	P	P
Bed and breakfast inn	P	P	P	P
Business service	P	P	P	P
Carpet cleaning	P	P	P	
Car wash		PC		
Community garden (commercial)	P	P	P	P
Community garden (non-commercial)	P	P	P	P
Custom assembly	P	P	P	P
Donation center, drop-off	PC	PC	PC	PC
Funeral home	P	P	P	P
Furniture store	P	P	P	P
Home improvement sales	P	P	P	P
Hotel/motel	P	P	P	P
Inventory stock	A	A	A	A
Kennel/stable				
Laundry plants		P	P	
Liquor sales	P	P	P	P
Major appliance repair	P	P	P	
Microbrewery	P	P	P	P
Mobile storage unit	PC	PC	PC	PC
Mobile vendor				
Personal care services	P	P	P	P
Restaurant, fast-food	P	P	P	P
Restaurant, full-service	P	P	P	P
Restaurant, take-out	P	P	P	P
Retail	P	P	P	P
Self-service storage	P	P	P	P
Vehicular rental/leasing	PC	PC	PC	PC

# Section III: Uses

## Use Tables

P Permitted by right PC Permitted with conditions SE Special Exception A Accessory O Overlay District	North	South	West	Central
Vehicular sales & services, limited		P		
Wrecker service		P		
<b>Communication Uses:</b>				
Amateur radio antenna	P	P	P	P
Audio/video tape transfer	P	P	P	P
Multi-media production	P	P	P	P
Printing and publishing	P	P	P	P
Radio/TV/satellite tower	PC	PC	PC	PC
Radio/TV studio	P	P	P	P
Satellite dish	P	P	P	P
Telephone services	PC	PC	PC	PC
<b>Industrial Uses:</b>				
Artisan distillery	P	P	P	P
Building contractor supply	PC	PC	PC	
Distributive business/wholesale	PC	PC	PC	
Fuel storage	A	A	A	
Heavy equipment sales & service				
Hazardous operation				
Manufacturing, heavy				
Manufacturing, medium				
Manufacturing, light	PC	PC	PC	
Research service	P	P	P	
Scrap operation				
Tank farm				
Warehouse	PC	PC	PC	
<b>Transportation Uses:</b>				
Airport/heliport				
Boatdock (commercial)	P	P		P
Bus station/landport	P	P	P	P
Bus transfer station	P	P	P	P
Commuter rail	P	P	P	P
Helistop	SE	SE	SE	SE
Motor freight				
Park and ride lot				
Railroad station	SE	SE	SE	
Railroad yard				
Water taxi station	P	P		P
<b>Utility Uses:</b>				
Power/gas substation	P	P	P	P
Power plant	A	A	A	A
Reservoir/water tank	P	P	P	P
Safety services	P	P	P	P
Waste water treatment	SE	SE	SE	SE
Water/sewer pump station	P	P	P	P
Water treatment plant	SE	SE	SE	SE

P Permitted by right PC Permitted with conditions SE Special Exception A Accessory O Overlay District	North	South	West	Central
<b>Waste Management Uses:</b>				
Collection center				
Construction/demolition landfill				
Medical waste	A	A	A	A
Recycling collection center	P	P	P	P
Recycling facility				
Sanitary landfill				
Waste transfer				
<b>Recreation and Entertainment Uses:</b>				
Adult entertainment	O	O	O	O
Camp				
Club	P	P	P	P
Commercial amusement (inside)	P	P	P	P
Commercial amusement (outside)	P	P	P	P
Country club	P	P	P	P
Drive-in movie				
Driving range				
Fairground				
Golf course				
Greenway	P	P	P	P
Park	P	P	P	P
Racetrack				
Recreation center	P	P	P	P
Rehearsal hall	P	P	P	P
Stadium arena/convention center	P	P	P	P
Temporary festival	P	P	P	P
Theater	P	P	P	P
Zoo				
<b>Other Uses:</b>				
Agricultural activity				
Cemetery	P	P	P	P
Domestic animals / wildlife				
Mineral extraction				
Pond/lake	P	P	P	P

# Section IV: General Standards

Amended by Ordinance No. BL2012-142  
as adopted on May 21, 2012

Amended by Ordinance No. BL2011-896  
as adopted on May 26, 2011



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# Section IV: General Standards

## Calculations

### Measurement from “Grade”

- Unless otherwise indicated, reference to measurements from “grade” shall be calculated using the average elevation along the public right-of-way fronting the property. Thus, grade will generally be measured from the public sidewalk, not from grade on site.
- When buildings are set back from the property line more than 15 feet, grade shall be measured as the average existing elevation at the building facade.
- In the event that the base flood elevation, as established by FEMA, is higher than the sidewalk or grade elevations, the height of the first story, shall be measured from 1 foot above the base flood elevation.

### Measurement of Height

- Unless otherwise specified herein, the height of buildings shall be measured in stories.
- The maximum height for an individual story shall not exceed 25 feet from finished floor to finished floor for each of the first 2 stories, 18 feet floor to floor above the second story, and 25 feet for the top story of buildings greater than 5 stories.
- The minimum building height shall be 25 feet. This applies to all buildings except those designed for single-family use, two-family use, or multi-family use with residential on the ground floor.
- The maximum height for a raised foundation is 6 feet above grade.
- Basements are not considered stories for the purposes of determining building height.
- Building height shall be measured from each Street Frontage (excluding Other streets alleys) or Open Space.
- The height of a parking structure concealed by a building liner may be equal to the height of the liner, regardless of the number of stories. If there is no liner to conceal the parking structure, its height is limited by the maximum number of stories allowed.
- The height of fences, walls and hedges shall be measured in feet from the average sidewalk elevation.

### Step-backs

- If a building step-back is specified to occur between a range of stories, that range shall include the lower and upper story bounds. For example, the James Robertson Subdistrict specifies a 15’ step-back between the 4th and 8th stories. Therefore the required stepback can occur on the 4th, 5th, 6th, 7th, or 8th story.
- To allow for facade variation, lower stories may be permitted to step-back to a lesser extent, or not at all, so long as the minimum step-back depth is met by the upper story bound of the required step-back range.

### Fenestration and Glazing

- Except as specifically referenced herein, facade glazing and opening standards shall include windows, doors and openings in parking structures and shall apply to all areas of the building facade facing a public street or open space (excluding Other streets and alleys) as follows:
  - First Floor: facade area measured from the finished floor to a height 14 feet above the finished floor.
  - Upper Floors: facade area from finished floor to finished floor.
  - Openings for vehicular access to parking structures on the first floor shall not be included in calculation of total facade area or glazed area.
  - The DTC recognizes the need for building systems and functionality including interior mechanical systems, fire safety egress, other building code issues and their impact of the feasibility of building fenestration. Areas of the facade affected by these elements shall not be counted toward minimum glazing requirements.

## Section IV: General Standards

### Street Character

The public right-of-way, including streets, sidewalks and public utility infrastructure, plays both a functional and social role in the life of the city and its citizens. Streets organize the city, help to define space, and link destinations. The street is also a public place where people congregate, shop, socialize and live. Active, attractive streets are critical to the continued growth and success of Downtown. The DTC includes urban design tools to make working, living and playing in Downtown lively, safe and comfortable.

The DTC uses Street Types as an urban design and organizing tool. All streets are classified on the Regulating Plan as Primary, Secondary, Tertiary, Other, or Alley.

Where alleys exist and are in working condition, or where new alleys can be created, the DTC prioritizes alleys for access and loading. The location of vehicular access from all other streets shall be determined on a case-by-case basis.

The Downtown Plan: 2007 Update calls for “a strong emphasis on expanding other modes of transportation including walking, cycling and transit.” The DTC emphasizes walking, cycling and transit as primary modes of transportation within Downtown through the urban design of individual buildings, blocks, and neighborhoods.

#### All Streets

- Streets refer to publicly or privately owned right-of-way. They are intended for use by pedestrian, bicycle, transit and vehicular traffic and provide access to property.
- Streets consist of vehicular lanes and the Sidewalk Corridor. The vehicular lanes, in a variety of widths, provide traffic and parking capacity and may include bicycle paths. The Sidewalk Corridor contributes to the urban character of each neighborhood. It may include pedestrian paths, landscaped planters, street furnishings and street trees.
- Pedestrian safety, comfort, and accessibility should be a primary consideration of street design and dimensioning.
- When alleys are present, vehicular access from alleys is preferred. Vehicular access from public streets shall be considered in the following order: Other Streets, Secondary Streets, Tertiary Streets and then Primary Streets as approved by Metro departments.



# Section IV: General Standards

## Street Character

### Street Types

- **Primary Street:** Primary Streets accommodate high levels of pedestrian activity and high levels of vehicular traffic. On Primary Streets, active uses – residential, retail, restaurant or office – lining parking structures and on the first floor of buildings, and restricted vehicular access enhance the pedestrian experience. Primary streets provide the opportunity for more intense, urban development including shallow Build-to Zones and, in some cases, increased building height. Pedestrian comfort on these streets is of highest importance. Primary streets should have a continuous street wall, wide sidewalks between 15 and 20 feet to provide room for street furniture such as benches, trash receptacles, and bicycle parking. Primary Streets have the highest level of urban activity such as, outdoor dining, retail displays, and community activities like markets, parades, and music. Street trees provide protection from the sun and rain, reduce stormwater runoff and air pollution, and provide aesthetic value to the city. Trees should be planted in wells with tree grates to allow for the uninterrupted flow of pedestrian traffic.
- **Secondary Street:** Secondary Streets have moderate levels of pedestrian activity and moderate levels of vehicular traffic. Secondary Streets may be mixed-use or more residential in character. The Build-to Zone is generally shallow, and building heights are limited. In mixed-use areas, a continuous street wall should be maintained and sidewalks should be between 12 and 15 feet wide to accommodate pedestrian traffic. In residential areas, the required minimum façade width is limited – allowing for more space between buildings – and sidewalks may be narrower. Both tree wells and open landscaped planters are appropriate depending on sidewalk width.
- **Tertiary Street:** Tertiary Streets are the less important than Primary and Secondary streets. They may function as “back of house” for buildings with multiple street frontages. Care should be taken to make these streets as pedestrian-friendly as possible while accommodating loading and access needs.



*Broadway is a Primary Street due to the high intensity of urban activity.*



*Third Avenue south of Broadway is a Secondary Street within a mixed-use area.*



*Ninth Avenue North is a Secondary Street within a residential area.*

## Section IV: General Standards

### Street Character

- **Other Street:** Other Streets are streets that do not fall into any of the other street categories. They may have high or moderate levels of vehicular traffic, but often have no access to property and limited pedestrian activity. Building height along these streets is regulated by the other property frontages. Buildings do not front on these streets and may be built up to the property line.
- **Alley:** Alleys are service roads that provide shared access to property. Public utilities as well as access to mechanical equipment and trash should be located off an alley whenever possible. Where alleys exist and are in working condition, or where new alleys can be created through the dedication of new right-of-way, alleys are prioritized for access and loading.

### Sidewalk Corridor

- The Sidewalk Corridor is the portion of the right-of-way between the vehicular lanes and the property line or building façade.
  - The primary function of the Sidewalk Corridor is to provide a safe, comfortable, and convenient route for pedestrian travel that is separated from vehicular movements.
  - The Sidewalk Corridor is a public space that should include pedestrian amenities such as seating, shade trees, places to congregate, trash receptacles and outdoor dining.
  - The Sidewalk Corridor may accommodate public utilities such as electric poles and vaults, water and sewer lines, bus stops and traffic signals.
- As property develops within the DTC boundaries, property owners shall consult with **Metro Planning and Public Works** to make the necessary improvements to the streetscape in accordance with *the Major and Collector Street Plan*, *the Downtown Streetscape Elements Design Guidelines*, *the Strategic Plan for Sidewalks and Bikeways* and Title 17.20.120 Provision of sidewalks.



*Other Streets are less traveled by pedestrians and have limited access for vehicles from adjoining lots.*



*Alleys provide access for parking and service areas.*

## Section IV: General Standards

### Street Character

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#### Street Trees

Shade-producing street trees shall be planted in the public right-of-way along the length of the lot frontage at a maximum spacing of fifty feet or in accordance with the regulations of Metro departments and agencies.

#### Tree Quality

Tree species shall be chosen from the *Urban Forestry Recommended and Prohibited Tree and Shrub List* based on tree size and planting area provided or an alternative species deemed appropriate by the Urban Forester.

- At planting trees, shall meet the requirements for street trees set out in the *American Standard for Nursery Stock*.
- All nursery stock used as street trees shall be vigorous, healthy and free of diseases or infestation.
- Planting Area Dimension
  - The following standards are minimum standards. All development is encouraged to provide street trees with the largest area of pervious surface and volume of soil that can be accommodated.
    - Trees shall be accommodated in planting areas with a minimum depth of 3 feet and a minimum soil volume of 400 cubic feet.
    - The minimum pervious opening at grade shall be 25 square feet.
    - Tree vaults shall have the capability to drain water.
  - Planting areas shall not inhibit ingress/egress from buildings or pedestrian traffic along the Sidewalk Corridor.

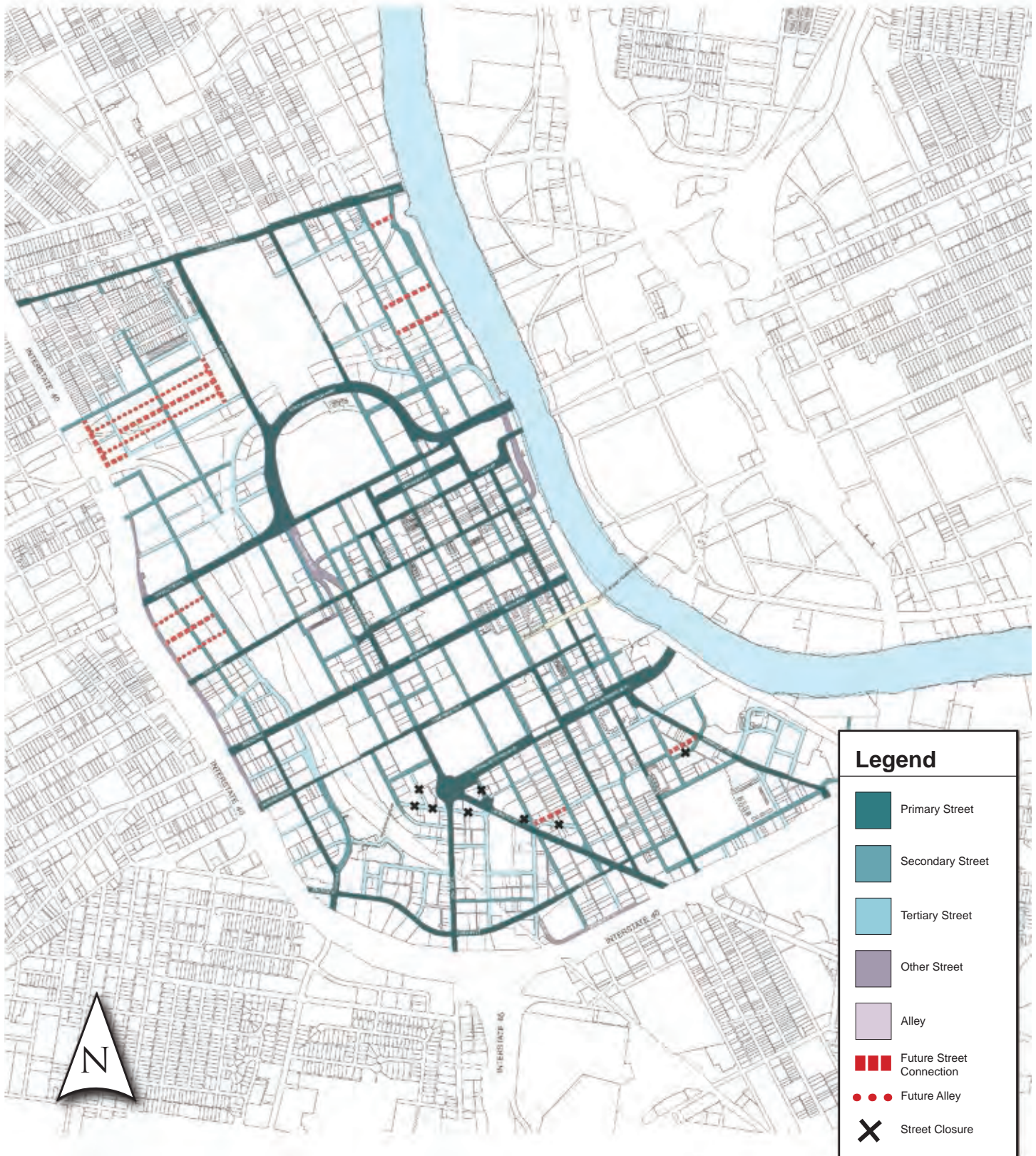
#### Future Streets

Downtown thrives on a connected system of streets which allow easy access within neighborhoods and to other parts of the city. There are, however, places for improvement. The Future Streets Plan show how streets could be realigned, connected and created in the future to improve mobility within Downtown.

Properties near an area highlighted for change on the Future Streets Plan shall consult with the Planning Department and the Department of Public Works to discuss the potential change.

# Section IV: General Standards

## Future Streets Plan



# Section IV: General Standards

## Lots and Frontages

### Frontages

A Frontage is the specific way in which the building face addresses the street. It is the transition and interaction between the private and public realms. Building Frontages define the character and form of the public spaces within each neighborhood. The following standards shall apply to all development within the DTC.

- Buildings shall front a street (excluding alleys), open space, or a pedestrian passage.
- Principal and Minor Frontages
  - Every property shall establish one Principal Frontage along a street.
  - When a lot fronts more than one street the following priority shall be given when establishing the Principal Frontage: Primary Street, Secondary Street, Tertiary Street, Other Street.
  - Any other frontage(s) shall be treated as a Minor Frontage.
  - In the instance a property fronts multiple Primary streets, any may be chosen as the Principal Frontage or all may be treated as the Principal Frontages.
  - Along a Minor Frontage, the façade width may be reduced to the maximum depth of the building along the Principal Frontage. The remaining lot width shall be defined with a knee-wall according to the Walls and Fences section of the DTC.
  - Along a Minor Frontage, modifications may be granted for the reduction of ground level garage liners and/or glazing requirements.
- Façade Width
  - The minimum façade width is the minimum amount of the frontage that must be defined by a building and is designated as a percentage of the frontage.
  - If a single lot frontage is greater than two hundred feet, the façade width may be reduced to a minimum of one hundred and fifty feet in length.
- Open Space Frontages
  - When building facades front on open space the standards of the adjacent street type (excluding Tertiary) shall apply.
  - All buildings fronting open space shall have a minimum of one primary pedestrian entrance on the open space.
- Pedestrian Passage Frontages
  - When building facades front on a pedestrian passage the standards of the adjacent street type (excluding Tertiary) shall apply.
  - All buildings fronting a pedestrian passage shall have a minimum of one primary pedestrian entrance on the pedestrian passage.

### Build-to Zone

- The Build-to Zone is the specified depth along a property's street frontage(s) in which the required minimum façade width must be located. The depth is Subdistrict and Street Type specific.
- Depending on site conditions the front of the Build-to Zone may begin at different locations.
  - When the existing sidewalk meets ~~with the Downtown Streetscape Design Guideline~~ Major and Collector Street Plan standards (including constrained right-of-way and local street dimensions as determined by Metro Planning) for sidewalk width, the Build-to Zone begins at the back of the sidewalk/property line.
  - When the existing sidewalk does not meet ~~with the Downtown Streetscape Design Guideline~~ the Major and Collector Street Plan standards (including constrained right-of-way and local street dimensions as determined by Metro Planning) for sidewalk width, the sidewalk ~~should~~ shall be widened on site and the Build-to Zone begins at the back of the new sidewalk.
  - When utility ~~or pedestrian~~ easements exist along the street frontage of a property the Build-to Zone shall begin at the back of the easement.
  - When buildings front an ~~Open~~ sSpace the Build-to Zone shall begin at the back of the ~~Open~~ sSpace.
- Attachments
  - Structures, including porches, stoops, and balconies shall not encroach beyond the front of the Build-to Zone.
  - Elements such as stairs, awnings, and landscaping may encroach beyond the front of the Build-to Zone. Any encroachments into the right-of-way must follow the Mandatory Referral process.



## Section IV: General Standards

### Lots and Frontages

- Entrances
  - All buildings shall have at least one pedestrian entrance on the Principal Building Frontage. This may be access to a lobby shared by individual tenants.
  - Corner entrances are appropriate on corner lots.
- Glazing
  - All street level exterior windows must have a minimum light transmission of 60 percent.
  - Modifications may be permitted insofar as it is determined that tinting does not substantially diminish the effect of the building wall or the pedestrian character of the street.
- Vehicular Access
  - When calculating the minimum façade width, access to structured parking shall be counted as part of the required façade width, and access to surface parking shall not be counted part of the required façade width. That is, access to surface parking is allowed in the “remaining” area, after the façade width requirement has been met. Surface parking is not allowed in the “remaining” area.

### Active Use

- An active ground floor use requirement shall mean a habitable space occupied by retail, office, residential, institutional or recreational uses, specifically excluding parking and mechanical uses.
- An active use is required on the ground floor of all Primary streets, Secondary streets, Open Space and pedestrian passages. An active use is encouraged on Tertiary streets, particularly if the Tertiary street is the only street frontage, but is not required.
- Any references to the requirement of an active use on the ground floor on “streets” or “public streets” shall exclude Tertiary streets, Other streets, and alleys and shall include Open Space and pedestrian passages. **However, if a Tertiary street is a project’s only street frontage, all active use requirements shall apply on that frontage.**
- The term “active use” and “building liner” are synonymous.

### Auto-oriented canopies and awnings

- Auto-oriented canopies and awnings, for uses such as drive-thrus and gas station pumps, may be attached to a building according to the following:
  - The building shall comply with all Frontage standards.
  - The canopy and/or awning shall be lower in height than the primary building.
  - The setback of the canopy and/or awning shall be a minimum of 15 feet from the back of the Build-to Zone.
- Exteriors
  - Any new roof or complete resurfacing of an existing roof must use a roofing material having an SRI of 29 or greater for roof slopes greater than 2:12 or SRI of 78 for slopes less than or equal to 2:12.

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## Section IV: General Standards

### Lots and Frontages: Specific to Storefront Frontage

The Storefront Frontage has a limited Build-to Zone that is close to the street, with building entrances accessible at sidewalk grade. The Storefront Frontage has substantial glazing on the facade at ground level, space for pedestrian-oriented signage, awnings, retail display, and other design features conducive with creating an active commercial streetscape.

The Storefront Frontage is commonly used for general commercial, office, retail, restaurant, lobby, etc.



# Section IV: General Standards

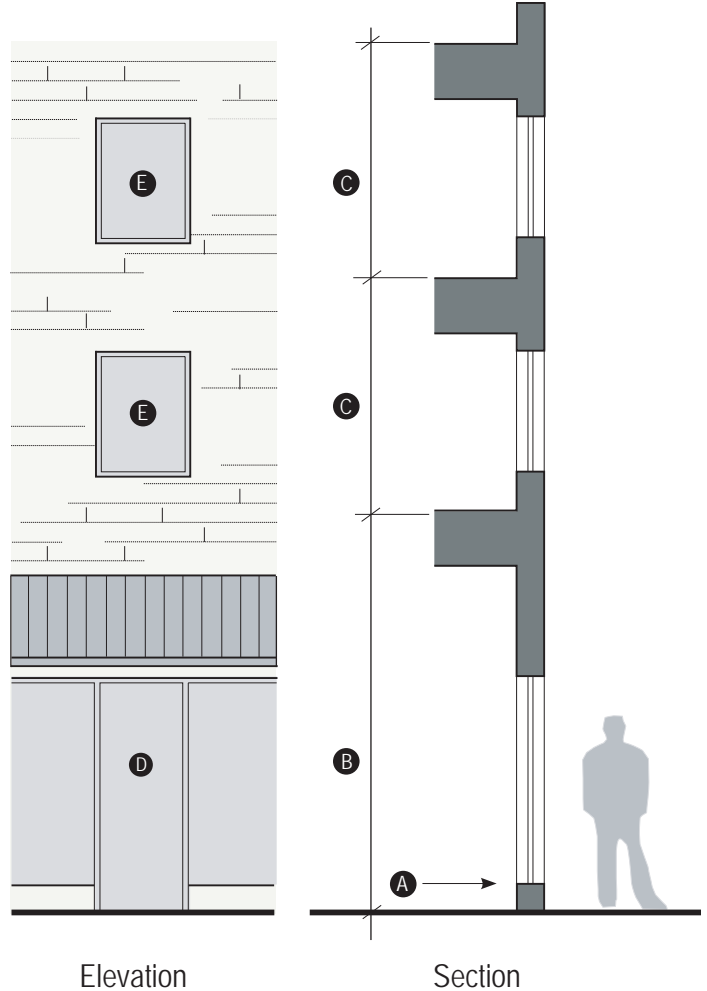
## Lots and Frontages: Specific to Storefront Frontage

### Storefront Frontage

<b>A</b> Max. sill height	3 ft
<b>B</b> Min. ground floor height	14 ft from grade
<b>C</b> Min. upper floor(s) height	10 ft floor to floor
<b>D</b> Min. ground floor glazing	
Principal Frontage	40% from grade to 14 ft
Minor Frontage	30 % from grade to 14 ft
<b>E</b> Min. upper floor(s) openings	25% from floor to floor

#### Notes

Where Storefront frontage is allowed, modifications may be given to allow for a Storefront arcade. All Storefront Frontage standards shall be met on the facade behind the arcade.



## Section IV: General Standards

### **Lots and Frontages: Specific to Stoop Frontage**

The Stoop Frontage has a limited to moderate Build-to Zone with the first floor elevated from the sidewalk grade. This frontage type utilizes a stoop - a small landing connecting a building entrance to the sidewalk by a stair or ramp - to transition from the public sidewalk or open space into the building.

Stoops are generally provided externally, but may be provided internally as necessitated for ADA compliance.

The Stoop Frontage is generally used for residential and live-work buildings, but may be appropriate for other uses.



# Section IV: General Standards

## Lots and Frontages: Specific to Stoop Frontage

### Stoop Frontage

<b>A</b> First floor elevation	
Min.	18" from grade
Max.	5 ft from grade
<b>B</b> Min. ground floor openings	30% floor to floor
<b>C</b> Min. upper floor(s) openings	25% from floor to floor

### Stoop

- D** Min. stoop width 5 ft
- E** Stoops may not extend beyond the front of the Build-to Zone.
- F** Steps may extend beyond Build-to Zone, but may not encroach into the public Right-of-Way.

### Notes

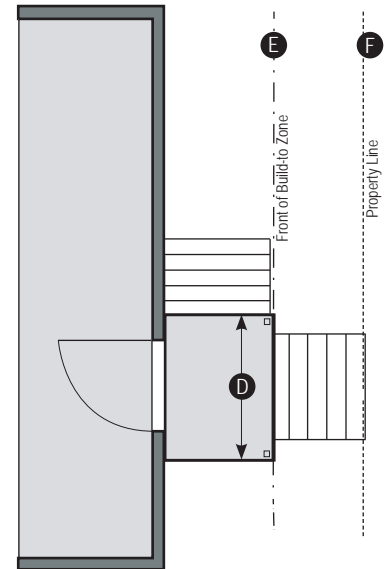
Greater first floor elevation allowed by modification for:

- Property with significant elevation change across the site at the street frontage.
- Development that incorporates below grade basement floors that are accessible from the exterior of the building.

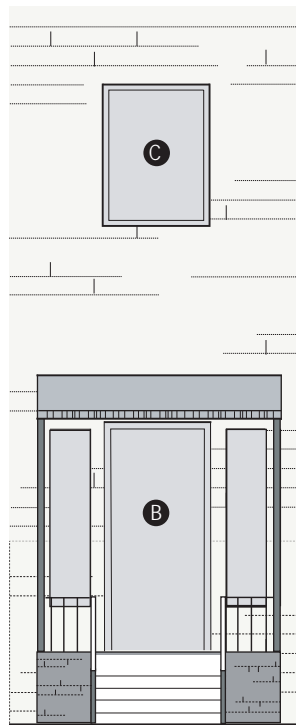
Transition to first floor elevation may be accommodated on the interior of the building to allow for compliance with ADA accessibility requirements.

Entries shall not be recessed more than 4 feet from the facade of the building.

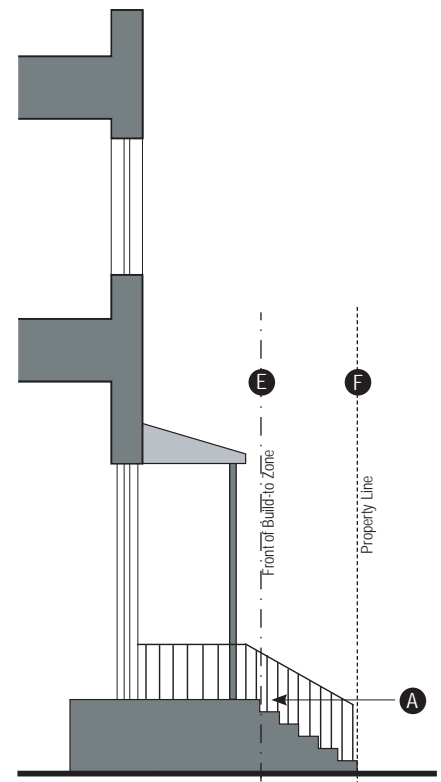
Doors shall face the street.



Plan



Elevation



Section

## Section IV: General Standards

### **Lots and Frontages: Specific to Porch Frontage**

The Porch Frontage has a moderate Build-to Zone with the first floor elevated from the sidewalk grade. The Porch Frontage utilizes a porch - an open air room appended to the mass of a building with floor and roof but no walls on at least two sides - to transition from the public sidewalk or open space into the building.

The Porch Frontage is primarily used for residential buildings.



# Section IV: General Standards

## Lots and Frontages: Specific to Porch Frontage

### Porch Frontage

#### A First floor elevation

Min.	18" from grade
Max.	5 ft from grade

#### B Min. ground floor openings

30% floor to floor

#### C Min. upper floor(s) openings

25% from floor to floor

#### Porch

- D Min. porch depth 5 ft
- E Stoops may not extend beyond the front of the Build-to Zone.
- F Steps may extend beyond Build-to Zone, but may not encroach into the public Right-of-Way.

#### Notes

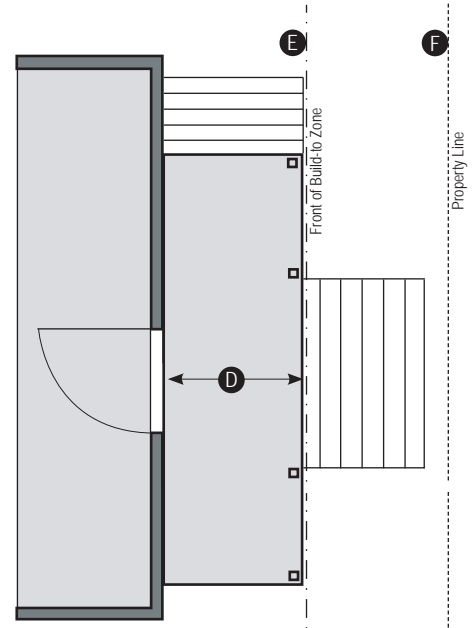
Greater first floor elevation allowed by modification for:

- Property with significant elevation change across the site at the street frontage.
- Development that incorporates below grade basement floors that are accessible from the exterior of the building.

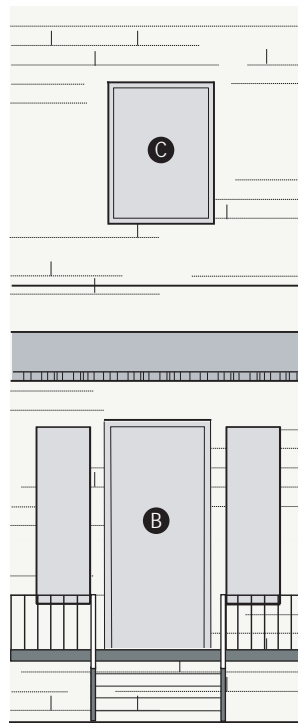
Transition to first floor elevation may be accommodated on the interior of the building to allow for compliance with ADA accessibility requirements.

Entries shall not be recessed more than 4 feet from the facade of the building.

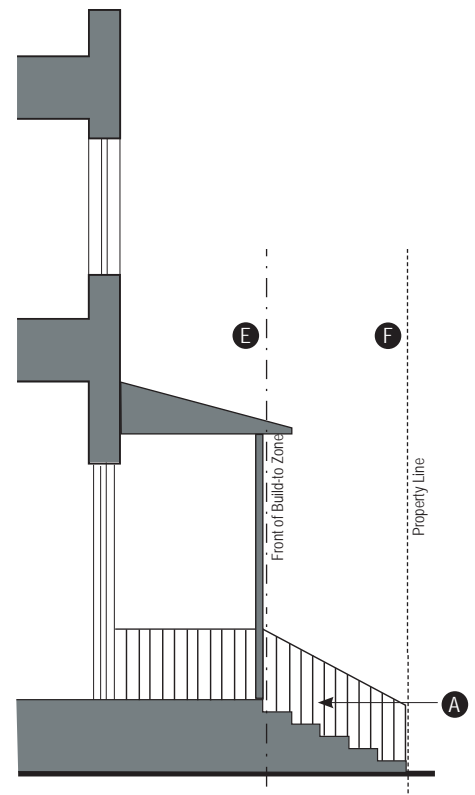
Doors shall face the street.



Plan



Elevation



Section



## Section IV: General Standards

### **Lots and Frontages: Specific to Industrial Frontage**

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The Industrial Frontage shall be used to adapt existing buildings to the standards of the DTC and for new construction of buildings intended for industrial uses. The Industrial Frontage shall be allowed only in specified subdistricts on specified streets and shall be prohibited on Primary Streets.

The Industrial Frontage mitigates the negative impact of the “blank wall” on the street by requiring the Build-to Zone to be entirely landscaped with drought-resistant plantings. All landscaping shall be in a functioning bioswale, or irrigated using drip irrigation or sub-surface irrigation. If drought-tolerant species are used, no irrigation is required.

A primary pedestrian entrance is required on the Principle Facade.

Associated vehicular entrances shall comply with the Parking and Access and Mechanical, Screening and Loading standards.

# Section IV: General Standards

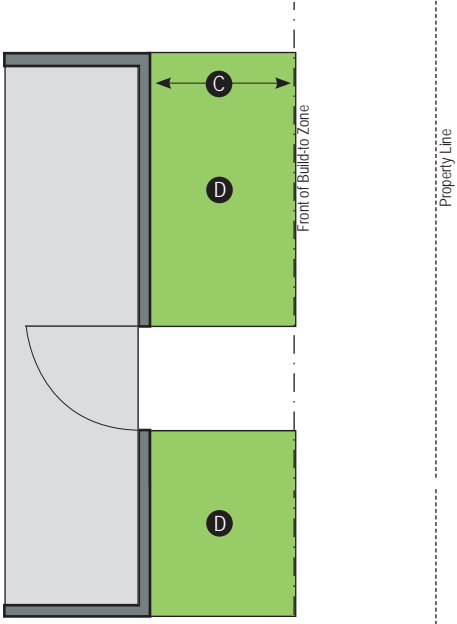
## Lots and Frontages: Specific to Industrial Frontage

### Industrial Frontage

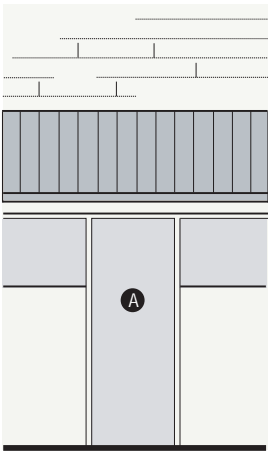
<b>A Front door</b>	Required on Principle Facade
<b>B Min. Building Height</b>	25 ft from grade
<b>C Build-to Zone</b>	5-10 ft
<b>D Landscaping</b>	Entire Build-to Zone shall be landscaped with drought resistant species; in a bioswale or irrigated 2'-6"
<b>E Min. landscaping height</b>	

**Notes**

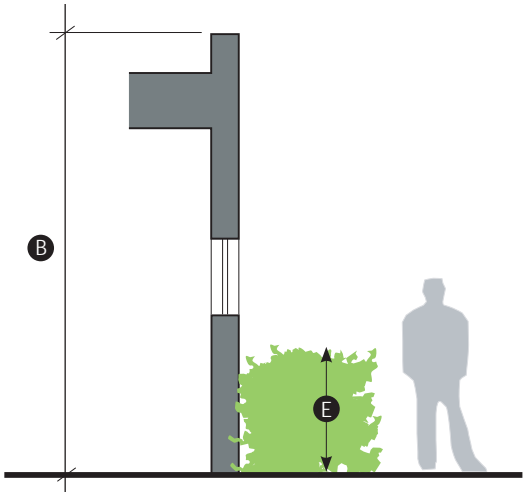
Industrial Frontage is prohibited on Primary streets. A building intended for an industrial use, with frontage on a Primary street shall comply with the standards of another frontage type on the Primary street.



Plan



Elevation



Section

## Section IV: General Standards

### **Lots and Frontages: Specific to Civic Frontage**

Civic buildings are designed and constructed for community use or benefit by governmental, cultural, educational, public welfare, or religious organizations. Civic buildings are inherently unique structures that present opportunities for unusual and iconic design within the urban fabric. Civic buildings should be designed with prominence and monumentality.

A Civic building shall be oriented to streets and public spaces and follow the intent of the particular subdistrict in which it is located with regard to pedestrian orientation, massing, and articulation.

Key architectural features should act as community focal points. Where possible, street axes should be terminated by the primary building form or architectural feature. Towers, spires, and other vertical forms are encouraged.

Civic buildings may include the following: community buildings, libraries, post offices, schools, religious institutions, publicly owned recreational facilities, museums, performing arts buildings, and municipal buildings.

Civic buildings shall be reviewed by modification.



# Section IV: General Standards

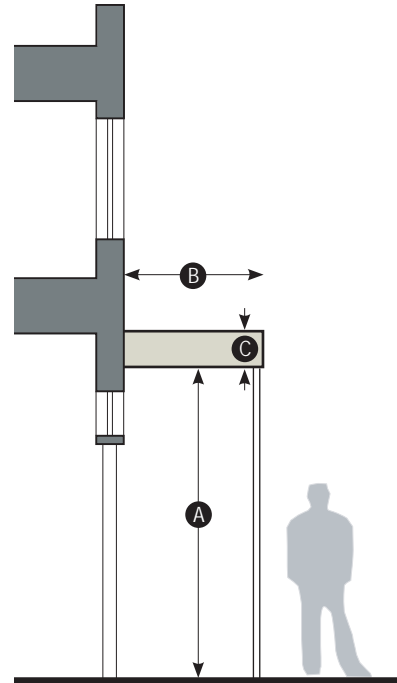
## Canopies and Awnings

### Canopies

<b>A Clearance</b>	
Minimum from sidewalk	8'
Minimum with ROW encroachment	14'
Maximum	25'
<b>B Maximum projection</b>	within 2' of curb
<b>C Maximum canopy height</b>	4'

#### Notes

Canopies shall be permitted only over pedestrian and vehicular building entrances, and shall not be permitted above windows. Canopies shall be constructed as a roof-like structure. Fabrics and non-rigid plastic are prohibited.



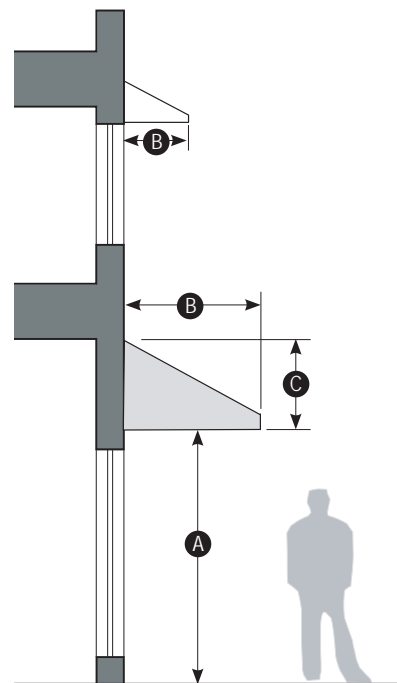
Section

### Canopies Awnings

<b>A Clearance</b>	
Minimum from sidewalk	8'
Minimum with ROW encroachment	14'
<b>B Maximum projection</b>	
First floor	4' from facade
Upper floors	2' from facade
<b>C Maximum awning height</b>	5'

See the Lots and Frontages section for details on auto-oriented canopies and awnings.

Encroachments in the public right-of-way must meet Metropolitan Government's current clearance standards and be approved under the mandatory referral process prior to installation.



Section

## Section IV: General Standards

### Parking and Access: General

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#### Parking Requirements

- No parking is required within the boundary of the DTC.

#### Parking and Access General Standards

- In addition to the Parking and Access standards of the DTC the following shall apply:
  - 17.20.050 Handicapped parking, 17.20.060 Parking area design standards, and 17.20.130 Loading space requirements.
- When alleys are present, vehicular access from alleys is **preferred required**. Access from public streets shall be considered in the following order: Other Streets, Secondary Streets, Neighborhood Streets and then Primary Streets as approved by Metro departments. Reviewers shall consider the public safety, street character, and pedestrian experience.
- Vehicular / Pedestrian Conflict
  - Valet and “drop-off” areas shall be located within the right-of-way when space allows.
  - **If not provided within the right-of-way, valet and drop-off areas should be located internal to the development.**
  - Where driveways to parking facilities or drop-off areas cross the Sidewalk Corridor, **priority shall be given to the pedestrian realm and** the following design elements shall be required:
    - **The DTC and MCSP sidewalks and tree planting standards shall be maintained for any pedestrian islands created.**
    - **Curbcuts shall not exceed 24’ in width for two-way traffic, and shall not exceed 20’ in width for one-way traffic.**
    - Bollards or other protective device shall be used to separate pedestrian and vehicular areas.
    - Distinction between vehicular lane and pedestrian areas shall be indicated through changes in grade, color, texture and/or material.
- To reduce stormwater fees and impact, utilize Low Impact Development strategies published in Metro Water Services Stormwater BMPs for hardscaping, including parking and drive lanes.

## Section IV: General Standards

### Parking and Access: Specific to Structured Parking

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#### Vehicular Access

- Vehicular openings to parking structures shall not exceed thirty-five feet in width.
- Vehicular openings shall have a minimum spacing of thirty-five feet.
- When alleys are present, the primary vehicular access shall come from the alley.

#### Pedestrian Access

- All parking structures with parking available to the public shall have a clearly marked pedestrian entrance, separate from vehicular access, on street frontages. A publicly accessible building lobby may meet this requirement.

#### Location and Lining

- On the ground level, parking structures shall be located behind a liner building with an active use that is a minimum of fifteen feet deep.
- Upper level habitable liners are encouraged on all streets and are required on the south side of Division Street (within the Gulch South Subdistrict) and on James Robertson Parkway. See the Bonus Height Program for more information on incentives for upper level garage liners.
- Upper level facade treatments /cladding is required on all public street frontages, including any facades visible from the Interstates. Facade treatments shall integrate or complement the architectural characteristics of the habitable portion of the building and the surrounding built context.
- Underground parking that is visible from the street, shall not extend beyond the façade of the building. Underground parking that is completely below grade may extend beyond the façade of the building. Underground parking may not encroach into the right-of-way.

## Section IV: General Standards

### Parking and Access: Specific to Surface Parking

#### General Standards for Surface Parking

- Parking area screening and landscaping standards shall apply to all surface parking lots including, but not limited to, public and private parking facilities, driveways and access aisles, the outdoor display of automobiles and other vehicles that are for sale or lease.

#### Perimeter Screening Standards for Surface Parking

- Parking areas adjacent to public streets and open space shall be separated from the edge of the right-of-way and/or property line by a perimeter landscape strip a minimum of five feet in width which shall be landscaped per the standards of this section.
  - All perimeter landscape strips adjacent to public streets and open space shall include a fence or wall in accordance with the Fence and Wall Standards.
- Parking areas shall be separated from adjacent side lot lines by a perimeter landscape strip a minimum of 5 feet in width, which shall be landscaped per the standards of this section.
  - A two and one-half foot landscape strip may be provided if the required trees are to be planted in tree islands located adjacent to the property line.
  - Two adjacent properties may share equally in the establishment of a seven-foot (minimum) planting strip along the common property line. In instances where the common perimeter planting strip is part of a plan for shared access, each owner may count the respective area contributed toward that common planting strip toward the interior planting area requirements for the lot.
- Berms are not permitted in any landscape strips.

#### Interior Planting Requirements

- Parking areas shall be landscaped in accordance with the interior planting requirements of Title 17.24.160.
- Parking areas with less than twelve thousand square feet in total area shall be exempt from the interior and side lot line planting requirements.

#### Landscape Materials

- Perimeter landscape strips along public streets, open space and side lot lines.
  - Trees shall be installed at a rate of one tree for every thirty feet of frontage. Spacing may be adjusted with the approval of the Urban Forester based upon tree species, the presence of utilities, and the dimensions of the planting strip.
  - Evergreen shrubs and trees shall be installed at appropriate spacing to fully screen vehicles to a minimum height of two and one-half feet.
    - Plantings within fifteen feet of driveways or street intersections shall be maintained to a maximum height of two and one-half feet.
    - Plantings shall not obstruct views onto site as to impede the security of users.
- Tree and shrub species shall be chosen from the Urban Forestry Recommended and Prohibited Tree and Shrub List or an alternative species deemed appropriate by the Urban Forester.
- At planting, trees shall be a minimum of six feet in height and two caliper inches.
- All landscaping shall be in a functioning bioswale, or irrigated using drip irrigation or sub-surface irrigation. If drought-tolerant species are used, no irrigation is required.
- At planting, all landscaping shall meet the standards for size, form and quality set out in the American Standard for Nursery Stock (ANSI Z60.1, latest edition).
- All nursery stock shall be vigorous, healthy and free of diseases or infestation.

## Section IV: General Standards

### Mechanical, Service, and Loading

#### Applicability

The following elements shall be shielded from view from adjacent public streets, pedestrian corridors, and open spaces.

- Refuse collection, dumpsters, recycling bins, and refuse handling areas that accommodate a dumpster or five or more trash or recycling cans.
- Building or ground-mounted mechanical equipment, including, but not limited, to transformers, backflow preventors, telephone risers, equipment cabinets, generators, or similar devices.
- Mechanical equipment on roofs.
- Air conditioning or similar HVAC equipment.
- Loading docks, berths, or similar spaces including, but not limited, to service entrances and maintenance areas.
- Outdoor storage of materials, equipment, and vehicles.

#### Location and Access

- Applicable site elements shall be located along the alley, along an interior property line, or internal to the property.
- Service elements, such as loading docks and trash collection locations, should not be accessible from Primary streets, unless a Primary street is the only frontage.
- **Where access to loading areas and service elements cross the Sidewalk Corridor, priority shall be given to the pedestrian realm and the following design elements shall be required:**
  - **The DTC and MCSP sidewalks and tree planting standards shall be maintained for any pedestrian islands created.**
  - **Bollards or other protective device shall be used to separate pedestrian and vehicular areas.**
  - **Distinction between vehicular lane and pedestrian areas shall be indicated through changes in grade, color, texture and/or material.**

#### Screening Standards

- Applicable site elements shall be fully screened at all times, including immediately following planting if vegetative materials are to be used.
- Refuse collection and refuse handling areas shall be screened by a walled enclosure with gates in accordance with the Fence and Wall Standards of the DTC.

#### Screening Methods

- **Vegetative Materials:**
  - Vegetative materials shall be planted in two rows in staggered fashion.
  - All trees shall be evergreen with a minimum height at time of planting of at least six feet above the root ball.
  - All shrubs shall be evergreen with the minimum height and spacing necessary to fully screen the item intended for screening (but no less than thirty inches in height) at the time of planting.
  - Vegetative material shall be located immediately adjacent to the element being screened in a planting area a minimum of four feet wide.
- **Fencing and Walls**
  - Screening is permitted through the use of a fence or wall constructed in accordance with the Fences and Walls Standards of the DTC.
- **Parapet Walls**
  - Parapet walls or other techniques included as an integral part of the building design shall be used to totally screen any rooftop mechanical equipment from view from adjacent public rights-of-way or open space.
- **Integrated Building Elements or Features**
  - Building design or other structural features (e.g., knee walls, alcoves, wing walls, roof extensions, etc.) may also be used to fully or partially enclose site features required to be screened.
- **Alternative Screening Methods**
  - Alternative screening methods or materials that are not listed may be used following approval by the Planning Commission or its designee, provided that they are determined to be comparable to screening methods described in this subsection.

\*\* In order to properly locate and screen mechanical equipment, approval may be required from applicable Metro departments and agencies.



## Section IV: General Standards

### Fences and Walls

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#### Location

- Permitted Locations: Fences and walls constructed in accordance with the standards in this section may be constructed within:
  - The Build-to Zone.
  - A utility easement only through the express written consent from the utility or entity holding the easement.
  - A required landscape area, Tree Protection Zone, or open space.
- Prohibited Locations: No fence or wall shall be installed that:
  - Encroaches into a right-of-way (without approval through the Mandatory Referral process).
  - Blocks or diverts a natural drainage flow on to or off of any other land.
  - Compromises safety by blocking vision at street intersections or obstructs the visibility of vehicles entering or leaving driveways or alleys.
  - Blocks access to any above ground or pad-mounted electrical transformer, equipment vault, fire hydrant or similar device.

#### Appearance

- All fences shall be installed so that the finished side shall face outward; all bracing shall be on the inside of the fence.
- Fences and walls shall be constructed of any combination of brick, stone, masonry materials, treated wood posts and planks, rot-resistant wood, or metal. Chain link fencing shall be coated with dark green or black vinyl when visible from a public street or open space (excluding alleys).
- Chain-link fences are prohibited within the Build-to Zone.
- Razor wire is prohibited within the Build-to Zone.
- Fences and walls used to screen refuse areas shall be opaque and include gates that prohibit unauthorized users to access the area.

#### Standards by function and location

- Fences and walls within the Build-to Zone shall not exceed four feet in height.
  - Modifications may be made in order to properly secure playgrounds and parks.
  - The height of fences and walls along a sidewalk shall be measure from sidewalk grade.
- Fences and walls within the Build-to Zone that are greater than three feet high shall be a minimum of thirty percent transparent to allow visibility into the property.
- Fences and walls used to screen parking shall be a minimum of two and one-half feet above the grade of the parking lot.
  - When a fence or wall is combined with plantings the majority of the plantings shall be between the right-of-way and the fence or wall.
- Fences and walls used to screen mechanical, loading and refuse elements shall be a minimum of two feet taller than the element being screened.
- All other fences and walls shall have a maximum height of ten feet measured from grade.
- Fences surrounding athletic fields and courts may exceed the previous height limitations.

## Section IV: General Standards

### Open Space

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The *Downtown Community Plan: 2007 Update* envisions accessible, enjoyable open spaces to help create vital and functioning neighborhoods within Downtown. To meet this goal, the DTC encourages many types of open spaces to serve the needs of both citizens and visitors for passive and active recreation. Public art, other amenities, and interactive features are encouraged in open spaces. Buildings conducive with the use of the Open Space and for public use may be approved by the Planning Commission and Parks Department.

It is a goal of the DTC to have open space within each ¼ mile radius neighborhood in Downtown. All public and private open spaces, greater than one-half acre have been mapped in order to determine the areas within the DTC that are in need of neighborhood open space. The one-quarter mile radius buffer area around existing open spaces is indicated in green on the Open Space Map. The areas that lack open space within one-quarter mile radius (deficiency areas) are shown in yellow. Public open space developed in any portion of a deficiency area will count toward fulfilling the open space need of that area. The development of the needed open space may be done by public or private entities.

The open space types and standards listed in the DTC shall be utilized by property owners in the development of public open space. The following standards shall apply to open space that is accessible to the public, including open space developed for credit under the Bonus Height Program. Private open space or amenity areas shall not be counted toward fulfilling the open space need and shall not be eligible for the Bonus Height Program.

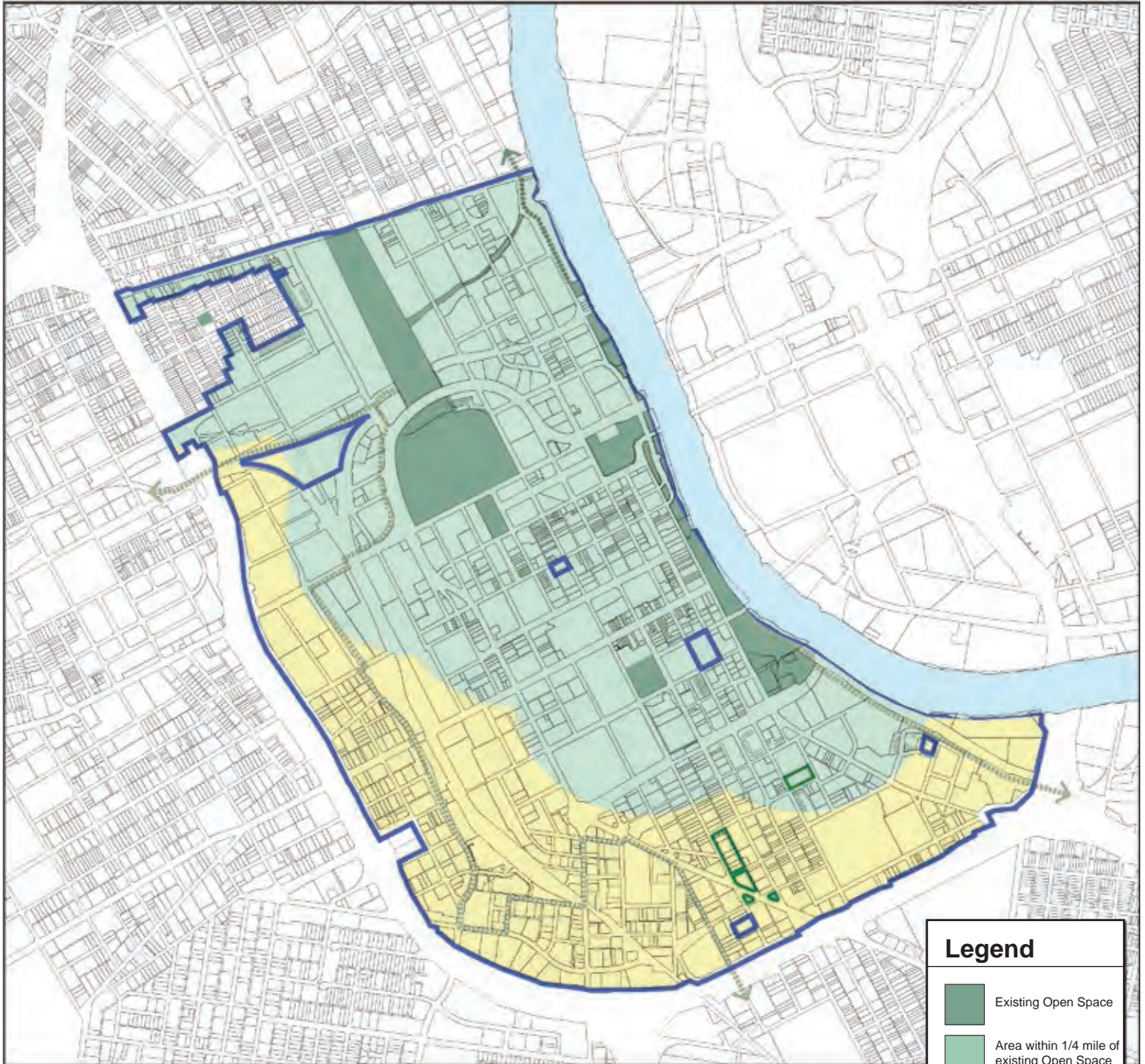
Open Space developed within the deficiency areas are eligible for greater bonuses through the Bonus Height Program than those developed in non-deficiency areas. See the Bonus Height Program section for more details.

The Open Space Plan also includes the existing and planned urban greenways.

Parks and greenways are publicly owned open space and shall follow the standards set out by the Parks Department.

# Section IV: General Standards

## Open Space Plan



**Legend**

- Existing Open Space
- Area within 1/4 mile of existing Open Space
- Area with an Open Space Deficiency
- Existing Urban Greenway
- Future Urban Greenway
- Potential parcels for Open Space
- DTC Boundary



# Section IV: General Standards

## Open Space: General Standards

Standards of Title 17 not varied by the following Open Space Standards shall apply within the DTC.

### Calculation

- When calculating the open space square footage or acreage, the footprint of any building, whether public or private, shall be subtracted first. The remaining square footage shall be used for all calculations and percentages.

### Access

- Every open space shall have a minimum of one primary pedestrian entrance along each street frontage and pedestrian frontage.
- All publicly accessible open space shall meet the appropriate standards of the American's with Disabilities Act.

### Building frontage

- Buildings that are part of the same development as the open space and abut the open space shall have a minimum of one pedestrian entrance on the open space.

### Seating

- Permitted types of seating include but are not limited to, moveable, fixed individual seats, fixed benches, seat walls, planter ledges and seating steps.

### Paving Materials

- Asphalt may be approved by the Planning Commission or its designee for recreational jogging or bicycle paths only.

### Landscaping

- Ground level green space shall consist of turf grass, shrubbery, perennial and annual beds, mulched areas and generally areas with "natural" material planted within six inches of grade.
  - Ground level green space does not include container plantings.
- Pervious surfaces include green space, porous concrete and modular pavers, areas with tree grates or areas that otherwise allow water to infiltrate into the soil.
- Trees shall be accommodated in planting areas with a minimum of 600 cubic feet of soil. When using structural soil, the planting area may be reduced to 300 cubic feet. The minimum opening at grade shall be 25 square feet.
- Planting areas shall not impede ingress/egress from buildings or pedestrian traffic.
- Tree Grates
  - When used, tree grates shall be modular and allow for removal as tree grows.
  - Tree grates shall be flush with grade to allow for unobstructed movement of pedestrian traffic.
- Tree and shrub species shall be chosen from the Urban Forestry Recommended and Prohibited Tree and Shrub List based on tree size and planting area provided or an alternative species deemed appropriate by the Urban Forester.

## Section IV: General Standards

### Open Space: Specific to Greens

Greens are larger, less formal Open Space consisting of a majority green space with laws, paths, and vegetation.

- A minimum of two sides shall be street frontages.
- Size ½ acres - 6 acres
- Greens shall maintain a minimum of 60 percent ground level green space and 70 percent pervious surface.
- Seating
  - A minimum of one linear foot of seating shall be required for each 900 square feet of gross open space.
  - Of the required seating, one linear foot for each 20 feet of street frontage shall be located within 15 feet of the property line.

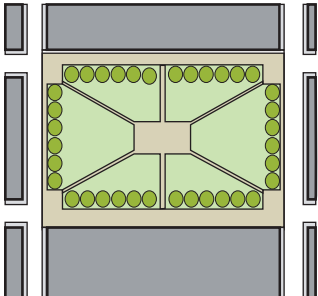


# Section IV: General Standards

## Open Space: Specific to Squares

Squares are Open Space used for unstructured recreational or civic uses. Landscaping consisting of lawns and trees is formally composed.

- A minimum of two sides shall be street frontages.
- Size: 1/2 acre – 5 acres
- Squares shall be required to maintain a minimum of 30 percent ground level green space and 50 percent pervious surface.
- Seating
  - A minimum of one linear foot of seating shall be required for each 700 square feet of gross open space.
  - Of the required seating, one linear foot for each 20 feet of street frontage shall be located within 15 feet of the property line.

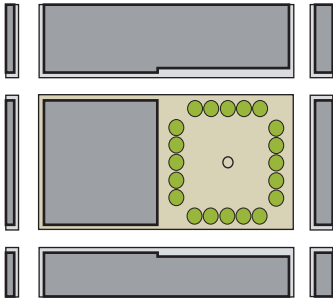


# Section IV: General Standards

## Open Space: Specific to Plazas

A Plaza is an Open Space used for unstructured civic and/or commercial purposes. A plaza is spatially defined by building frontages.

- Size: 2500 square feet – 20,000 square feet
- Plazas are required to maintain a minimum of 10 percent ground level green space and 40 percent pervious surface.
- Seating
  - A minimum of 40 linear feet of seating shall be required for Plazas, plus a minimum of one linear foot of seating for every 500 square feet of gross open space.
  - Of the required seating one linear foot for each 20 feet of street frontage shall be located within 15 feet of the property line.

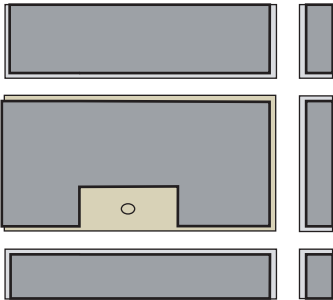


# Section IV: General Standards

## Open Space: Specific to Courts

A Court is Open Space accessible from the street and used for entry into a building. A Court is spatially defined by building frontages and is generally tucked back into the building.

- Size: 400 square feet – 2,500 square feet
- Seating
  - A minimum of 10 linear feet of seating shall be required for Courts, plus one linear foot of seating for each 300 square feet of open space minimum.
- Accessibility
  - Courts are permitted to be closed to the public by use of a gate. See the Fence and Wall Standards for details.



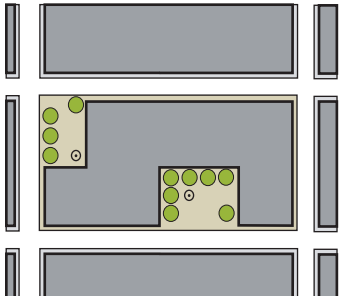


# Section IV: General Standards

## Open Space: Specific to Pocket Parks and Playgrounds

Pocket Parks and Playgrounds are Open Space that are accessible from the street and used for structured recreation, gardening or other community use.

- Size: 800 square feet – 1 acre
- Pocket Parks shall provide a community benefit such as a garden or playground.
- Pocket Parks are required to maintain a minimum of 20 percent ground level green space.
  - Additional ground area may be impervious provided space is structured for active recreation.
- Seating:
  - A minimum of one linear foot of seating shall be required for every 300 square feet of gross open space.
- Trees
  - The tree requirement may be waived for pocket parks that are designed for structured active recreation, such as basketball or tennis courts.
- Accessibility
  - Pocket Parks may be fenced for safety but shall remain open to the public during daylight hours.



## Section IV: General Standards

### Bonus Height Program

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The Bonus Height Program (BHP) allows additional building height in Downtown in exchange for contribution to specified programs that provide benefits to the public. The Bonus Height shall be permitted if the proposed development contributes to specific public benefits in the amount and manner set forth herein.

Bonus Height shall be permitted in exchange for the following public benefit contributions: Leadership in Energy and Environmental Design (LEED) certification of individual buildings, LEED for Neighborhood Development, pervious surface, **Historic Building Preservation**, publicly-accessible Open Space, Workforce Housing, Civil Support Space, upper level garage liners, and underground parking.

~~Density bonuses for the preservation of historic buildings are given through the Transfer of Development Rights Program as outlined in Section 17.12.120 of the zoning code.~~

### Bonus Height Standards

- Upon providing a binding commitment for the specified public benefit, the proposed development project shall be allowed to build within the restrictions of the Subdistrict, up to the Bonus Height Maximum as established within this section.
- Multiple height bonuses may be compounded insofar as the total additional height does not exceed the Bonus Height Maximum for the Subdistrict.
- Additional development rights achieved through the BHP may be transferred to another site within the DTC **one time to one receiving site**, provided the transferred height does not exceed the Bonus Height Maximum of the receiving site. By-right height may not be transferred; only bonus height received through the BHP may be transferred.
- Bonus height transfers shall be based on the square footage of the sending site, not the receiving site.
- No building permit shall be issued for bonus height until the Planning Commission has certified compliance with the provisions of this section, upon referral and assurance of compliance from applicable departments.

## Section IV: General Standards

### Bonus Height Program

#### LEED and LEED ND

The U.S. Green Building Council (USGBC) is a non-profit organization that oversees the Leadership in Energy and Environmental Design (LEED) Green Building Rating System.

LEED for Neighborhood Development integrates the principles of smart growth, urbanism and green building into the first national system for neighborhood design. LEED ND goes beyond the building to address sustainability on a neighborhood-wide basis.

The bonuses are specific to each Subdistrict. See the BHP Chart for details.

A different nationally-recognized, third-party system of overseeing green building and/or sustainable development practices may be substituted for LEED. Bonuses will be determined by the Planning Commission based on ratings equivalent to LEED silver, gold, and platinum.

Bonuses for individual buildings are given upon pre-certification of LEED silver, gold and platinum. Bonuses for neighborhoods are given upon pre-certification of LEED ND. Every property within the LEED ND neighborhood may utilize the bonus height. The bonuses are specific to each Subdistrict. See the BHP Chart for details.

The following shall apply to all new construction that utilizes the Bonus Height Program for LEED:

- Prior to issuance of a temporary certificate of occupancy for any use of the development, a report shall be provided for the review of the Department of Codes Administration and the Planning Commission by a LEED accredited professional. The report shall certify that all construction practices and building materials used in the construction are in compliance with the LEED certified plans and shall report on the likelihood of certification. If certification appears likely, temporary certificates of occupancy (as set forth below) may be issued. Monthly reports shall be provided as to the status of certification and the steps being taken to achieve certification. Once certification is achieved, the initial certificate of LEED compliance, as set forth herein, and a final certificate of occupancy (assuming all other applicable conditions are satisfied) shall be issued.
- To ensure that LEED certification is attained the Department of Codes Administration is authorized to issue a temporary certificate of occupancy once the building is otherwise completed for occupancy and prior to attainment of LEED certification. A temporary certificate of occupancy shall be for a period not to exceed three (3) months (with a maximum of two extensions) to allow necessary time to achieve final certification. Fees for the temporary certificate (and a maximum of two extensions) shall be \$100 or as may otherwise be set by the Metro Council. Once two extensions of the temporary certificate of occupancy are granted, any additional extensions shall be granted only in conjunction with a valid certificate of LEED noncompliance as set forth herein.
- If the property fails to achieve LEED certification, the Department of Codes Administration is authorized to issue a short-term certificate of LEED noncompliance. This certificate will allow the building to retain its certificate of occupancy pending attainment of LEED certification. A certificate of LEED noncompliance shall be for a period not to exceed three (3) months and may be renewed as necessary to achieve certification. The fee for noncompliance shall be issued every time the certificate is issued for up to ten years.
- The fee for a certificate of LEED noncompliance shall be based on the following formula:  $F = [(CN-CE)/CN] \times CV \times 0.0075$ , where:
  - F is the fee;
  - CN is the minimum number of credits to earn the level of LEED certification for which the project was pre-certified;
  - CE is the number of credits earned as documented by the report; and
  - CV is the Construction Value as set forth on the building permit for the structure.

## Section IV: General Standards

### Bonus Height Program

#### Pervious Surface

The integration of pervious surfaces into site design and building design benefits the individual development, the neighborhood and the city. Pervious surfaces can reduce stormwater runoff, flood risk, irrigation needs and the burden on infrastructure. Examples of pervious surfaces include impervious pavement, green roofs, bio-swales, landscaping, and green screens. As technology in this field advances, additional pervious surfaces may meet the intent of this standard.

- The number of square feet of Bonus Height shall be twice that of the number of square feet of Pervious Surface. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Green roofs that are utilized to meet LEED certification may not be “double counted” for both the LEED height bonus and the Pervious Surface height bonus. If the level of LEED certification would be met without the green roof, then the green roof may be counted for the Pervious Surface height bonus.

#### Historic Building Preservation

The preservation and re-use of Downtown’s historic buildings is critical to maintaining the character and identity of Nashville. Use of this bonus shall require a recommendation from the Metropolitan Historical Commission, or its designee, on the worthiness of preserving a building outside of a Historic Overlay District, including but not limited to buildings worthy of conservation, and buildings listed on or eligible to be listed on the National Register of Historic Places. A recommendation shall also be provided on the square feet of the preserved historic building footprint.

- The number of square feet of Bonus Height shall be equal to the development rights being forfeited by the preservation of the building, calculated as follows: the number of by-right stories permitted minus the number of stories of the historic building, multiplied by the square feet of the historic building footprint. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.

- The binding commitments shall consist of an instrument recorded in the register of deeds, that records the transferred bonus height and square feet calculations as first certified by the Planning Commission; records the preservation of the historic building in perpetuity; records the forfeiture or any future claim for demolition or for additional building intensity of development, including any type of rezoning or variance of the preserved historic building; and records the project’s and/or receiving site’s additional square feet and building height bonus derived from the transfer.

#### Publicly-Accessible Open Space

Accessible, enjoyable open spaces are essential for vital and functioning neighborhoods. Open space provides the community with opportunities to be in an outdoor setting, while encouraging social interaction. See the BHP Chart for details for a list of Subdistricts in which the Open Space bonus may be utilized.

Open Space must be designed to the open space standards of the DTC. To be eligible for the Height Bonus, open space must be a minimum of ¼ acre in area.

- Plazas are not eligible for the BHP.
- In Open Space deficiency areas (See the Open Space section of the General Standards), the number of square feet of Bonus Height shall be seven times that of the number of square feet in open space. Outside of Open Space deficiency areas, the number of square feet of Bonus Height shall be four times that of the number of square feet in open space. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Bonuses are available only for publicly accessible (whether publicly or privately owned) open space.

## Section IV: General Standards

### Bonus Height Program

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#### Workforce Housing

Housing encompassing a range of sizes, costs and tenure (both rental and owner-occupied), to accommodate the diverse range of employees and their families, is key to Downtown's continued economic health and to sustainable development patterns for Nashville/Davidson County.

Metro Government shall require the developer to execute an agreement, restrictive covenant, or other binding restriction on land use that preserves affordability and establishes the manner in which the affordability will be monitored for the required period before final site plan review.

As listed below, the specified percentage of total units shall be reserved for ownership or rental by households with incomes below the specified percentage of the current Average Median Income (AMI) in Davidson County, as determined by MDHA.

Height bonuses are based on a percentage of the Maximum Height allowed on the property as dictated by the Subdistrict. In all cases, fifty percent (50%) of the additional stories shall be dedicated to Workforce Housing with twenty-five percent (25%) of the height bonus provided as housing for persons below one hundred percent (100%) of AMI and twenty-five percent (25%) of the height bonus provided as housing for persons below one hundred-twenty percent (120%) of AMI.

Workforce Housing Height Bonuses are:

- Level 1 - 10% increase in stories
- Level 2 - 20% increase in stories
- Level 3 - 30% increase in stories
- Level 4 - 40% increase in stories

Not all Levels are available in every Subdistrict. See the BHP Chart for details.

When percentage calculations result in a fraction of a story, the number of stories shall be rounded up.

The following shall apply to all construction that utilizes the BHP for Workforce Housing:

- Owner-occupied units shall remain affordable for 30 years.
- Renter-occupied units shall remain affordable for 30 years.
- Units that are converted from renter-occupied to owner-occupied shall remain affordable (as determined above) for 30 years beyond conversion.
- The size of all Workforce Housing units shall be at least 80% of the average size of market rate units.

## Section IV: General Standards

### Bonus Height Program

#### Civil Support Space

The dedication of Civil Support Space offers height bonus for the developer's contribution of space to a specific use or entity that serves to better the neighborhood or community. See the BHP Chart for details for a list of Subdistricts in which the Civil Support Space bonus may be utilized.

- Civil Support Space is typically on the ground level. Upper levels may be appropriate depending on the intended use.
- The number of square feet of Bonus Height shall be twice that of the number of square feet donated to Civil Support Space. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Civil Support Space shall be dedicated to the chosen use or uses for 15 years. Adherence to this standard shall be checked yearly by the Planning Commission or its designee.

The Planning Commission may require the developer to execute an agreement, restrictive covenant, or other binding restriction on land use that preserves the use of Civil Support Space for the required period before final site plan review.

The following uses are appropriate for Civil Support Spaces:

- Institutional Uses
  - Cultural center
  - Day care center
  - School day care
- Education
  - Community education
- Transportation Uses
  - Water taxi station
- Waste Management Uses
  - Recycling collection center
- Recreational and Entertainment Uses
  - Community playground
- Other Uses
  - Community garden

Other uses may be appropriate for Civil Support Space. The applicant may propose a different use for Civil Support Space to be approved by the Planning Commission.

#### Upper Level Garage Liner and Underground Parking

The public realm of the streetscape is improved by providing parking in underground structures and lining above ground parking structures with habitable space. See the BHP Chart for a list of Subdistricts in which the Garage Liner and Underground Parking bonuses may be utilized.

- Height bonuses are given for upper levels of habitable space, a minimum of ~~15'~~ 20' in depth, which masks a parking structure from view along public streets and open space. The minimum depth may be reduced by the Planning Commission provided the intent of an active streetscape is met.
- The number of square feet of Bonus Height shall be twice that of the number of square feet in Garage Liners. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- The number of square feet of Bonus Height shall be equal to the number of square feet in Underground Parking. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Height bonuses are not given for ground level liners, or upper level liners that are required by the DTC.

#### Public Parking

Parking accessible to the general public is important to the continued growth and vitality of Downtown. See the BHP Chart for a list of Subdistricts in which the Public Parking bonuses may be utilized.

- The number of square feet of Bonus Height shall be twice that of the number of square feet in Public Parking. The additional square footage may be used to the Bonus Height Maximum as determined on the BHP Chart.
- Public Parking shall be clearly marked as public, and shall be accessible to the public, at all hours that the garage is open, for the lifetime of the building.

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# Section IV: General Standards

## Bonus Height Chart

Maximum height within the Subdistrict	LEED	LEED ND	Pervious Surface	Historic Building Preservation	Open Space	Workforce Housing	Civil Support Space	Upper Level Garage Liner & Underground Parking	Public Parking	Bonus Height Maximum	
<b>Central</b>											
James Robertson	elevation of 560'	Any = 2 stories	2 stories	2 stories	2 stories	2 stories	All Levels to a maximum of elevation above sea level of 560'	2 stories below 560' elevation	2 stories below 560' elevation	elevation 560'	
<b>Core</b>											
Core	30 stories	Silver = 4 stories; Gold = 8 stories Platinum = 12 stories	2 stories	2 stories	8 stories	8 stories	All Levels	8 stories	8 stories	2 stories	Unlimited
Core-historic	6 stories on the interior of blocks; 10 stories on the corners	No bonus	No bonus	No bonus	4 stories	No bonus	No bonus	No bonus	No bonus	No bonus	6 stories on the interior of blocks; 10 stories on the corners- 10 stories
<b>Upper Broadway</b>											
Upper Broadway	100'	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	100'
<b>Second and Broadway</b>											
Second	8 stories within 105'	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	8 stories within 105'
Broadway	5 stories within 65'	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	5 stories within 65'
<b>SoBro</b>											
General	30 stories	Silver = 4 stories; Gold = 8 stories Platinum = 12 stories	2 stories	2 stories	8 stories	8 stories	All Levels	8 stories	8 stories	2 stories	Unlimited
west side of 1st and east side of 2nd	15 stories within 220'	Silver = 4 stories; Gold or Platinum = 5 stories	2 stories	2 stories	5 stories	5 stories	Levels 1, 2, and 3	5 stories	5 stories	2 stories	20 stories within 300'
<b>Central River</b>											
Central River	10 stories within 145'	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	10 stories within 145'
<b>South</b>											
<b>Lafayette</b>											
General	8 stories	Any = 2 stories	2 stories	2 stories	2 stories	2 stories	Levels 1, 2, and 3	2 stories	2 stories	2 stories	11 stories
Lafayette Street	12 stories	Any = 2 stories	2 stories	2 stories	3 stories	3 stories	Levels 1, 2, and 3	3 stories	3 stories	2 stories	16 stories
<b>Rutledge Hill</b>											
Primary and Secondary	6 stories	Any = 1 story	1 story	1 story	1 story	1 story	Levels 1, and 2	1 story	1 story	2 stories	7 stories
Neighborhood Tertiary	3 stories	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	3 stories
<b>Rolling Mill Hill</b>											
Rolling Mill Hill	65 feet with an SEP of 1:1.5	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	65 feet with an SEP of 1:1.5
<b>Rutledge River</b>											
Rutledge River	9 stories	Any = 2 stories	2 stories	2 stories	2 stories	2 stories	Levels 1, 2, 3 and 4	2 stories	2 stories	2 stories	12 stories
<b>West</b>											
<b>Gulch North</b>											
General	7 stories	Any = 2 stories	2 stories	2 stories	2 stories	2 stories	All Levels	2 stories	2 stories	2 stories	10 stories below 560' elevation
fronting Herman Street	4 stories	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	2 stories	4 stories
<b>Gulch South</b>											
General	10 stories	Any = 2 stories	2 stories	2 stories	2 stories	2 stories	All Levels	2 stories	2 stories	2 stories	16 stories
fronting Church, Broadway, Demonbreun	15 stories	Any = 2 stories	2 stories	2 stories	3 stories	3 stories	All Levels	3 stories	3 stories	2 stories	21 stories
at key intersections	20 stories	Silver or Gold = 2 stories; Platinum = 3 stories	2 stories	2 stories	4 stories	4 stories	All Levels	4 stories	4 stories	2 stories	28 stories
<b>North</b>											
<b>Hope Gardens</b>											
Primary	7 stories	Any = 1 story	1 story	1 story	1 story	1 story	Level 1	1 story	1 story	1 story	8 stories
Secondary	4 stories	Any = 1 story	1 story	1 story	1 story	1 story	Level 1	1 story	1 story	1 story	5 stories
Neighborhood Tertiary	3 stories	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	No bonus	<del>4-story</del> No Bonus	3 stories
<b>Sulphur Dell</b>											
Primary	7 stories	Any = 1 story	1 story	1 story	1 story	1 story	Level 1	Any = 1 story	1 story	1 story	8 stories
Secondary	5 stories	Any = 1 story	1 story	1 story	1 story	1 story	Level 1	Any = 1 story	1 story	1 story	6 stories
Neighborhood Tertiary	4 stories	Any = 1 story	1 story	1 story	1 story	1 story	Level 1	Any = 1 story	1 story	1 story	5 stories



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# Section V

## SIGN STANDARDS



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# Section V: Sign Standards

## Introductory Provisions

### Intent

The purpose of these regulations is to set specific sign standards that accomplish the following:

- Establish reasonable and improved standards for Downtown business identification;
- Encourage creative and innovative approaches to regulating signs consistent with the principles of the Downtown Community Plan;
- Promote economic vitality in Downtown;
- Enhance the overall visual environment in Downtown by discouraging signs that contribute to the visual clutter of the streetscape;
- Ensure signs are designed for the purpose of identifying a business in an attractive and functional manner; and
- Ensure signs reinforce the existing and envisioned character and are complementary to the architectural design of Downtown.

### Applicability

- These sign regulations apply to all properties zoned DTC and are not in an Historic Zoning Overlay. See map on Page 107.
- In addition to the standards set forth within this section, the following Sections of the Metro Zoning Code shall apply to all regulated signage within the DTC.
  - Section 17.04.06 - Definitions of general terms
  - Within Chapter 17.32 SIGN REGULATIONS:
    - Section 17.32.020 - General Provisions
    - Section 17.32.040 - Signs allowed without a permit
    - Section 17.32.050 - Prohibited signs
    - Section 17.32.060 - Permitted on-premises temporary signs
    - Section 17.32.145 - Landmark signs
    - Section 17.32.160 - Computations
  - Within Article XI. Sign Procedures
    - 17.40.490 - Permits and compliance tag.
    - 17.40.510 - Unsafe, illegal, dilapidated and abandoned signs

### Sign Permit Applications

Applications for sign permits shall be made with and reviewed by the Codes Department. All sign applications that do not involve Modifications shall only require Codes Department approval, regardless of whether the property is subject to additional design guidelines (e.g. MDHA redevelopment districts).

Applicants for sign permits shall submit the following information. Incomplete applications will not be accepted.

- Design and details of the signage depicting size and shape (including height, width and depth), anchoring, materials, lighting and other data necessary to determine compliance with the requirements of this section and with the requirements of the Metropolitan building code and the Metropolitan electrical code. Additional information may be required by Codes.
- Drawings and specifications, including building elevations or artist's rendering depicting the sign faces, and dimensions indicating sign placement on the building.
  - For ground signs and signs seeking a ROW encroachment, the distance of the signs from the corner of the building or property line should be included.
- A site plan, drawn to scale, depicting the location of the proposed signage and all relevant features of the site, including location and size of other regulated signs.
- The property address, applicant and sign designer's name and contact information.

### Common Sign Plan

A common signage plan regulates signage for multiple businesses or tenants within one building or complex. A common sign plan is mandatory for all new developments and sign Modifications.

- A common signage plan shall provide for consistency among signs with regard to at least four of the following: materials; location of each sign on the building; sign proportions; color scheme; lighting; lettering or graphic style.
- The common signage plan shall establish an allowable area of signage for existing and future tenants with regard to all allowed signs types.
- The common signage plan shall indicate existing nonconforming signs as well as the amount and location of on-premises signage to be allocated to each tenant under the new plan.

# Section V: Sign Standards

## Modifications

### Sign Permit Modifications

Requests for modifications to sign standards are reviewed through the process outlined on pages 14 and 15.

Modifications are reviewed by Planning staff and either the MDHA Design Review Committee or the DTC Design Review Committee.

The applicant may appeal a decision through the process described on pages 14 and 15.

All sign Modifications are Major Modifications. Two additional types of Modifications for signage related permits may be requested and are outlined below.

### Modifications for Exceptional Design

Creative signage that does not fit the specific regulations of this section may be considered by the appropriate reviewing body (listed above), based on its merits, as they relate to all of the following design criteria:

- architecture
- the configuration or location of the building or property
- building scale
- legibility
- technical competence and quality in design, construction and durability

Applications for this type of Modification require submittal of a common sign plan for the property in question. Approval of any related structures (i.e. canopy) will require review by all applicable agencies. Exceptional design modifications shall not permit electronic changeable copy where it is otherwise not permitted.

### Modifications for Tourist-oriented Businesses

Tourist-oriented businesses within DTC zoning may receive Modifications to allow greater sign area and use of digital technology. To qualify as a tourist-oriented business a business shall:

- have a minimum permanent fixed seating capacity of 500; and
- offer lawful activities or services to the general public of cultural, historical, recreational, educational, or entertainment purposes.

Applications for this type of Modification require submittal of a common sign plan for the property in question. Approval of any related structures (e.g. canopy) will require review by all applicable agencies. Tourist-oriented business Modifications shall be reviewed according to the design criteria listed under Modifications for Exceptional Design, above, in addition to the following:

- Large electronic or illuminated signs shall not adversely impact residential or hotel uses.
- All signs shall conform to the lighting standards of the DTC.
- See page 119 for additional information on changeable copy.

### Right-of-way Encroachments

Where a sign is proposed to encroach into the public right-of-way an application shall be made with the Public Works Department in addition to the sign permit application with the Codes Department. Both applications shall include the requirements of the Public Works Department available at their website: <http://www.nashville.gov/pw/permits.asp>, in addition to the submittal requirements listed on the previous page.

### Nonconforming Signs

#### Nonconforming Signs

Sections 17.40.660 and 17.40.690 of the Metro Zoning Code apply. This section further clarifies them.

#### Building Signs

A sign shall be brought into compliance with the provisions of the above mentioned sections if a sign permit is required to rebuild the sign. This does not include a panel change in a non-conforming cabinet sign, which shall be permitted. All new panels shall conform to all illumination standards herein.

#### Ground Signs

An existing ground sign may change the face or panel of a sign that does not meet the area or height standards within this section. However, in no instance shall there be an increase in the degree of nonconformity. All new panels shall conform to all illumination standards herein.

A sign shall be brought into compliance with the provisions of this title if at any time the sign is altered, repaired, restored or rebuilt to the extent that the cost exceeds fifty percent (50%) of the estimated replacement cost of the sign (in current dollar value). All permits within any six consecutive calendar months shall be aggregated for purposes of measuring the fifty percent standard.

### Repair and Maintenance

If the alteration or repair is caused by involuntary damage or casualty, the sign may be altered or repaired to its pre-damaged condition.

A sign may be removed and taken off-site for repair and maintenance. The sign must be returned to the original location within 120 days of removal.

# Section V: Sign Standards

## General Standards

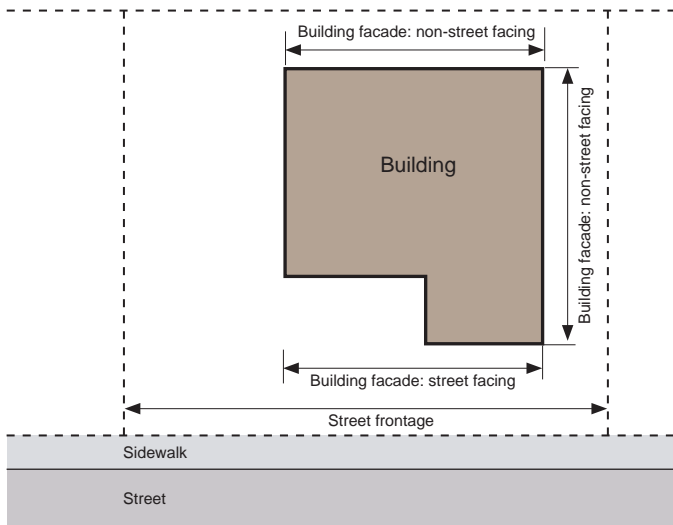
### Materials

All permanent, on-premises signs shall be constructed of a rigid, weatherable material such as hard plastic, wood, MDO plywood, aluminum, steel, PVC, glass and/or Plexiglas. On-premises, permanent signs shall not be constructed of nonrigid materials including, but not limited to, vinyl, fabric, canvas, or corrugated plastic. The provisions of this subsection shall not apply to approved, permitted canopies, awnings, and porticoes.

### Voluntary Removal of a Legally Non-conforming Ground Sign

Any property voluntarily removing a legally non-conforming ground sign shall be permitted to a bonus to one hundred fifty percent (150%) of the building signs allocated to the property. For example, if a property is allocated 200 sq ft of building signs, the property will be allowed 300 sq ft of buildings signs if a legally non-conforming ground sign is removed.

### Building Facade and Street Frontage Measurement



## Other Sign Types

### Non Street-Facing Signs

Non street-facing building facades and alley frontages, not otherwise regulated, are allocated 1 square foot of sign area per 1 linear foot of building facade, to a maximum of the sign area permitted for the primary street frontage.

This includes lots adjoining open spaces, pedestrian walkways, or parking areas. Ground signs are not permitted on non street-facing building facades or alley frontages.

### Temporary Signs

Temporary signs shall follow the standards of 17.32.060.

### Murals

Only the company name, text relating directly to products or services sold on site, and logos shall count toward the sign area allocation. Otherwise, murals are exempt from this code.

### Auto-oriented canopy/awning Signs

The allocation of signage for auto-oriented canopies and awnings shall be measured as walls signs and shall only be used on the canopy/awning. See the Auto-oriented Canopies and Awnings section of the Downtown Code for information on the design of canopies and awnings.

### Parking Lot Signs

A pole-mounted projecting sign is allowed for surface parking lots with no associated building. One sign per street frontage is allowed. The maximum size shall be 36 square feet per sign. The side of the sign shall be attached to the pole, and the pole will be considered the "building façade". All projecting sign standards shall apply (Page 114); parking lot signs shall follow the standards of a 1-story building.

# Section V: Sign Standards

## Street Types

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### **Pedestrian Streets**

- Pedestrian streets are roadways with high pedestrian activity and slower moving vehicular traffic. Buildings along these streets are located at the back of the sidewalk creating a streetscape with active uses including retail, office and entertainment businesses.
- Pedestrian streets are generally located in the Downtown core, where more of the original street wall remains intact, and less opportunities exist for surface parking.
- Some streets outside of the Downtown core, such as Korean Veterans Boulevard (KVB), require Pedestrian designation. The MDHA and UDO standards for KVB require pedestrian-focused building design and streetscape. The standards for Pedestrian Street signage allow greater flexibility for such streets.

### **Gateway Streets**

- Gateway streets are wider roadways that serve pedestrians, but focus primarily on automobile traffic and typically carry traffic at higher speeds than Pedestrian Streets.
- Generally located on the fringe of Downtown, Gateway streets serve as the key automobile entry points into Downtown.

### **Transitional Streets**

- Transitional streets currently contain a mix of different building characters, transitioning from more auto oriented buildings and uses to a more mixed use pedestrian environment.
- As these street redevelop, buildings will be sited close to the sidewalk with a more pedestrian focus.

### **Interstate**

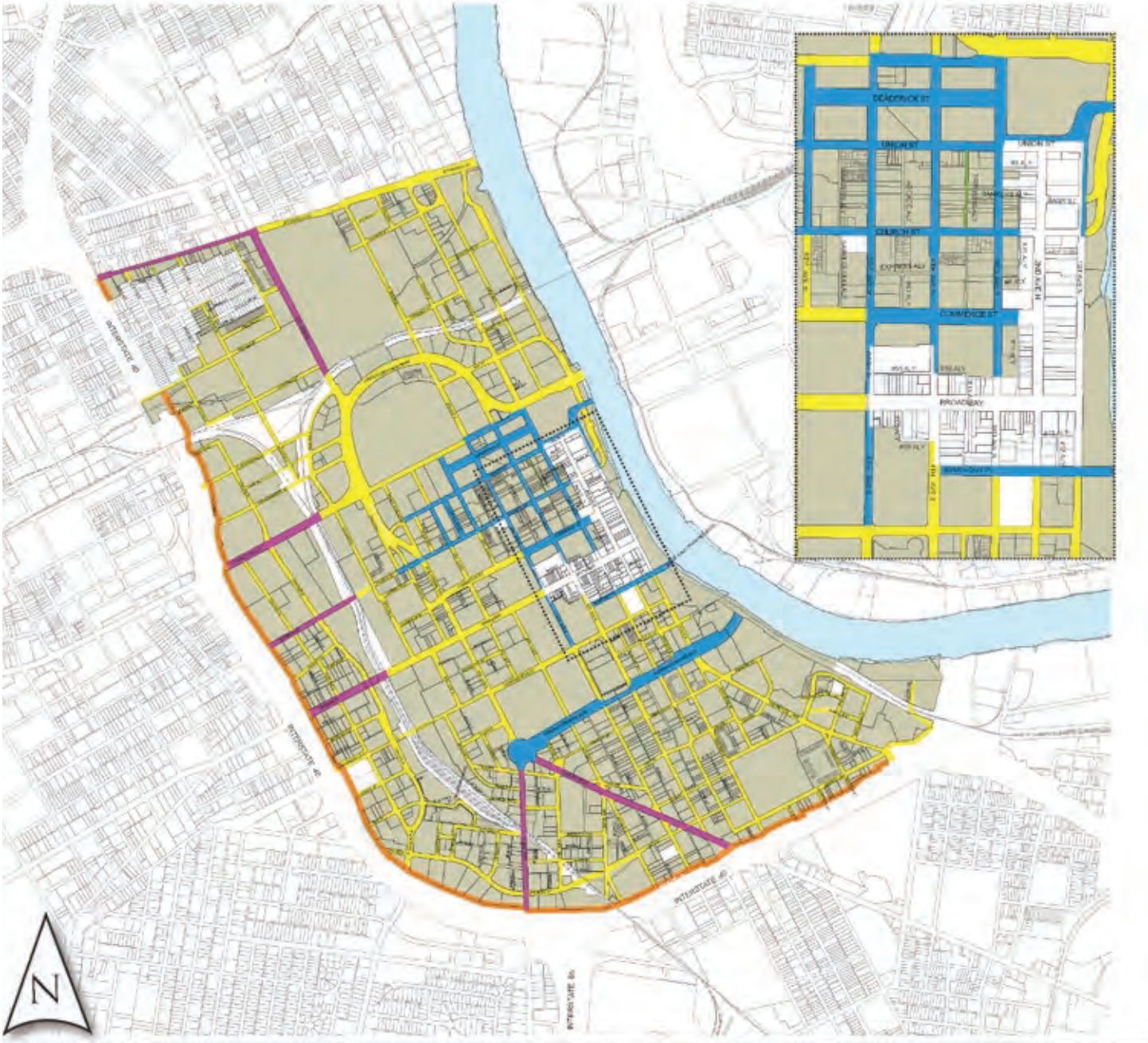
- Many properties are visible from the interstate loop which moves vehicles into and through Downtown.
- The opportunity to advertise to the interstate must be balanced with the safety of drivers and preservation of Nashville's iconic skyline.

### **Printers' Alley**







- Printers' Alley is a unique, iconic alley in Downtown. As a public, pedestrian-only street with businesses on the ground floor and upper floors, unconventional standards apply to this street only.

# Section V: Sign Standards

**Signs:** Map of Street Types for Signage Standards



### Legend

	Transitional Street		Interstate
	Pedestrian Street		Printers' Alley
	Gateway Street		Applicable properties

- New streets that are not in existence as of the adoption of this ordinance shall be categorized as Transitional Streets.
- Properties within an Historic Zoning Overlay and/or with SP zoning are not subject to the sign standards of DTC zoning.



# Section V: Sign Standards

## Determining Sign Entitlements

Use this page as a guide to determine the sign entitlements of a property. You will need to know the length of the building on all street frontages, the length of alley frontages, and the length of all interior property lines.

### STEP 1

What Street Types is the property on? Page 107

- Pedestrian
- Transitional
- Gateway
- Interstate
- Printers' Alley

### STEP 2

How much square footage of signage is allowed? Pages 109-110

- Pedestrian
  - Building \_\_\_\_\_
  - Ground \_\_\_\_\_
  - Skyline \_\_\_\_\_
- Transitional
  - Building \_\_\_\_\_
  - Ground \_\_\_\_\_
  - Skyline \_\_\_\_\_
- Gateway
  - Building \_\_\_\_\_
  - Ground \_\_\_\_\_
  - Skyline \_\_\_\_\_
- Interstate
  - Building \_\_\_\_\_
  - Skyline \_\_\_\_\_
- Printers' Alley
  - Building \_\_\_\_\_
  - Skyline \_\_\_\_\_

### STEP 3

What are the standards for the signs? Pages 111-117

### STEP 4

What are the illumination standards for signs? Pages 118-119

### STEP 5

Is changeable copy allowed? Page 119

### STEP 6

- For a multi-tenant development: submit a Common Sign Plan to the Codes Department. Page 103
- For an individual tenant: submit a Sign Plan Proposal to the Codes Department. Page 103
- Additional types of signs may be allowed, depending on site conditions. Page 105

### KEEP IN MIND

- There is no limit to the number of Building Signs per property.
- Sign entitlements are limited only by the total amount of square footage of signs allowed on the property, the maximum sizes of signs and the required placement of signs.
  - For example, if a building is allowed 100 sq ft of Building Signs, that can be used in one 100 sq ft sign or in five 20 sq ft signs. The only limit is the maximum dimensions of the sign type.
- Non street-facing facades are allowed signs. Page 105
- Contact the Codes Department with questions.

# Section V: Sign Standards

## Allocation of Sign Area by Street Type

The maximum sign area for each type of sign is determined by the Street Type and is established in the following tables. Specific requirements for each sign type are shown on the subsequent pages.

For each cell in the table below, there is a maximum allowed sign area that may be utilized with any combination and any number of signs associated with that cell, unless otherwise noted.

The measurements for "linear feet" shall be at grade.

Pedestrian Street Type	
<b>Building Signs</b>	
Wall Sign	1.5 square foot of sign area per 1 linear foot of building facade or 36 square feet, whichever is greater. When a Projecting Sign is used on the building, an additional 0.5 square feet of sign area per 1 linear foot of building facade shall be permitted, for a total of 2.0 square feet per 1 linear feet of building facade.
Awning Sign	
Canopy Sign	
Projecting Sign	
Shingle Sign	
Shingle Sign	9 square feet per sign
<b>Ground Signs</b>	
Monument Sign	24 square feet
<b>Skyline Signs</b> - area determined by average height of building	
75' to 100'	480 square feet
101' - 200'	600 square feet
201' and taller	720 square feet

Gateway Street Type	
<b>Building Signs</b>	
Wall Sign	1.5 square foot of sign area per 1 linear foot of building facade or 36 square feet, whichever is greater. Where no ground sign exists, an additional 0.5 square feet of sign area per 1 linear foot of building facade shall be permitted, for a total of 2.0 square feet per 1 linear feet of building facade.
Awning Sign	
Canopy Sign	
Projecting Sign	
Shingle Sign	
Shingle Sign	9 square feet per sign
<b>Ground Signs</b>	
Monument Sign	64 square feet Properties with 300 or more feet of frontage are allowed one additional monument sign of an additional 64 square feet
<b>Skyline Signs</b> - area determined by average height of building	
75' to 100'	480 square feet
101' - 200'	600 square feet
201' and taller	720 square feet

Transitional Street Type	
<b>Building Signs</b>	
Wall Sign	1.5 square foot of sign area per 1 linear foot of building facade or 36 square feet, whichever is greater. Where no ground sign exists, an additional 0.5 square feet of sign area per 1 linear foot of building facade shall be permitted for a total of 2.0 square feet per 1 linear feet of building facade.
Awning Sign	
Canopy Sign	
Projecting Sign	
Shingle Sign	
Shingle Sign	9 square feet per sign
<b>Ground Signs</b>	
Monument Sign	32 square feet Properties with 300 or more feet of frontage are allowed one additional monument sign of an additional 32 square feet
<b>Skyline Signs</b> - area determined by average height of building	
75' to 100'	480 square feet
101' - 200'	600 square feet
201' and taller	720 square feet

**\*\* More Street Types on next page\*\***

# Section V: Sign Standards

## Allocation of Sign Area by Street Type

The maximum sign area for each type of sign is determined by the Street Type and is established in the following tables. Specific requirements for each sign type are shown on the subsequent pages.

For each cell in the table below, there is a maximum allowed sign area that may be utilized with any combination and any number of signs associated with that cell, unless otherwise noted.

The measurements for "linear feet" shall be at grade.

Interstate Street Type	
<b>Building Signs</b>	
Wall Sign	1 square foot of sign area per 1 linear foot of street frontage
Awning Sign	
Canopy Sign	
Projecting Sign	
Shingle Sign	9 square feet per sign
<b>Ground Signs</b>	Not allowed
<b>Skyline Signs</b> - area determined by average height of building	
75' to 100'	480 square feet
101' - 200'	600 square feet
201' and taller	720 square feet

Printers' Alley Street Type	
<b>Building Signs</b>	
Wall Sign	Ground floor: 2.0 square foot of sign area per 1 linear foot of street frontage Second floor: 1.5 square foot of sign area per 1 linear foot of street frontage Upper floors: 1 square foot of sign area per 1 linear foot of street frontage Signage allowed for each floor shall be used on that floor and shall not be redistributed to other parts of the building.
Awning Sign	
Canopy Sign	
Projecting Sign	
Shingle Sign	
<b>Ground Signs</b>	Not allowed
<b>Skyline Signs</b> - area determined by average height of building	
75' to 100'	480 square feet
101' - 200'	600 square feet
201' and taller	720 square feet

# Section V: Sign Standards

## Building Sign: Wall Sign

### Description

A wall sign is a building sign that is attached flat to, or mounted away from but parallel to, the building facade.

A wall sign may be painted on the building facade.

### General Provisions

- A wall sign shall either be located lower than the window sills of the third story or at the top of the building, not to be placed below the windows of the highest floor.
- No portion of a wall sign may extend above the roof line or above a parapet wall of a building with a flat roof.
- No portion of a wall sign may extend above the lower eave line of a building with a pitched roof.
- A wall sign cannot cover windows or architectural details.
- An exposed raceway shall be finished to match the background wall or be integrated into the overall design of the sign.
- A wall sign can be externally or internally illuminated in accordance with pages 118.

### Design Standards

- |                                 |  |
|---------------------------------|--|
| Ⓐ Overall area allocation (max) | See pages 109-110  |
| Ⓑ Projection (max)              | 13 inches  |
| Ⓒ Exposed Raceway height (max)  | 50% of the letter height, OR if the Raceway is used as the sign background, the Raceway may extend 3 inches beyond the largest part of the sign. |

Refer to the Illumination section (pages 118) for additional raceways standards and permitted locations.



Example of raceway incorporated as sign background

# Section V: Sign Standards

## Building Sign: Awning Sign

### Description

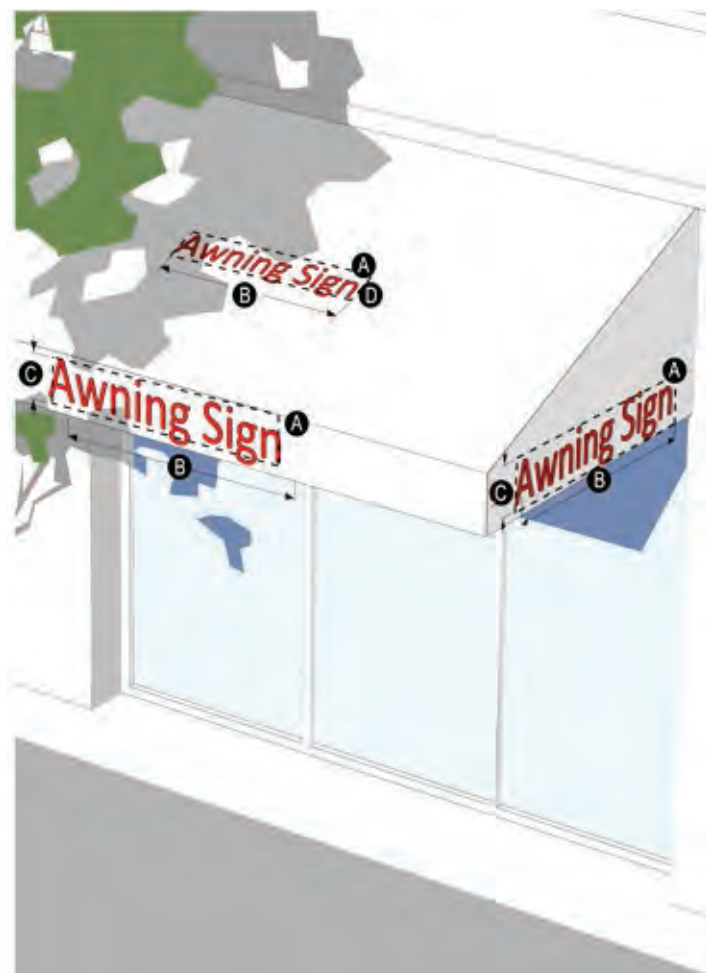
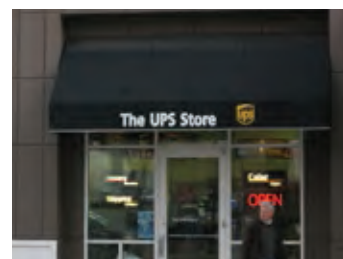
An awning sign is a type of building sign. Graphics and symbols are painted, sewn, or otherwise adhered to the awning material as an integrated part of the awning itself.

### General Provisions

- Only awnings on first and second story windows may contain signs.
- A maximum of one sign is allowed per awning face.
- An awning sign may only be externally illuminated.
- See the Awnings section of the Downtown Code for additional information on the design of awnings.

### Design Standards

- |   |                   |
|---|-------------------|
| Ⓐ Overall area allocation (max)                   | See pages 109-110 |
| Ⓑ Sign Width<br>(max % of awning width/depth)     | 75%               |
| Ⓒ Height of text and graphics on valance<br>(max) | 2 feet            |
| Ⓓ Max area of sloping plane covered by sign       | 50%               |



# Section V: Sign Standards

## Building Sign: Canopy Sign

### Description

A canopy sign is a type of building sign that is attached above, below or to the face of a canopy.

### General Provisions

- A canopy sign cannot extend outside the overall length or width of the canopy. However, a canopy sign may extend above or below the canopy provided the sign meets all other design standards.
- Maximum of one sign per canopy face.
- Raceways are permitted for signs extending below or above the canopy.
- A canopy sign can be externally or internally illuminated in accordance with page 118.
- Cabinet signs are not permitted as canopy signs.

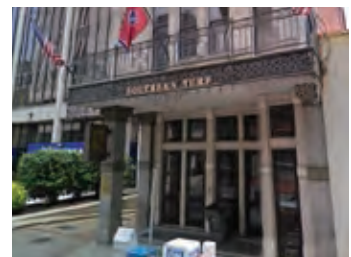
See the Canopies section of the Downtown Code for additional information on the design of canopies.

- This definition does not include freestanding canopies over fuel pumps. See the Auto-oriented canopy/awning Signs on page 105.

### Design Standards

- |   |  |
|---|--|
| Ⓐ Overall area allocation (max)                   | See pages 109-110  |
| Ⓑ Sign Width<br>(max % of canopy length or depth) | 75%  |
| Ⓒ Height of text and graphics (max)               | 2 feet   |
| Ⓓ Depth (max)                                     | 13 inches  |
| Ⓔ Exposed Raceway height (max)                    | 50% of the letter height, OR if the Raceway is used as the sign background, the Raceway may extend 3 inches beyond the largest part of the sign. |

Refer to the Illumination section (pages 118) for additional raceways standards and permitted locations.



# Section V: Sign Standards

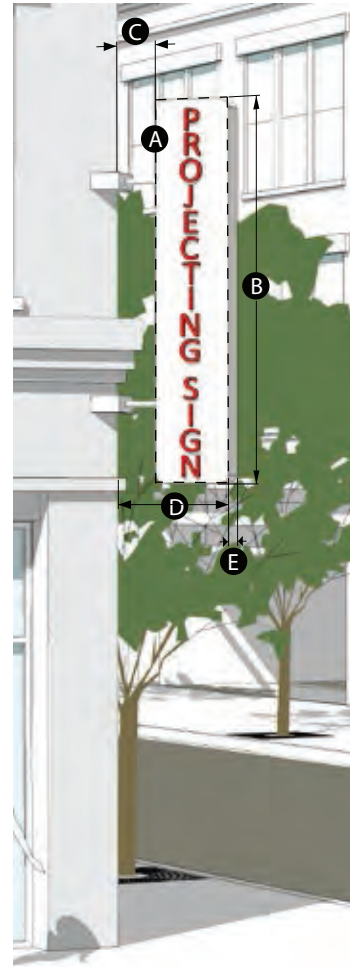
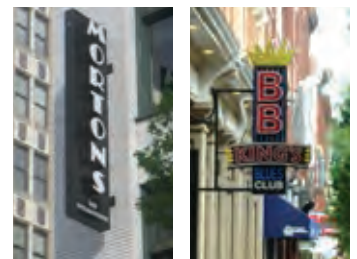
## Building Sign: Projecting Sign

### Description

A projecting sign is a type of building sign that projects outward from the facade, typically at a ninety degree angle. Projecting signs are typically, but not always, vertically oriented and generally mounted above the first floor.

### General Provisions

- A projecting sign must be located at least 25 feet from any other projecting sign. When building width prohibits adherence to this standard, flexibility shall be permitted though a Minor Modification to be reviewed by staff.
- A projecting sign may be erected on a building corner when the building corner adjoins the intersection of two streets. Allocation of sign area from both streets may be used, however, in no case shall the sign exceed the maximum dimensional standards below.
- A projecting sign shall be located below the window sills of the fourth story.
- The top of a projecting sign shall not extend above the building eave or top of parapet, except on one story buildings where the top of a projecting sign may have a maximum of 20% of the sign height above the top of the building.
- A projecting sign can be externally or internally illuminated in accordance with pages 118.



### Design Standards

<b>A</b> Overall area allocation (max)	See pages 109-110
<b>B</b> Height (max)	
1 story buildings	10 feet
2 and 3 story buildings	16 feet
4 or more story buildings	20 feet
<b>C</b> Spacing from facade (min)	1 foot
<b>D</b> Projection Width (max)	6 feet
<b>E</b> Depth of cabinet (max)	18 inches

# Section V: Sign Standards

## Building Sign: Shingle Sign

### Description

A shingle sign is a smaller building sign that projects outward, typically at a ninety degree angle, and hangs from a bracket or support that is located over or near a building entrance.

### General Provisions

- Signs shall be located within 8 feet of an active pedestrian building entrance. This does not include service entries or entries that primarily remain locked.
- An active pedestrian entrance at the corner of a building is allowed signs on both streets.
- A shingle sign shall be located below the window sills of the second story.
- A shingle sign shall not be internally illuminated.

### Design Standards

Ⓐ Area (max)	9 square feet
Ⓑ Height (max)	3 feet
Ⓒ Spacing from facade (min)	6 inches
Ⓓ Width (max)	3 feet
Ⓔ Depth (max)	6 inches





# Section V: Sign Standards

## Ground Sign: Monument Sign

### Description

A monument sign is a ground sign that is attached along its entire length to a continuous pedestal.

### General Provisions

- Each property fronting on a Street Type which allows a monument sign is permitted one per street frontage. One additional monument sign is allowed for properties with 300 or more feet of street frontage on one street. Where more than one sign is permitted, signs along the same street frontage shall be spaced a minimum of 200 feet apart.
- A monument sign must be set back at least 5 feet from the front property line.
- A sign erected on the top of a retaining wall is required to meet the standards for a monument sign. The height of the wall shall be included in the overall height calculation. In this case, the 5 foot minimum setback is not required.
- A sign affixed to the face of a retaining wall or seat wall that is an integral part of a plaza or streetscape design may utilize the sign area allocated to wall signs. In this case, the 5 foot minimum setback is not required.
- A monument sign can be externally or internally illuminated in accordance with page 118.



### Design Standards

<ul style="list-style-type: none"> <li><b>A</b> Sign area (max per sign)</li> <li><b>B</b> Height (max)</li> <li style="padding-left: 20px;">Pedestrian Street</li> <li style="padding-left: 20px;">Transitional Street</li> <li style="padding-left: 20px;">Gateway Street</li> <li><b>C</b> Depth (max)</li> </ul>	<p>See pages 109-110</p> <p>5 feet</p> <p>10 feet</p> <p>15 feet</p> <p>18 inches</p>
--	---



# Section V: Sign Standards

## Skyline Sign

### Description

A building sign is attached flat to or mounted away from the building facade. Sign may be parallel to the building facade or vertical. Located on the upper band of a building.

### General Standards

- A skyline sign is only allowed on buildings greater than 75 feet in height.
- A skyline sign must be located within the top third of the building.
- No portion of a skyline sign may extend above the roof line or above a parapet wall of a building with a flat roof.
- No more than one skyline sign per facade is allowed. However, additional skyline signs may be allowed as a Modification for Exceptional Design. See additional information on page 104.
- Raceways are not permitted on skyline signs.
- A skyline sign can be internally (but not externally) illuminated in accordance with page 118.

### Design Standards

- |                                  |                   |
|----------------------------------|-------------------|
| Ⓐ Area (max)                     | See pages 109-110 |
| Ⓑ Height (max)                   | 14 feet           |
| Ⓒ Width (max % of facade length) | 60%               |



# Section V: Sign Standards

## Illumination

Illumination of signs shall be in accordance with the following requirements:

### External Illumination

- External light sources shall be placed close to, and directed onto, the sign and shielded to minimize glare into the street, sidewalks or onto adjacent properties.
- Projecting light fixtures used for externally illuminated signs shall be simple and unobtrusive in appearance. They should not obscure the sign.

### Internal Illumination

- Channel letters may be internally lit or back-lit.
- For cabinet signs, the background must be opaque. Only graphics, text and logos may be illuminated, and a halo of one inch around graphics, text, and logos may be non-opaque.
- Exposed neon may be used for lettering or as an accent.

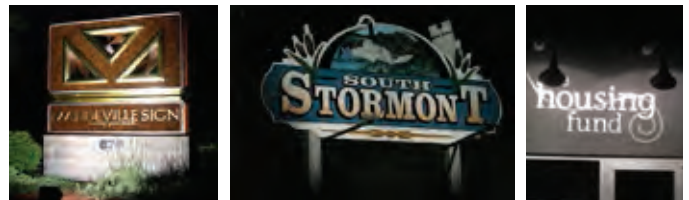
### Prohibited Light Sources

The following light sources are prohibited on every Street Type except Printers' Alley:

- Blinking, flashing, chasing, and sequential lighting.
- Bare bulb illumination.

### Raceways and Transformers

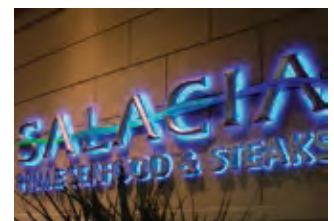
- Within Redevelopment Districts, raceways may only be permitted by Modification.
- Visible transformers are prohibited.



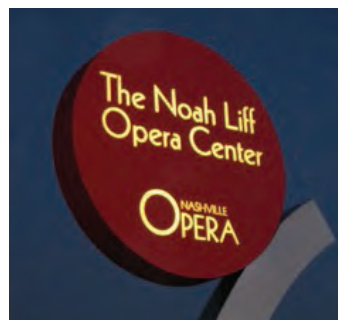
External light sources



Internally lit channel letters



Back lit channel letters



Internally lit cabinet signs with darker backgrounds

# Section V: Sign Standards

## Changeable Copy Signs (Manual or Electronic)

Changeable copy signs shall be in accordance with the following requirements:

### Description

- Manual changeable copy sign. A sign or portion of a sign that has a readerboard for the display of text information in which each alphanumeric character, graphic or symbol is defined by objects, not consisting of an illumination device. Changeable copy is changed or re-arranged manually or mechanically without altering the face or the surface of the sign.
- Electronic changeable copy sign. A sign or portion of a sign that displays information in which each alphanumeric character, graphic, or symbol is defined by a small number of matrix elements using different combinations of light emitting diodes (LED's), fiber optics, light bulbs or other illumination devices within the sign.

### Usage

- Manual changeable copy is allowed in conjunction with a wall sign, pylon sign or monument sign.
- Electronic Changeable copy is allowed in conjunction with a wall sign or a monument sign provided the changeable copy portion is no greater than 50% of the built sign area.
- An electronic changeable copy sign is not allowed in a Redevelopment District or on Interstate frontage.
- Tourist-oriented businesses within Downtown Nashville may receive Modifications to allow greater sign area and use of digital technology in accordance to the process on page 104.

### Spacing

- The closest edge of an electronic changeable copy sign must be a minimum distance of 100 feet from any residential zoning district or historic district boundary.
- An electronic changeable copy sign must be separated from another electronic changeable copy sign by at least 50 feet.

### Duration

- Any image or message or portion of the image or message must have a static display for minimum duration of eight seconds.
- Transition time must be immediate.
- No portion of the image or message may flash, scroll, twirl, change color or in any manner imitate movement.

### Brightness

- The sign must not exceed a maximum illumination of 7,500 nits during daylight hours and a maximum illumination of 750 nits between dusk to dawn as measured from the sign's face at maximum brightness.
- Electronic changeable copy signs must have an automatic dimmer control to produce a distinct illumination change from a higher illumination level to a lower level.