

Metropolitan Planning Commission



Staff Reports

July 23, 2015



Metro Planning Commission Meeting of 07/23/2015

Mission Statement: The Planning Commission is to guide the future growth and development for Nashville and Davidson County to evolve into a more socially, economically and environmentally sustainable community with a commitment to preservation of important assets, efficient use of public infrastructure, distinctive and diverse neighborhood character, free and open civic life, and choices in housing and transportation.

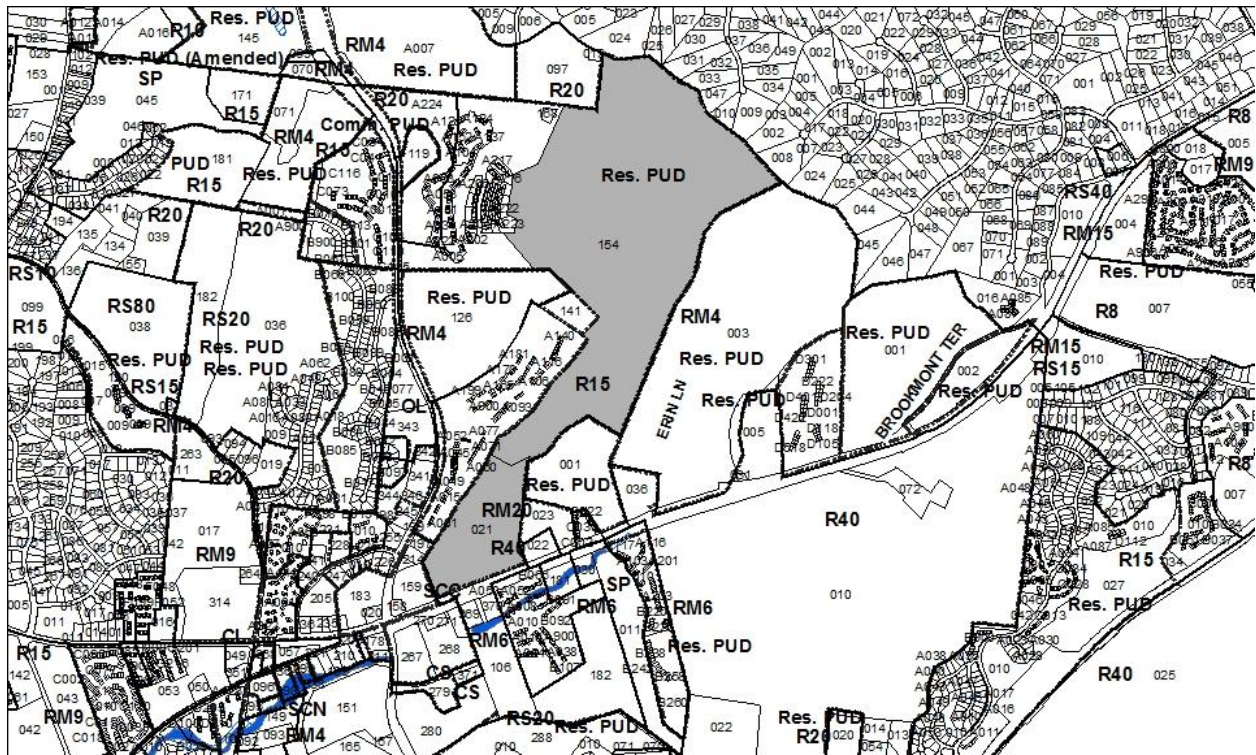


PREVIOUSLY CONSIDERED ITEMS

- **Specific Plans**
- **Zone Changes**
- **Planned Unit Developments**
- **Subdivision (Concept Plans)**



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73-85-P
NASHVILLE HIGHLANDS (PERIODIC REVIEW)
Map 128, Parcel(s) 154
Map 142, Parcel(s) 021
06, Bellevue
23 (Emily Evans)



Project No.	Planned Unit Development 73-85-P
Project Name	Nashville Highlands (Periodic Review)
Council District	23 – Evans
School District	9 – Frogge
Requested by	Metro Planning Commission.
Staff Reviewer	Swaggart
Staff Recommendation	<i>Find the PUD to be inactive and advise Council to cancel the portion of PUD under review and rezone to Specific Plan.</i>

APPLICANT REQUEST

Periodic review of a Planned Unit Development.

Periodic PUD Review

A request for a periodic review for a portion of the Nashville Highlands Residential Planned Unit Development Overlay District located at Highway 70 S (unnumbered) and Old Hickory Boulevard (unnumbered), approximately 650 feet east of Old Hickory Boulevard, zoned One and Two-Family Residential (R15) (202 acres), approved for 864 multi-family units.

Existing Zoning

One and Two-Family Residential (R15) requires a minimum 15,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 3.09 dwelling units per acre including 25 percent duplex lots. *In this case the land use and density is controlled by the PUD.*

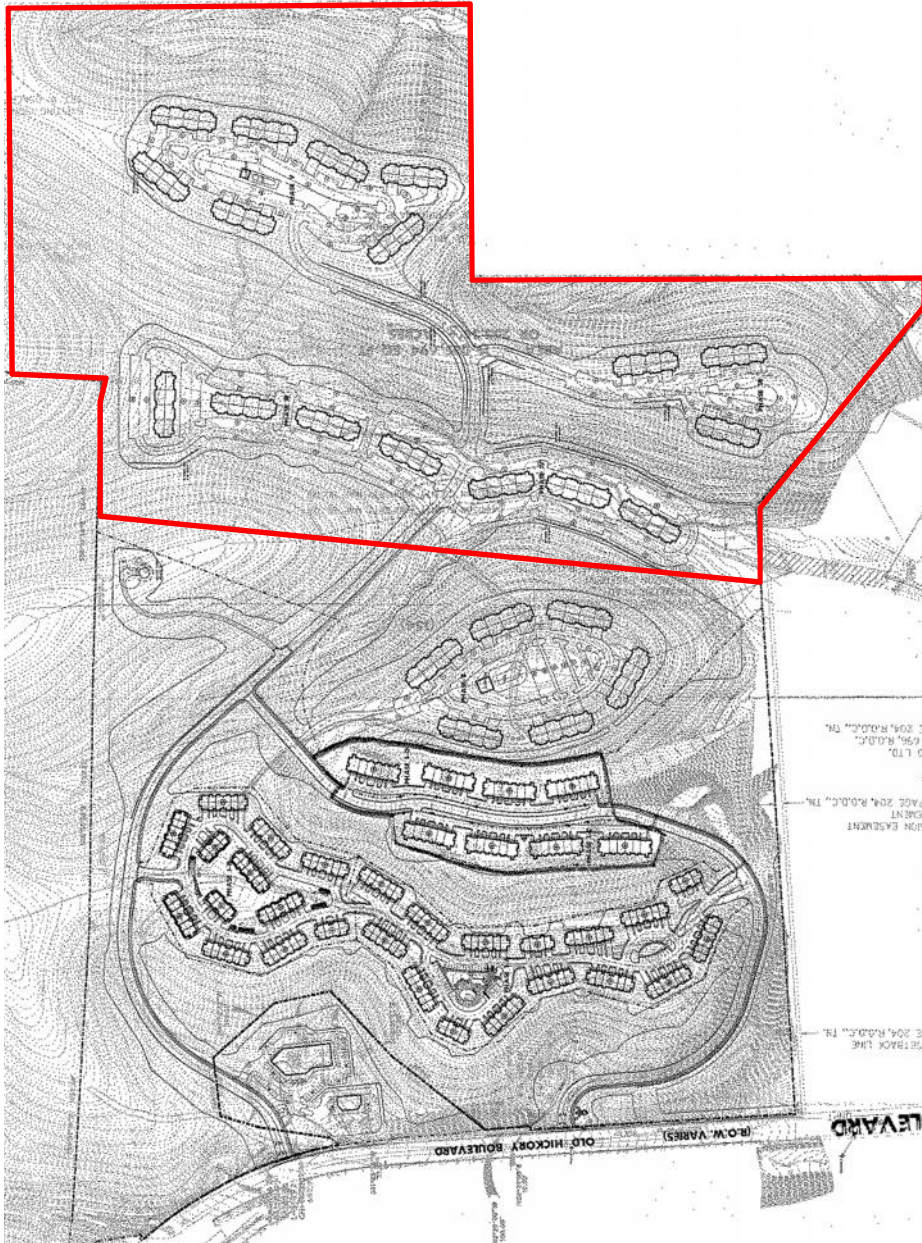
Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provision of an adequate roadway system or essential utilities and services. In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provision of essential utilities and streets.

PUD DETAILS

The Nashville Highlands PUD was originally approved in 1985 for 1,062 multi-family units. The development has also been referred to as Eagle Ridge at the Reserve, The Reserve and Robertson Highlands. Through the years the PUD has been revised several times. There have also been request to amend the PUD to permit single-family uses and commercial uses; however, these amendments were never approved. The overall PUD boundary consists of approximately 271 acres. Currently 198 units, a clubhouse and water tower have been constructed on approximately 69 acres. The remaining approximately 202 acres, which has been requested for review, remains undeveloped.



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Approved Preliminary Site Plan (The outlined area indicates the area that is not developed)



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The PUD would permit a maximum of an additional 864 units within this undeveloped portion of the PUD.

The boundary of the PUD extends from Old Hickory Boulevard southward to Highway 70. The entire site is encumbered with very steep slopes in excess of 25 percent. The steep slopes on the site contain Bodine-Sulphura which is a problem soil due to its proneness to slide. While the PUD permits a large number of units, the density is under four units an acre. The PUD preserves a majority of the site, approximately 227 acres (83% of the site) in open space.

PERIODIC PUD REVIEW

Section 17.40.120 H of the Metro Zoning Ordinance authorizes the Planning Commission, a councilmember or the property owner of the area to be reviewed to request the Metropolitan Planning Commission to review, any Planned Unit Development (PUD) overlay district, or portion thereof, to determine whether the PUD is “inactive,” and if so, to recommend to the Council what action should be taken with respect to the PUD. The Commission determines whether the PUD is “inactive” by examining whether development activity has occurred within six years from the date of the initial enactment, subsequent amendment, or re-approval by the Metro Council. If the Planning Commission determines the PUD to be inactive, the Commission is required to recommend legislation to the Council to re-approve, amend, or cancel the PUD.

Timeline for Planning Commission Action

The Zoning Code requires that, within 90 days from the initiation of its review, the Planning Commission must hold a public hearing to make a determination of activity, and if necessary, make a recommendation to the Council. At the May 28, 2015, Planning Commission meeting the Commission voted to hear the periodic review at the July 23, 2015, meeting. The 90 day period extends to August 26, 2015, *which would permit the request to be deferred to the August 13, 2015, meeting if the Commission desired.* If the Planning Commission does not make a determination within 90 days from the initiation of a review, it is considered to be a recommendation to re-approve by ordinance the existing PUD overlay district without alteration.

Classification of the PUD (Active or Inactive)

Under 17.40.120 H., the Commission is first required to determine whether the portion of the Nashville Highlands PUD requested for periodic review is active or inactive by examining whether development activity has occurred within six years from the date of the initial enactment, subsequent amendment, or re-approval by the Metro Council.

Section 17.40.120 H.3.a. of the Metro Code requires the Planning Commission to make three findings in order to determine whether a PUD has been active or inactive:

- i. Six or more years have elapsed since the latter of*
 - 1) The effective date of the initial enacting ordinance of the PUD,*
 - 2) The effective date of any ordinance approving an amendment to the PUD,*
 - 3) The effective date of any ordinance re-approving or amending a PUD after it has been reviewed and decided in accordance with subsection 5.a. or b. of this section,*
or
 - 4) The deadline for action by the metropolitan council in accordance with subsection 5.d. of this section, and*



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Approved, Revised Final Site Plan for existing development, as constructed



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The initial enacting ordinance for the PUD became effective in 1985. No amendments have been approved for the PUD including the subject portion of the PUD that required Metro Council approval since the initial enactment date.

- ii. *Construction has not begun on the portion of the PUD under review; construction shall mean physical improvements such as, but not limited to, water and sewer lines, footings, and/or foundations developed on the portion of the PUD under review; clearing, grading, the storage of building materials, or the placement of temporary structures shall not constitute beginning construction, and*

The owner/developer has indicated that a portion of a 1,800 foot sewer main was constructed and serves the existing portion of the PUD and was appropriately sized to serve the unbuilt portion. They also assert that other infrastructure in Phase 1 was sized to accommodate the unbuilt portion of the PUD. This includes private drives and a water tower necessary to provide water to this development and to another development, both located in Phase 1. All of the infrastructure in Phase 1 was necessary to construct Phase 1. Staff is not aware that any infrastructure specifically for Phase 2 has been constructed.

- iii. *Neither right-of-way acquisition from a third party nor construction has begun on off-site improvement(s) required to be constructed by the metropolitan council as a condition of the PUD approval.*

The owner/developer has indicated that a traffic light was installed on Old Hickory Boulevard. The light was included in the findings of a Traffic Impact Study (TIS) that accounted for the built portion of the PUD and the remaining unbuilt portion (1,062 units). There was no condition in the Council bill requiring the traffic signal.

Section 17.40.120 H.3.a. states that the Commission “*may also take into consideration the aggregate of actions, if any, taken by the owner of the PUD within the prior 12 months to develop the portion of the PUD under review.*”

The owner/developer has indicated that the following actions have been taken to develop the unbuilt portion of the PUD within the last 12 months:

- A civil engineer was contracted in February 2014, to conduct engineering studies for the site. Conceptual layout plans, grading plans and environmental plans have been prepared within the last 12 months, at a cost of \$16,000.

Staff is not aware of any infrastructure or other construction that has occurred within the last 12 months that could be counted towards the aggregate of actions. Therefore, staff recommends that there has not been an aggregate of actions that constitutes activity.



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Planning Commission Recommendation to Metro Council

If the Planning Commission determines the PUD to be active, then no further action is required. If the Commission determines the PUD to be inactive, then the Commission is required to recommend legislation to the Council to re-approve, amend, or cancel the PUD.

With respect to the legislation to be recommended to the Metro Council, the Planning Commission is directed by the Code to take two distinct steps.

First, the Commission is to determine whether the *“existing PUD is consistent with the goals, policies, and objectives of the General Plan and any applicable specific redevelopment, historic, neighborhood, or community plans.”*

Second, the Commission is to recommend the legislation, and include, as required:

- (a) The appropriate base zoning district(s), if different from current base zoning, to retain and implement the PUD overlay district as it exists.
- (b) Any amendment(s) to the inactive PUD's master development plan and base zoning district(s) to reflect existing conditions and circumstances, including the land use policies of the general plan and the zoning of properties in the area.
- (c) Base zoning district(s) consistent with the adopted general plan, should the PUD overlay district be recommended for cancellation.

BELLEVUE COMMUNITY PLAN

Conservation (CO) is intended to preserve and enhance environmentally sensitive land within all Transect Categories except T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils.

Suburban Neighborhood Maintenance (T3 NM) is intended to preserve the general character of suburban neighborhoods as characterized by their development pattern, building form, land use and associated public realm. T3 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood, in terms of its development pattern, building form, land use, and the public realm. Where not present, enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Consistent with Policy?

Conservation policy can support development that is lower in intensity and preserves the areas the policy is intended to protect. While the approved PUD plan protects some steep slopes, there is development approved that impacts environmental features, such as slopes and streams. While some development may be appropriate on this site, the current PUD plan is not designed to protect steep slopes, streams and other environmental features. A plan that protects the environmental features and maintains open space could comply with the policies.



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STAFF RECOMMENDATION

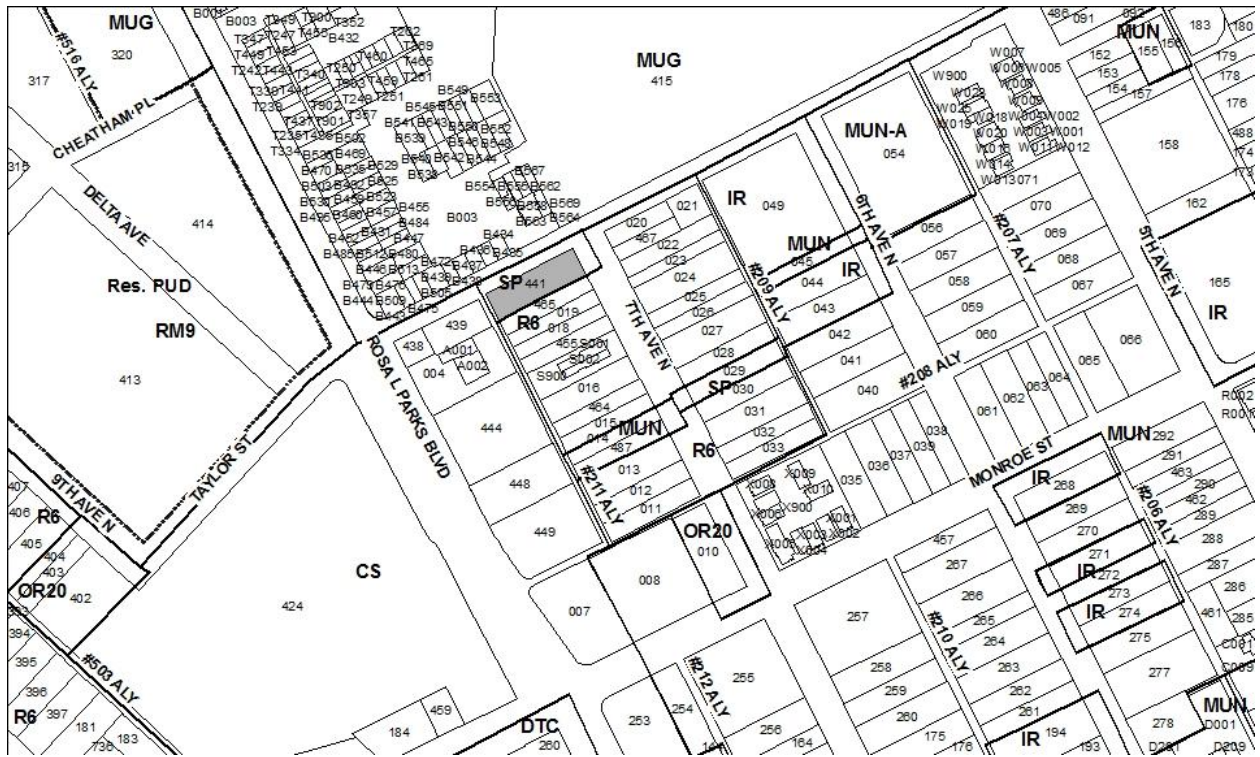
In accordance with the requirements of 17.40.120 H, staff recommends that the Planning Commission find the PUD to be inactive. While the applicant has indicated that actions have been taken to develop the portion of the PUD under review, staff is not able to verify if those actions were conducted specifically for the unbuilt portion of the PUD under review or for the existing portion of the PUD. Also, a final site plan has not been approved for the portion of the PUD under review. While the water and sewer infrastructure may have been sized for full build out, it was necessary for the construction of the first phases of development. Finally, staff finds the PUD inactive based on a lack of an aggregate of actions taken within the last 12 months.

If the Commission finds the PUD inactive, then staff recommends that the Commission recommend that Council cancel the portion of the PUD under review and rezone the property to SP. At a minimum, any SP should reduce the overall foot print of the development, utilize environmentally sensitive building practices and protect the view shed by reducing the visibility of the development from the surrounding area. Furthermore, the Commission should encourage the property owner to work with Council, Metro Parks and/or conservation groups in possibly transferring the open space areas to Metro Parks and/or a land trust so that the open space areas can be preserved in perpetuity.

The information submitted by the owner/developer and its representatives can be found in exhibit A at the end of this report.



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2006SP-075-001

1329 7TH AVENUE NORTH
Map 081-12, Parcel(s) 441
08, North Nashville
19 (Erica S. Gilmore)



Project No.	Zone Change 2006SP-075-001
Project Name	1327 7th Avenue North
Council Bill	BL2015-1299
Council District	19 - Gilmore
School District	1 - Gentry
Requested by	Barge Cauthen & Associates, applicant; Village People, LLC, owner

Deferral This request was previously deferred from the June 11, 2015, and June 25, 2015, Planning Commission meeting.

Staff Reviewer Birkeland
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

SP amendment to permit 4 townhome units and 2 live/work units.

SP Amendment

A request to amend an existing Specific Plan-Residential (SP-R) to permit four townhome units plus two live/work unit for a total of six units, where four detached units had previously been approved, approximately 174 feet north of Rosa Parks Blvd (0.18 acres), within the Germantown Historic Preservation Zoning Overlay District.

Existing and Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes attached residential buildings.

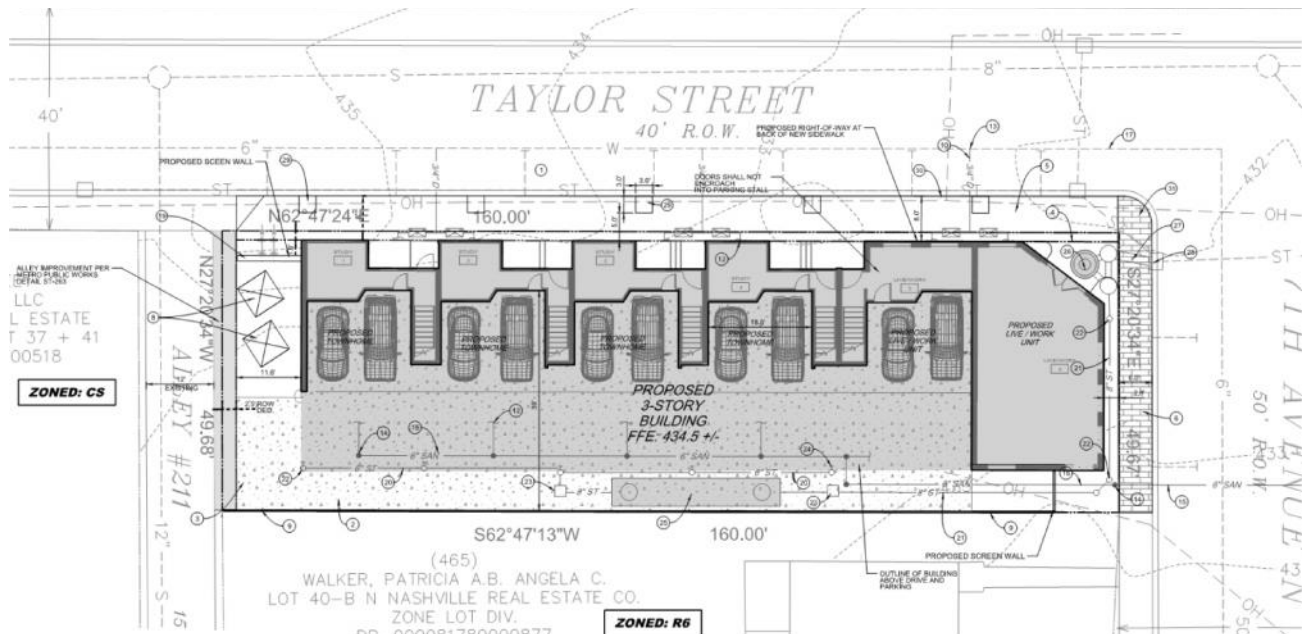
CRITICAL PLANNING GOALS

- Supports a Variety of Transportation Choices
- Creates Walkable Neighborhoods
- Provides a Range of Housing Choices

The proposal meets several critical planning goals based on its location and design. This site is located in an area that is served by adequate infrastructure. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure such as roads, water and sewer, because it does not burden Metro with the cost of maintaining new infrastructure. Bus service is present just to the west of the site on Rosa L. Parks Boulevard. Increased density through infill development makes bus service and similar transit services more feasible because it generates more riders. An improved sidewalk along Taylor Street will provide an improved pedestrian connection by providing pedestrians safe and access to other areas.



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Proposed Site Plan



Proposed Elevations



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NORTH NASHVILLE COMMUNITY PLAN

T4 Urban Mixed Use Neighborhood (T4 MU) is intended to preserve, enhance, and create urban, mixed use neighborhoods characterized by a development pattern that contains a diverse mix of residential and nonresidential land uses, and that are envisioned to remain or develop in a mixed use pattern. T4 MU areas are areas intended to be mixed use in nature with the presence of commercial and even light industrial uses, but also a significant amount of moderate to high density residential development.

Consistent with Policy?

Yes. The policy supports residential and live/work uses. The plan provides an urban form by placing the residential units up to the new sidewalk along Taylor Street and provides a private drive from the alley to access the buildings. The proposed multifamily residential units provide that mixture of housing types including live/work units in a strategic location within North Nashville.

PLAN DETAILS

The site is located at the corner of Taylor Street and 7th Avenue North, within the Germantown Historic Perseveration District in North Nashville. The site consists of one parcel that has an existing non-contributing quad-plex. In 2007, this site was approved for SP zoning that allowed four single-family homes with four parking spaces. The proposed SP amendment includes four attached multifamily dwelling units, and two live/work units, for a total of six multi-family attached units, within one building.

The proposed site plan positions the building to frame both Taylor Street and 7th Avenue North. The proposed site plan includes porches or stoop for each unit, as well as sidewalk connections, creating a pedestrian presence along the streetscape and helping to further advance the goals of the policy.

Vehicular access will be provided by a new drive aisle from the existing alley, along the southern property line. The SP also includes right-of-way dedication along both Taylor Street and the alley. Ten parking spaces will be provided on-site, underneath the cantilevered building. A fence is proposed along the southern property line. A screen wall is proposed along the northern side to buffer the trash and recycling receptacles. Another screen wall is proposed along the eastern side of the building to buffer vehicular noise and headlight pollution onto 7th Avenue North.

Taylor Street and 7th Avenue North are local streets. This SP includes an eight foot sidewalk along Taylor Street. The existing brick sidewalk along 7th Avenue North will remain. In addition, the SP is located in close proximity to existing transit routes that run along Rosa L. Parks Boulevard. Five street trees will be installed along the new Taylor Street sidewalk.

Architectural elevations have been included on the plan and shall be provided with the final site plan. The proposed residential units shall have a maximum height of 40 feet measured to roofline. The applicant will have to return to the Metro Historic Zoning Commission for final building elevation approvals. The plans have been submitted for review to Metropolitan Development and Housing Agency's (MDHA) Phillips Jackson Design Review Committee. The Committee has approved the concept plans.

The two live/work units are defined as a single unit consisting of a commercial space on the first floor and a residential component that is occupied by the proprietor of the commercial space above.



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The live/work unit may also serve as solely residential use. Examples of appropriate commercial uses include retail, office and personal care services.

ANALYSIS

The proposed four multi-family residential units and two live/work units are consistent with the T4 Mixed Use Neighborhood Policy and meet several critical planning goals. The six total units will provide a range of housing types in the strategic location in North Nashville.

HISTORIC ZONING COMMISSION RECOMMENDATION

Approved with conditions

- Recommend approval with the conditions that the final design is reviewed by the MHZC.

METROPOLITAN DEVELOPMENT AND HOUSING AGENCY

Approved

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review. Hydrants shall be located within 500' of all parts of every structure via approved hard-surfaced roads.

STORMWATER RECOMMENDATION

Approved

WATER SERVICES RECOMMENDATION

Approved

- Approved as Prelim SP. Applicant will need to pay required Capacity Fees before it can be approved as a Final SP.

TRAFFIC AND PARKING RECOMMENDATION

No exception taken

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Indicate the dedication and construction of ½ MPW standard ST-263. Will require a 2.5' ROW dedication.
- Dedicate ROW to the face of the proposed building (approx. 2.3') to facilitate an ADA compliant path of travel within the ROW.
- The existing site appears to have a newer dumpster pad and container. Indicate the removal on the plans.



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Maximum Uses in Existing Zoning District: SP

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.18	-	4 U	39	3	5

Maximum Uses in Proposed Zoning District: SP

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (220)	0.18	-	6 U	40	4	4

Traffic changes between maximum: SP and SP

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 2 U	+1	+1	-1

METRO SCHOOL BOARD REPORT

Projected student generation existing SP-R district: 0 Elementary 0 Middle 0 High
Projected student generation proposed SP-R district: 0 Elementary 0 Middle 0 High

Based on data from the Metro School Board last updated September 2014, the proposed SP permitting up to 6 multifamily residential dwelling units will not generate additional students from what is generated by the existing SP zoning district.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Uses within this SP shall be limited to six residential units, with two of the six units located along the corner of Taylor Street and 7th Avenue North, allowed to be live/work.
2. Live/work unit is defined as a single unit consisting of a commercial space on the first floor and a residential component that is occupied by the proprietor of the commercial space above. The live/work unit may also serve as solely residential use. Commercial uses include retail, office and personal care services only.
3. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM20-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
4. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
5. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.



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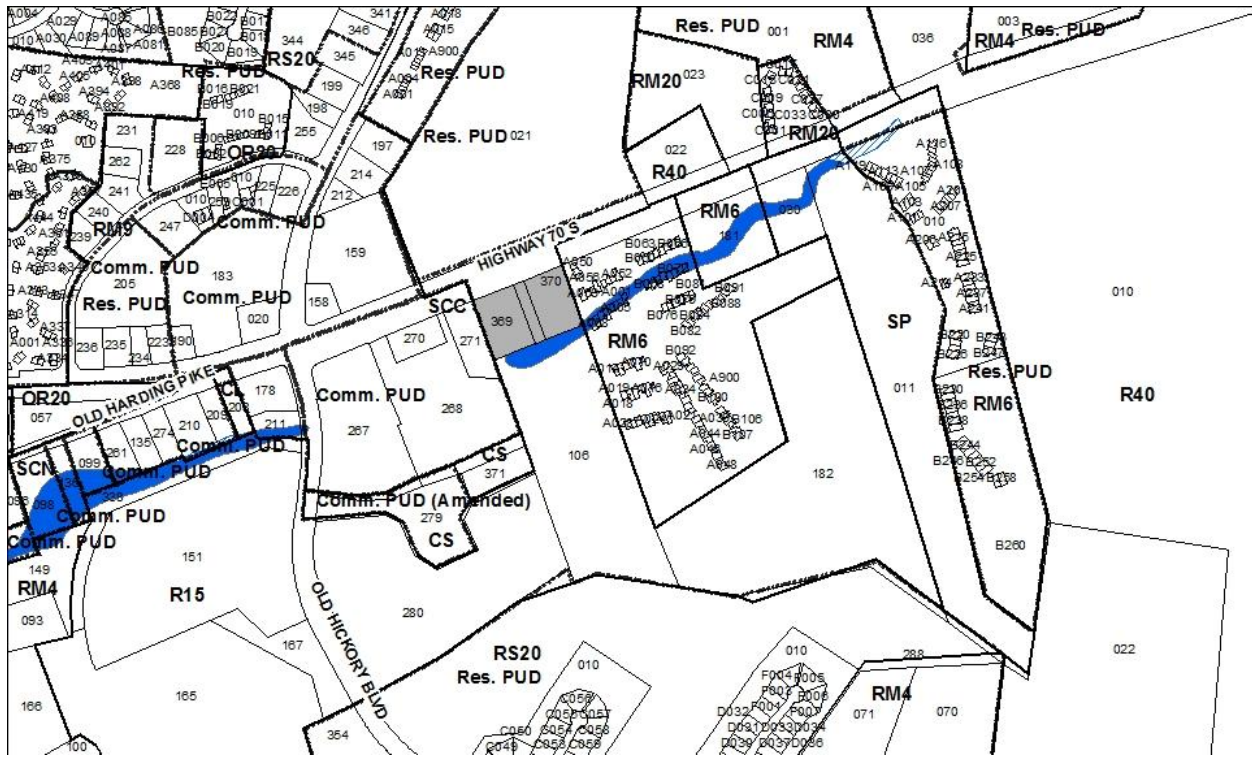
6. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
7. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



SEE NEXT PAGE



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2015SP-043-001

HWY 70 RETAIL CENTER

Map 142, Parcel(s) 369-370, Part of Parcel 106

06, Bellevue

34 (Carter Todd)



Project No. 2015SP-043-001
Project Name Highway 70 Retail Center
Council Bill BL2015-1207
Council District 34 – Todd
School District 9 – Frogge
Requested by Civil Design Consultants, LLC, applicant; Hicks Family Bellevue, LLC, owner.

Deferral This request was previously deferred from the May 28, 2015 and June 11, 2015, Planning Commission meetings.

Staff Reviewer Sajid
Staff Recommendation *Disapprove.*

APPLICANT REQUEST
Permit commercial and office uses.

Preliminary SP

A request to rezone from Single-Family Residential (RS15) to Specific Plan-Commercial (SP-C) zoning for properties located at 7037 Highway 70 S and Highway 70 S (unnumbered), approximately 1,065 feet east of Old Hickory Boulevard (3.17 acres), to permit a commercial/office development.

Existing Zoning

Single-Family Residential (RS15) requires a minimum 15,000 square foot lot and is intended for single-family dwellings at a density of 2.47 dwelling units per acre. *RS15 would permit a maximum of 9 units.*

Proposed Zoning

Specific Plan-Commercial (SP-C) is a zoning District category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes commercial uses.

CRITICAL PLANNING GOALS

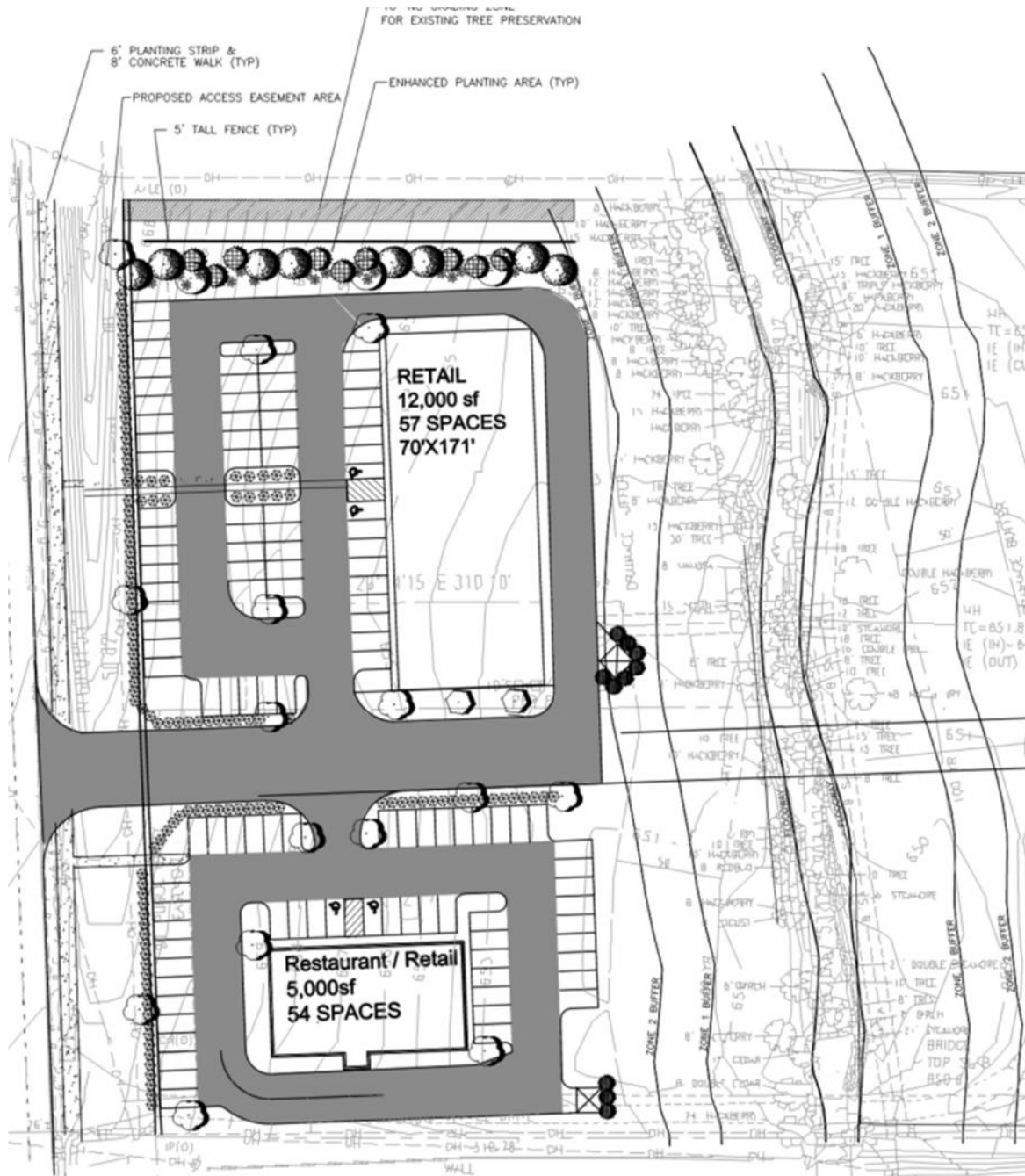
N/A

BELLEVUE COMMUNITY PLAN

Suburban Community Center (T3 CC) is intended to enhance suburban community centers encouraging their redevelopment as intense mixed use areas that are compatible with the general character of suburban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T3 Suburban Community Centers are pedestrian friendly areas, generally located at prominent intersections. T3 Suburban Community Centers serve suburban communities within a 10 to 20 minute drive.



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Proposed Site Plan



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Conservation (CO) is intended to preserve and enhance environmentally sensitive land within all Transect Categories except T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils.

Consistent with Policy?

No. The SP is not consistent with the goals of the Suburban Community Center and Suburban Neighborhood Maintenance policies. While the uses may be consistent with the policies, the design of the SP does not meet the policy objective to enhance the pedestrian environment. The commercial uses include four rows of parking between the building and Highway 70 South, where the policy calls for no more than one row in order to foster a more pedestrian oriented feel. Given the context along Highway 70 South, planning staff finds up to two rows of parking between the building and the street may be appropriate. In addition, the SP does not incorporate an interior sidewalk connection from the proposed buildings to the public sidewalk network.

PLAN DETAILS

The site is located on Highway 70 South, east of Old Hickory Boulevard. The site is currently vacant, and access to the site is from Highway 70 South.

Site Plan

The SP includes two parcels and a portion of a third that total 3.17 acres; all parcels include frontage on Highway 70 South. The plan shows two buildings totaling 17,000 square feet, and uses for these buildings are proposed to include a variety of commercial and office uses. A stream is located along the southern boundary of the site. The proposed development does not encroach into the required stream buffer.

The proposed buildings are oriented toward Highway 70 South. Surface parking is provided throughout the site, including multiple rows between the buildings and Highway 70. The Council bill has been filed for this rezoning request and limits building area to up to 23,500 square feet. The plan currently shows parking for only 17,000 square feet of building area. However, the Council bill includes a note that “any addition of square footage in excess of 17,000 square feet or any vehicular access points not currently present or approved must be approved by Public Works and by the Planning Commission as a modification of the SP.”

Access to the site is from one driveway onto Highway 70 South. The plan includes sidewalks per the Major and Collector Street Plan but does not provide interior sidewalk connections from the commercial/office buildings to the public sidewalk. The plan does not include elevations or design standards for either of the buildings. The maximum height proposed for the retail/office buildings is 35’.

ANALYSIS

While the proposed uses may be consistent with the Suburban Community Center policy, the SP includes several design elements that do not align with the pedestrian-oriented goals of the policy. The plan proposes four rows of parking proposed in front of the retail building located to the east of the site which is not consistent with the policy which permits one row of parking between the building and the street in order to create a context that fosters a pedestrian friendly environment. As stated previously, staff finds up to two rows of parking between the building and the street may be



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appropriate given the context along Highway 70 South. While staff recognizes that the existing character in this area does provide for large parking lots in front of buildings, it does not provide for appropriate pedestrian scaled development. One of the goals of the land use policy for this area is to provide an opportunity for new development to engage the sidewalk area better than it has in the past. Over time, as new development and redevelopment occur, the goal is to transition the area into a more pedestrian oriented streetscape. While this may not happen quickly, each new development is an opportunity to move closer to that goal.

As the SP is not consistent with the goals of the Suburban Community Center, staff recommends disapproval of the SP.

FIRE MARSHAL RECOMMENDATION

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review

STORMWATER RECOMMENDATION

Approve

TRAFFIC & PARKING RECOMMENDATION

Conditional if approved

In accordance with May 22, 2015 TIS findings, developer shall

- Construct an EB right turn lane on Hwy 70 at access drive with 125 ft storage length and taper per AASHTO standards.
- Project Access drive shall have 2 exiting lanes and 1 entering lane with appropriate lane widths.
- A minimum of 75ft of storage for exiting lanes shall be provided. Adequate queue storage shall be provided between the first interior drive and Hwy 70.
- Adequate sight distance shall be provided at intersection with Hwy 70. The TIS identified existing Berms along this property which will need removal to allow adequate sight distance. Developer shall submit a field run survey confirming adequate sight distance with final SP.
- Final SP and Construction documents shall include a pavement marking plan and signage plan and construction plans for right turn lane. Roadway improvements on a hwy 70 may require approval by TDOT.
- The developer shall submit a signal modification plan to upgrade the pedestrian signals at Hwy70 /OHB signalized intersection to modular countdown units on a minimum of 2 approaches and modify traffic signal as approved by metro traffic engineer. The signal plans shall be included in final SP and construction documents.
- Per the TIS, developer shall work with MTA to incorporate a transit stop in project plans if feasible.
- Plans shall show cross access easement to the R15 zoned property and relocation of the existing single family house drive. Internal road design shall show intersection design with relocated single family drive .
- All internal drives shall provide two way travel with adequate turning radii for truck turning movements



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HARPETH VALLEY UTILITY DISTRICT RECOMMENDATION

Approve with conditions

- Water and sewer plans must be submitted and approved by HVUD at the appropriate time.

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- All construction within the ROW will require a permit from TDOT. TDOT permit must be obtained and copy remitted to MPW prior to any work within the ROW.
- If sidewalks are required, indicate the installation of standard curb and gutter at the existing EOP, 6' grass strip and 8' standard sidewalk along the property frontage. The sidewalks must be in ROW.

Maximum Uses in Existing Zoning District: **RS15**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	14.99	2.90 D	43 U	479	40	51

Maximum Uses in Proposed Zoning District: **SP-MU**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (814)	14.99	-	12,000 SF	552	17	51

Maximum Uses in Proposed Zoning District: **SP-MU**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Restaurant (932)	14.99	-	5,000 SF	636	58	56

Maximum Uses in Proposed Zoning District: **RM20**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (220)	14.99	20 D	238 U	1566	121	149



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Traffic changes between maximum: RS15 and SP-MU

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+2,275	+156	+205

STAFF RECOMMENDATION

As the SP is not consistent with the goals of the Suburban Community Center in regard to design and enhancing the pedestrian realm, staff recommends disapproval of the SP.

CONDITIONS (if approved)

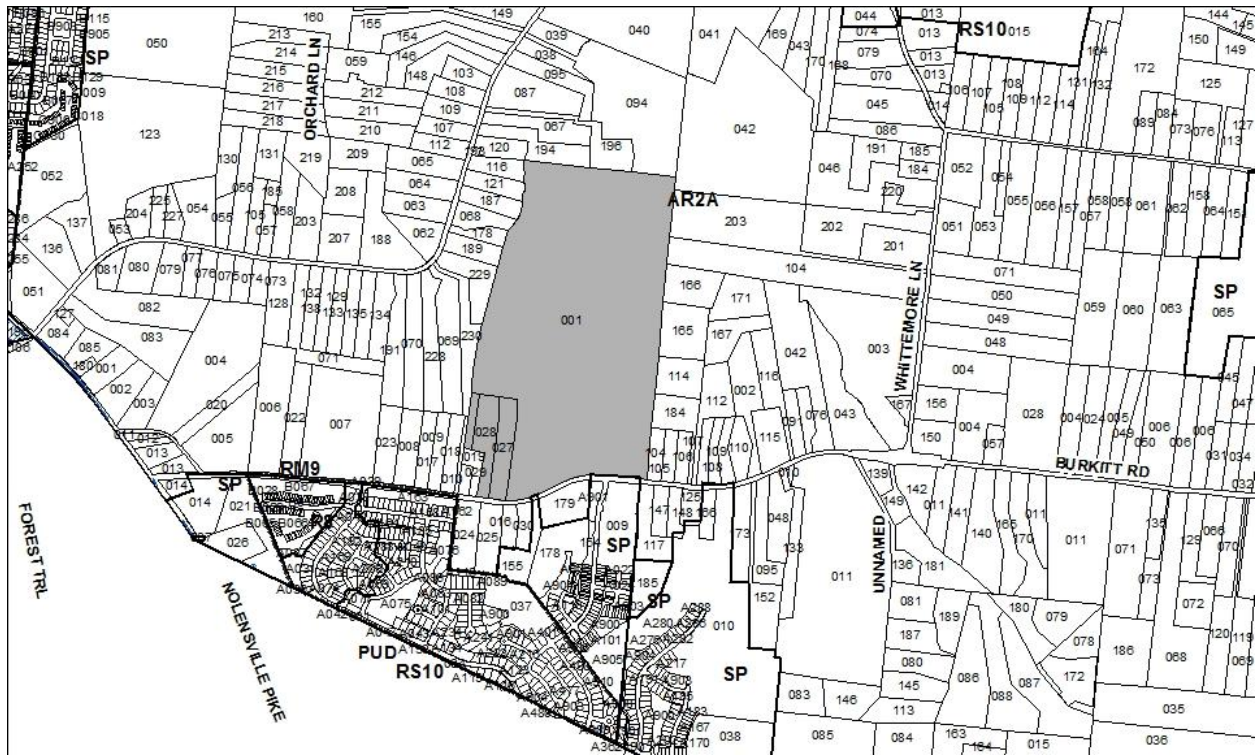
1. Uses within the SP shall permit up to 23,500 square feet of commercial/office space.
2. For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations and requirements of the SCC District as of the date of the applicable request or application.
3. Uses shall be limited to retail, restaurant, financial institution, general office, ATM, automobile parking, outpatient clinic, medical office, personal care services, business services, furniture store, day care, home improvement sales and commercial amusement (inside).
4. Maximum building height shall be measured to the top of the roofline.
5. Interior sidewalks shall be provided throughout the development and connect to the public sidewalk proposed along the Highway 70 South frontage.
6. Add note to plan: Be it further enacted, minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, or eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance. Any addition of square footage in excess of 17,000 square feet or any vehicular access points not currently present or approved must be approved by Public Works and by the Planning Commission as a modification of this SP.
7. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
8. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2015SP-061-001
HILLSHIRE GROVE
Map 186, Parcel(s) 027, 028, Part of Parcel 029
Map 187, Parcel(s) 001
12, Southeast
31 (Fabian Bedne)



Project No.	Specific Plan 2015SP-061-001
Project Name	Hillshire Grove
Council Bill	BL2015-1300
Council District	31 – Bedne
School District	2 – Brannon
Requested by	Ragan-Smith & Associates; Timothy and Kimberly Weddle, Gary Price, Jr. et ux, Starker Services, Inc., and Stacy Carter et al, owners.

Deferral This request was previously deferred from the June 11, 2015, and the June 25, 2015, Planning Commission meeting.

Staff Reviewer Swaggart
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

Preliminary SP to permit 303 single-family lots and 94 multi-family units.

Preliminary SP

A request to rezone from Agricultural/Residential (AR2a) to Specific Plan – Mixed Residential (SP-MR) for properties located at 6994 and 6998 Burkitt Road and Burkitt Road (unnumbered), approximately 1,200 feet east of Canonbury Drive (155.3 acres), to permit up to 303 single-family lots and 94 multi-family units.

Existing Zoning

Agricultural/Residential (AR2a) requires a minimum lot size of two acres and intended for uses that generally occur in rural areas, including single-family, two-family, and mobile homes at a density of one dwelling unit per two acres. The AR2a District is intended to implement the natural conservation or rural land use policies of the general plan. *AR2a would permit a maximum of 77 lots with 19 duplex lots for a total of 96 units.*

Proposed Zoning

Specific Plan-Residential (SP-MR) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only two residential building types.

CRITICAL PLANNING GOALS

- Preserves Environmental Resources
- Creates Walkable Neighborhoods
- Provides a range of housing options

The plan preserves a majority of the sensitive environmental features which include blue line streams and steep slopes. The plan provides for street connectivity, which not only helps distribute



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Proposed Site Plan



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traffic, but also connects neighborhoods. Sidewalks are proposed along all streets which help foster walkable neighborhoods. The plan provides and additional housing options for the area which provide living opportunities for a more diverse group of people.

SOUTHEAST COMMUNITY PLAN

Suburban Neighborhood Evolving (T3 NE) policy is intended to create and enhance suburban residential neighborhoods with more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate density development patterns with moderate setbacks and spacing between buildings. T3 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network, block structure, and proximity to centers and corridors. T3 NE areas are developed with creative thinking in environmentally sensitive building and site development techniques to balance the increased growth and density with its impact on area streams and rivers.

Conservation (CO) policy intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.

Consistent with Policy?

Yes. The proposed SP is consistent with both policies. The proposed layout maintains the overall suburban character/lot pattern of the surrounding area but also provides for additional housing options consistent with the TSNE policy. The plan provides for future connections to adjacent, undeveloped property including a collector street. The plan preserves a majority of the environmentally sensitive areas on the site such as streams and steep slopes consistent with the conservation policy. The disturbance of these sensitive features is minimal and is consistent with the Conservation policy.

PLAN DETAILS

The approximately 155 acres site is located on the north side of Burkitt Road east of Nolensville Pike. The property has open fields/pasture as well as large areas contiguous wooded areas. The site contains several blue line streams as well as a pond. The property also contains some steep slopes in excess of 25 percent.

Site Plan

The plan calls for a total of 397 units consisting of 303 single-family lots and 94 multi-family units with an overall density of approximately 2.5 units per acre. The plan calls for front loaded as well as alley loaded single-family lots. The proposed bulk standards are as follows:

Single-Family Lots (front loaded)

Minimum lot area: 5,000 sq. ft.

Maximum lot coverage: 0.75



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Minimum street setback: 20'
Minimum rear setback: 20'
Minimum side setback: 3' w/10' aggregate and 10' building setback
Maximum Height: 3 stories
Maximum height at setback: 48' (measured from ground floor F.F.E. to top of eaves or roof deck)

Single-Family Lots (alley loaded)

Minimum lot area: 3,000 sq. ft.
Maximum lot coverage: 0.75
Minimum street setback: 10'
Minimum rear setback: Between 5 and 9' or equal to or greater than 18' from edge of alley
Minimum side setback: 3' w/10' aggregate and 10' building setback
Maximum height: 3 stories
Maximum height at setback: 48' (measured from ground floor F.F.E. to top of eaves or roof deck)

Multi-Family (Townhomes)

Minimum lot area: N/A
Maximum ISR: 0.70
Minimum street setback: 20'
Minimum rear setback: 10'
Minimum side setback (common wall): 0'
Minimum side setback: 20'
Maximum Height: 3 stories
Maximum height at setback: 48' (measured from ground floor F.F.E. to top of eaves or roof deck)

The plan provides numerous design standards. The standards are related to but not limited to porches, stoops and entry placement, utility meter placement, rear entry, and front and side entry. The plan also provides signage standards.

As proposed, the primary access into the site is from two new public roads onto Burkitt Road. The plan calls for all streets and alleys to be public with the exception of a few alleys that are designated as private. The plan provides for future street connections to the north, east and west. Most of the streets are local, but two streets are designated as a collector and runs from the eastern property line to the western property line. Sidewalks are shown along both streets.

ANALYSIS

Staff recommends approval because the proposed SP plan is consistent with the Southeast Community Plan as detailed in the above policy section of this report. The plan also supports several critical planning goals. The proposed layout maintains the overall suburban character/lot pattern of the surrounding area but also provides for additional housing options consistent with the TSNE policy. The plan provides for future connections to adjacent, undeveloped property including a collector street. The plan preserves a majority of the environmentally sensitive areas on the site such as streams and steep slopes consistent with the conservation policy. The disturbance of these sensitive features is minimal consistent with the Conservation policy. It is also important to note that the proposed development is within the Infrastructure Deficiency Area (IDA) and requires 4,503 linear feet of major roadways (or an equivalent transportation improvement) within the



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identified infrastructure deficiency area to a two-lane cross section at a level acceptable to the Department of Public Works.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Approved

PUBLIC WORKS RECOMMENDATION

No exceptions taken

TRAFFIC & PARKING RECOMMENDATION

Conditions if approved

In accordance with the TIS findings, developer shall construct the following roadway improvements with development of phase 1 and phase 2 with 200 dwelling units. Prior to development of the remaining phases and dwelling units, the TDOT roadway plans for the Nolensville Pike widening and Burkitt Rd intersection improvements shall be constructed by TDOT and operational.

Nolensville Pike at Burkitt Road

1. Developer shall modify SB left turn lane pavement markings on Nolensville Pike at Burkitt Rd to increase storage per the TIS addendum exhibit.

Burkitt Road at Site Access West

1. The Hillshire Grove access road shall include one lane for traffic entering the site and two lanes for traffic exiting the site (one right turn lane, one left turn lane). The total pavement width of the access should be a minimum of 33 feet.
2. The Hillshire Grove Final SP shall include clearing and grading work necessary to mitigate the existing sight distance obstructions east of the proposed access on Burkitt Road.
3. Appropriate intersection sight distance per AASHTO guidelines should be shown on the Hillshire.

Grove Final SP.

1. While the existing grade and alignment of Burkitt Road appear adequate for intersection sight distance after mitigating measures are completed, proposed landscaping and development signage shall be set back from Burkitt Road to provide appropriate intersection sight distance per AASHTO guidelines.
2. A left turn lane shall be constructed on Burkitt Road for traffic entering Hillshire Grove. This turn lane shall have storage length of 180 feet, a bay taper length of 180 feet, and lane transition tapers based upon AASHTO and MUTCD guidelines. The total length of the roadway improvements is not currently available and will be based upon final design and lane transition offsets.
3. A right turn lane on Burkitt Rd shall be installed at west access road.



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Burkitt Road at Westcott Lane / Site Access East

1. The Hillshire Grove access road shall include one lane for traffic entering the site and two lanes for traffic exiting the site (one shared through/right turn lane, one left turn lane). The total pavement width of the access should be a minimum of 36 feet.
2. While the existing grade and alignment of Burkitt Road appear adequate for intersection sight distance after mitigating measures are completed, proposed landscaping and development signage shall be set back from Burkitt Road to provide appropriate intersection sight distance per AASHTO guidelines.
3. A signal warrant analysis shall be prepared by the Hillshire Grove developer when a total of 100 lots or units are permitted or underway at Hillshire Grove or when directed by metro traffic engineer. When a traffic signal is warranted and approved by the Metro Nashville - Davidson County Public Works Traffic Engineering Division, the developer shall prepare a traffic signal plan and install a traffic signal with pedestrian infrastructure at this intersection.
4. A left turn lane shall be constructed on Burkitt Road for traffic entering Hillshire Grove. This turn lane should have storage length of 180 feet, a bay taper length of 180 feet, and lane transition tapers based upon AASHTO and MUTCD guidelines. The total length of the ES – 3 roadway improvements is not currently available and will be based upon final design and lane transition offsets.
5. A right turn lane shall be constructed on Burkitt Road for traffic entering Hillshire Grove. This turn lane should have a storage length of 250 feet and a taper length of 100 feet.

INFRASTRUCTURE DEFICIENCY AREA (IDA)

This request is located in the Infrastructure Deficiency Area (IDA), and requires that improvements be made to roadway within the IDA. The applicant will be required to improve approximately 4,503 linear feet of roadway within the IDA. Roadway to be improved will be determined by Public Works' staff. This is in addition to any other off site roadway improvements required by Public Works.

WATER SERVICES RECOMMENDATION

Approve with conditions

Approved as a Preliminary SP only. The required public construction plans must be submitted and approved before the Final SP is approved. The required capacity fees must be paid prior to Final SP approval.

METRO SCHOOL BOARD REPORT

Projected student generation existing AR2a district: 5 Elementary 4 Middle 3 High

Projected student generation proposed SP-R district: 89 Elementary 61 Middle 58 High

The proposed SP-R zoning district is expected to generate 196 additional students than what is typically generated under the existing AR2a zoning district. Students would attend Maxwell Elementary School, Marshal Middle School, and Cane Ridge High School. Both Maxwell Elementary and Cane Ridge High School are over capacity. There is additional capacity in the cluster for elementary students in the cluster, but there is no additional capacity for high school students. This information is based upon data from the school board last updated October 2014.



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Fiscal Liability

The fiscal liability of 84 new elementary school students is \$1,806,000 (84 X 21,500 per student). The fiscal liability of 57 new middle school students is \$1,482,000 (57 X 26,000 per student). The fiscal liability of 55 new high school students is \$1,980,000 (55 X 36,000 per student). This is only for information purposes to show the potential impact of this proposal, it is not a staff condition of approval.

School Site Dedication

Due to the potential impact of this development on the public school system, the applicant is required by Planning Commission policy to offer for dedication a school site in compliance with the standards of Section 17.16.040 for a middle school with a capacity of 800 students.

This land dedication requirement is proportional to the development's student generation potential. Such site shall be in accordance with the site condition and location criteria of the Metropolitan Board of Education and shall be within the Cain Ridge High School cluster. The Board of Education may decline such dedication if it finds that a site is not needed or desired. No final plat for development of any residential uses on the site shall be approved until a school site has been dedicated to the Metro Board of Education or the Board has acted to relieve the applicant of this requirement. However, failure of the Board of Education to act prior to final plat consideration and approval by the Metropolitan Planning Commission in accordance with its schedule and requirements shall constitute a waiver of this requirement by the Board of Education.

STAFF RECOMMENDATION

Approve with conditions and disapprove without all conditions as the proposed SP is consistent with the Southeast Community Plan and meets several critical planning goals.

CONDITIONS (if approved)

1. Uses within the SP shall be limited to a maximum of 303 single-family residential lots and 94 multi-family units.
2. Non alley loaded lots shall be a minimum of 50 feet wide.
3. Lots on any collector streets shall meet Section 3-4.4 of the Metro Subdivision Regulations in regards to access onto collector streets (requiring joint access easements).
4. Structures shall be limited to 45 feet in height measured from the finished floor elevation (FFE) to the roofline.
5. An eight foot planting strip and six foot sidewalk shall be provided along Burkitt Road.
6. All local streets shall provide a four foot planting strip and a five foot sidewalk.
7. The collector street shall provide a six foot planting strip and a six foot sidewalk.
8. Any garage facing a primary street frontage shall be recessed at least five feet from the front façade.
9. The following design standards shall be added to the plan:
 - a. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.
 - c. EIFS, vinyl siding and untreated wood shall be prohibited.
 - d. Porches shall provide a minimum of six feet of depth.



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10. 4,503 linear feet of major roadways (or an equivalent transportation improvement) shall be improved by the applicant within the identified infrastructure deficiency area to a two-lane cross section at a level acceptable to the Department of Public Works. Such improvements shall be undertaken within available right-of-way and at a level commensurate with the development entitlements appropriate on the site. Improvements are to accommodate additional traffic volumes within the Infrastructure Deficiency Area. When appropriate improvements cannot be physically made, then the applicant may make a financial contribution for future roadway improvements within the identified infrastructure deficiency area. The Department of Public Works shall determine the appropriate contribution based on the linear feet of roadway to be improved.
11. Due to the potential impact of this development on the public school system, the applicant is required by Planning Commission policy to offer for dedication a school site in compliance with the standards of Section 17.16.040 for middle schools with a capacity of 800 students. This land dedication requirement is proportional to the development's student generation potential. Such site shall be in accordance with the site condition and location criteria of the Metropolitan Board of Education and shall be within the Cane Ridge High School cluster. The Board of Education may decline such dedication if it finds that a site is not needed or desired. No final plat for development of any residential uses on the site shall be approved until a school site has been dedicated to the Metro Board of Education or the Board has acted to relieve the applicant of this requirement. However, failure of the Board of Education to act prior to final plat consideration and approval by the Metropolitan Planning Commission in accordance with its schedule and requirements shall constitute a waiver of this requirement by the Board of Education.
12. The plan shall identify the limits of disturbance and any disturbance beyond those limits shall not be permitted without Council approval.
13. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
14. All traffic requirements specified by the TIS as indicated in the Traffic and Parking of the staff report and adopted by the Metro Planning Commission resolution shall be met, unless Public Works determines that specific requirements are not necessary or that additional requirements are necessary with future development.
15. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RS5 zoning district for single-family lots and the RM2 for multi-family units as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
16. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
17. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2015SP-065-001

SCOVEL HOMES DEVELOPMENT PHASE 1

Map 081-15, Parcel(s) 471-472

08, North Nashville

21 (Edith Taylor Langster)



Project No.	Zone Change 2015SP-065-001
Project Name	Scovel Homes Development Phase 1
Council District	20 - Baker
School District	9 - Frogge
Requested by	Scovel Homes Development, LLC, applicant and owner.

Deferral This request was previously deferred from the June 11, 2015, Planning Commission meeting.

Staff Reviewer Birkeland
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

Zone change to permit five detached residential units.

Preliminary SP

A request to rezone from Single-Family Residential District (RS5) to Specific Plan-Residential (SP-R) zoning for properties located at 1818 and 1818 B Scovel Street, approximately 265 feet east of 21st Avenue North, (0.4 acres), to permit up to five detached residential units.

Existing Zoning

Single-Family Residential District (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre. *RS5 would permit a maximum of 3 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes attached residential buildings.

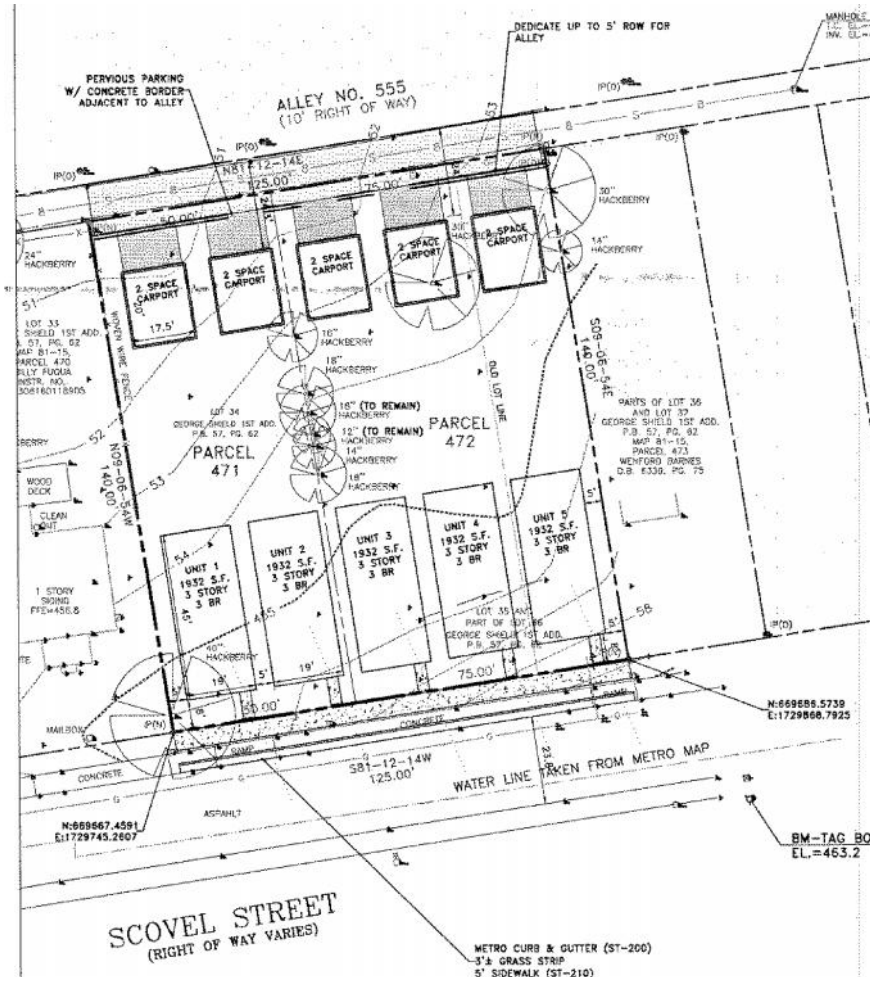
CRITICAL PLANNING GOALS

- Provides a Range of Housing Choices
- Supports Infill Development

The proposed SP creates an opportunity for infill housing in an area that is served by existing infrastructure. The request provides additional housing opportunities in the area. Housing options are important to serve a wide range of people with different housing needs. An existing sidewalk along Scovel Street will remain and continue to provide safe pedestrian access to other areas.



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Proposed Site Plan



ELEVATIONS FACING SCOVEL STREET LOT B - PHASE ONE(1)
ALL FACING SOUTH
CASE NUMBER 2015SP-065-001

Proposed Elevations



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NORTH NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

Consistent with Policy?

Yes. The policy supports the proposed detached residential units through form and interaction with the public realm. The SP will provide a broad range of housing types in a strategic location within North Nashville. The Neighborhood Evolving Policy also encourages improved alley access, which already exists along the rear of the property.

PLAN DETAILS

The site consists of two vacant residential parcels located at 1818 and 1818B Scovel Street in North Nashville. The proposed SP includes five detached dwelling units that will have frontage along Scovel Street.

The existing alley along the rear of the property will provide vehicular access to the proposed dwelling units. Vehicular access to the site will be provided from the alley to the five, two-car carports along the rear of the site. On street parking is currently allowed on Scovel Street; curb length allows up to five additional vehicles to park adjacent to the site. The carports shall be designed to meet the Metro Zoning Code parking requirement.

Scovel Street is identified as a local street in the Major and Collector Street Plan (MCSP). An existing five foot sidewalk and foot planting strip is already installed along Scovel Street. Each residential unit will have a connection to this sidewalk. All proposed sidewalks shall be a minimum of five feet. The applicant has not proposed landscaping within the SP. Staff recommends a type "A" landscape buffer yard be installed along the eastern and western property lines.

The applicant has provided proposed architectural elevations. Architectural standards have also been included on the plan. Elevations shall be provided with the final site plan. The proposed residential units shall have a maximum height limitation of three stories within 35 feet, measured to roofline.

ANALYSIS

The SP is consistent with the T4 Neighborhood Evolving policy and meets two critical planning goals. The eight detached residential units will provide a well-designed development within North Nashville that needs additional housing.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review



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STORMWATER RECOMMENDATION

Approved

WATER SERVICES RECOMMENDATION

Approved with conditions

- Approved as a Preliminary SP only. Public sewer construction plans must be submitted and approved prior to Final SP approval. The required capacity fees must also be paid prior to Final SP approval. The applicant should update their availability study by Final SP stage, so the appropriate fee amounts are paid (unit count has been reduced since latest study).

TRAFFIC AND PARKING RECOMMENDATION

Conditions if approved

- Provide parking per metro code. Carport width appears insufficient for 2 parking spaces.

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- ROW is to be dedicated prior to building permit signoff by MPW.
- Coordinate stormwater outfall with MPW and Metro Stormwater. Point source discharge into public ROW is not permitted.

Maximum Uses in Existing Zoning District: **RS5**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.4	8.71 D	3 U	29	3	4

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (210)	0.4	-	5 U	48	4	6

Traffic changes between maximum: **RS5** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 2 U	+19	+1	+2



Metro Planning Commission Meeting of 07/23/2015

METRO SCHOOL BOARD REPORT

Projected student generation existing RS5 district: 0 Elementary 0 Middle 0 High
Projected student generation proposed SP-R district: 0 Elementary 0 Middle 0 High

The proposed SP-R zoning district could generate no more students than what is typically generated under the existing RS5 zoning district. Students would attend Churchwell Elementary School, John Early Middle School, and Pearl-Cohn High School. All three schools have been identified as over capacity. There is capacity within the cluster for elementary and middle school students. There is capacity within the adjacent cluster for high school students. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions. The proposed SP is consistent with the T4 Neighborhood Evolving policy of the North Nashville Community Plan.

CONDITIONS

1. Uses within this SP shall be limited to a maximum of five detached residential units.
2. Revised plans shall be to scale and include sidewalk location and width.
3. Design of carports shall be of similar design as the principle structure and meet zoning code requirement.
4. Install a type "A" landscape buffer along the eastern and western property lines.
5. Architectural elevations for units 1-5 shall be varied. This may include varied window design, door location, type of materials, etc.
6. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM15-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
7. The final site plan shall include architectural elevations showing raised foundations of 18-36" for residential buildings.
8. No structure shall be more than three stories and shall be limited to a maximum height of 35 feet, measured to the roofline. Building elevations for all street and courtyard facing facades, including all carports, shall be provided with the final site plan. The following standards shall be met:
 - a. Building facades fronting a street and courtyard shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except in dormers.
 - c. EIFS and vinyl siding shall be prohibited.
9. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
10. The preliminary SP plan approved by the metropolitan council is of such detail that the executive director of the planning department or his designee may waive the submittal of a final site plan.
11. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
12. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site



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conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.

13. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2015SP-066-001

SCOVEL HOMES DEVELOPMENT PHASE 2

Map 081-15, Parcel(s) 468-469

08, North Nashville

21 (Edith Taylor Langster)



Project No.	Zone Change 2015SP-066-001
Project Name	Scovel Homes Development Phase 2
Council District	20 - Baker
School District	9 - Frogge
Requested by	Scovel Homes Development, LLC and Harold Love, Jr. et ux, owners.

Deferral This request was previously deferred from the June 11, 2015, Planning Commission meeting.

Staff Reviewer Birkeland
Staff Recommendation *Approve with conditions and disapprove without all conditions.*

APPLICANT REQUEST

Zone change to permit four residential units.

Preliminary SP

A request to rezone from Single-Family Residential District (RS5) to Specific Plan-Residential (SP-R) zoning for properties located at 1822 and 1824 Scovel Street, approximately 58 feet east of 21st Avenue North, (0.296 acres), to permit up to four residential units.

Existing Zoning

Single-Family Residential District (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre. *RS5 would permit a maximum of 3 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes attached residential buildings.

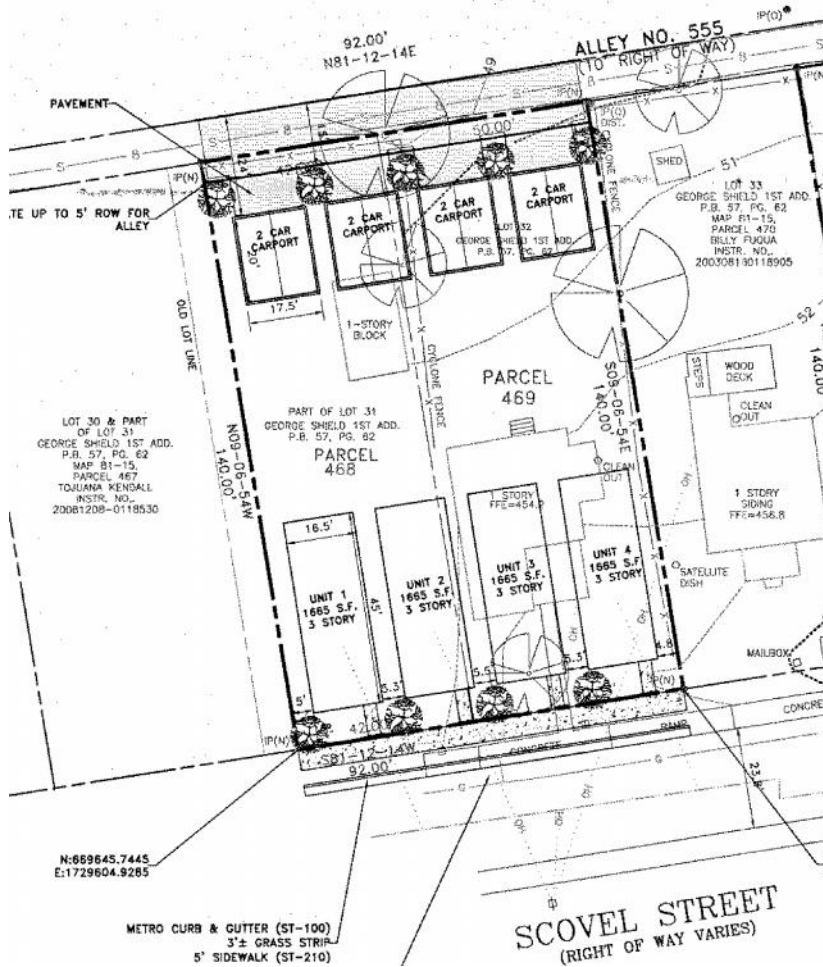
CRITICAL PLANNING GOALS

- Provides a Range of Housing Choices
- Supports Infill Development

The proposed SP creates an opportunity for infill housing in an area that is served by existing infrastructure. The request provides additional housing opportunities in the area. Housing options are important to serve a wide range of people with different housing needs. An existing sidewalk along Scovel Street will remain and continue to provide safe pedestrian access to other areas.



Metro Planning Commission Meeting of 07/23/2015



Proposed Site Plan



ELEVATIONS FACING SCOVEL STREET LOT A - PHASE TWO(2)
ALL FACING SOUTH CASE NUMBER 2015SP-066-001

Proposed Elevations



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NORTH NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban neighborhoods that are compatible with the general character of existing urban neighborhoods as characterized by their development pattern, building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. The resulting development pattern may have higher densities than existing urban neighborhoods and/or smaller lots sizes, with a broader range of housing types providing housing choice. This reflects the scarcity of easily developable land (without sensitive environmental features) and the cost of developing housing.

Consistent with Policy?

Yes. The policy supports the proposed detached residential units through form and interaction with the public realm. The SP will provide a broad range of housing types in a strategic location within North Nashville. The Neighborhood Evolving Policy also encourages improved alley access, which already exists along the rear of the property.

PLAN DETAILS

The site consists of two residential parcels located at 1822 and 1824 Scovel Street in North Nashville. The proposed SP includes four detached residential units with frontage along Scovel Street. Vehicular access to the site will be provided from the alley to the four, two car carports along the rear of the site. On street parking is currently allowed on Scovel Street; curb length allows up to four additional vehicles to park adjacent to the site. The carports shall be designed to meet the Metro Zoning Code parking requirement.

Scovel Street is identified as a local street in the Major and Collector Street Plan (MCSP). An existing five foot sidewalk and foot planting strip is already installed along Scovel. Each residential unit will have a connection to this sidewalk. All proposed sidewalks shall be a minimum of five feet. The applicant has not proposed landscaping within the SP. Staff recommends a type "A" landscape buffer yard be installed along the eastern and western property lines.

Architectural standards have been included on the plan and shall be provided with the final site plan. The proposed residential units shall have a maximum height limitation of three stories within 35 feet, measured to roofline.

ANALYSIS

The SP is consistent with the T4 Neighborhood Evolving policy and meets two critical planning goals. The residential units will provide additional housing in North Nashville.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review

STORMWATER RECOMMENDATION

Approved



Metro Planning Commission Meeting of 07/23/2015

WATER SERVICES RECOMMENDATION

Approved with conditions

- Approved as a Preliminary SP only. Public sewer construction plans must be submitted and approved prior to Final SP approval. The required capacity fees must also be paid prior to Final SP approval. The applicant should update their availability study by Final SP stage, so the appropriate fee amounts are paid (unit count has been reduced since latest study).

TRAFFIC AND PARKING RECOMMENDATION

Conditions if approved

- Provide parking per metro code. Carport width appears insufficient for 2 parking spaces.

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- ROW is to be dedicated prior to building permit signoff by MPW.
- Coordinate stormwater outfall with MPW and Metro Stormwater. Point source discharge into public ROW is not permitted.

Maximum Uses in Existing Zoning District: **RS5**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.296	8.71 D	2 U	20	2	3

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (210)	0.296	-	4 U	39	3	5

Traffic changes between maximum: **RS5** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 2 U	+19	+1	+2



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METRO SCHOOL BOARD REPORT

Projected student generation existing RS5 district: 0 Elementary 0 Middle 0 High
Projected student generation proposed SP-R district: 1 Elementary 1 Middle 1 High

The proposed SP-R zoning district could generate 3 more students than what is typically generated under the existing RS5 zoning district. Students would attend Churchwell Elementary School, John Early Middle School, and Pearl-Cohn High School. All three schools have been identified as over capacity. There is capacity within the cluster for elementary and middle school students. There is capacity within the adjacent cluster for high school students. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions. The proposed SP is consistent with the T4 Neighborhood Evolving policy of the North Nashville Community Plan.

CONDITIONS

1. Uses within this SP shall be limited to a maximum of four residential units.
2. Revised plans shall be to scale and include sidewalk location and width.
3. Design of the carports shall be of similar design as the principles structures and meet the Zone Code requirements.
4. Install a type "A" landscape buffer along the eastern and western property lines.
14. Architectural elevations for units 1-4 shall be varied. This may include varied window design, door location, type of materials, etc.
5. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM15-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
6. The final site plan shall include architectural elevations showing raised foundations of 18-36" for residential buildings.
7. No structure shall be more than three stories and shall be limited to a maximum height of 35 feet, measured to the roofline. Building elevations for all street and courtyard facing facades, including all carports, shall be provided with the final site plan. The following standards shall be met:
 - a. Building facades fronting a street and courtyard shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except in dormers.
 - c. EIFS and vinyl siding shall be prohibited.
8. Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
9. The preliminary SP plan approved by the metropolitan council is of such detail that the executive director of the planning department or his designee may waive the submittal of a final site plan.
10. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
11. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of



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the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.

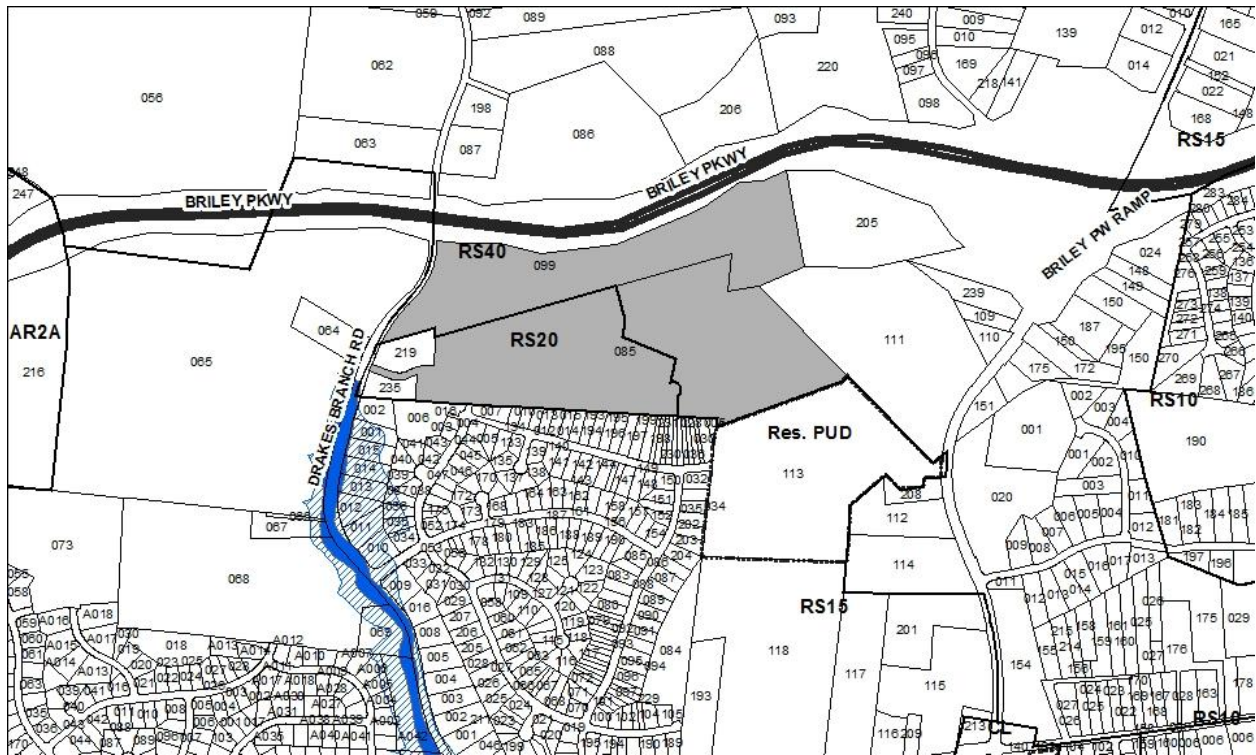
12. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2015SP-067-001

THE CROSSING AT DRAKES BRANCH

Map 058, Parcel(s) 085, 099

03, Bordeaux - Whites Creek

01 (Lonnell Matthews, Jr.)



Project No. 2015SP-067-001
Project Name The Crossing at Drake’s Branch
Council Bill BL2015-1301
Council District 1 - Matthews
School District 1 - Gentry
Requested by Dewey-Estes Engineering, applicant; Drakes Branch Development, LLC, owner.

Deferral This request was previously deferred from the June 11, 2015, and June 25, 2015, Planning Commission meeting.

Staff Reviewer Thomas
Staff Recommendation *Disapprove.*

APPLICANT REQUEST

Preliminary SP to permit up to 108 residential units.

Preliminary SP

A request to rezone from Single-Family Residential (RS20 and RS40) to Specific Plan-Residential (SP-R) zoning for properties located at 4834 and 4856 Drakes Branch Road, approximately 1,400 feet north of Judd Drive (76.13 acres), to permit up to 108 residential units.

Existing Zoning

Single-Family Residential (RS20) requires a minimum 20,000 square foot lot and is intended for single-family dwellings at a density of 1.85 dwelling units per acre. *RS20 would permit a maximum of 51 units.*

Single-Family Residential (RS40) requires a minimum 40,000 square foot lot and is intended for single-family dwellings at a density of .93 dwelling units per acre. *RS40 would permit a maximum of 57 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

CRITICAL PLANNING GOALS

N/A

BORDEAUX – WHITES CREEK COMMUNITY PLAN

T3 Suburban Neighborhood Evolving (T3 NE) is intended to create and enhance suburban residential neighborhoods with more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate density development patterns with moderate setbacks and spacing between buildings. T3 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing



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Proposed Site Plan



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and some elements of the existing developed character, such as the street network, block structure, and proximity to centers and corridors. T3 NE areas are developed with creative thinking in environmentally sensitive building and site development techniques to balance the increased growth and density with its impact on area streams and rivers.

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.

Consistent with Policy?

The proposed SP is inconsistent with the principles of the Community Character Manual policies. The proposed plan does not integrate the two-family residences throughout the development, and does not cohesively mix with the single-family residences. Additionally, the layout of the lots and road network is designed in a way that does not preserve some areas of steep slope, as would normally be required.

PLAN DETAILS

The 76.13-acre site is located along Drakes Branch Road, immediately south of Briley Parkway and north of the Royal Hills subdivision. The dead end terminus of Drakes Branch Road is approximately three-quarters of a mile north of the proposed entrance to this plan. The Royal Hills subdivision includes a mixture of single- and two-family residences.

Site Plan

The plan proposes 56 single-family residences and 52 two-family residences, referred to by the applicant as villas, for a total of 108 residential units. The two-family residences are primarily concentrated around 'Road A' in the northern part of the development, with additional two-family residences clustered on the cul-de-sacs of 'Road C' and 'Road E'. Staff has requested the applicant disperse the two-family residences throughout the development, primarily on larger lots located at intersections. All residences are front loaded. To ensure that the garage is not the prominent feature and that the pedestrian realm is enhanced, staff is recommending that any garage be setback a minimum of five feet from the front façade of the unit for which the garage serves.

The layout of the lots and road network proposes significant grading within the steep slopes associated with the large hillside in the eastern half of the development. Staff has requested the applicant remove Lots 36 through 49 to preserve the hillside and reduce disturbance due to grading.

Primary access to the development is provided by a connection of 'Road A' to Drakes Branch Road. 'Road B' provides a second access point, connecting to existing Lunn Drive in the Royal Hills subdivision to the south. All streets within the project are public and include detached five foot sidewalks. A stream bisects the site from east to west and multiple areas have been reserved for stormwater. An active open space is proposed in the southeast portion of the proposed development.



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ANALYSIS

The preliminary SP is inconsistent with the principles of the policies. The grading proposed on the plan is not sensitive to the natural contours associated with the steep slopes present on the site. The proposal does not incorporate single- and two-family residences into a unified development, consistent with the subdivision to the south. In addition, Stormwater and Traffic & Parking have not recommended approval of the request. Traffic & Parking is awaiting submittal of a Traffic Impact Study for review and Stormwater is awaiting a revised plan that depicts all streams and associated buffers. While SP zoning is intended to provide for flexible design standards, it is not intended to allow development that is not consistent with the Community Plan’s land use policies. Therefore, staff recommends disapproval of the SP.

FIRE MARSHAL’S OFFICE

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Returned

- Not all streams are shown with the associated buffers (see area near culdesac and the grading near lot 24).
- Show adequate buffers or provide a determination showing the conveyances as non-jurisdictional.

WATER SERVICES

Approve with conditions

- Approved as a Preliminary SP only.
- Public water and sewer construction plans must be submitted and approved before the Final SP is approved.
- The required capacity fees must also be paid prior to Final SP approval.

PUBLIC WORKS RECOMMENDATION

Conditions if approved

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.

TRAFFIC AND PARKING RECOMMENDATION

Returned

- A TIS is required prior to preliminary SP approval.
- TIS was scoped on May 26, but has not been received.

Maximum Uses in Existing Zoning District: **RS20**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	23.69	2.17 D	51 U	489	39	52



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Maximum Uses in Existing Zoning District: **RS40**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	52.44	1.08 D	57 U	546	43	58

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	76.13	-	56 U	536	42	57

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (210)	76.13	-	52 U	498	39	53

Traffic changes between maximum: **RS20, RS40 and SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	-	-	-

METRO SCHOOL BOARD REPORT

Projected student generation existing RS20 & RS40 districts: 21 Elem. 17 Middle 17 High

Projected student generation proposed SP-R district: 23 Elementary 21 Middle 20 High

The proposed SP-R zoning district would generate 9 additional students than what is typically generated under the existing RS20 and RS40 zoning districts. Students would attend Cumberland Elementary School, Joelton Middle School, and Whites Creek High School. Cumberland Elementary School has been identified as over capacity; however, there is capacity within the cluster for elementary school students. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends disapproval because the request is inconsistent with the principles of the Community Character Manual policies.

CONDITIONS (if approved)

1. Uses within the SP shall be limited to a maximum of 94 residential units.



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2. Remove Lots 36 through 49 due to proposed grading that is not sensitive to the steep slopes present on the site.
3. Two-family lots along 'Road A' shall be dispersed throughout the site on corner lots that are a minimum 10,000 square feet.
4. Any garage facing a primary street frontage shall be recessed a minimum of five feet from the front façade of any attached unit.
5. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the R6 zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
6. The following design standards shall be added to the plan:
 - a. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.
 - c. EIFS, vinyl siding and untreated wood shall be prohibited.
 - d. Porches shall provide a minimum of six feet of depth.
 - e. A raised foundation of 18"- 36" is required for all residential structures, unless the structure is located on a hill where site conditions preclude a raised foundation.
7. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
8. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
9. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
10. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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2015Z-057PR-001

Map 119-14, Parcel(s) 003

11, South Nashville

16 (Tony Tenpenny)



Project No.	Zone Change 2015Z-057PR-001
Council District	16- Tenpenny
School District	7 - Pinkston
Requested by	Land Development Solutions - Nashville, applicant; Terry Woodall, owner.
Deferral	This request was previously deferred from the June 25, 2015, Planning Commission meeting.
Staff Reviewer	Birkeland
Staff Recommendation	<i>Defer indefinitely.</i>

APPLICANT REQUEST
Zone change from RS10 to R10.

Zone Change

A request to rezone from Single-Family Residential (RS10) to One and Two-Family Residential (R10) zoning for property located at 2913 Simmons Avenue, at the northwest corner of Simmons Avenue and Tanksley Avenue (0.35 acres).

Existing Zoning

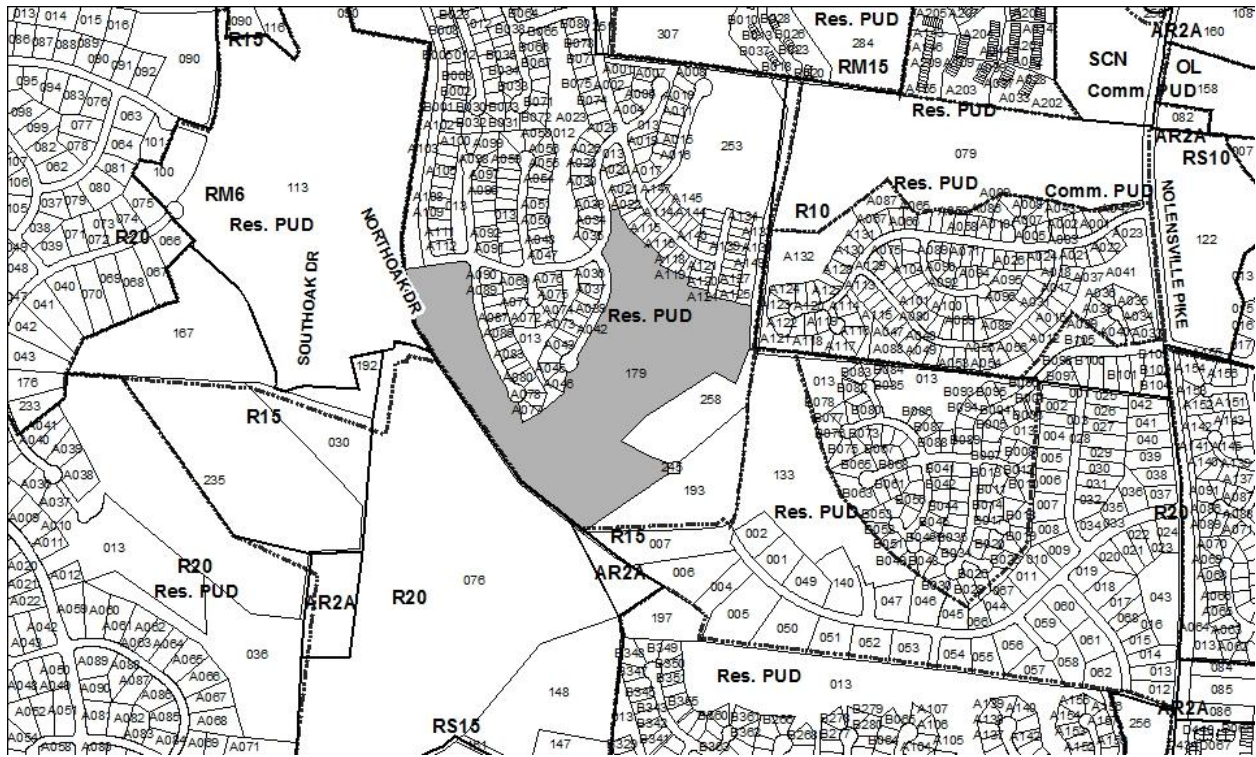
Single-Family Residential (RS10) requires a minimum of 10,000 square foot lot and is intended for single-family dwellings at a density of 3.7 dwelling units per acre. *RS10 would permit a maximum of 1 unit.*

STAFF RECOMMENDATION

Staff recommends an indefinite deferral at the request of the applicant.



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122-83P-001

THE WOODLANDS, PHASE 3

Map 172, Parcel(s) 179

12, Southeast

04 (Brady Banks)



Project No.
Project Name
Council District
School District
Requested by

Planned Unit Development 122-83P-001
The Woodlands, Phase 3
4 – Banks
2 – Brannon
Dewey-Estes Engineering, applicant; Woodlands Development, LLC, owner.

Deferral

This case was previously deferred from the May 28, 2015, June 11, 2015, and June 25, 2015, Planning Commission meetings.

Staff Reviewer
Staff Recommendation

Milligan
Defer indefinitely.

APPLICANT REQUEST

Revise preliminary plan and for final site plan for The Woodlands, Phase 3 Planned Unit Development.

Revise Preliminary PUD and Final Site Plan

A request to revise a portion of The Woodlands Residential Planned Unit Development Overlay District and for final site plan on property located at 1203 Pineview Lane, at the terminus of Boxwood Drive (31.54 acres), zoned One and Two-Family Residential (R15), to revise the lots lines of Phase 3.

STAFF RECOMMENDATION

Staff recommends an indefinite deferral at the request of the applicant.



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2015S-066-001
RIVERSIDE DRIVE, SECTION 2
Map 083-11, Parcel(s) 080
05, East Nashville
06 (Peter Westerholm)



Project No.	Subdivision 2015S-066-001
Project Name	Riverside Drive, Sec. 2 (Concept Plan)
Council District	6 – Westerholm
School District	5 – Kim
Requested by	Chandler Surveying, applicant; Riverside Development, LLC, owner.

Deferrals This request was previously deferred at the June 25, 2015, Planning Commission meeting.

Staff Reviewer Swaggart
Staff Recommendation *Defer to the August 27, 2015, Planning Commission meeting.*

APPLICANT REQUEST
Create 18 single-family lots.

Concept Plan
A request for concept plan approval to create 18 lots on property located at Riverside Drive (unnumbered), approximately 335 feet north of Paden Drive, zoned One and Two-Family residential (R10) (4.44 acres).

STAFF RECOMMENDATION
Staff recommends deferral to the August 27, 2015, Planning Commission meeting.



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RECOMMENDATIONS TO METRO COUNCIL

- **Zoning Text Amendments**
- **Specific Plans**
- **Zone Changes**
- **Historic Preservation Overlays**
- **Historic Landmark Overlays**



NO SKETCH



Project No.	Text Amendment
	2015Z-013TX-001
Project Name	Lafayette Subdistrict Text Amendment
Council Bill	BL2015-1253
Council District	19 – Gilmore
School District	5 – Kim
Requested by	Tune, Entrekin, & White PC, applicant; various property owners.
Staff Reviewer	Collins
Staff Recommendation	<i>Approve with an amendment.</i>

APPLICANT REQUEST

Amend the Lafayette Subdistrict of the Downtown Code (Chapter 17.37 of the Zoning Code) to allow Transitional Properties 15 stories by-right.

TEXT AMENDMENT

A request to amend Title 17 of the Metropolitan Code, to modify the maximum height for transitional properties in the Lafayette subdistrict within Chapter 17.37, the Downtown Code.

ANALYSIS

The proposed text amendment seeks to increase the by-right height for designated Transitional Properties in the Lafayette Subdistrict of the DTC from 8 stories to 15 stories. The Bonus Height Program Chart will also be updated to include a Transitional Properties category, with potential Bonus Height up to 3 additional stories, if complying with the standards of the DTC’s Bonus Height Program.

The DTC defines Transitional Properties as those properties located within the Lafayette Subdistrict, that border the SoBro Subdistrict boundary along Koreans Veteran’s Boulevard (KVB). These are the properties that comprise the blocks located between Koreans Veteran Boulevard and Peabody Street from 1st Avenue South to the KVB Roundabout. Per the DTC, if these properties have frontage or consolidate to have any frontage on KVB, then they are considered to be within the SoBro Subdistrict, permitting up to 30 stories by-right. However, if they do not have frontage onto KVB, they remain in the Lafayette Subdistrict, where the by-right height is 8 stories for Subdistrict General properties, and 12 stories for properties with frontage on Lafayette Street.

These height standards have created a condition where Transitional Properties on the same block between KVB and Peabody Street, have differing height entitlements of either 30 stories or only 8 stories if without KVB frontage. The proposed text amendment would allow the Transitional Properties 15 stories by-right, creating a transition in height between the 30 story and the 8 story height entitlements. The DTC’s intent is to create a transition within this block between more intense development in the SoBro Subdistrict, and mid-rise development in the Lafayette Subdistrict. However, by not prescribing a transitional height, it currently creates juxtaposition in height rather than a transition. The amendment would allow for a truer transition in height to occur: 30 stories, 15 stories, and 8 stories.



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Hatched properties are transitional properties



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DOWNTOWN COMMUNITY PLAN

The Land Use Policy is T6 Downtown Core for the affected areas, and the Downtown Community Plan calls for a transition in height to occur at this location with Special Policy 09-T6DN-SOBRO-02 - Properties long Peabody Street:

“If properties south of Korean Veterans Boulevard (on Peabody Street or numbered streets such as 1st through 6th streets) are consolidated and developed with frontage on Gateway (KVB), then the properties shall be considered part of SoBro and the goals and objectives of SoBro and the T6 Downtown Core policy shall apply. See the Downtown Code for codification of this policy.”

“If properties south of Korean Veteran Boulevard are developed without frontage on KVB, then they shall be required to transition in height down from the T6 Core to T6 Downtown Neighborhood policy. The actual height will take in to consideration the context of the individual property and achievable heights in adjacent policy areas, but it is expected these heights will range between 8 and 20 stories, with higher heights seen closer to Korean Veterans Boulevard and lower heights transitioning into the adjacent neighborhoods.”

STAFF RECOMMENDATION

Staff recommends approval with an amendment. The amendment is to fully incorporate the Downtown Code Text Amendment 2015Z-003TX-001, approved by the Planning Commission on May 28, 2015 (the associated Council Bill 2015-1053 is scheduled for 3rd reading at Metro Council on August 4, 2015), into the applicant’s proposed text amendment exhibit (pages 39 and 99 of the DTC document).

The applicant’s proposed text amendment is consistent with the Downtown Community Plan’s Special Policy for creating a transition in height at this location, and serves to create a true transition in height between 30 stories and 8 stories, by permitting up to 15 stories by-right for Transitional Properties.

ORDINANCE NO. BL2015-1253

An ordinance to amend Title 17 of the Metropolitan Code of Laws, the Zoning Ordinance of The Metropolitan Government of Nashville and Davidson County, to modify the maximum height for transitional properties in the Lafayette subdistrict within Chapter 17.37, all of which is described herein (Proposal No. 2015Z-013TX-001).

NOW, THEREFORE, BE IT ENACTED BY THE COUNCIL OF THE METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY:

Section 1. That Chapter 17.37 of the Metropolitan Code is hereby amended by deleting pages 39 and 99 and replacing with pages 39 and 99 in the attached Exhibit A.

Section 2. Be it further enacted, that this ordinance take effect immediately after its passage and such change be published in a newspaper of general circulation, the welfare of The Metropolitan Government of Nashville and Davidson County requiring it.

Sponsored by: Erica Gilmore



Section II: Subdistrict Standards

Lafayette Subdistrict: Building Regulations

Frontage

A Allowed Frontage Types with Required Build-to Zone

Primary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
Secondary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'

Tertiary Street	
• Storefront Frontage	0'-10'
• Stoop Frontage	5'-10'
• Porch Frontage	10'-15'

Industrial Frontage is allowed in this Subdistrict.

B Facade width

Primary Street	80% of lot frontage min.
Secondary Street	80% of lot frontage min.
Tertiary Street	60% of lot frontage min.

Remaining lot frontage may be used for pedestrian amenities and shall not be used for parking.

C Min. building depth 15' from building facade

Height

D Max.

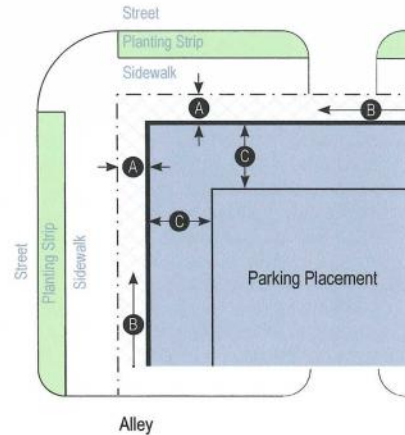
• Transitional properties	15 stories
• On Lafayette St	12 stories
• Subdistrict general	8 stories

Additional height available through the Bonus Height Program

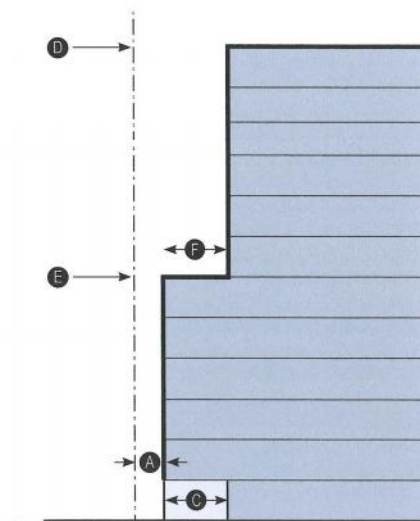
Step-back

E Step-back required for all buildings fronting public streets

• Step-back after	6 stories
• Min. step-back depth	15'



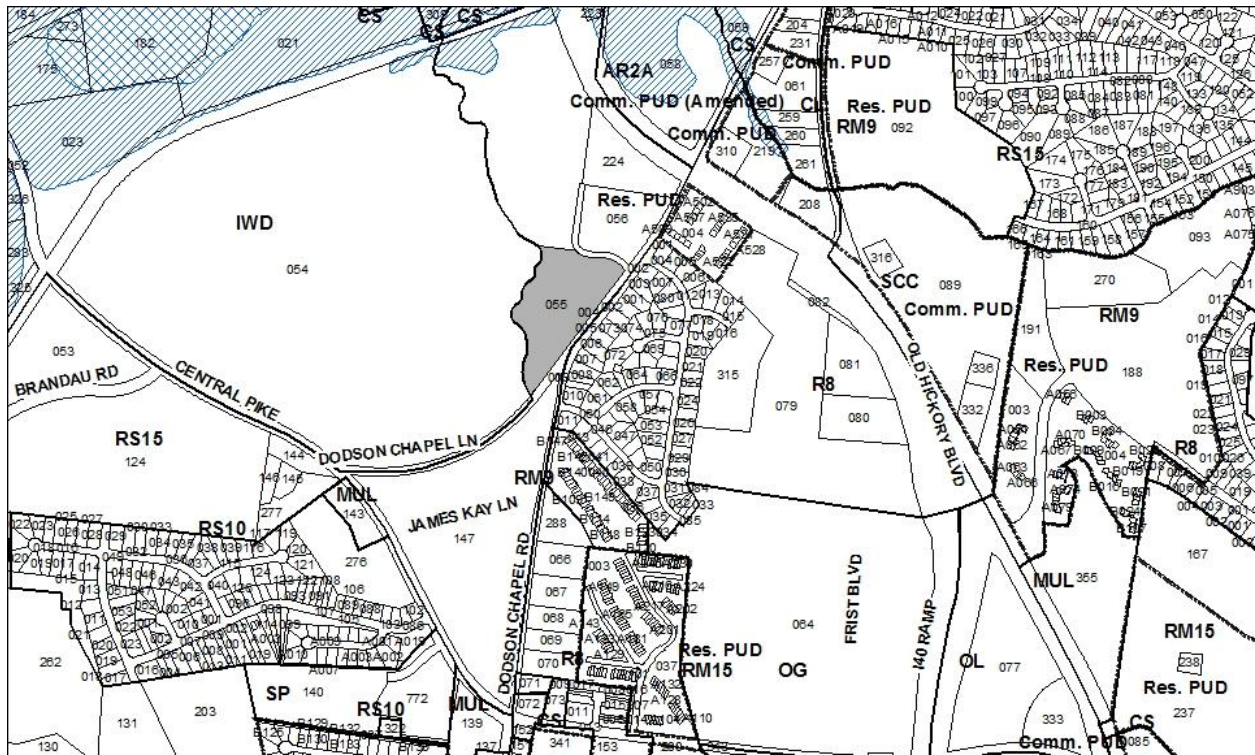
Building Plan



Building Section



Metro Planning Commission Meeting of 07/23/2015



2014SP-006-001

DODSON CHAPEL CHILDCARE II, PH II (PRELIM & FINAL)

Map 086, Parcel(s) 055

14, Donelson - Hermitage

14 (James Bruce Stanley)



Project No.	Zone Change 2014SP-006-001
Project Name	Dodson Chapel Childcare
Council District	14- Stanley
School District	4 – Shepard
Requested by	Anderson Architects, applicant; Dodson Chapel United Methodist Church, owner.
Staff Reviewer	Birkeland
Staff Recommendation	<i>Approve with conditions and disapprove without all conditions.</i>

APPLICANT REQUEST

Preliminary and Final SP to permit a daycare facility for up to 207 persons.

Preliminary and Final Site Plan

A request to rezone from Single Family Residential (RS15) to Specific Plan-Institutional (SP-INS) zoning and for final site plan approval for property located at 4190 Dodson Chapel Road, at the southwest corner of Dodson Chapel Road and Dodson Chapel Court (7.2 Acres), to permit a daycare of up to 207 persons within an existing facility.

Existing Zoning

Single Family Residential (RS15) requires a minimum 15,000 square foot lot and is intended for single-family dwellings at a density of 2.47 dwelling units per acre. *RS15 would permit a maximum of 20 units.*

Proposed Zoning

Specific Plan-Institutional (SP-INS) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes institutional uses.

CRITICAL PLANNING GOALS

N/A

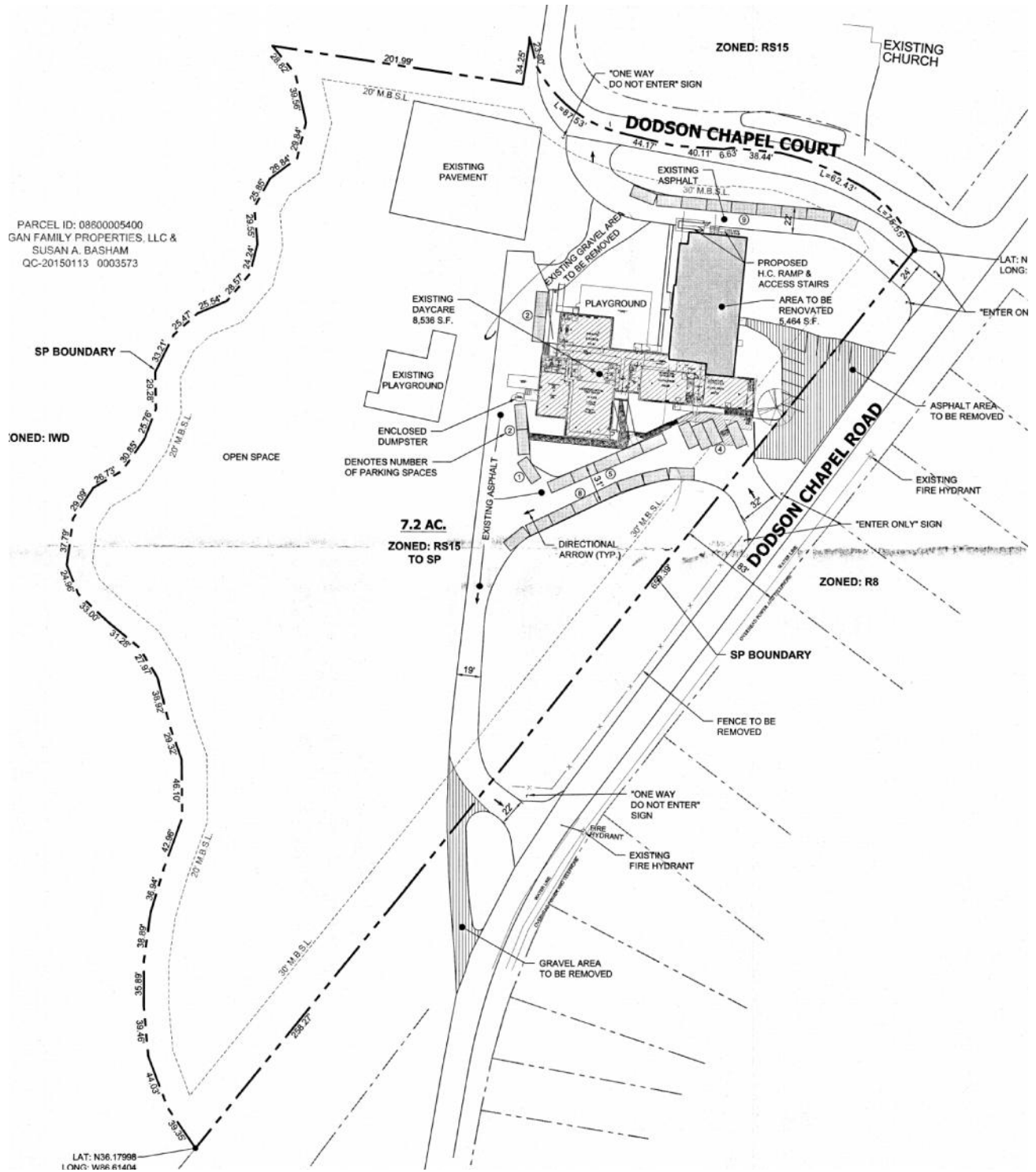
DONELSON – HERMITAGE COMMUNITY PLAN

T3 Suburban Neighborhood Evolving (T3 NE) is intended to create and enhance suburban residential neighborhoods with more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate density development patterns with moderate setbacks and spacing between buildings. T3 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network, block structure, and proximity to centers and corridors. T3 NE areas are developed with creative thinking in environmentally sensitive building and site development techniques to balance the increased growth and density with its impact on area streams and rivers.



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PARCEL ID: 08600005400
GAN FAMILY PROPERTIES, LLC &
SUSAN A. BASHAM
QC-20150113 0003573



Proposed Site Plan



Metro Planning Commission Meeting of 07/23/2015

Consistent with Policy?

Yes. The policy supports institution uses within residential districts. Expanding the existing daycare use in the T3 Suburban Neighborhood Evolving policy will provide additional daycare opportunities to accommodate the evolving residential uses surrounding the property.

PLAN DETAILS

The approximate 7.2 acre site is located at 4190 Dodson Chapel Road and is located within the Dodson Chapel Worthy of Conservation district. This property currently has an existing daycare facility located on the site. The Dodson Chapel Child Care Center currently has programs for up to 75 children, from 6 weeks to 12 years old. The proposed expansion of the childcare program includes renovating 5,494 square feet of the existing 14,000 square foot, one-story building to allow for a total capacity of 207 students and 26 employees. This property has been designated Worthy of Conservation.

The site currently has six access points; five along Dodson Chapel Road and one along Dodson Chapel Court. One access drive and one parking area will be removed along Dodson Chapel Road. The remaining four access points will be established as "one-way" only. Vehicles on the north side of the facility will exit onto Dodson Chapel Court while vehicles on the south side of the facility will exit onto Dodson Chapel Road.

The site will provide 31 parking stalls placed along the drive isles for parent/student morning drop offs and afternoon pick-ups. The 26 staff members will park in the existing parking lot of the Dodson Chapel United Methodist Church, on the north side of Dodson Chapel Court. This off-site parking area will also hold overflow parking for yearly events at the daycare. The parking provided meets the Metro Codes parking standards.

Sidewalks are not proposed on the SP plan. However, staff recommends that six foot sidewalks shall be installed to facilitate safe pedestrian movement. A six foot sidewalk shall be installed along the northern side of the southern drive isle, south of the existing daycare building. A six foot sidewalk shall be installed along the southern side of the northern drive isle, extending from Dodson Chapel Court to Dodson Chapel Road.

ANALYSIS

The expansion of the existing daycare use is supported by the T3 Neighborhood Evolving policy. Based upon the T3 Neighborhood Evolving policy and the standards in the proposed SP, staff recommends approval of the SP.

METROPOLITAN HISTORIC ZONING COMMISSION STAFF RECOMMENDATION

No exception taken

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review

STORMWATER RECOMMENDATION

Approved



Metro Planning Commission Meeting of 07/23/2015

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- MPW understands that the operation of childcare facilities is such that guardians must walk each student into the facility and sign them in, thus additional parking may be required.
- If sidewalks are required, then they should be shown on the plan per Public Works standards with the required curb and gutter and grass strip.

TRAFFIC AND PARKING RECOMMENDATION

Approve with conditions

- Comply with the parking and arrival and dismissal study recommendations dated 7/10/15.

METRO WATER RECOMMENDATION

Approve with conditions

- As all our previous issues have been addressed via the latest SP plan revision (stamped received June 30th), including payment of capacity fees, we recommend approval on the following conditions:
 - 1) Approval does not apply to private water and sewer design. These must be submitted via a separate review process with Metro Water Permits, and approved before connection permits are issued.

Maximum Uses in Existing Zoning District: **RS15**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	7.2	2.90 D	20 U	192	15	21

Maximum Uses in Proposed Zoning District: **SP-INS**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Daycare (565)	7.2	-	207 persons	937	157	143

Traffic changes between maximum: **RS15** and **SP-INS**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+745	+142	+122



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METRO SCHOOL BOARD REPORT

According to information provided by Metro Schools, this request will not generate additional students. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

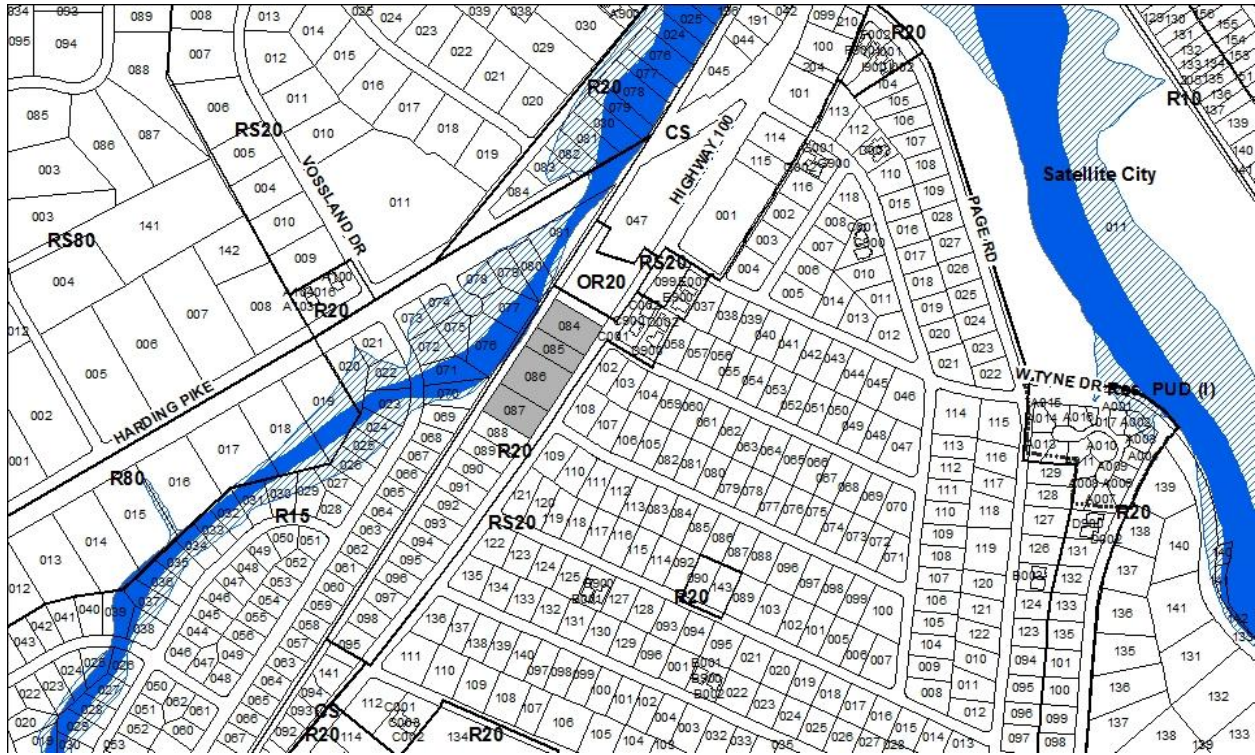
Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS

1. Uses within this SP shall be limited to permit a daycare use of up to 207 kids.
2. A six foot sidewalk shall be installed along the northern side of the southern drive isle, south of the existing daycare building. A six foot sidewalk shall be installed along the southern side of the northern drive isle, extending from Dodson Chapel Court to Dodson Chapel Road.
3. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
4. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RS15 zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
5. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
6. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



Metro Planning Commission Meeting of 07/23/2015



2015SP-072-001

VILLAGES AT RAIL STATION

Map 129-08, Parcel(s) 084-087

07, West Nashville

23 (Emily Evans)



Project No. Zone Change 2015SP-072-001
Project Name Villages at Rail Station
Council Bill No. BL2015-1259
Council District 23 – Evans
School District 8 – Pierce
Requested by Dale and Associates, applicant; various property owners.

Staff Reviewer Swaggart
Staff Recommendation *Approve with conditions and disapprove without all conditions if the Commission directs staff to amend the policy from Transition to Suburban Neighborhood Center.*

APPLICANT REQUEST

Zone change from R20 to SP-O to permit a mixed-use development.

Zone Change

A request to rezone from One and Two-Family Residential (R20) to Specific Plan – Office (SP-O) for properties located at 6200, 6202, 6300 Highway 100 and Highway 100 (unnumbered), approximately 630 feet south of Harding Pike (3.44 acres), to permit a mixed-use development.

Existing Zoning

One and Two-Family Residential (R20) requires a minimum 20,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 2.31 dwelling units per acre including 25 percent duplex lots. *R20 would permit a maximum of seven lots with one duplex lot for a total of eight units.*

Proposed Zoning

Specific Plan-Mixed-Use (SP-MU) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes residential uses in addition to office and/or commercial uses.

HISTORY

This request was originally proposed for 60,200 square feet of office space. The Planning Commission recommended that Council disapprove the request at the June 25, 2015, Planning Commission meeting. An amended request was approved by Council on first reading on July 7, 2015, and referred back to the Commission. The plan has been amended to include residential, office and commercial uses. Because there is a new plan to consider, the applicant has sent new notices. Therefore, staff recommends reopening the public hearing.

CRITICAL PLANNING GOALS

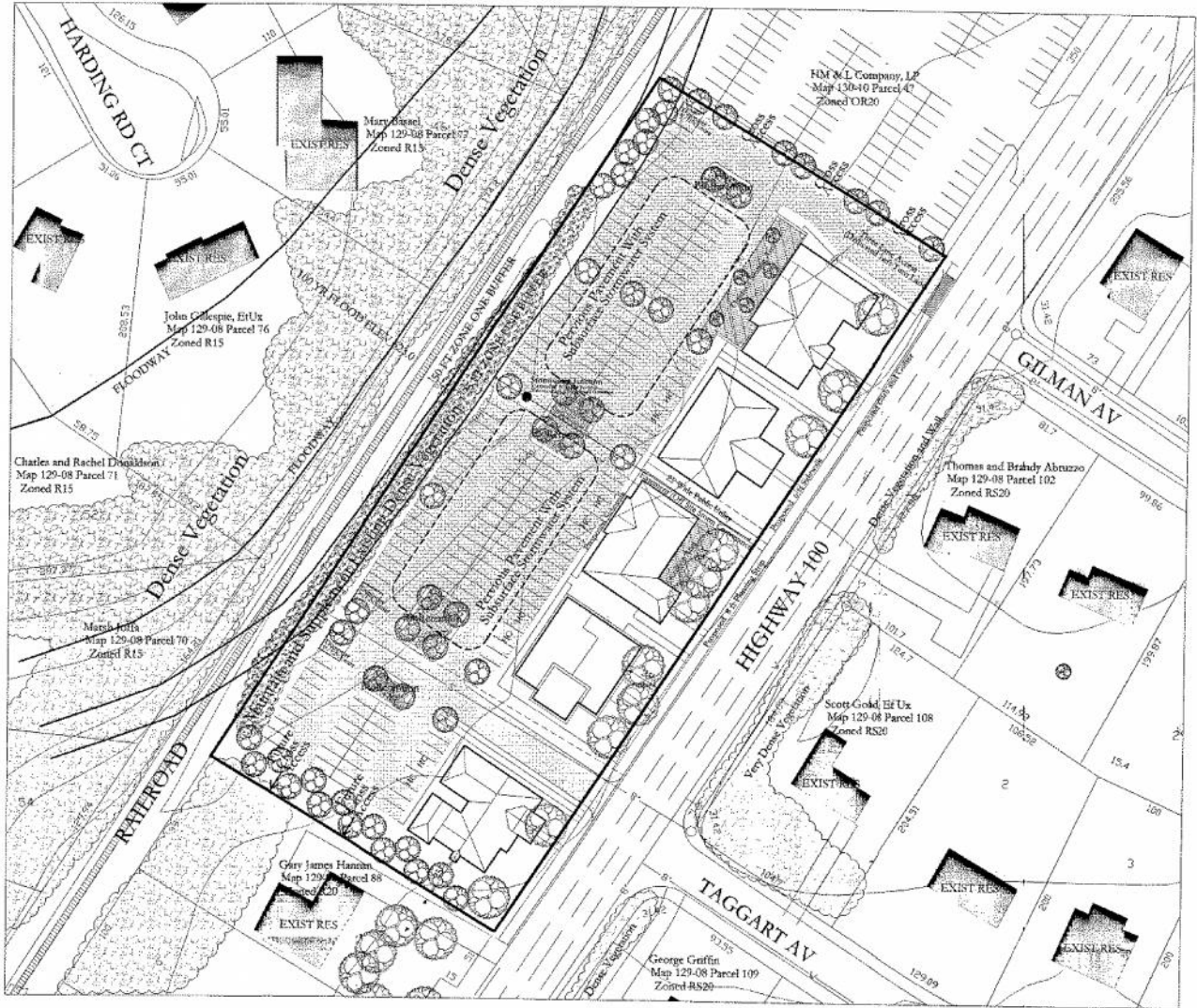
N/A

WEST NASHVILLE COMMUNITY PLAN

Transition (TR) policy is intended to preserve, enhance, and create areas whose primary purposes are to serve as transitions between higher intensity uses or major thoroughfares and lower density residential neighborhoods while providing opportunities for small scale offices and/or residential



Metro Planning Commission Meeting of 07/23/2015



Proposed Site Plan



Metro Planning Commission Meeting of 07/23/2015

development. Housing in Transition areas can include a mix of types and is especially appropriate for “missing middle” housing such as duplexes, bungalow courts, and multifamily housing with small to medium-sized footprints. Predominant uses in Transition areas are small scale offices and moderate density residential of all types. Transition areas may be used in situations where it would otherwise be difficult to provide a transition between higher intensity development or a major thoroughfare and an adjacent residential neighborhood and where there is a market for a compatibly scaled office, live-work, and/or residential uses.

Consistent with Policy?

The request is not consistent with the Transition policy because it permits commercial uses. The Transition policy does not support commercial uses, but only supports residential and office uses intended to provide a transition from more intense areas that are zoned and/or have policy for commercial and less intense residential areas. The policy can also be applied to areas between commercial with the intent of not stripping out the area for which the Transition policy is applied.

PLAN DETAILS

The site consists of four individual properties totaling approximately 3.44 acres. The site which is on the east side of Highway 100 and is bounded by Highway 100 and a CSX railroad that runs along the western property boundary. The site is listed as Worthy of Conservation (WOC). The WOC designation recognizes the Red House Spring House. While the designation recognizes the spring house, the applicant and Metro Historic staff has not been able to locate the spring.

Site Plan

The plan calls for a mixed-use development that would permit residential, office and commercial. Commercial uses are limited to used permitted under MUL-A. The maximum floor area is limited to 59,975 square feet. Retail is limited to a maximum of 40% of the overall floor area which is 23,975 square feet. Residential uses are limited to 20 units. Buildings are proposed to be two stories in height and the SP limits the maximum height to two stories in 35 feet. Buildings are setback 25 feet from Highway 100. The plan provides images of building types and includes some architectural standards.

Access into the site is proposed from two drives from Highway 100. Both drives line up with the two streets on the opposite side of Highway 100 (Gilman Avenue and Taggart Avenue). All parking is located to the rear of the buildings and includes 202 spaces (~1 space per 300 sq. ft.). The plan provides a six foot sidewalk and eight foot planting strip along Highway 100.

ANALYSIS

As proposed, the plan in not consistent with the Transition policy because it permits commercial uses which are not supported by the policy. Staff would support a change in policy to Suburban Neighborhood Center (T3 NC). Following is a description of the policy:

Suburban Neighborhood Center (T3 NC) policy is intended to enhance and create suburban neighborhood centers that are compatible with the general character of suburban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhance infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. T3 Suburban Neighborhood Centers are pedestrian friendly areas, generally located at



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intersections of suburban streets that contain commercial, mixed use, civic and public benefit land uses, with residential present only in mixed use buildings. T3 Suburban Neighborhood Centers serve suburban neighborhoods within a 5 minute drive.

While a transition policy can work on the subject site, the T3-NC policy is also appropriate given it is adjacent to Highway 100 and an active railroad track. The T3 NC policy is also located on the same side of Highway 100 to the north and the south of the subject site. As proposed the plan provides architectural standards that are in keeping with a residential character. Staff is also looking into pedestrian improvements which could provide pedestrian access from the opposite side of Highway 100.

FIRE MARSHAL’S OFFICE

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Approved

PUBLIC WORKS RECOMMENDATION

Approved with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- ROW dedication to the back of sidewalk may be required if the proposed sidewalk is not contained within the existing ROW.

TRAFFIC AND PARKING

Conditions if approved

- A TIS is required prior to final SP plan approval.
- At a minimum a center turn lane along Hwy 100 frontage with appropriate tapers shall be required.

Maximum Uses in Existing Zoning District: **R20**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (210)	3.44	2.17 D	8 U*	77	6	9

*Based on one two-family lot.

Maximum Uses in Proposed Zoning District: **SP-MU**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (220)	3.44	-	20U	245	14	29



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Maximum Uses in Proposed Zoning District: **SP-MU**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Office (710)	3.44	-	36,000 SF	608	83	120

Maximum Uses in Proposed Zoning District: **SP-MU**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (814)	3.44	-	23,975 SF	1064	27	80

Traffic changes between maximum: **R20** and **SP-MU**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+1,840	+118	+220

WATER SERVICES RECOMMENDATION

Approved

STAFF RECOMMENDATION

Staff recommends the proposed SP be approved with the condition and disapproved without all staff conditions if the Commission directs staff to amend the Transition policy to Suburban Neighborhood Center (T3 NC). The T3 NC policy is already in place immediately to the north and south of this site and would provide additional opportunities for neighborhood scaled development. If the Commission does not instruct staff to amend the policy, then staff recommends disapproval as the proposed SP is not consistent with the existing Transition policy.

CONDITIONS (if approved)

1. Uses are limited to all uses permitted under MUN-A.
2. Prior to the issuance of a grading permit the site must be evaluated per Tennessee State law to determine if grave sites are present on the site. If graves are found on-site, the final site plan shall be revised or the grave sites shall be moved in compliance with all applicable laws.
3. Prior to final site plan approval, a Traffic Impact Study (TIS) shall be completed. In addition to standard Public Works requirements, the TIS must explore the possibility of a signalized pedestrian crossing at either Taggart Avenue or Gillman Avenue. If Planning and Public Works determines that a signalized crosswalk is not feasible, then other pedestrian improvements may be required.
4. Commercial uses (non-office) shall be limited to 40% of the total permitted floor area.
5. A minimum of 10,000 square feet of commercial is required.
6. Multi-family shall be limited to no more than 20 units.
7. The maximum height is limited to two stories in 35 feet.
8. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of



Metro Planning Commission Meeting of 07/23/2015

the MUN-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.

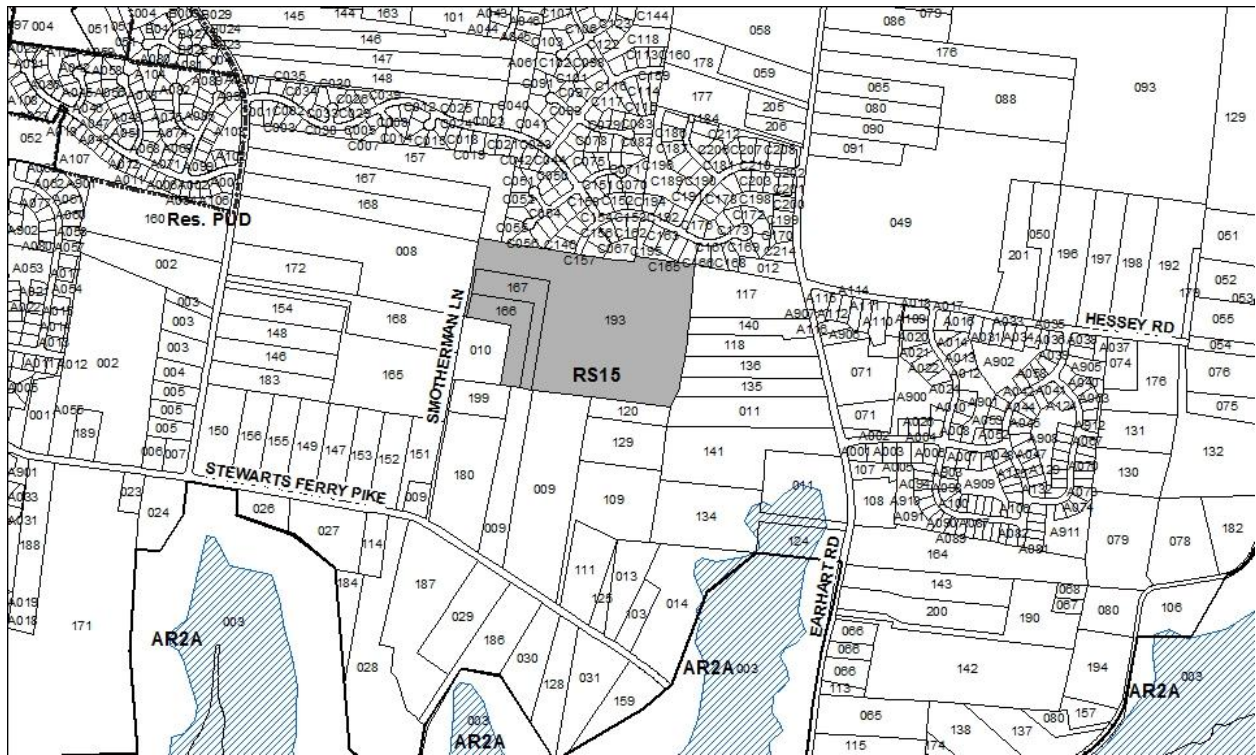
9. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
10. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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Metro Planning Commission Meeting of 07/23/2015



2015SP-075-001

THE VILLAS OF THE MEADOWS OF SEVEN POINTS

Map 110, Parcel(s) 166, 167, 193

14, Donelson - Hermitage

12 (Steve Glover)



Project No.	2015SP-075-001
Project Name	The Villas of the Meadows of Seven Points
Council District	12 - Glover
School District	4 - Shepherd
Requested by	Joe C. McConnell, applicant; David Fisher, Trustee, owner.
Staff Reviewer	Thomas
Staff Recommendation	<i>Approve with conditions and disapprove without all conditions.</i>

APPLICANT REQUEST

Preliminary SP to permit up to 86 residential units.

Preliminary SP

A request to rezone from Single-Family Residential (RS15) to Specific Plan-Residential (SP-R) zoning for properties located at 2237 Seven Points Circle and 4103 and 4109 Smotherman Lane, approximately 1,500 feet north of Stewarts Ferry Pike (32.06 acres), to permit up to 86 residential units on 45 lots.

Existing Zoning

Single-Family Residential (RS15) requires a minimum 15,000 square foot lot and is intended for single-family dwellings at a density of 2.47 dwelling units per acre. *RS15 would permit a maximum of 92 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

DONELSON - HERMITAGE - OLD HICKORY COMMUNITY PLAN

T3 Suburban Neighborhood Evolving (T3 NE) is intended to create and enhance suburban residential neighborhoods with more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate density development patterns with moderate setbacks and spacing between buildings. T3 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network, block structure, and proximity to centers and corridors. T3 NE areas are developed with creative thinking in environmentally sensitive building and site development techniques to balance the increased growth and density with its impact on area streams and rivers.



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Proposed Site Plan



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Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.

Consistent with Policy?

The proposed SP is inconsistent with the T3 NE policy. The request proposes 86 residential units, 82 of which are two-family. The T3 NE policy is designed to provide a thorough mix of housing types, versus groupings of single types of housing. Additionally, the lack of a vehicular connection to Smotherman Lane restricts the ability of the project to provide moderate to high levels of connectivity.

Staff has requested the applicant reduce the number of two-family units and disperse their location at key intersections and larger lots throughout the development. In addition, staff has requested a direct connection from Seven Points Circle to Smotherman Lane. Subject to these conditions, staff finds the SP would be consistent with the T3 NE policy.

PLAN DETAILS

The 32.06 acre site is located south of the existing Meadows of Seven Points subdivision, and at the terminus of Smotherman Lane. The site is approximately 1,500 feet north of Stewarts Ferry Pike and 1,000 feet west of Earhart Road. The Meadows of Seven Points subdivision to the north consists of single-family residences on one-quarter acre or larger lots. To the south, east, and west are single-family residences on properties that are approximately 2 acres to 16 acres in size.

Site Plan

The plan proposes 41 two-family lots and 4 single-family lots, for a total of 86 residential units. The single-family lots are proposed along Smotherman Lane along the west boundary of the site. The remaining 41 lots are proposed to consist entirely of two-family residences. Staff recommends the plan be limited to 15 two family lots, which would account for 33% of the lots, and disperse the two-family residences to intersections and larger lots. This ratio of single- and two-family lots is consistent with the Bridgewater subdivision on John Hagar Road to the north.

The two-family lots take access from the existing Meadows of Seven Points subdivision to the north. Future connections are planned to the east and south. Staff recommends a future connection from the northeast corner of the site, connecting to Smotherman Lane along the west boundary, in order to accommodate the Hessey Road extension identified in the Major and Collector Street Plan.

The single-family lots are proposed to access onto Smotherman Lane where it currently dead ends in the site. As proposed, a variance to the Subdivision Regulations would be required for the single-family lots as the length of Smotherman Lane exceeds the maximum 750 foot length of dead end streets with turnarounds. A variance would not be required with staff's condition to provide a direct connection from Seven Points Circle to Smotherman Lane.



Metro Planning Commission Meeting of 07/23/2015

ANALYSIS

The proposal groups a single type of housing type rather than providing an appropriate, thorough mix of housing. Staff recommends the applicant reduce the number of two-family lots and disperse them throughout the development at intersections and on larger lots.

Additionally, the applicant has not provided adequate connectivity to establish multiple routes for residents and reduce congestion on primary roads. Staff finds a direct connection from Seven Points Circle to Smotherman Lane would meet the intent of the T3 NE policy of providing a higher level of connectivity. The existing width of Smotherman Lane varies and improvements may be required if a connection is made.

FIRE MARSHAL’S OFFICE

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Approved

WATER SERVICES

Approve with conditions

- Approved as a Preliminary SP only.
- FYI - significant public water and sewer construction plan work will be required for the Final SP. This work must be approved prior to Final SP approval.
- The required capacity fees must also be paid prior to Final SP approval.

PUBLIC WORKS RECOMMENDATION

No exceptions taken

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.

TRAFFIC AND PARKING RECOMMENDATION

Conditions if approved

- Comply with road comments.
- Any future subdivision connected to this subdivision and accessed from Earhart Rd and S New Hope Rd may require a traffic access study.

Maximum Uses in Existing Zoning District: **RS15**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	32.06	2.90 D	92 U	963	75	100



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Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	32.06	-	86 U	906	70	94

Traffic changes between maximum: **RS15** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	- 6 U	-57	-5	-6

METRO SCHOOL BOARD REPORT

Projected student generation existing RS15 district: 9 Elementary 4 Middle 4 High

Projected student generation proposed SP-R district: 30 Elementary 16 Middle 14 High

The proposed SP-R zoning district would generate 43 additional students than what is typically generated under the existing RS15 zoning district. Students would attend Ruby Major Elementary School, Donelson Middle School, and McGavock High School. Ruby Major Elementary School and Donelson Middle School have been identified as over capacity; however, there is capacity within the cluster for elementary and middle school students. McGavock High School is also identified as over capacity; however, there is capacity in adjacent clusters. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends approval with conditions and disapproval without all conditions.

CONDITIONS (if approved)

1. Uses within the SP shall be limited to a maximum of 60 residential units.
2. Two-family lots shall be dispersed throughout the site and primarily on corner lots.
3. All garages shall be side or rear loaded, consistent with the images on the plan.
4. The development plan shall include a direct connection from Seven Points Circle to Smotherman Lane.
5. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM4 zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
6. The following design standards shall be added to the plan:
 - a. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.
 - c. EIFS, vinyl siding and untreated wood shall be prohibited.
 - d. Porches shall provide a minimum of six feet of depth.
 - e. A raised foundation of 18”- 36” is required for all residential structures.
7. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.



Metro Planning Commission Meeting of 07/23/2015

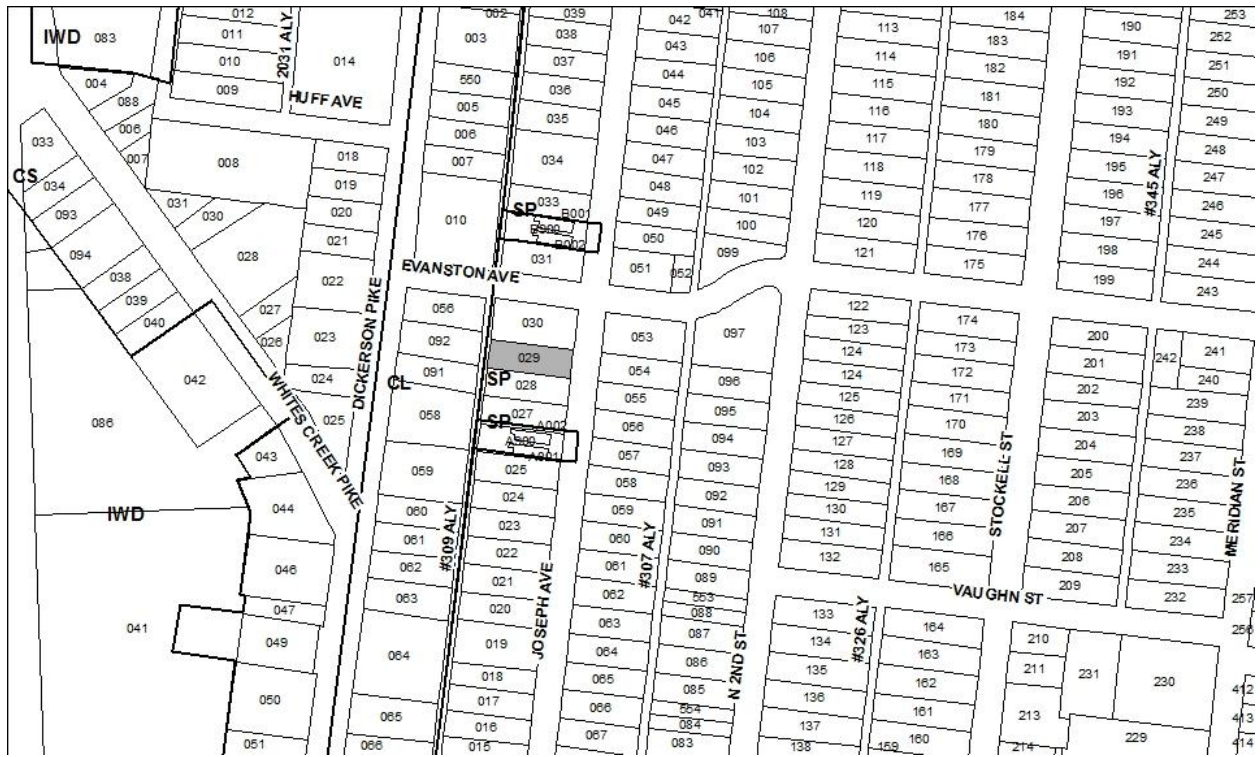
8. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
9. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
10. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



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Metro Planning Commission Meeting of 07/23/2015



2015SP-076-001
1014 JOSEPH AVENUE SP
Map 082-03, Parcel(s) 029
05, East Nashville
05 (Scott Davis)



Project No. 2015SP-076-001
Project Name 1014 Joseph Avenue SP
Council District 5 – Davis
School District 5 – Kim
Requested by Duke & Duke, LLC, applicant and owner.

Staff Reviewer Sajid
Staff Recommendation *Disapprove.*

APPLICANT REQUEST

Permit up to two detached residential units.

Preliminary SP

A request to rezone from Specific Plan-Residential (SP-R) to Specific Plan-Residential (SP-R) zoning for property located at 1014 Joseph Avenue, approximately 90 feet south of Evanston Avenue, (0.16 acres), to permit up to 2 detached residential units.

Existing Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

CRITICAL PLANNING GOALS

N/A

EAST NASHVILLE COMMUNITY PLAN

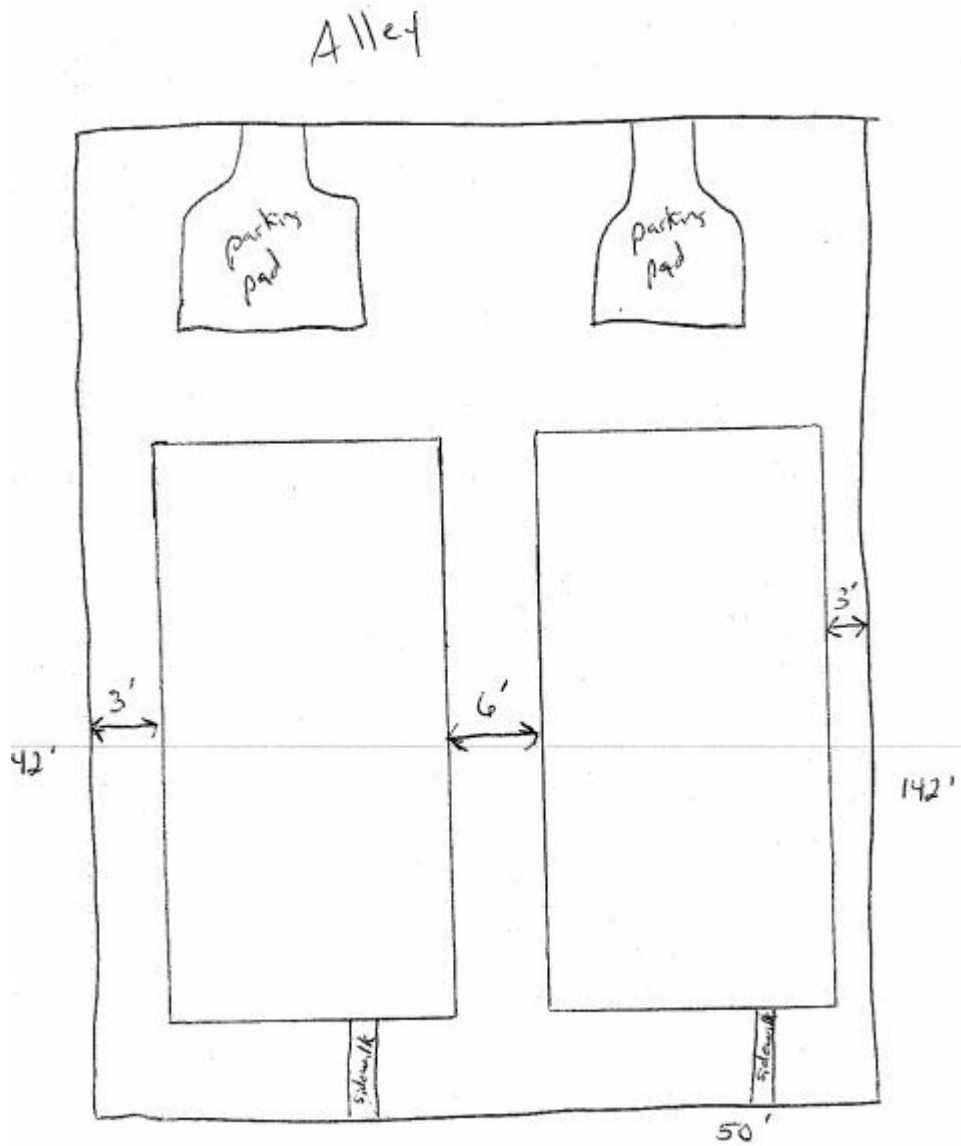
Urban Neighborhood Maintenance (T4 NM) is intended to preserve the general character of existing urban residential neighborhoods. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T4 NM areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Consistent with Policy?

No. The proposed SP is not consistent with the Urban Neighborhood Maintenance policy. Policy was previously Urban Community Center. Through the NashvilleNext process, the neighborhood requested that an Urban Neighborhood Maintenance policy be applied to this area to preserve the character of the existing neighborhood. The SP proposes a detached duplex, and while there are some existing duplexes scattered throughout the neighborhood, the predominant use in the neighborhood is single-family residential. An attached two-family structure may be appropriate at this location if the plan incorporates contextually appropriate design so that it is consistent with the character of the neighborhood.



Metro Planning Commission Meeting of 07/23/2015



1014 Joseph

Proposed Site Plan



Metro Planning Commission Meeting of 07/23/2015

PLAN DETAILS

The site is located at 1014 Joseph Avenue, south of Evanston Avenue. Access to the property is from the existing improved alley that abuts the site to the east.

Site Plan

The plan proposes 2 detached residential units fronting Joseph Avenue. Access appears to be limited to the alley to the east, and parking pads are provided behind the units. The site is served by existing sidewalks along Joseph Avenue and proposes sidewalk connections from the units to the public sidewalk.

The plan as submitted is not to scale so staff cannot determine if the proposed street setback is contextual or if the proposed parking pads are adequate to meet the parking required by the Zoning Code. The plan does not include information addressing building height, landscaping or design standards. Additionally, the proposed plan does not include all elements of the development plan as required by Section 17.40-106.B of the Zoning Code.

ANALYSIS

The SP proposes a detached duplex on an interior lot which is not consistent with the form and rhythm of the predominantly single-family residential character of the neighborhood and therefore, is not consistent with the Urban Neighborhood Maintenance policy. An attached two-family structure may be appropriate at this location if the plan incorporates contextually appropriate design that resembles the form and scale that resembles the existing character of the neighborhood. In addition, the SP as submitted is incomplete and does not have recommendations of approval from Stormwater, Public Works, and Traffic & Parking.

FIRE MARSHAL RECOMMENDATION

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Returned

- Drawing should be a CADD drawing that is to scale.
- Add Preliminary Note.
- Show existing topo.
- Add FEMA note and information.
- Show North Arrow and Bearing information.
- Provide a Vicinity Map.
- Proposed site layout.
- Add 78-840 note to plans
- Add Preliminary Note.
- Add Access Note.
- Add C/D Note.
- Provide water quality concept.
- Provide room for detention (if necessary).

TRAFFIC & PARKING RECOMMENDATION



Metro Planning Commission Meeting of 07/23/2015

Returned

- Submit dimensioned site plan and alley name.

WATER SERVICES RECOMMENDATION

Approve with conditions

- Approved as a Preliminary SP only, on the condition all four private service lines (one water and one sewer for each dwelling) tie directly to a public line from the dwelling it serves (no shared private water and sewer lines). The required capacity fees must be paid prior to Final SP approval.

PUBLIC WORKS RECOMMENDATION

Returned

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Comply with MPW Traffic Engineer
- Submit a dimensioned site plan and include, at a minimum, the following: existing sidewalks, curb location, existing edge of pavement, existing width of Joseph Ave pavement and ROW, alley ROW width and pavement width, etc.
- The proposed parking pads on the alley are to be a minimum of 24' from the opposite ROW line.

No traffic table was prepared for this case, as it is not expected to generate significant additional traffic.

SCHOOL BOARD REPORT

Projected student generation existing SP-R district: 0 Elementary 0 Middle 0 High

Projected student generation proposed SP-R district: 0 Elementary 0 Middle 0 High

The proposed SP-R zoning district would not generate any more students than what is typically generated under the existing SP-R zoning district. Students would attend Glenn Elementary School, Jere Baxter Middle School, and Maplewood High School. All three schools have been identified as having additional capacity. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends disapproval of the SP as it is not consistent with the Urban Neighborhood Maintenance land use policy and does not have recommendations of approval from all reviewing agencies.

CONDITIONS (if approved)

1. Uses within the SP shall be limited to two attached residential units.
2. The following design standards shall be added to the plan: (For preliminary SPs)
 - a. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.
 - c. EIFS, vinyl siding and untreated wood shall be prohibited.
 - d. Porches shall provide a minimum of six feet of depth.



Metro Planning Commission Meeting of 07/23/2015

- e. A raised foundation of 18"- 36" is required for all residential structures.
3. Building height shall be limited to two stories in 35'.
4. Vehicular access for all units shall be limited to the alley.
5. The final site plan shall include a contextual street setback.
6. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
7. For any development standards, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations and requirements of the R6 zoning district as of the date of the applicable request or application.
8. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
9. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
10. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



Metro Planning Commission Meeting of 07/23/2015



2015SP-077-001
1436 LISCHEY AVENUE SP
Map 071-11, Parcel(s) 282
05, East Nashville
05 (Scott Davis)



Project No. 2015SP-077-001
Project Name 1436 Lischey SP
Council Bill BL2015-1302
Council District 5-Davis
School District 5-Kim
Requested by Bryan D. Spicer, applicant and owner.

Staff Reviewer Milligan
Staff Recommendation *Disapprove.*

APPLICANT REQUEST
Zone change from RS5 to SP

Zone Change

A request to rezone from Single-Family Residential (RS5) to Specific Plan-Residential (SP-R) zoning for property located at 1436 Lischey Avenue, approximately 144 feet south of Gatewood Avenue, (0.37 acres) to permit up to three residential units.

Existing Zoning

Single-Family Residential (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre. *RS5 would permit a maximum of 3 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

CRITICAL PLANNING GOALS

N/A

EAST NASHVILLE COMMUNITY PLAN

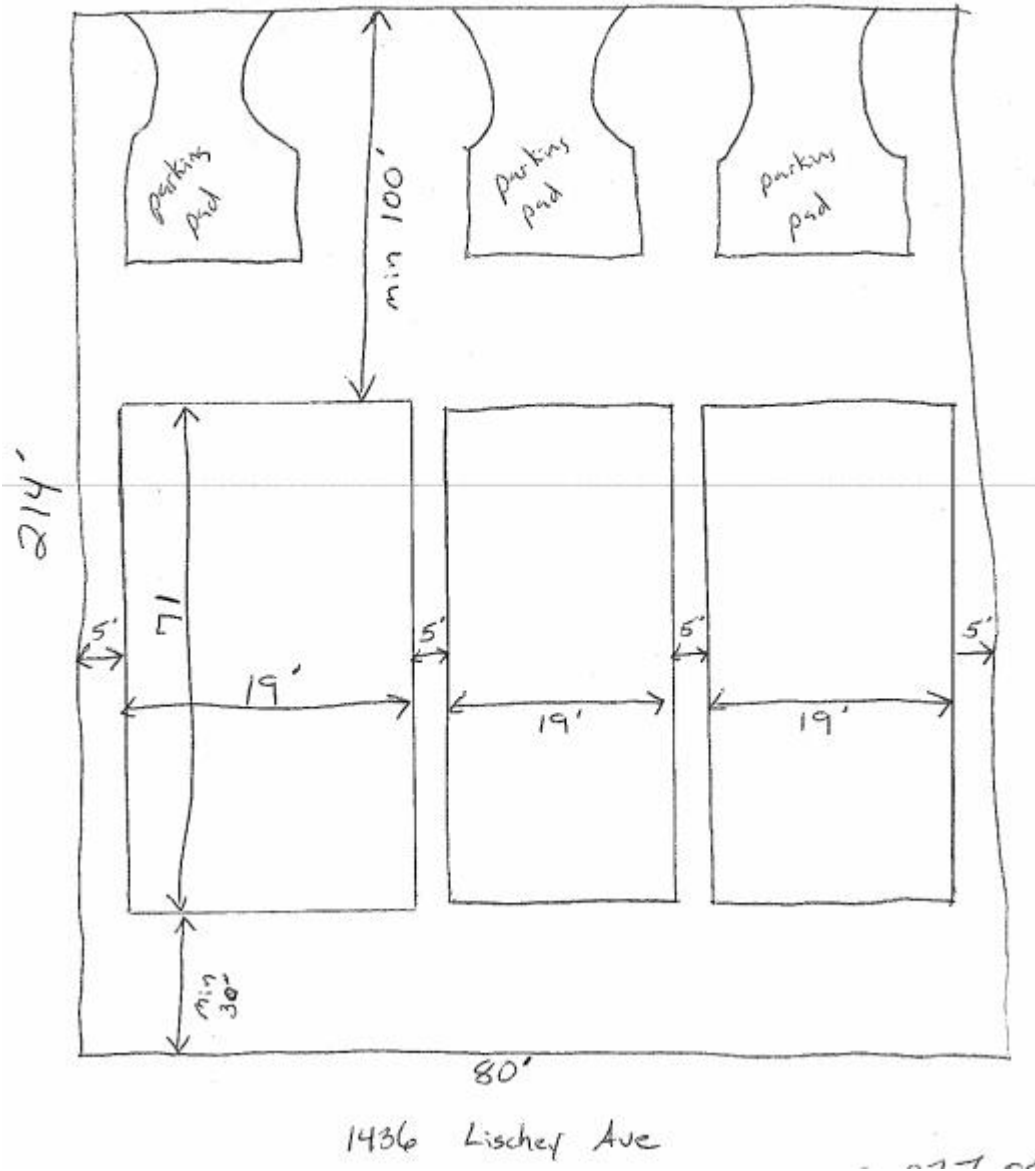
T4 Urban Neighborhood Maintenance (T4 NM) is intended to preserve the general character of existing urban residential neighborhoods. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T4 NM areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.



Metro Planning Commission Meeting of 07/23/2015

Alley Access



Proposed Site Plan



Metro Planning Commission Meeting of 07/23/2015

Consistent with Policy?

No. The rezoning request to allow for 3 residential units is inconsistent with the T4 Urban Neighborhood Maintenance policy. While some change would be expected within the Neighborhood Maintenance area, the change should be sensitive to the existing neighborhood character. There is also an area of Conservation Policy along the front of the property due to a stream that bisects the property. The plan does not take this feature into consideration in regards to building placement or possible needed remediation.

ANALYSIS

The property at 1436 Lischey Avenue is currently zoned RS5 which allows for single-family residential uses. All surrounding property is also zoned RS5. There is a mixture of single-family detached units and duplexes within the immediate area. The duplex units are small, single-story structures that appear more as single-family units. The spacing of units along the street is uniform and the introduction of 3 units on 1 lot will be inconsistent with the current pattern along the street. The applicant has proposed no standards in regards to building type, height, or architectural style.

FIRE MARSHAL'S OFFICE RECOMMENDATION

Approved with conditions

- Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Returned for corrections

- Drawing should be a CADD drawing that is to scale.
- Add Preliminary Note.
- Show existing topo.
- Add FEMA note and information.
- Show undisturbed buffers (or provide variance).
- Add Buffer Note to plans.
- Show North Arrow and Bearing information.
- Provide a Vicinity Map.
- Proposed site layout.
- Add 78-840 note to plans
- Add Preliminary Note.
- Add Access Note.
- Add C/D Note.
- Provide water quality concept.
- Provide room for detention (if necessary).

PUBLIC WORKS RECOMMENDATION

Returned for corrections

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Comply with MPW Traffic Engineer



Metro Planning Commission Meeting of 07/23/2015

- Submit a dimensioned site plan and include, at a minimum, the following: existing sidewalks, curb location, existing edge of pavement, existing width of Lischey Ave pavement and ROW, alley ROW width and pavement width, etc.
- The proposed parking pads on the alley are to be a minimum of 24' from the opposite ROW line.
- Depending on the width of Lischey Ave, applicant may be required to submit to Traffic and parking Commission to restrict parking along property frontage

TRAFFIC AND PARKING RECOMMENDATION

Returned for corrections

- Submit dimensioned site plan with alley name.
- Provide parking per code.

Maximum Uses in Existing Zoning District: **RS5**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.37	8.71	3 U	29	3	4

Maximum Uses in Proposed Zoning District: **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (210)	0.37	-	3 U	29	3	4

Traffic changes between maximum: **RS5** and **SP-R**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	-	-	-

STAFF RECOMMENDATION

Staff recommends disapproval as the rezoning is inconsistent with the policy for the area and a recommendation of approval has not been received from all reviewing agencies. Additionally, no standards have been provided in regards to the character of the units.

CONDITIONS (if approved)

1. Uses shall be limited to up to two residential units.
2. Add Case No. 2015SP-077-001 to the plan.
3. Provide a scaled site plan.
4. Indicate on the plan that access shall be from the alley only.
5. Provide the right-of-way dimensions of Lischey Avenue and the existing alley. Right-of-way dedications may be necessary.
6. The following design standards shall be added to the plan:
 - a. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - b. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.



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- c. EIFS and vinyl siding shall be prohibited.
- d. Porches shall provide a minimum of six feet of depth.
- e. A raised foundation of 18" - 36" is required for all residential structures.
- f. Setbacks shall be contextual and consistent with surrounding properties.
7. The Preliminary SP plan is the site plan and associated documents. If applicable, remove all notes and references that indicate that the site plan is illustrative, conceptual, etc.
8. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM9-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.
9. A corrected copy of the preliminary SP plan incorporating the conditions of approval by Metro Council shall be provided to the Planning Department prior to or with final site plan application.
10. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
11. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



Metro Planning Commission Meeting of 07/23/2015



2015SP-078-001
HARVEST HILLS HOMES SP
Map 105-04, Parcel(s) 044-046
11, South Nashville
17 (Sandra Moore)



Project No.	2015SP-078-001
Project Name	Harvest Hills Homes SP
Council District	17 - Moore
School District	5 - Kim
Requested by	FMBC Investments, LLC, applicant; Harvest Hands Community Development Corporation, owner.
Staff Reviewer	Thomas
Staff Recommendation	<i>Defer to the August 13, 2015, Planning Commission meeting.</i>

APPLICANT REQUEST

Preliminary SP to permit up to 10 dwelling units.

Preliminary SP

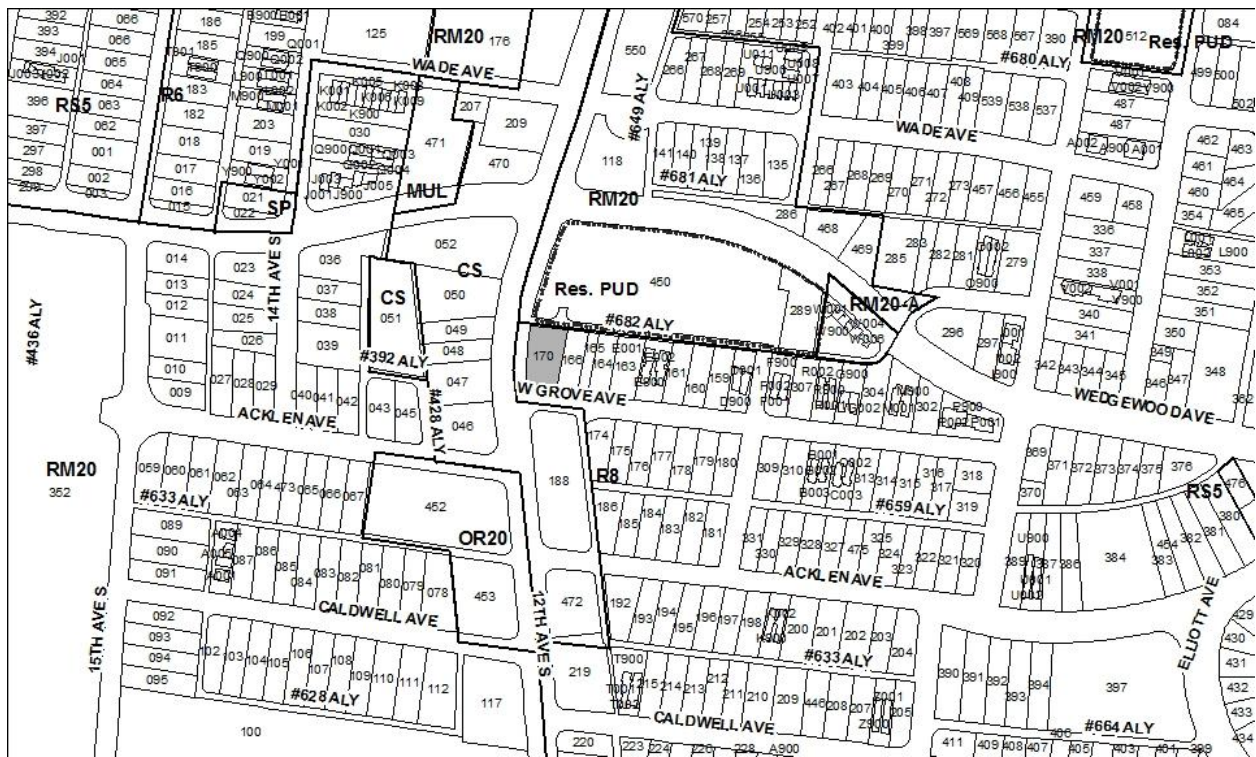
A request to rezone from One and Two-Family Residential (R6) to Specific Plan-Residential (SP-R) zoning for property located at 20, 22, and 24 N. Hill Street, approximately 100 feet west of Lincoln Street (0.54 acres), to permit up to 10 residential units.

STAFF RECOMMENDATION

Staff recommends deferral to the August 13, 2015, Planning Commission meeting at the request of the applicant.



Metro Planning Commission Meeting of 07/23/2015



2015SP-079-001
1820 12TH AVENUE SOUTH SP
Map 105-09, Parcel(s) 170
10, Green Hills - Midtown
17 (Sandra Moore)



Project No.	2015SP-079-001
Project Name	1820 12th Avenue South SP
Council District	17 - Moore
School District	8 - Pierce
Requested by	FMBC Investments, LLC, applicant and owner.
Staff Reviewer	Thomas
Staff Recommendation	<i>Defer to the August 13, 2015, Planning Commission meeting.</i>

APPLICANT REQUEST

Preliminary SP to permit a mixed-use development with up to 6 residential units.

Preliminary SP

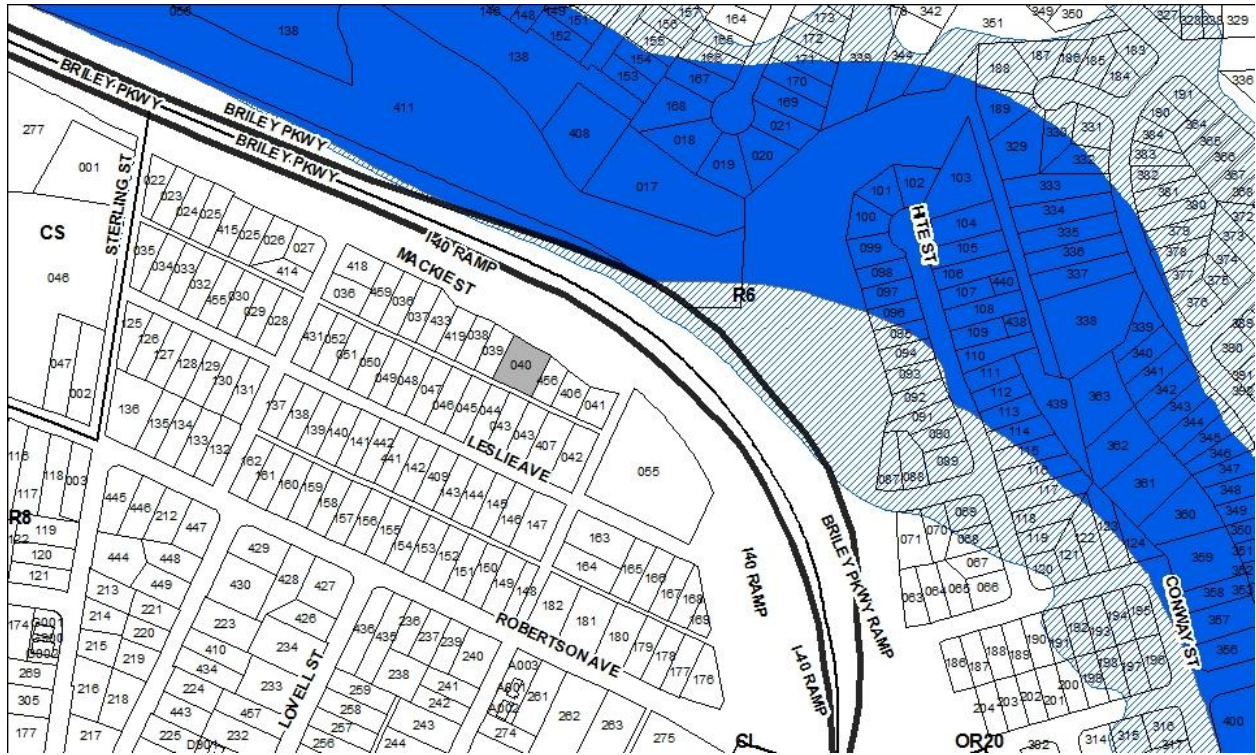
A request to rezone from One and Two-Family Residential (R8) to Specific Plan-Mixed Use (SP-MU) zoning for property located at 1820 12th Avenue S, on the northeast corner of 12th Avenue S and W Grove Avenue (0.22 acres), to permit a mixed-use development with up to 6 residential units and 800 square feet of general office.

STAFF RECOMMENDATION

Staff recommends deferral to the August 13, 2015, Planning Commission meeting at the request of the applicant.



Metro Planning Commission Meeting of 07/23/2015



2015SP-082-001

5807 MACKIE PLACE SP
Map 091-10, Parcel(s) 040
07, West Nashville
20 (Buddy Baker)



Project No.	2015SP-082-001
Project Name	5807 Mackie Place SP
Council District	20-Baker
School District	9-Frogge
Requested by	John W. Golden, applicant and owner.
Staff Reviewer	Milligan
Staff Recommendation	<i>Approve with conditions and disapprove without all conditions.</i>

APPLICANT REQUEST
Zone change from R8 to SP

Zone Change

A request to rezone from One and Two-Family Residential (R8) to Specific Plan-Residential (SP-R) zoning for property located at 5807 Mackie Place, approximately 465 feet east of Ethel Street (0.27 acres), to permit up to 3 residential units.

Existing Zoning

One and Two-Family Residential (R8) requires a minimum 8,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 5.79 dwelling units per acre including 25 percent duplex lots. *R8 would permit a maximum of 1 lot with 1 duplex lot for a total of 2 units.*

Proposed Zoning

Specific Plan-Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type.

CRITICAL PLANNING GOALS

- Supports Infill Development

This proposal meets one critical planning goal. Development in areas with adequate infrastructure is more appropriate than development not served with adequate infrastructure such as roads, water and sewer, because it does not burden Metro with the cost of maintaining new infrastructure. The project will intensify development on an infill site.

WEST NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Evolving (T4 NE) is intended to create and enhance urban residential neighborhoods that provide more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate to high density development patterns with shallow setbacks and minimal spacing between buildings. T4 NE areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. T4 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to



Metro Planning Commission Meeting of 07/23/2015

5807 Mackie Place SP

Development Summary	
SP Name	5807 Mackie Place Specific Plan
SP Number	2015SP-082-001
Council District	20
Applicant	Councilmember Buddy Baker

Site Data Table	
Site Data	0.27 ac
Existing Zoning	R8
Proposed Zoning	SP
Allowable Land Uses	Single-Family; Two-Family; Multi-family

The purpose of this SP zoning district is to provide development standards for a single, two-family, or multi-family residential structure.

1. Permitted uses shall be limited to up to 3 residential units.

If the existing structure is demolished, the following standards shall apply to any newly constructed residential units:

1. Permitted uses shall be limited to up to 3 residential units.
2. A Final SP Plan must be submitted and approved.
3. Height shall be limited to 2 stories in 35 feet.
4. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
5. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.
6. EIFS and vinyl siding shall be prohibited.
7. Porches shall provide a minimum of six feet of depth.
8. A raised foundation of 18"- 36" is required for all residential structures.
9. Access shall be limited to 1 driveway and parking shall be located behind the residential structures.
10. Sidewalks will be required along Mackie Place with redevelopment.

Proposed Site Standards



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take into account considerations such as timing and some elements of the existing developed character, such as the street network and block structure and proximity to centers and corridors.

Consistent with Policy?

Yes. The proposed SP is consistent with the T4 Urban Neighborhood Evolving policy in that it is providing additional housing options within an existing urban neighborhood. The plan proposes to utilize the existing structure, maintaining the character of the neighborhood.

PLAN DETAILS

The site is located at 5807 Mackie Place, on the north side of Mackie Place, south of I-40. The site is approximately 0.27 acres in size and is currently in use as a duplex.

Site Plan

The SP consists of a regulatory document that will regulate any future development on the site. The SP is intended to permit up to three residential units on the property. The current proposal is to utilize the existing structure. The structure was at one time utilized as a triplex but was converted to a duplex. The applicant now wishes to change it back to a triplex. The SP allows for the use of the existing structure but includes specific guidelines should the property be redeveloped. The standards are as follows:

If the existing structure is demolished, the following standards shall apply to any newly constructed residential units:

1. Permitted uses shall be limited to up to 3 residential units.
2. A Final SP Plan must be submitted and approved.
3. Height shall be limited to 2 stories in 35 feet.
4. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
5. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.
6. EIFS and vinyl siding shall be prohibited.
7. Porches shall provide a minimum of six feet of depth.
8. A raised foundation of 18"- 36" is required for all residential structures.
9. Access shall be limited to 1 driveway and parking shall be located behind the residential structures.
10. Sidewalks will be required along Mackie Place with redevelopment.

ANALYSIS

This SP would permit residential development consistent with the land use policy. The SP also supports infill development which is a critical planning goal.

FIRE MARSHAL'S OFFICE RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

N/A



Metro Planning Commission Meeting of 07/23/2015

TRAFFIC AND PARKING RECOMMENDATION

Conditions if approved

- Traffic study may be required at time of development.

Maximum Uses in Existing Zoning District: **R8**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (210)	0.27	5.44 D	2 U*	20	2	3

*Based on one two-family lot.

Maximum Uses in Proposed Zoning District: **SP**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (210)	0.27	15 D	4 U	39	3	5

Traffic changes between maximum: **R8** and **SP**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	+ 2 U	+19	+1	+2

STAFF RECOMMENDATION

Staff recommends approval as the rezoning is consistent with the policy for the area and provides for additional housing within an existing urban neighborhood.

CONDITIONS

1. Uses shall be limited to up to three residential units.
2. The following standard shall apply if the property is redeveloped:
 - a. A Final SP Plan must be submitted and approved.
 - b. Height shall be limited to 2 stories in 35 feet.
 - c. Building façades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 25% glazing.
 - d. Windows shall be vertically oriented at a ratio of 2:1 or greater, except for dormers.
 - e. EIFS and vinyl siding shall be prohibited.
 - f. Porches shall provide a minimum of six feet of depth.
 - g. A raised foundation of 18"- 36" is required for all residential structures.
 - h. Access shall be limited to 1 driveway and parking shall be located behind the residential structures.
3. Sidewalks will be required along Mackie Place with redevelopment.
4. If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the RM9-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.

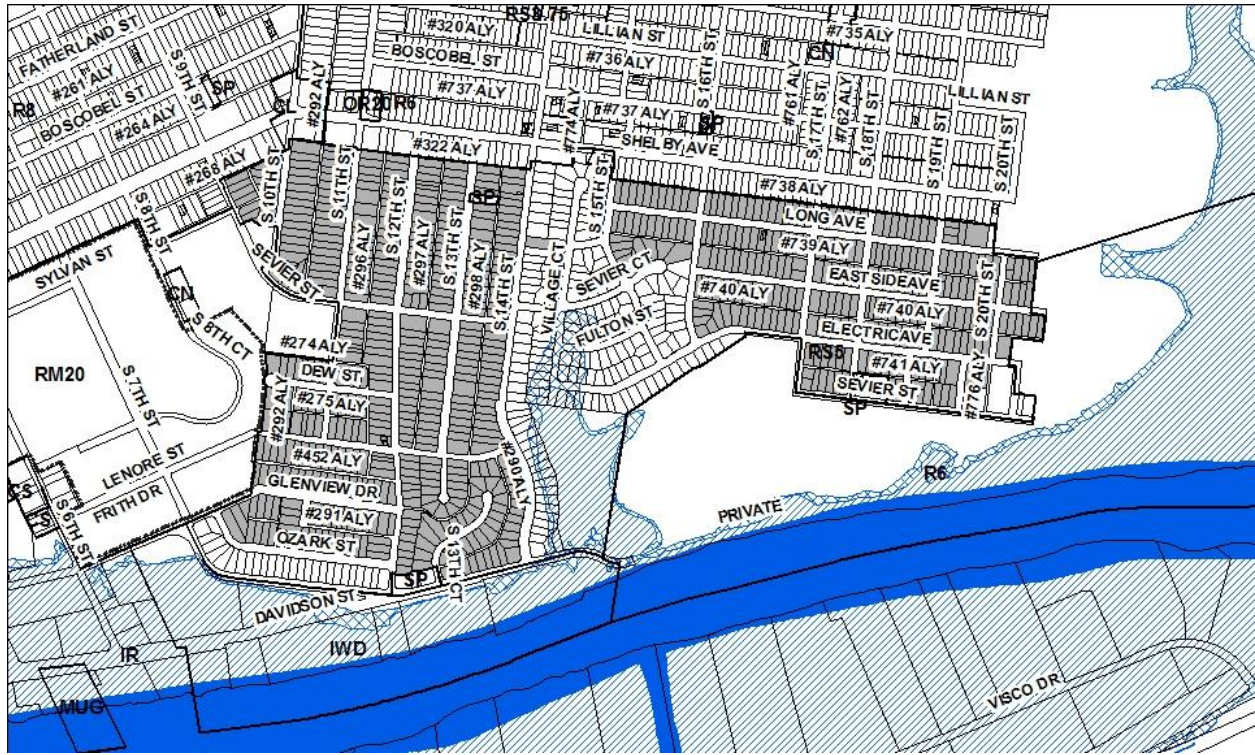


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5. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.
6. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.



Metro Planning Commission Meeting of 07/23/2015



2015Z-044PR-001

Map Various, Parcels Various

05, East Nashville

06 (Peter Westerholm)



Project No.	2015Z-044PR-001
Council Bill	BL2015-1155
Council District	6 – Westerholm
School District	5 - Kim
Requested by	Councilmember Peter Westerholm, applicant; various property owners
Staff Reviewer	Milligan
Staff Recommendation	<i>Approve.</i>

APPLICANT REQUEST

Apply a Detached Accessory Dwelling Unit (DADU) overlay.

Zone Change

A request to apply the Detached Accessory Dwelling Unit (DADU) overlay to various properties located along Davidson Street, Dew Street, Eastside Avenue, Electric Avenue, Glenview Drive, Lenore Street, Long Avenue, Ozark Street, S 9th Street, S 10th Street, S 11th Street, S 12th Street, S 13th Court, S 13th Street, S 14th Street, S 15th Street, S 16th Street, S 17th Street, S 18th Street, S 19th Street, S 20th Street, Sevier Court, and Sevier Street (approximately 129 acres).

Existing Zoning

Single-Family Residential (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre.

Proposed Zoning

Detached Accessory Dwelling Unit (DADU) Overlay provides additional housing options.

HISTORY

The Planning Commission recommended that Council disapprove the request at the June 25, 2015, Planning Commission meeting to provide additional time for discussions with neighbors in regards to the details of the request and what would be allowed. The request was approved by Council on second reading on July 7, 2015, and referred by to the Commission by Council. There has been no change to the request.

CRITICAL PLANNING GOALS

N/A

EAST NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Maintenance (T4 NM) is intended to preserve the general character of existing urban residential neighborhoods. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T4 NM areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

T4 Urban Neighborhood Evolving (T4 NE) is intended to create and enhance urban residential neighborhoods that provide more housing choices, improved pedestrian, bicycle and vehicular



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connectivity, and moderate to high density development patterns with shallow setbacks and minimal spacing between buildings. T4 NE areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. T4 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network and block structure and proximity to centers and corridors.

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.

Consistent with Policy?

Yes. The proposed Detached Accessory Dwelling Unit (DADU) Overlay is consistent with the policies for the area. The overlay provides for an additional housing option while maintaining the existing character of the area. The design standards that are incorporated into the overlay ensure proper placement, design, and access to the units.

ANALYSIS

The Metro Council is currently considering legislation to establish the Detached Accessory Dwelling Unit Overlay District. On July 7, 2015, the Metro Council approved the text amendment on 2nd reading. The Detached Accessory Dwelling Unit Overlay District provides additional housing options within Davidson County. The proposed text amendment would allow DADUs as an accessory use in areas where the overlay is applied, with the same standards that currently apply to DADUs. This application is proposing to apply a Detached Accessory Dwelling Unit Overlay in the Shelby Hills area.

As per the existing standards, in order for a lot to be eligible for a detached accessory dwelling unit, it must first meet the lot size standards of the base zoning district, in this case RS5 which requires a minimum lot size of 5,000 square feet. Additionally the lot must:

- Be located within a historic overlay district; OR
- Be located within a Urban Design Overlay with standards for DADUs; OR
- Have an improved alley abutting the rear or side lot line; OR
- Exceed 15,000 square feet in size.

The Detached Accessory Dwelling Unit Overlay outlines standards for DADUs including specifications for ownership, location, driveway access, bulk, massing, as well as design standards. The Design Standards are established in the zoning ordinance and are not established by the application of an overlay to a specific area. The Design Standards as established cannot be modified.



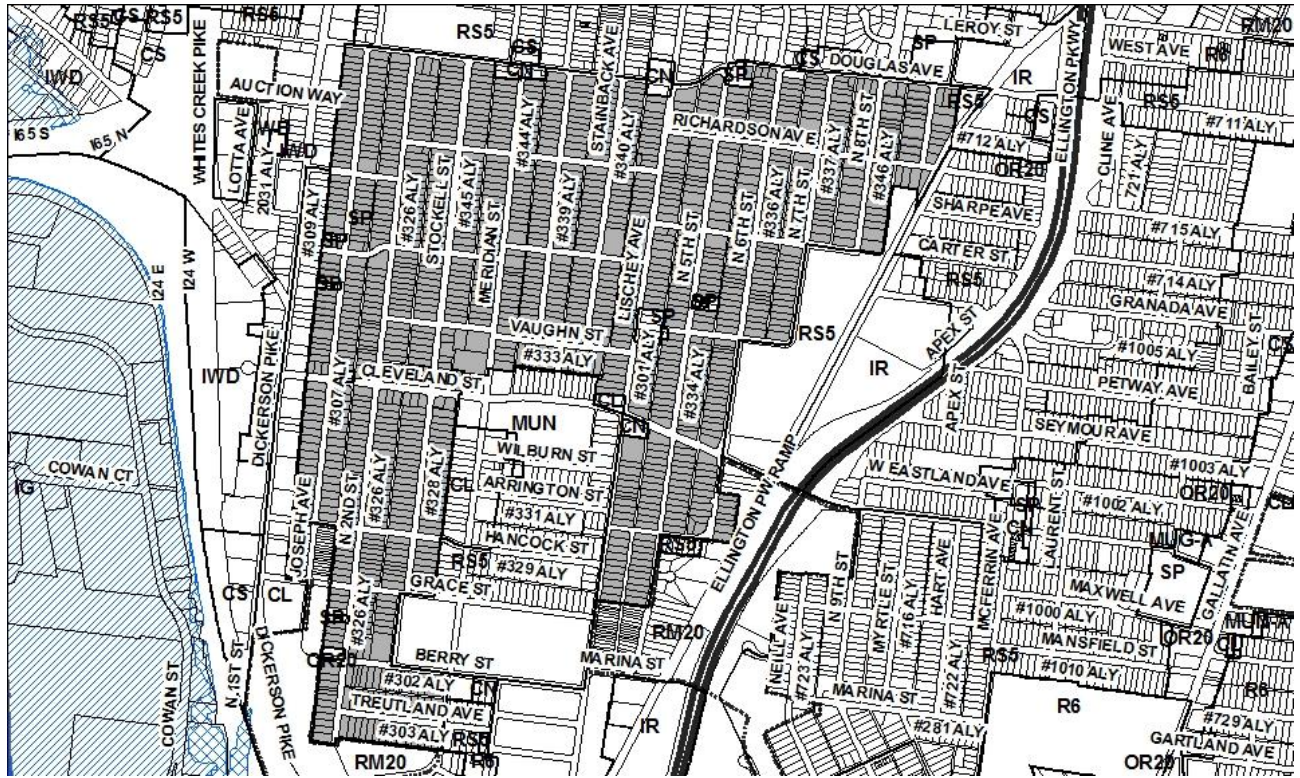
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STAFF RECOMMENDATION

Staff recommends approval as the establishment of a detached accessory dwelling unit overlay is consistent with the policy for the area and allows for the introduction of an additional dwelling unit type while maintaining the existing character of the community.



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2015Z-053PR-001
Map Various, Parcels Various
05, East Nashville
05 (Scott Davis)



Project No.	Zone Change 2015Z-053PR--001
Council District	5 - Davis
School District	5 - Kim
Requested by	Councilman Scott Davis, applicant; various property owners.
Staff Reviewer	Milligan
Staff Recommendation	<i>Approve.</i>

APPLICANT REQUEST

Zone change from SP to RS5.

Zone Change

A request to rezone from Specific Plan – Residential (SP-R) to Single-Family Residential (RS5) zoning for various properties located along Arrington Street, Berry Street, Cleveland Street, Douglas Avenue, Evanston Avenue, Joseph Street, Lischey Avenue, Meridian Street, Montgomery Avenue, N. 2nd Street, N. 5th Street, N. 6th Street, N. 7th Street, N. 8th Street, Pennock Avenue, Richardson Avenue, Stainback Avenue, Stockell Street, Treutland Avenue, Vaughn Street and Vernon Winfrey Avenue, south of Douglas Avenue (approximately 238 acres).

Existing Zoning

Specific Plan – Residential (SP-R) is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes only one residential building type. The approved SP allows for detached accessory dwelling units in addition to uses allowed within RS5.

Proposed Zoning

Single-Family Residential (RS5) - requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre.

CRITICAL PLANNING GOALS

N/A

EAST NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Maintenance (T4 NM) is intended to preserve the general character of existing urban residential neighborhoods. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T4 NM areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

T4 Urban Neighborhood Evolving (T4 NE) is intended to create and enhance urban residential neighborhoods that provide more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate to high density development patterns with shallow setbacks and minimal spacing between buildings. T4 NE areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. T4 NE policy may be



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applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network and block structure and proximity to centers and corridors.

T4 Urban Neighborhood Center (T4 NC) is intended to preserve, enhance, and create urban neighborhood centers that serve urban neighborhoods that are generally within a 5 minute walk. T4 NC areas are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, residential, and institutional land uses. Infrastructure and transportation networks may be enhanced to improve pedestrian, bicycle and vehicular connectivity.

Civic (CI) is intended to serve two purposes. The primary intent of CI is to preserve and enhance publicly owned civic properties so that they can continue to serve public purposes over time, even if the specific purpose changes. This recognizes that locating sites for new public facilities will become more difficult as available sites become scarcer and more costly. The secondary intent of CI is to guide rezoning of sites for which it is ultimately determined that conveying the property in question to the private sector is in the best interest of the public.

Consistent with Policy?

Yes. The RS5 zoning is consistent with the policies for the area. There is a mix of policies in the area which creates a vibrant urban neighborhood with a mixture of uses. The RS5 allows for single-family detached homes on relatively small lots providing needed housing in an urban area.

ANALYSIS

In February 2015, these properties were rezoned from RS5 to SP (Specific Plan) at the request of Councilman Davis. The SP allowed for the construction of detached accessory dwelling units (DADUs) in addition to all uses allowed under RS5. Design Standards were included in the SP to ensure that DADUs would be compatible with the existing neighborhood character of single-family homes.

The Metro Council is currently considering legislation that would create a Detached Accessory Dwelling Unit Overlay (DADU). The overlay would allow for the construction of DADUs where it is applied, with the same restrictions that currently exist for DADUs.

Working with Planning staff, Councilman Davis has proposed to rezone the property back to its original RS5 and then, by separate ordinance, apply a DADU overlay. See the staff report for 2015Z-054PR-001 for additional information on the area proposed for the DADU overlay.

STAFF RECOMMENDATION

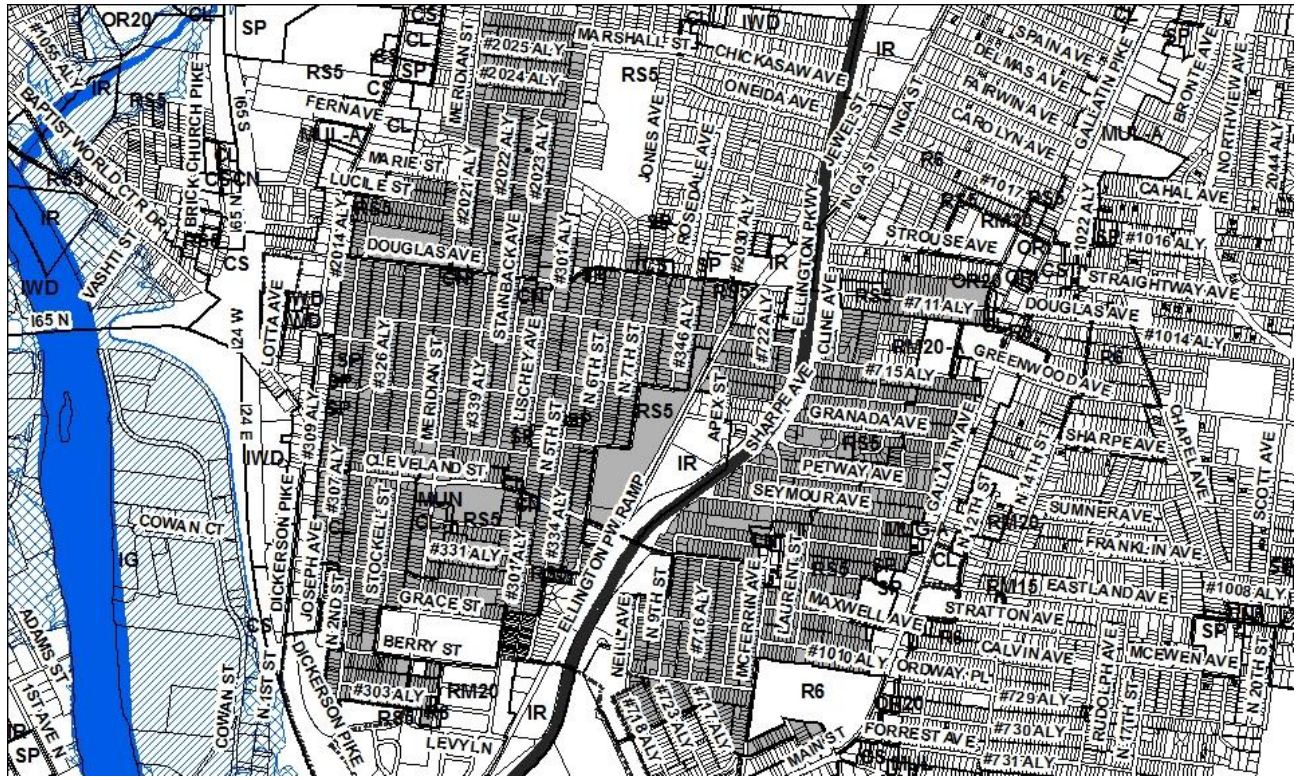
Staff recommends approval of the rezoning and is it consistent with the policies for the area.



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2015Z-054PR-001

Map Various, Parcels Various

05, East Nashville

05 (Scott Davis)



Project No. 2015Z-054PR-001
Council Bill BL2015-1272
Council District 5 - Davis
School District 5 - Kim
Requested by Councilmember Scott Davis, applicant; various property owners

Staff Reviewer Milligan
Staff Recommendation *Approve.*

APPLICANT REQUEST

Apply a Detached Accessory Dwelling Unit (DADU) overlay.

Zone Change

A request to apply the DADU overlay for various properties located along Arrington Street, Berry Street, Carter Street, Cleveland Street, Cline Avenue, Douglas Avenue, Eastmoreland Street, Edith Avenue, Emmett Avenue, Evanston Avenue, Foster Street, Gatewood Avenue, Grace Street, Granada Avenue, Hancock Street, Hart Avenue, Jones Avenue, Joseph Avenue, Laurent Street, Lischey Avenue, Lucile Street, Mansfield Street, Marie Street, Marina Street, Marshall Street, McFerrin Avenue, Meridian Street, Montgomery Avenue, Myrtle Street, N. 2nd Street, N. 3rd Street, N. 5th Street, N. 6th Street, N. 7th Street, N. 8th Street, N. 9th Street, Neill Avenue, Pennock Avenue, Pullen Avenue, Richardson Avenue, Sharpe Avenue, Smiley Street, Stainback Avenue, Stockell Street, Treutland Avenue, Vaughn Street, Vernon Winfrey Avenue, W. Greenwood Avenue, W. McKennie Avenue, and Wilburn Street, south of E. Trinity Lane and west of Gallatin Pike (approximately 627 acres).

Existing Zoning

Commercial Limited (CL) is intended for retail, consumer service, financial, restaurant, and office uses.

Commercial Neighborhood (CN) is intended for very low intensity retail, office, and consumer service uses which provide for the recurring shopping needs of nearby residential areas.

Commercial Service (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.

Mixed-Use Neighborhood (MUN) is intended for a low intensity mixture of residential, retail, and office uses.

Single-Family Residential (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre.

Proposed Zoning

Detached Accessory Dwelling Unit (DADU) Overlay provides additional housing options.

CRITICAL PLANNING GOALS

N/A



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EAST NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Maintenance (T4 NM) is intended to preserve the general character of existing urban residential neighborhoods. T4 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T4 NM areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

T4 Urban Neighborhood Evolving (T4 NE) is intended to create and enhance urban residential neighborhoods that provide more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate to high density development patterns with shallow setbacks and minimal spacing between buildings. T4 NE areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. T4 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network and block structure and proximity to centers and corridors.

T4 Urban Neighborhood Center (T4 NC) is intended to preserve, enhance, and create urban neighborhood centers that serve urban neighborhoods that are generally within a 5 minute walk. T4 NC areas are pedestrian friendly areas generally located at intersections of urban streets that contain commercial, mixed use, residential, and institutional land uses. Infrastructure and transportation networks may be enhanced to improve pedestrian, bicycle and vehicular connectivity.

D Major Institutional (D MI) is intended to preserve, enhance, and create Districts where major institutional uses are predominant and where their development and redevelopment occurs in a manner that complements the character of surrounding communities. Land uses include large institutions such as medical campuses, hospitals, and colleges and universities as well as uses that are ancillary to the principal use.

Civic (CI) is intended to serve two purposes. The primary intent of CI is to preserve and enhance publicly owned civic properties so that they can continue to serve public purposes over time, even if the specific purpose changes. This recognizes that locating sites for new public facilities will become more difficult as available sites become scarcer and more costly. The secondary intent of CI is to guide rezoning of sites for which it is ultimately determined that conveying the property in question to the private sector is in the best interest of the public.

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.



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Consistent with Policy?

Yes. The proposed Detached Accessory Dwelling Unit (DADU) Overlay is consistent with the policies for the area. The overlay provides for an additional housing option while maintaining the existing character of the area. The design standards that are incorporated into the overlay ensure proper placement, design, and access to the units.

ANALYSIS

The Metro Council is currently considering legislation to establish the Detached Accessory Dwelling Unit Overlay District. On July 7, 2015, the Metro Council approved the text amendment on 2nd reading. The Detached Accessory Dwelling Unit Overlay District provides additional housing options within Davidson County. The proposed text amendment would allow DADUs as an accessory use in areas where the overlay is applied, with the same standards that currently apply to DADUs. This application is proposing to apply a Detached Accessory Dwelling Unit Overlay in an area of East Nashville. A portion of the proposed overlay is within the Maxwell Heights Neighborhood Conservation District.

Per the existing standards, in order for a lot to be eligible for a detached accessory dwelling unit, it must first meet the lot size standards of the base zoning district, in this case RS5 which requires a minimum lot size of 5,000 square feet. Additionally the lot must:

- Be located within a historic overlay district; OR
- Be located within a Urban Design Overlay with standards for DADUs; OR
- Have an improved alley abutting the rear or side lot line; OR
- Exceed 15,000 square feet in size.

The Detached Accessory Dwelling Unit Overlay outlines standards for DADUs including specifications for ownership, location, driveway access, bulk, massing, as well as design standards. The Design Standards are established in the zoning ordinance and are not established by the application of an overlay to a specific area. The Design Standards as established cannot be modified.

EXISTING DESIGN STANDARDS

Site Requirements

- May only be located behind the principal structure

Driveway Access

- No alley – no more than 1 curb-cut from any public street
- Alley – Any additional access shall be from the alley and no new curb cut from public streets
- Parking accessed from any public street shall be limited to one driveway for the lot with a maximum width of 12 feet

Bulk and Massing

- Living space shall not exceed 700 square feet
- Footprint ranges from a maximum of 750 square feet (lots less than 10,000 sq ft) to 1,000 square feet (lots 10,000 sq ft and over)



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- DADU shall maintain a proportional mass, size, and height to ensure it is not taller than the principal structure on the lot. Height shall not exceed the height of the principal structure as measured to the eave line, with a maximum eave height of 10 feet for single-story and 17 feet for two-story
- The roof ridge line must be less than the primary structure and shall not exceed 27 feet in height.

Design Standards

- Shall be of similar style, design, and material color as used for the principal structure and shall use similar architectural characteristics, including roof form and pitch
- May have dormers that relate to the style and proportion of windows on the DADU and shall be subordinate to the roofslope by covering no more than 50% of the roof
- May have dormers that are setback a minimum of 2 feet from the exterior wall

Historic Properties

- Any properties within a historic district shall comply with the adopted standards of the applicable historic overlay

STAFF RECOMMENDATION

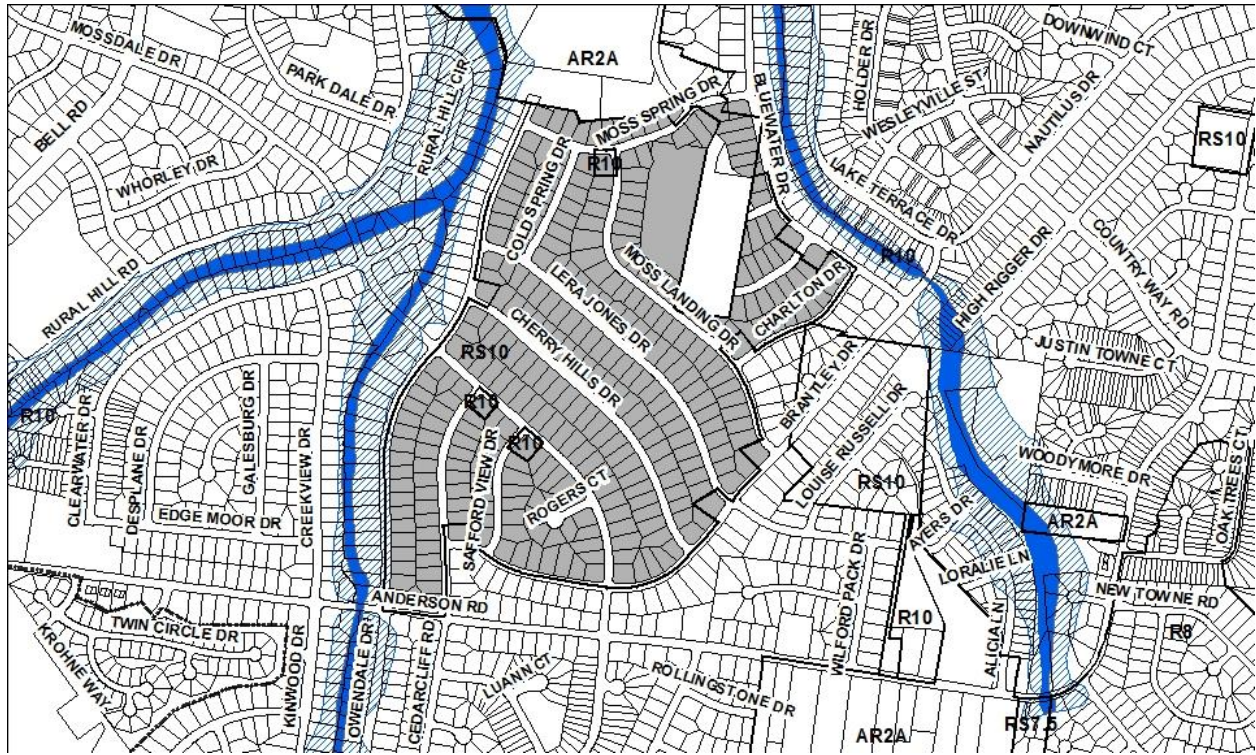
Staff recommends approval as the establishment of a detached accessory dwelling unit overlay is consistent with the policy for the area and allows for the introduction of an additional dwelling unit type while maintaining the existing character of the community.



SEE NEXT PAGE



Metro Planning Commission Meeting of 07/23/2015



2015Z-059PR-001

Map Various, Parcels Various

13, Antioch - Priest Lake

29 (Karen Y. Johnson)



Project No.	2015Z-059PR-001
Council Bill	BL2015-1269
Council District	29 - Johnson
School District	6 - Hunter 7 - Pinkston
Requested by	Councilmember Karen Johnson, applicant; various property owners.
Staff Reviewer	Milligan
Staff Recommendation	<i>Approve.</i>

APPLICANT REQUEST

Apply a Contextual Overlay District.

Zone Change

A request to apply the contextual overlay to various properties located along Anderson Road, Bluewater Drive, Bluewater Trace, Bluewater Way, Brantley Drive, Cedarcliff Road, Charlton Drive, Cherry Hills Drive, Cold Spring Drive, Lera Jones Drive, Moss Landing Drive, Moss Spring Drive, Mossdale Drive, Owendale Drive, Rogers Court and Safford View Drive, north of Anderson Road (97.8 acres).

Existing Zoning

Agricultural/Residential (AR2a) requires a minimum lot size of two acres and intended for uses that generally occur in rural areas, including single-family, two-family, and mobile homes at a density of one dwelling unit per two acres. The AR2a District is intended to implement the natural conservation or rural land use policies of the general plan.

Single-Family Residential (RS10) requires a minimum of 10,000 square foot lot and is intended for single-family dwellings at a density of 3.7 dwelling units per acre.

One and Two-Family Residential (R10) requires a minimum 10,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 4.63 dwelling units per acre including 25 percent duplex lots.

Proposed Zoning

Contextual Overlay provides appropriate design standards for residential areas necessary to maintain and reinforce an established form or character of residential development in a particular area.

CRITICAL PLANNING GOALS

N/A

ANTIOCH-PRIEST LAKE COMMUNITY PLAN

T3 Suburban Neighborhood Maintenance (T3 NM) is intended to preserve the general character of developed suburban residential neighborhoods. T3 NM areas will experience some change over time, primarily when buildings are expanded or replaced. When this occurs, efforts should be made to retain the existing character of the neighborhood. T3 NM areas have an established development



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pattern consisting of low to moderate density residential development and institutional land uses. Enhancements may be made to improve pedestrian, bicycle and vehicular connectivity.

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.

Consistent with Policy?

Yes. The proposed Contextual Overlay is consistent with the policy. The Contextual Overlay would help to preserve the general character of the existing neighborhood with specific standards for new construction that are directly related to the existing residential structures in the area.

ANALYSIS

The Contextual Overlay District provides appropriate design standards for residential areas necessary to maintain and reinforce an established form or character of residential development in a particular area.

The Design Standards established through the Contextual Overlay include specific standards in regards to street setback, building height, building coverage, access, driveways, garages, and parking areas. Street setbacks, building height, and building coverage are directly tied to the lots abutting on either side of a lot proposed for new construction. Access, driveway, garage and parking Design Standards are intended to help control new accesses on the public streets as well as location of garages and parking to lessen the impact of new construction on existing homes. The Design Standards as established cannot be modified.

CONTEXTUAL OVERLAY STANDARDS

- A. Street setback. The minimum required street setback shall be the average of the street setback of the two developed lots abutting each side of the lot. When one or more of the abutting lots is vacant, the next developed lot on the same block face shall be used. The minimum provided in 17.12.030A and the maximum provided in 17.12.030C.3 shall not apply. Where there is only one abutting lot on the same block face, it shall be used for this calculation. When the subject lot is on a corner, the minimum required street setback shall be calculated and met for each street.
- B. Height.
 1. The maximum height, including the foundation, of any primary structure shall not be greater than 35 feet or 125% of the average height of the principal structures on the two lots abutting each side of the lot, whichever is less. When one of the abutting lots is vacant, the next developed lot on the same block face shall be used. Where there is only one abutting lot on the same block face, it shall be used for this calculation. When the subject lot is on a corner, the maximum height shall be calculated for each street and limited to 35 feet or 125% of the average height of the lesser value. When 125% of the average of the abutting structures is less than 27 feet, a maximum height of 1.5 stories in 27 feet shall be permitted.
 2. The maximum height, including the foundation, of any accessory structure shall not be greater than 27 feet.



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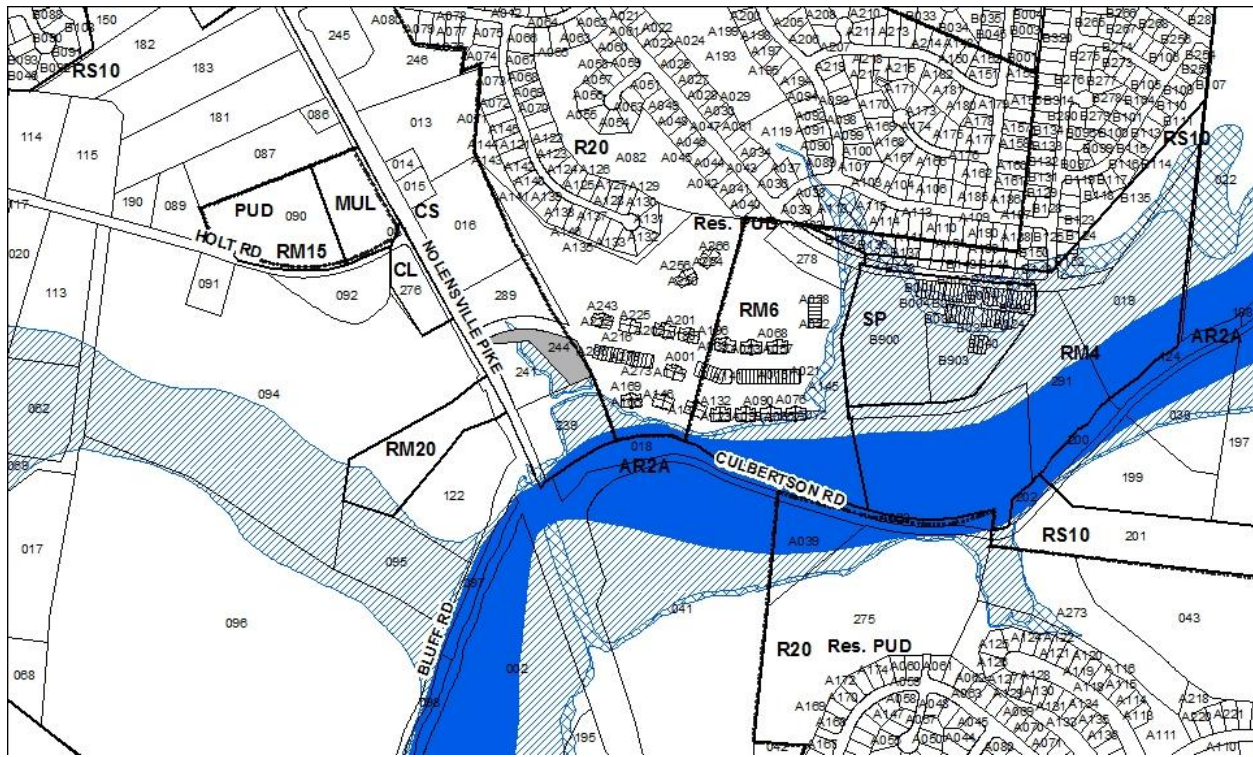
3. For the purposes of this section, height shall be measured from grade or, if present, the top of a foundation which shall not exceed three feet above grade, to the roof line.
- C. Maximum building coverage. The maximum building coverage (excluding detached garages and other accessory buildings) shall be a maximum of 150% of the average of the building coverage (excluding detached garages and other accessory buildings) of the two abutting lots on each side. When the abutting lot is vacant, the next developed lot shall be used. Where there is only one abutting lot on the same block face, it shall be used for this calculation. When the subject lot is on a corner, the maximum building coverage shall be calculated and met for each street.
- D. Access and driveways, garages and parking areas.
 1. Access and Driveways.
 - a. Where existing, access shall be from an improved alley. Where no improved alley exists, a driveway within the street setback may be permitted.
 - b. For a corner lot, the driveway shall be located within 30 feet of the rear property line.
 - c. Driveways are limited to one driveway ramp per public street frontage.
 - d. Parking, driveways and all other impervious surfaces in the required street setback shall not exceed twelve feet in width.
 2. Garages.
 - a. Detached. The front of any detached garage shall be located behind the rear of the primary structure. The garage door of a detached garage may face the street.
 - b. Attached. The garage door shall face the side or rear property line

STAFF RECOMMENDATION

Staff recommends approval as the establishment of a contextual overlay is consistent with the policy for the area.



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2015Z-060PR-001
Map 181, Parcel(s) 244
12, Southeast
31 (Fabian Bedne)



Project No.	Zone Change 2015Z-060PR-001
Council District	31 – Bedne
School District	4 - Shepherd
Requested by	Anderson, Delk, Epps & Associates, applicant; Yazdian Construction, Inc, owner.
Staff Reviewer	Birkeland
Staff Recommendation	<i>Approve.</i>

APPLICANT REQUEST

Zone change from CS to OR20-A.

Zone Change

A request to rezone from Commercial Service (CS) to Office/Residential-A (OR20-A), for property located at Nolensville Pike (unnumbered) at the southeast intersection of Nolensville Pike and Lenox Creekside Drive (1.39 acres).

Existing Zoning

Commercial Service (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.

Proposed Zoning

Office/Residential-A (OR20-A) is intended for office and/or multi-family residential units at up to 20 dwelling units per acre and is designed to create walkable neighborhoods through the use of appropriate building placement and bulk standards. *OR20-A would permit a maximum of 27 units.*

CRITICAL PLANNING GOALS

N/A

SOUTHEAST COMMUNITY PLAN

T3 Suburban Mixed Use Corridor (T3 CM) policy is intended to enhance suburban mixed use corridors by encouraging a greater mix of higher density residential and mixed use development along the corridor. T3 CM areas are located along pedestrian friendly, prominent arterial-boulevard and collector-avenue corridors that are served by multiple modes of transportation and are designed and operated to enable safe, attractive and comfortable access and travel for all users. T3 CM areas provide high access management and are served by highly connected street networks, sidewalks, and existing or planned mass transit.

Consistent with Policy?

Yes. The proposed OR20-A zoning district allows uses that are consistent with the T3 CM Suburban Mixed Use Corridor land use policy. Changing the zoning on the subject properties to OR20-A is appropriate given the policy and the existing office and multifamily residential zoning and uses around the area.

FIRE MARSHAL RECOMMENDATION

N/A



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PUBLIC WORKS RECOMMENDATION N/A

TRAFFIC AND PARKING RECOMMENDATION Conditions of Approval

- Traffic study may be required at the time of development

WATER SERVICES RECOMMENDATION No agency review required

STORMWATER RECOMMENDATION No agency review required

Maximum Uses in Existing Zoning District: CS

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (814)	1.35	0.6 F	35,283 SF	1548	36	107

Maximum Uses in Proposed Zoning District: OR20

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (220)	1.39	20 D	27 U	288	17	33

Traffic changes between maximum: CS and OR20

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	-1,260	-19	-74

METRO SCHOOL BOARD REPORT

Projected student generation existing CS district: 2 Elementary 1 Middle 1 High

Projected student generation proposed OR20-A district: 2 Elementary 1 Middle 1 High

The proposed zone change would generate no more students than what is typically generated under the existing CS zoning district. Students would attend Shayne Elementary School, Oliver Middle School, and Overton High School.

STAFF RECOMMENDATION

The proposed zone change to OR20-A will encourage a greater mix of higher density residential and mixed uses along the Nolensville Pike corridor. Staff recommends approval because the zoning change from CS to OR20-A zoning district allows uses that are consistent with the T3 Suburban Mixed Use Corridor Policy.



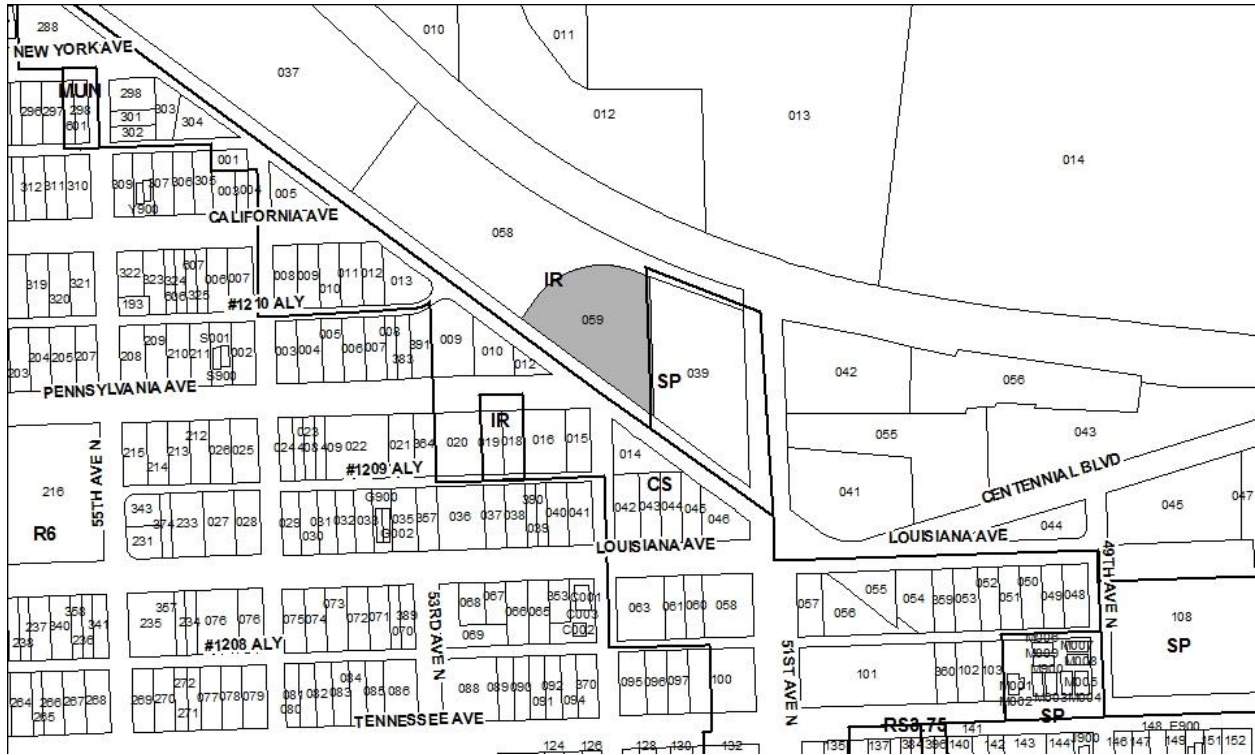
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CONDITIONS

1. Work with Public Works to identify requirements of the Infrastructure Deficiency Area (IDA), equal to 238 linear feet, prior to the issuance of building permits. Prior to the issuance of the use and occupancy, complete IDA requirements.



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2015Z-061PR-001
Map 091, Parcel(s) 059
07, West Nashville
20 (Buddy Baker)



Project No.	Zone Change 2015Z-061PR-001
Council District	20- Baker
School District	01-Gentry
Requested by	Dale and Associates, applicant; 5202 Centennial, LLC, owner.
Staff Reviewer	Deus
Staff Recommendation	<i>Approve.</i>

APPLICANT REQUEST
Rezone from IR to MUL-A.

Zone Change

A request to rezone from Industrial Restrictive (IR) to Mixed Use Limited-A (MUL-A) for property located at 5202 Centennial Boulevard, approximately 375 feet west of 51st Avenue N (1.49 acres).

Existing Zoning

Industrial Restrictive (IR) is intended for a wide range of light manufacturing uses at moderate intensities within enclosed structures.

Proposed Zoning

Mixed Use Limited-A (MUL-A) is intended for a moderate intensity mixture of residential, retail, restaurant, and office uses and is designed to create walkable neighborhoods through the use of appropriate building placement and bulk standards.

CRITICAL PLANNING GOALS

- Supports Infill Development
- Creates Walkable Neighborhoods

This request directs development to areas where infrastructure is already existing (i.e. sewer lines, roads) as opposed to where there are not adequate public facilities. This reduces the service constraints placed on Metro’s resources. The proposed request would also enhance walkability in a corridor through the orientation of buildings.

WEST NASHVILLE COMMUNITY PLAN

T4 Urban Mixed Use Neighborhood (T4 MU) is intended to preserve, enhance, and create urban, mixed use neighborhoods with a development pattern that contains a variety of housing along with mixed, use, commercial, institutional, and even light industrial development. T4 MU areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit.

Consistent with Policy?

Yes, the proposed MUL-A district is consistent with land use policy.

ANALYSIS

This site covers 1.49 acres and is zoned IR. The proposed zoning district would allow for a variety of uses to be introduced into a neighborhood that is expected to transition from an industrial area to



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a mixed use neighborhood. The MUL-A district would create walkable neighborhoods through building placements and bulk standards along a corridor. Structures would have a shallow build-to-zone that situates the building to address the public realm and create a pedestrian oriented design.

Table 1 & 2 display the differences in bulk regulations between the existing zoning and the proposed zoning.

Table 1. Industrial Restrictive (IR) District

Existing Zoning	Max. FAR	Max. Height at Setback Line	Slope of Height Control Plane (V to H)
Industrial Restrictive (IR)	0.60	45 feet	1.5 to 1

Table 2. Mixed Use Limited (MUL-A) District

Proposed Zoning	Max. FAR	Max. Height in Build- to Zone	Min. Step-back	Max. Height
Mixed Use Limited (MUL-A)	1.00	3 stories in 45 feet	15 feet	4 stories in 60 feet

FIRE MARSHAL RECOMMENDATION

N/A

STORMWATER RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

N/A

TRAFFIC & PARKING RECOMMENDATION

- Traffic study may be required at time of development.

Maximum Uses in Existing Zoning District: **IR**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Warehousing (150)	1.49	0.6 F	38,942 SF	139	12	13

Maximum Uses in Proposed Zoning District: **MUL-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (820)	1.49	1.0 F	64,904 SF	5128	120	477



Metro Planning Commission Meeting of 07/23/2015

Traffic changes between maximum: IR and MUL-A

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+4,989	+108	+464

METRO SCHOOL BOARD REPORT

Projected student generation IR district 0 Elementary 0 Middle 0 High

Projected student generation MUL-A district 10 Elementary 5 Middle 5 High

The proposed MUL- A district would generate 20 more students than what is typically generated as the IR district does not allow residential. Students would attend Cockrill Elementary, McKissack Middle School and Pearl- Cohen High School. Cockrill Elementary and Pearl-Cohen High School are identified as being over capacity. There is capacity for additional high school students within an adjacent cluster.

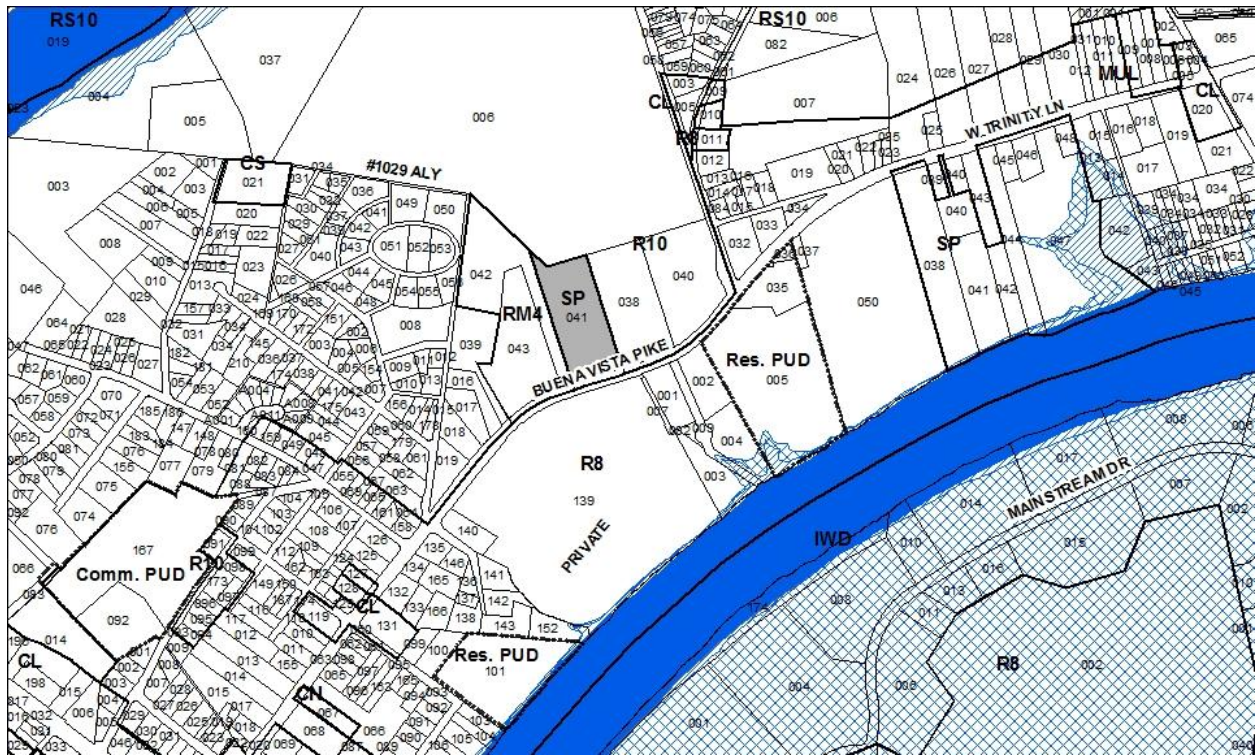
This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

Staff recommends approval as this request is consistent with policy.



Metro Planning Commission Meeting of 07/23/2015



2015Z-062PR-001

Map 070, Parcel(s) 041

03, Bordeaux - Whites Creek

02 (Frank R. Harrison)



Project No.
Council District
School District
Requested by

Zone Change 2015Z-062PR-001
2 - Harrison
1 - Gentry
Angela J. High, applicant; Zion Hill First African Baptist Church, owner.

Staff Reviewer
Staff Recommendation

Birkeland
Approve.

APPLICANT REQUEST
Zone change from SP to R10.

Zone Change

A request to rezone from Specific Plan (SP) to One and Two-Family Residential (R10) for property located at 2433 Buena Vista Pike, approximately 930 feet west of Tucker Road (5.01 acres).

Existing Zoning

Specific Plan (SP) Specific Plan-Residential is a zoning district category that provides for additional flexibility of design, including the relationship of streets to buildings, to provide the ability to implement the specific details of the General Plan. This Specific Plan includes residential and institutional uses.

Proposed Zoning

One and Two-Family Residential (R10) requires a minimum 10,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 4.63 dwelling units per acre including 25 percent duplex lots. *R10 would permit a maximum of 21 lots with 5 duplex lots for a total of 26 units.*

CRITICAL PLANNING GOALS

N/A

BORDEAUX-WHITES CREEK COMMUNITY PLAN

T3 Suburban Neighborhood Evolving (T3 NE) is intended to create and enhance suburban residential neighborhoods with more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate density development patterns with moderate setbacks and spacing between buildings. T3 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network, block structure, and proximity to centers and corridors. T3 NE areas are developed with creative thinking in environmentally sensitive building and site development techniques to balance the increased growth and density with its impact on area streams and rivers.



Metro Planning Commission Meeting of 07/23/2015

Consistent with Policy?

Yes. The proposed R10 zoning district allows uses that are consistent with the T3 Suburban Neighborhood Evolving land use policy. Changing the zoning on the subject properties to R10 is appropriate given the policy and the residential zoning surrounding this site.

FIRE MARSHAL RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

N/A

TRAFFIC AND PARKING RECOMMENDATION

Conditions of Approval

- Traffic study may be required at the time of development

WATER SERVICES RECOMMENDATION

No agency review required

STORMWATER RECOMMENDATION

No agency review required

Maximum Uses in Existing Zoning District: **SP**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Religious Institution (560)	5.01	-	250 seats	N/A	15	13

Maximum Uses in Existing Zoning District: **SP**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Multi-Family Residential (230)	5.01	-	23 U	180	16	19

Maximum Uses in Proposed Zoning District: **R10**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Two-Family Residential (210)	5.01	4.35 D	26 U*	249	20	27

*Based on five two-family lots.

Traffic changes between maximum: **SP** and **R10**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+69	-11	-5



Metro Planning Commission Meeting of 07/23/2015

METRO SCHOOL BOARD REPORT

Projected student generation existing SP district: 2 Elementary 1 Middle 1 High

Projected student generation proposed R10 district: 4 Elementary 4 Middle 3 High

The proposed SP-R zoning district could generate 7 more students than what is typically generated under the existing SP zoning district. Students would attend Cumberland Elementary School, Joelton Middle School, and Whites Creek High School. Cumberland Elementary has been identified as over capacity. There is capacity within the cluster for elementary school students. This information is based upon data from the school board last updated October 2014.

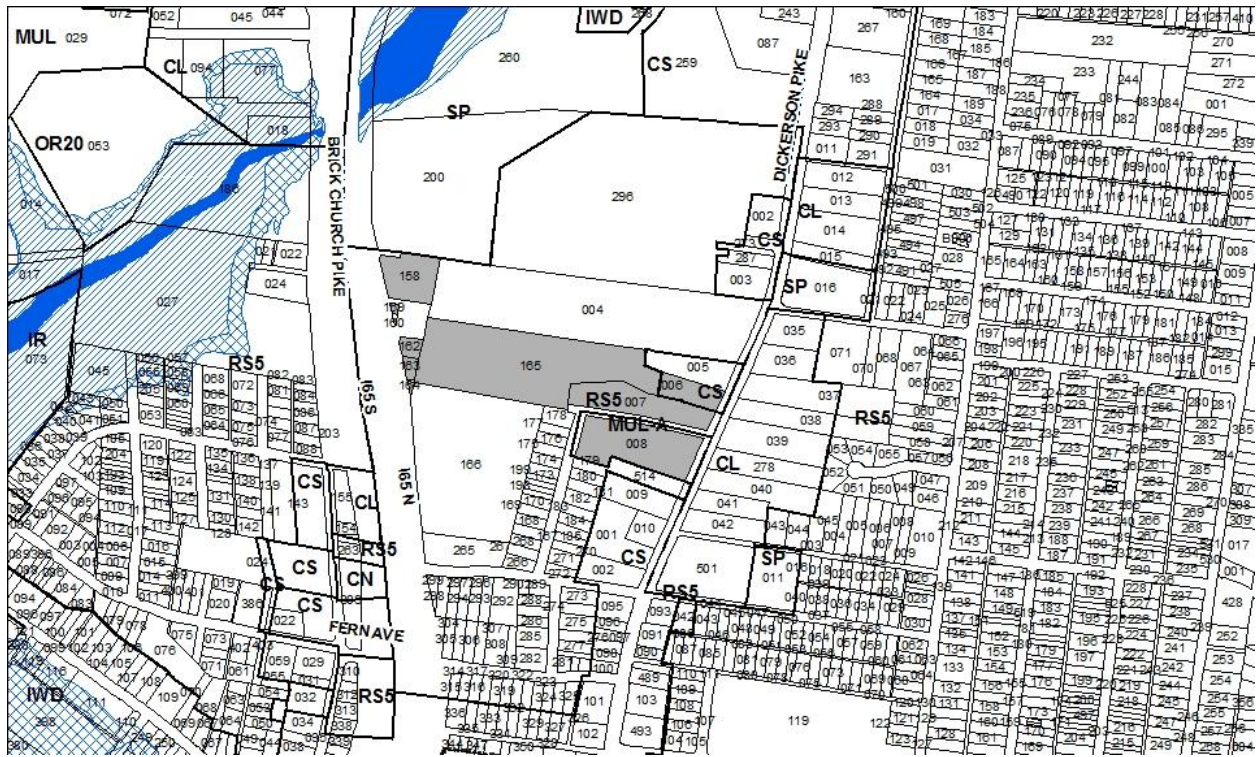
STAFF RECOMMENDATION

In 2007 this property was approved for a zone change from R10 to SP zoning that allowed 23 multi-family units and a 250 seat religious institution within a two-story, 27,000 square foot building. The subject site is vacant; no construction activity has started for any portion of the SP. The applicant is requesting to rezone this property back to R10 zoning.

The proposed zone change to R10 zoning will encourage the development of one and two family residential uses along an already residential portion of Buena Vista Pike. Staff recommends approval because the zoning changes from SP to R10 zoning district allows uses that are consistent with the T3 Neighborhood Evolving Policy.



Metro Planning Commission Meeting of 07/23/2015



2015Z-063PR-001

Map 071-10, Parcel(s) 158-159, 162-165

Map 071-11, Parcel(s) 006-008

05, East Nashville

05 (Scott Davis)



Project No.	Zone Change 2015Z-063PR-001
Council Bill No.	BL2015-1257
Council District	5 – Davis
School District	5 – Kim
Requested by	Dale and Associates, applicant; Phillip Parkerson and Regal Homes, owners.
Staff Reviewer	Swaggart
Staff Recommendation	<i>Disapprove.</i>

APPLICANT REQUEST

Zone change from CS, MUL-A and RS5 to MUG-A.

Zone Change

A request to rezone from Commercial Services (CS), Mixed Use Limited – A (MUL-A), and Single-Family Residential (RS5) to Mixed Use General – A (MUG-A) and within the Dickerson Road Urban Design Overlay for properties located at 1404, 1410, and 1412 Dickerson Pike and Dickerson Pike (unnumbered) and Penning Avenue (unnumbered) and Public Street (unnumbered), approximately 230 feet north of Fern Avenue (14.03 acres).

Existing Zoning

Single-Family Residential (RS5) requires a minimum 5,000 square foot lot and is intended for single-family dwellings at a density of 7.41 dwelling units per acre. *RS5 would permit a maximum of 89 units.*

Commercial Service (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.

Mixed Use Limited-A (MUL-A) is intended for a moderate intensity mixture of residential, retail, restaurant, and office uses and is designed to create walkable neighborhoods through the use of appropriate building placement and bulk standards.

Urban Design Overlay (UDO) is a zoning tool that requires specific design standards for development in a designated area. UDOs overlay the current base zoning and allow for development standards above and beyond those in the base zoning. This UDO only provides additional requirements for signs.

Proposed Zoning

Mixed Use General-A (MUG-A) is intended for a moderately high intensity mixture of residential, retail, and office uses and is designed to create walkable neighborhoods through the use of appropriate building placement and bulk standards.

CRITICAL PLANNING GOALS

N/A



Metro Planning Commission Meeting of 07/23/2015

EAST NASHVILLE COMMUNITY PLAN

Urban Mixed Use Corridor (T4 CM) policy is intended to enhance urban mixed use corridors by encouraging a greater mix of higher density residential and mixed use development along the corridor, placing commercial uses at intersections with residential uses between intersections; creating buildings that are compatible with the general character of urban neighborhoods; and a street design that moves vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit.

Urban Neighborhood Evolving (T4 NE) policy is intended to create and enhance urban residential neighborhoods that provide more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate to high density development patterns with shallow setbacks and minimal spacing between buildings. T4 NE areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. T4 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network and block structure and proximity to centers and corridors.

Conservation (CO) policy is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.

Consistent with Policy?

The request consists of various parcels that are within various policies. The proposed MUG-A district is consistent with the T4 CM policy area along Dickerson Pike; however, MUG-A is not consistent with the T4 NE policy and CO policy that applies to all areas outside of the T4 CM policy. The proposed MUG-A permits nonresidential uses which are not consistent with the T4 NE policy which only supports residential uses.

FIRE MARSHAL’S OFFICE RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

N/A

TRAFFIC AND PARKING RECOMMENDATION

Traffic study may be required at time of development.

Maximum Uses in Existing Zoning District: CS

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (814)	0.75	0.6 F	19,602 SF	877	23	69



Metro Planning Commission Meeting of 07/23/2015

Maximum Uses in Existing Zoning District: MUL-A

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (820)	2.97	1.0 F	129,373 SF	8029	180	756

Maximum Uses in Existing Zoning District: RS5

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	10.31	8.71 D	89 U	935	73	97

Maximum Uses in Proposed Zoning District: MUG-A

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (820)	14.03	3.0 F	1,833,440 SF	44985	857	4467

Traffic changes between maximum: CS, MUL-A, RS5 and MUG-A

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+35,144	+581	+3,545

STORMWATER RECOMMENDATION

Ignore

WATER SERVICES RECOMMENDATION

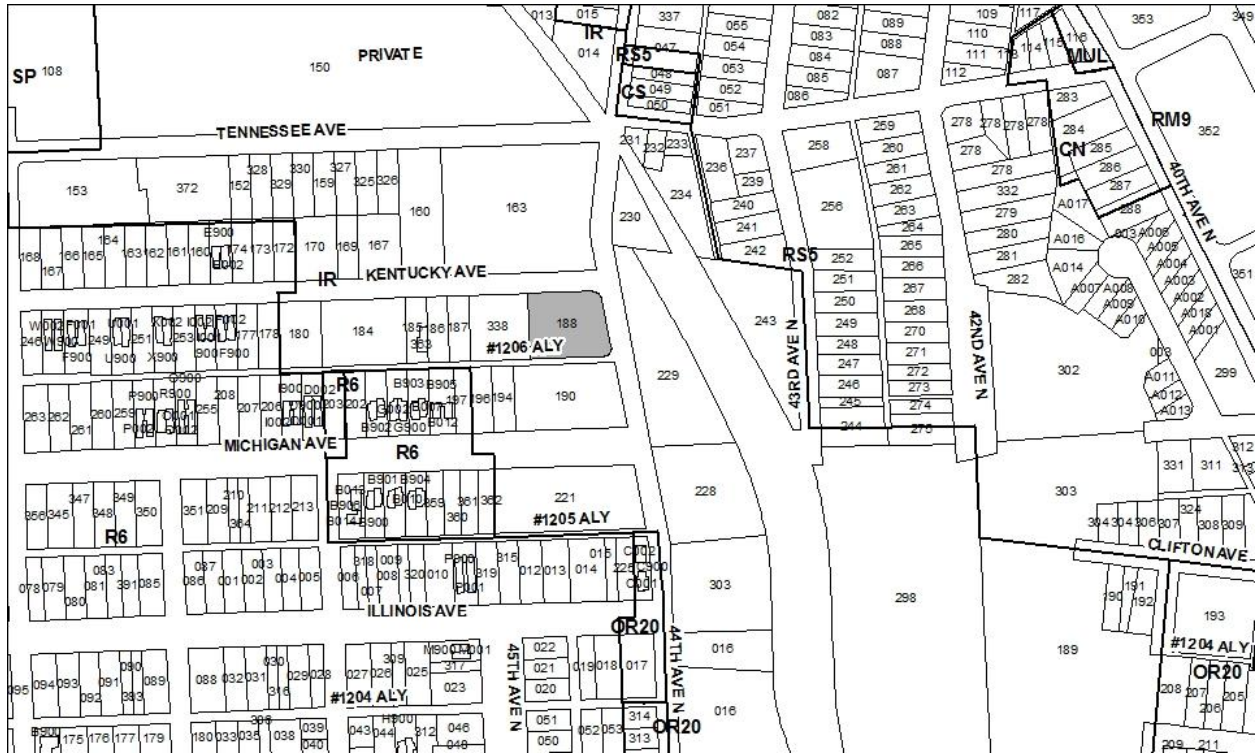
N/A

STAFF RECOMMENDATION

Staff recommends disapproval of the proposed MUG-A district because it is not consistent with all the policies that apply to the area requested to be rezoned.



Metro Planning Commission Meeting of 07/23/2015



2015Z-065PR-001

Map 091-08, Parcel(s) 188

07, West Nashville

20 (Buddy Baker)



Project No.	Zone Change 2015Z-065PR-001
Council Bill No.	BL2015-1263
Council District	20- Baker
School District	1 - Gentry
Requested by	Dale and Associates, applicant; Leo Elton DeMoss, owner.
Staff Reviewer	Birkeland
Staff Recommendation	<i>Approve.</i>

APPLICANT REQUEST

Zone change from IR to MUL-A

Zone Change

A request to rezone from Industrial Restrictive (IR) to Mixed Use Intensive-Alternative (MUL-A) for property located at 4401 Kentucky Avenue, at the southwest corner of 44th Avenue N and Kentucky Avenue (0.65 acres).

Existing Zoning

Industrial Restrictive (IR) is intended for a wide range of light manufacturing uses at moderate intensities within enclosed structures.

Proposed Zoning

Mixed Use Intensive-Alternative (MUL-A) is intended for a high intensity mixture of residential, retail, and office uses and is designed to create walkable neighborhoods through the use of appropriate building placement and bulk standards.

CRITICAL PLANNING GOALS

N/A

WEST NASHVILLE COMMUNITY PLAN

T4 Urban Mixed Use Neighborhood (T4 MU) is intended to preserve, enhance, and create urban, mixed use neighborhoods with a development pattern that contains a variety of housing along with mixed use, commercial, institutional, and even light industrial development. T4 MU areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit.

Consistent with Policy?

Yes. The proposed MUL-A district allows uses that are consistent with the T4 CC Urban Mixed Use Neighborhood land use policy. Changing the zoning on the subject property to MUL-A is appropriate for this policy area.

FIRE MARSHAL RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

N/A



Metro Planning Commission Meeting of 07/23/2015

TRAFFIC AND PARKING RECOMMENDATION

Conditions of Approval

- Traffic study may be required at the time of development

WATER SERVICES RECOMMENDATION

No agency review required

STORMWATER RECOMMENDATION

No agency review required

Maximum Uses in Existing Zoning District: **IR**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Warehousing (150)	0.65	0.6 F	16,988 SF	61	6	6

Maximum Uses in Proposed Zoning District: **MUL-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (814)	0.65	1.0 F	28,314 SF	1249	30	90

Traffic changes between maximum: **IR** and **MUL-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+1,188	+24	+84

METRO SCHOOL BOARD REPORT

Projected student generation existing IR district: 3 Elementary 1 Middle 1 High

Projected student generation proposed MUL-A district: 5 Elementary 2 Middle 2 High

The proposed zone change would generate four more students than what is typically generated under the existing IR zoning district assuming a 1,000 square foot residential unit since MUL-A does not have a maximum density. Students would attend Cockrill Elementary School, McKissack Middle School, and Pearl-Cohn High School. Both the elementary and high school has been identified as over capacity. There is room within the cluster for elementary students and there is room within an adjacent cluster for high school students. This information is based upon data from the school board last updated October 2014.

STAFF RECOMMENDATION

The subject property is located at the corner of Kentucky and 44th Avenue North and surrounded by IR zoning. However, this industrial area has transitioned to the T4 Urban Mixed Use Neighborhood policy which encouraged a mixture of uses. Staff recommends approval because the zoning change

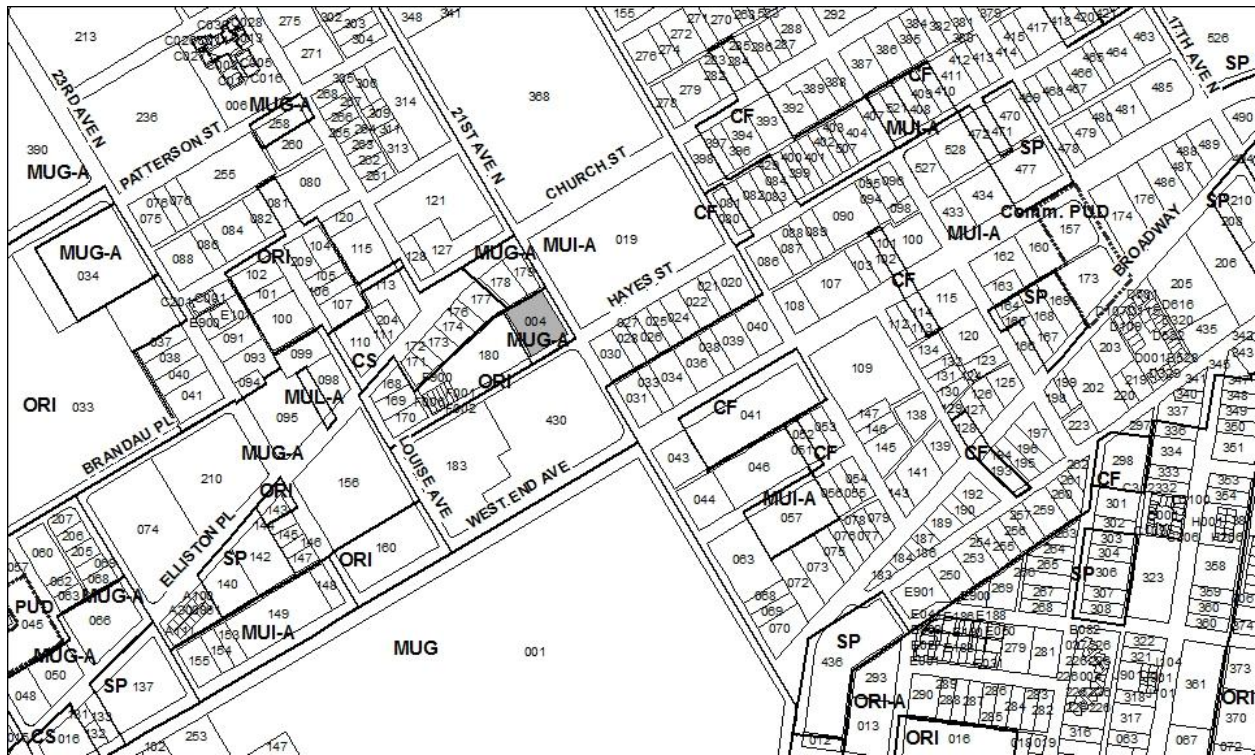


Metro Planning Commission Meeting of 07/23/2015

from IR to MUL-A zoning district allows uses that are consistent with the T4 Mixed Use Neighborhood Policy.



Metro Planning Commission Meeting of 07/23/2015



2015Z-066PR-001

Map 092-16, Parcel(s) 004
10, Green Hills - Midtown
21 (Edith Taylor Langster)



Project No.	Zone Change 2015Z-066PR-001
Council Bill No.	BL2015-1262
Council District	21 – Langster
School District	5 – Kim
Requested by	Baker, Donelson, Bearman, Caldwell & Berkowitz, PC, applicant; 21st Plaza Partners, owner.
Staff Reviewer	Swaggart
Staff Recommendation	<i>Disapprove.</i>

APPLICANT REQUEST

Zone change from MUG-A to MUI-A.

Zone Change

A request to rezone from Mixed Use General-A (MUG-A) to Mixed Use Intensive-A (MUI-A) for property located at 121 21st Avenue N, at the northwest corner of 21st Avenue N and Hayes Street (0.62 acres).

Existing Zoning

Mixed Use General-A (MUG-A) is intended for a moderately high intensity mixture of residential, retail, and office uses and is designed to create walkable neighborhoods through the use of appropriate building placement and bulk standards.

Proposed Zoning

Mixed Use Intensive-A (MUI-A) is intended for a high intensity mixture of residential, retail, and office uses and is designed to create walkable neighborhoods through the use of appropriate building placement and bulk standards.

CRITICAL PLANNING GOALS

N/A

GREENHILLS-MIDTOWN COMMUNITY PLAN

Urban Mixed Use Neighborhood (T5 MU) policy is intended to preserve, enhance, and create high-intensity urban mixed use neighborhoods with a development pattern that contains a diverse mix of residential and non-residential land uses. T5 MU areas are intended to be among the most intense areas in Davidson County. T5 MU areas include some of Nashville’s major employment centers such as Midtown that represent several sectors of the economy including health care, finance, retail, the music industry, and lodging. T5 MU areas also include locations that are planned to evolve to a similar form and function.

Special Policy

A special policy also applies to this area. The special policy applies to a larger area and species appropriate heights depending on location. The special policy supports a maximum of eight stories at the subject location.



Metro Planning Commission Meeting of 07/23/2015

Consistent with Policy?

No. While the proposed MUI-A district would permit uses and design that is consistent with the T5 MU policy, the district permits a height that is not supported by the policy. A special policy regarding height applies to this area. The policy supports a maximum of eight stories at this location. The proposed MUI-A district would permit a maximum of 15 stories. It is also important to note that the neighboring property to the south is the Robert E. Lee Apartments which is National Register Eligible and is only six stories in height.

FIRE MARSHAL'S OFFICE RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

N/A

TRAFFIC AND PARKING RECOMMENDATION

Traffic study may be required at time of development.

Maximum Uses in Existing Zoning District: **MUG-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (820)	0.62	3.0 F	81,021 SF	5923	137	553

Maximum Uses in Proposed Zoning District: **MUI-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (820)	0.62	5.0 F	135,036 SF	8256	184	778

Traffic changes between maximum: **MUG-1** and **MUI-A**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+2,333	+47	+225

STORMWATER RECOMMENDATION

Ignore

WATER SERVICES RECOMMENDATION

N/A

STAFF RECOMMENDATION

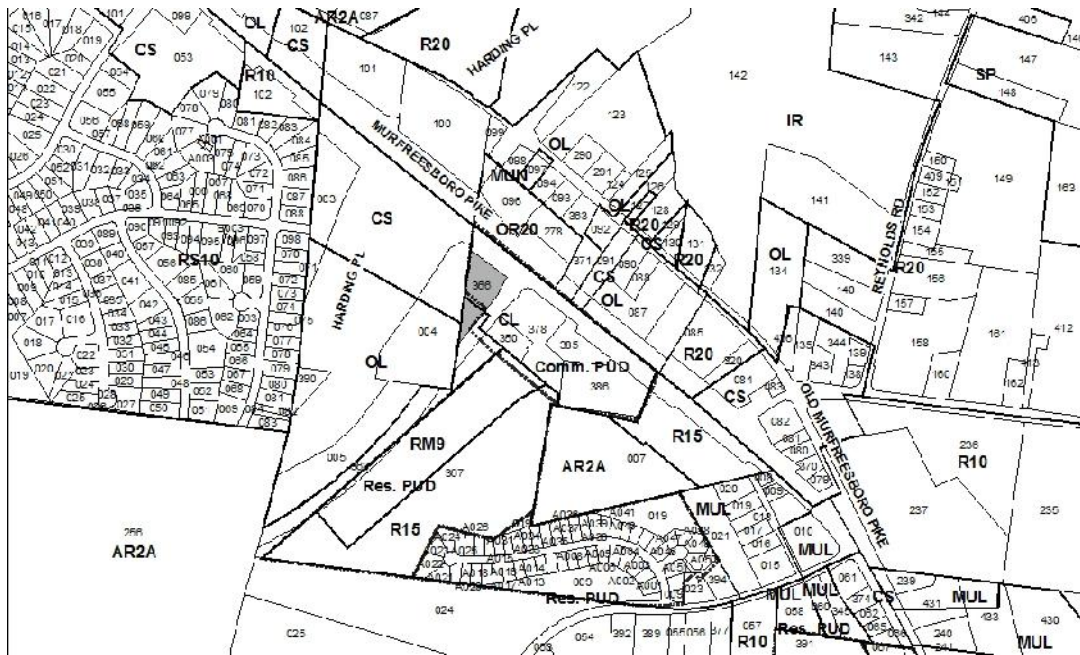
Staff recommends disapproval of the proposed MUG-A district because it permits a height that is not supported by the Green Hills-Midtown Community Plan.



SEE NEXT PAGE

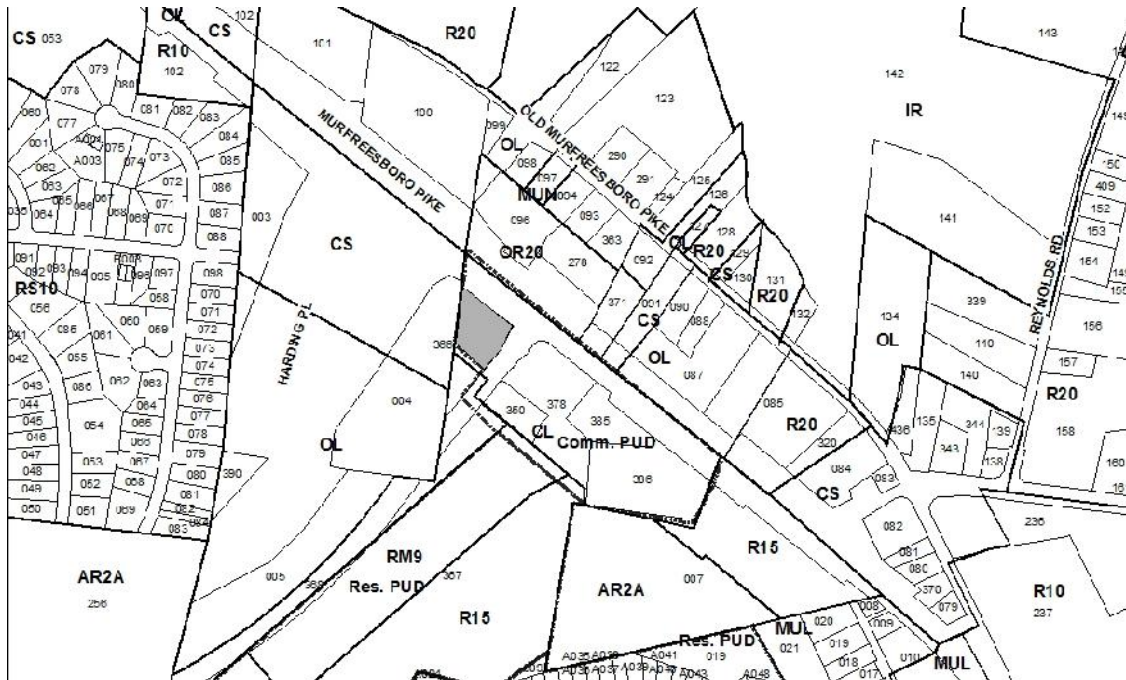


Metro Planning Commission Meeting of 07/23/2015



2015Z-067PR-001

Map 135, Parcel(s) 366
13, Antioch - Priest Lake
28 (Duane A. Dominy)



239-84P-003

CANTER CHASE (PUD CANCELLATION)
Map 135, Part of Parcel(s) 366
13, Antioch - Priest Lake
28 (Duane A. Dominy)



Project No.	2015Z-067PR-001 & 239-84P-003
Project Name	Zone Change & Canter Chase PUD Cancellation
Council District	28-Dominy
School District	7-Pinkston
Requested by	Councilmember Duane Dominy, applicant; James R. Jones, owner.
Staff Reviewer	Thomas
Staff Recommendation	<i>Disapprove.</i>

APPLICANT REQUEST

Zone change from CL and AR2a to CS and PUD cancellation.

Zone Change

A request to rezone from Commercial Limited (CL) and Agricultural/Residential (AR2a) to Commercial Service (CS) zoning for property located at 1827 Murfreesboro Pike, immediately west of the intersection of Murfreesboro Pike and Harding Place (1.47 acres).

Cancel PUD

A request to cancel a portion of the existing Canter Chase Planned Unit Development District for property located 1827 Murfreesboro Pike, immediately west of the intersection of Murfreesboro Pike and Harding Place (1.2 acres).

Existing Zoning

Commercial Limited (CL) is intended for retail, consumer service, financial, restaurant, and office uses.

Agricultural/Residential (AR2a) requires a minimum lot size of two acres and is intended for uses that generally occur in rural areas, including single-family, two-family, and mobile homes at a density of one dwelling unit per two acres. The AR2a District is intended to implement the natural conservation or rural land use policies of the general plan. *The portion of the existing site zoned AR2a is approximately 0.36 acres, which is not large enough to permit any residential units.*

Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provision of an adequate roadway system or essential utilities and services. In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provision of essential utilities and streets. *The subject PUD is approved for a financial institution.*

Proposed Zoning

Commercial Service (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.



Metro Planning Commission Meeting of 07/23/2015

CRITICAL PLANNING GOALS

N/A

ANTIOCH - PRIEST LAKE COMMUNITY PLAN

D Office Concentration (D OC) is intended to preserve, enhance, and create Districts where office use is predominant and where opportunities for the addition of complementary uses are present. The development and redevelopment of such Districts occurs in a manner that is complementary of the varying character of surrounding communities.

Conservation (CO) is intended to preserve environmentally sensitive land features through protection and remediation. CO policy applies in all Transect Categories except T1 Natural, T5 Center, and T6 Downtown. CO policy identifies land with sensitive environmental features including, but not limited to, steep slopes, floodway/floodplains, rare or special plant or animal habitats, wetlands and unstable or problem soils. The guidance for preserving or enhancing these features varies with what Transect they are in and whether or not they have already been disturbed.

Consistent with Policy?

No. The proposed rezoning and PUD cancellation are not consistent with the Office Concentration District or Conservation policies. The additional uses permitted by the proposed CS zoning district do not contribute to the preservation, enhancement, or creation of the District.

ANALYSIS

The proposed CS zoning district does not permit any uses that further the intent of the Office Concentration District beyond those uses already permitted within the existing CL zoning district. The only additional uses that CS zoning would permit for the site are uses that are not consistent with the policy, including, but not limited to automobile repair, used automobile sales, laundry plants, major appliance repair, self-service storage, vehicular sales/service, building contractor supply, and light manufacturing.

WATER SERVICES RECOMMENDATION

PUD Cancellation – Approved

STORMWATER RECOMMENDATION

PUD Cancellation – Approved

PUBLIC WORKS RECOMMENDATIONS

Rezone – N/A

PUD Cancellation – N/A

TRAFFIC AND PARKING RECOMMENDATIONS

Rezone – Conditions if approved

- Traffic study may be required at time of development.

PUD Cancellation – Conditions if approved

- Traffic study may be required at time of redevelopment.



Metro Planning Commission Meeting of 07/23/2015

Maximum Uses in Existing Zoning District: **CL**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (814)	1.11	0.6 F	29,010 SF	1279	31	92

Maximum Uses in Existing Zoning District: **AR2a**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Single-Family Residential (210)	0.36	0.50 D	0 U	-	-	-

Maximum Uses in Proposed Zoning District: **CS**

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (814)	1.47	0.6	38,419 SF	1682	38	114

Traffic changes between maximum: **CL, AR2a and CS**

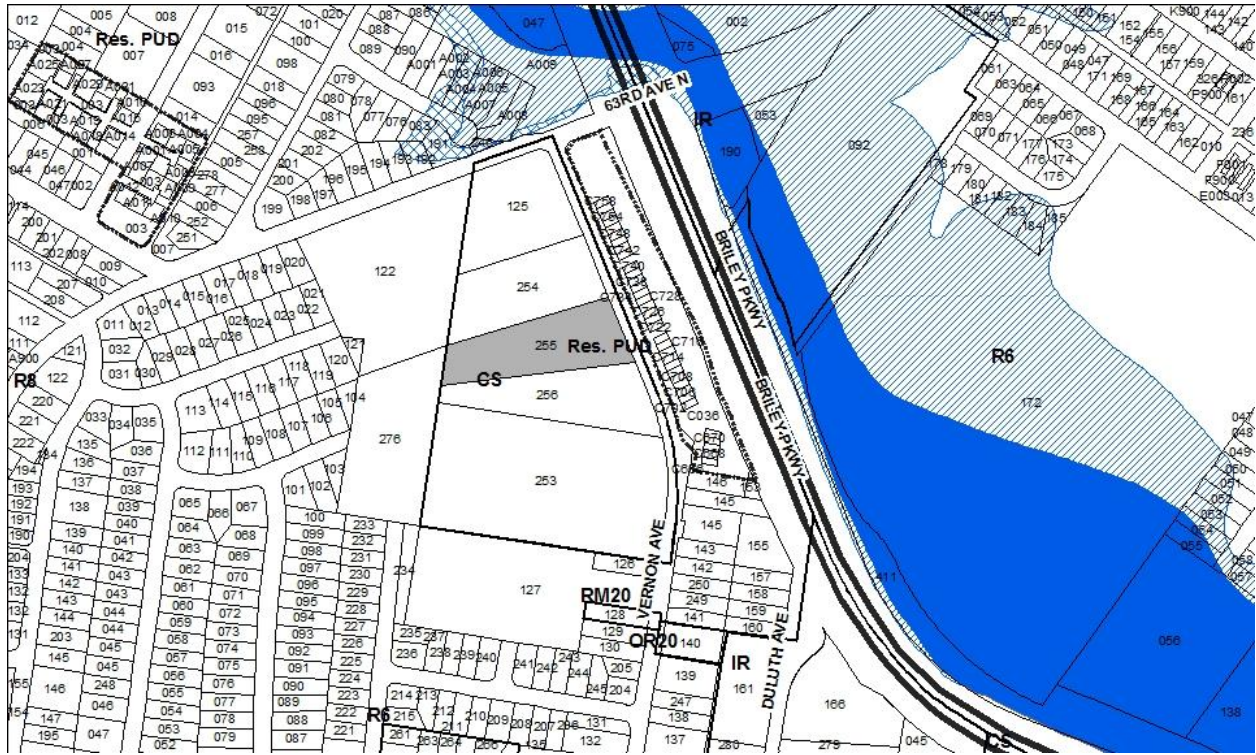
Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	+403	+7	+22

STAFF RECOMMENDATION

Staff recommends disapproval of the zone change request and disapproval of the PUD cancellation.



Metro Planning Commission Meeting of 07/23/2015



2015Z-068PR-001

Map 091-05, Parcel(s) 255

07, West Nashville

20 (Buddy Baker)



Project No.
Council District
School District
Requested by

Zone Change 2015Z-068PR--001
20-Baker
9-Frogge
Prewett Enterprises, Inc., applicant; Prewett Holdings, LLC, owner.

Staff Reviewer
Staff Recommendation

Milligan
Disapprove.

APPLICANT REQUEST

Zone change from CS to IWD.

Zone Change

A request to rezone from Commercial Service (CS) to Industrial Warehousing/Distribution (IWD) for property located at 677 Vernon Avenue, approximately 480 feet south of James Avenue (1.96 acres).

Existing Zoning

Commercial Service (CS) is intended for retail, consumer service, financial, restaurant, office, self-storage, light manufacturing and small warehouse uses.

Proposed Zoning

Industrial Warehousing/Distribution (IWD) is intended for a wide range of warehousing, wholesaling, and bulk distribution uses.

CRITICAL PLANNING GOALS

N/A

WEST NASHVILLE COMMUNITY PLAN

T4 Urban Neighborhood Evolving (T4 NE) is intended to create and enhance urban residential neighborhoods that provide more housing choices, improved pedestrian, bicycle and vehicular connectivity, and moderate to high density development patterns with shallow setbacks and minimal spacing between buildings. T4 NE areas are served by high levels of connectivity with complete street networks, sidewalks, bikeways and existing or planned mass transit. T4 NE policy may be applied either to undeveloped or substantially under-developed “greenfield” areas or to developed areas where redevelopment and infill produce a different character that includes increased housing diversity and connectivity. Successful infill and redevelopment in existing neighborhoods needs to take into account considerations such as timing and some elements of the existing developed character, such as the street network and block structure and proximity to centers and corridors.

Consistent with Policy?

No. The rezoning request to IWD is inconsistent with the T4 Urban Neighborhood Evolving Policy. The T4 NE policy is a residential only policy intended to create a diverse mix of housing types in existing urban residential neighborhoods. The existing zoning of CS is inconsistent with the policy as well. Rezoning to a more intense zoning district that allows for industrial uses is moving further away from the goals of the policy.



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ANALYSIS

The property at 677 Vernon Avenue is currently zoned CS, which allows for a wide range of uses. IWD zoning would allow for more intense uses, including industrial uses, such as light manufacturing and heavy equipment sales and service. The current CS zoning is inconsistent with the T4 Neighborhood Evolving policy, which encourages a mixture of different types of residential uses but no non-residential uses. A rezoning to IWD would move the zoning of the lot further away from the goals of the Neighborhood Evolving policy. There is a residential development directly across the street from the proposed rezoning which could be negatively impacted by the introduction of an industrial use in the area.

FIRE MARSHAL'S OFFICE RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

N/A

TRAFFIC AND PARKING RECOMMENDATION

Conditions if approved

- Traffic study may be required at time of development

Maximum Uses in Existing Zoning District: CS

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Retail (820)	1.96	0.6 F	51,226 SF	4397	104	407

Maximum Uses in Proposed Zoning District: IWD

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
Warehousing (150)	1.96	0.8 F	68,302 SF	244	21	22

Traffic changes between maximum: CS and IWD

Land Use (ITE Code)	Acres	FAR/Density	Total Floor Area/Lots/Units	Daily Trips (weekday)	AM Peak Hour	PM Peak Hour
-	-	-	-	-4,153	-83	-385

STAFF RECOMMENDATION

Staff recommends disapproval as the rezoning is inconsistent with the land use policy for the area.



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2015HP-001-001

Map 093-02-3, Parcel(s) 162

Map 093-06-1, Parcel(s) 057-059, 060, 099

09, Downtown

19 (Erica S. Gilmore)



Project No. Historic Preservation Overlay District
2015HP-001-001

Project Name Downtown Historic Preservation Overlay District

Council Bill BL2015-1271
Council District 19 – Gilmore
School District 5 – Kim
Requested by Metro Historical Commission and the Metro Planning Department, applicants; various property owners.

Staff Reviewer Sajid
Staff Recommendation *Approve.*

APPLICANT REQUEST

Apply Historic Preservation Overlay District.

Historic Preservation Overlay District

A request to apply the provisions of the Broadway Historic Preservation Overlay District to various properties located along Broadway, 2nd Avenue South, and 3rd Avenue South, at the corner of Broadway and 2nd Avenue South (approximately 1.16 acres).

Existing Base Zoning

Downtown Code (DTC) District is designed for a broad range of residential and non-residential activities associated with an economically healthy, socially vibrant, and sustainable Downtown. Included among the common goals for the DTC district is the efficient use of land capitalizing on a high level of services, reduced reliance on the automobile with enhanced usage of mass transit, and the creation of a vibrant and safe pedestrian streetscape. The DTC district is intended to implement the policies of the general plan.

Proposed Overlay

Historic Preservation Overlay Districts (HP) are geographical areas which possess a significant concentration, linkage or continuity of sites, buildings, structures or objects which are united by past events or aesthetically by plan or physical development.

CRITICAL PLANNING GOALS

- Preserves Historic Resources

The Historic Preservation Overlay District is intended to preserve historic structures through the implementation of development and design guidelines by the Metro Historic Zoning Commission and staff.

DOWNTOWN COMMUNITY PLAN

T6 Downtown Core (T6 DC) is intended to preserve and enhance the “core” of Downtown such that it will remain the commercial, civic and entertainment center of Nashville and Middle Tennessee. T6 DC is intended to have the highest intensity of development in the County. Offices are the predominant type of development, although the T6 DC contains a diverse array of land uses



Metro Planning Commission Meeting of 07/23/2015

including retail, entertainment, institutional uses, government services, and higher density residential. The highest intensity development is in the central portion of the Core (north of Broadway), with less intensive uses locating in the surrounding “frame” area of T6 DC, in the SoBro neighborhood.

Consistent with Policy?

Yes. The policy encourages the preservation and protection of historic features. The proposed Downtown Historic Preservation Overlay District will aid implementation of the design principles in the land use policy.

REQUEST DETAILS

The six properties included in the request are located east of the intersection of 4th Avenue North and Church Street and are listed in the Printer’s Alley National Register of Historic Places District.

The historic buildings are described by Metro Historic Zoning Commission staff:

The proposed overlay includes six properties. Four of the buildings are historic and identified as contributing properties to the National Register of Historic Places district listed in 1982. Two are non-historic buildings and they will be removed.



First National Bank, (JC Bradford Building) 170 4th Avenue North was Nashville’s first skyscraper in 1905. Barnett, Hayes and Barnett, architects, designed the twelve-story steel frame structure with brick curtain walls for the First National Bank. Subsequently it has been called the Independent Life Building, Third National Bank Building, the JC Bradford Building and the Marriott Hotel. The original structure was built in the Neo-classical style. When Third National Bank acquired the building, they built an addition to the east of the original structure which doubled the size of the building and removed virtually all original exterior ornamentation. New Art Deco details were added to both the interior and exterior and have become historic themselves. The base of the building which consists of the first two levels is faced with limestone. Pilasters with stylized capitals establish the rhythm of bays, and ornamental

cast metal spandrel panels separated the first and second level within each bay.



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Noel Hotel (US Bank Building), 200 4th Avenue North: Built in 1929 this twelve-story, reinforced concrete structure with brick curtain walls is a stripped-down 1920s modern adaptation of the Neo-classical style. It was designed by Nashville architects Marr and Holman. The ground story base with mezzanine has a rusticated and coursed ashlar limestone finish with large expanses of glass on the front and elegant arched windows on the rear extension. The ground story has a cornice with Greek Fret molding. Capping the parapet on the rear extension are classical urns. The shaft is very plain with evenly spaced individual single-light rectangular windows. Stone spandrel panels with diamond insets separate the windows on either end of the façade. The interior, unlike the exteriors, is quite ornate with its polished marble and brass, and plaster work detailed in stylized classical motifs. After the hotel was closed, the building was adapted as a bank in 1973 with the upper floors serving as offices and the lobby as the main banking room.



Utopia Hotel, 206 4th Avenue North, was constructed in 1891. The six-story solid masonry structure with stone Romanesque style façade was designed by Nashville architect Hugh C. Thompson and was listed individually on the National Register in 1978. The Hotel was a reflection of the growth and prosperity of Nashville at that time. In 1892 City Directory, the hotel's listing read "European Hotel and Restaurant, Saloon, Cigars, and Tobacco." The Hotel was one of several establishments that made up the "men's quarter" along what then named Cherry Street. In the Gay Nineties, men gathered in the quarter to eat, drink and gamble in the restaurants, saloons and the new Utopia Hotel. The lively social life centered there was for men only.



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Climax Saloon, 210 4th Avenue North was built in 1887. The three-story solid masonry structure has an Italianate style galvanized iron façade that in all probability was chosen from a catalogue. This is the only such example know in Nashville. Four narrow, round-arched windows on the second and third floors each are recessed into a wall of rusticated and coursed ashlar and separated by slender Corinthian columns with capitals at the impost level. The end walls have rusticated and coursed ashlar buttresses with Corinthian capitals also at the impost level. A belt course separates the floors. Above the third floor is a frieze with decorated panels. Caryatids formerly separated these panels and supported the parapet. The three-story block extends only half the length of the alley. The basement and first story extend the full length and probably originally house the kitchen. The Printers Alley façade has no noteworthy details.

The properties meet criterion 5 as they are listed in the Printer's Alley National Register of Historic Places District. Staff suggests that the Commission recommend approval of the overlay for these eligible properties to the Council and adopt the design guidelines proposed for this new district.

METRO HISTORIC ZONING COMMISSION RECOMMENDATION

On July 15, 2015, the Metro Historic Zoning Commission recommended approval and adoption of the design guidelines for the Downtown Historic Preservation Zoning Overlay.

STAFF RECOMMENDATION

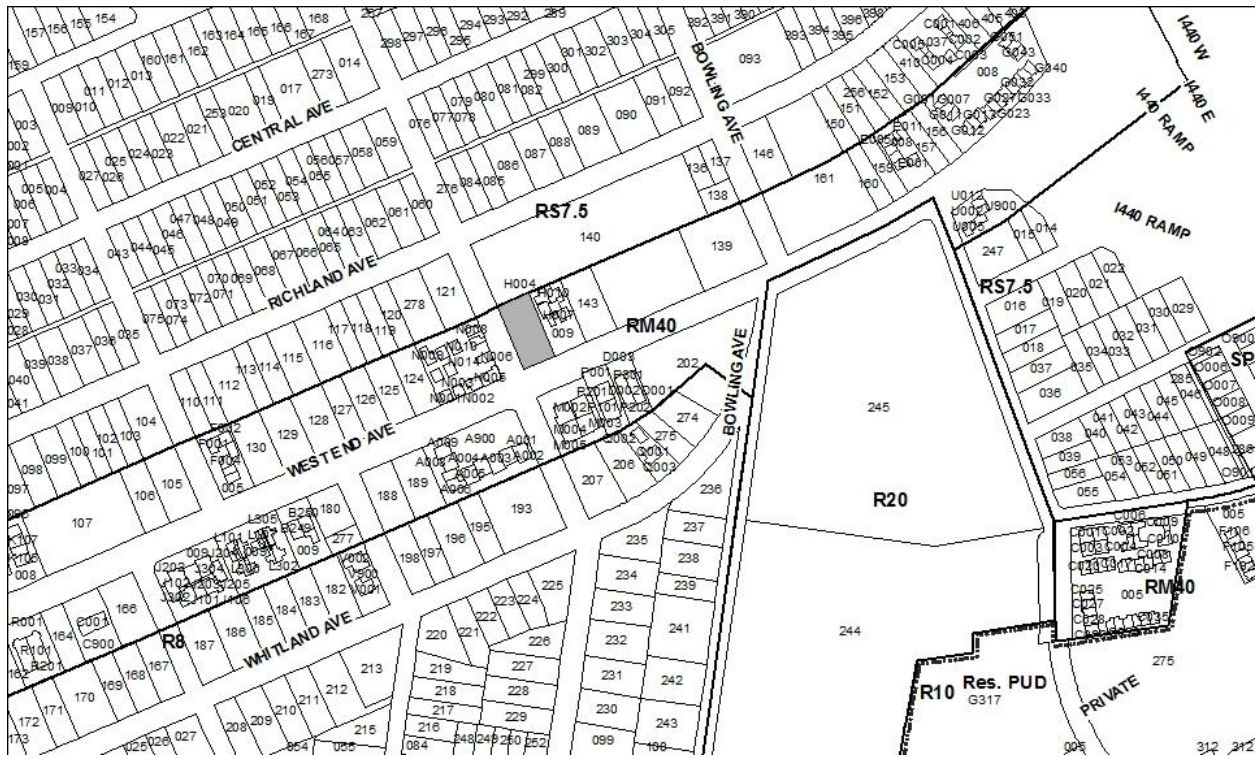
Staff recommends approval of the historic preservation overlay district for the requested properties.



SEE NEXT PAGE



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2015HL-015-001

WELCH LIBRARY HISTORIC LANDMARK OVERLAY DISTRICT

Map 104-09, Part of Parcel(s) 140

10, Green Hills - Midtown

24 (Jason Holleman)



Project No.	Historic Landmark Overlay 2015HL-015-001
Project Name	Welch Library
Council Bill	BL2015-1267
Council District	24 – Holleman
School District	9 – Frogge
Requested by	Welch College, applicant and owner.
Staff Reviewer	Sajid
Staff Recommendation	<i>Approve.</i>

APPLICANT REQUEST

Apply Historic Landmark Overlay District.

Historic Landmark Overlay

A request to apply a Historic Landmark Overlay District to a portion of property located at 3606 West End Avenue, at the northeast corner of Craighead Avenue and West End Avenue (0.46 acres), zoned Multi-Family Residential (RM40).

Existing Zoning

Multi-Family Residential (RM40) is intended for single-family, duplex, and multi-family dwellings at a density of 40 dwelling units per acre.

Proposed Zoning

Historic Landmark Overlay District (HL) A Historic Landmark is a building, structure, site or object, its appurtenances and the property it is located on, of high historical, cultural, architectural or archaeological importance; whose demolition or destruction would constitute an irreplaceable loss to the quality and character of Nashville and Davidson County.

CRITICAL PLANNING GOALS

- Preserves Historic Resources

The proposed Historic Landmark Overlay District is intended to preserve the historic structure on the property through the implementation of development guidelines by the Metro Historic Zoning Commission and staff.

GREEN HILLS - MIDTOWN COMMUNITY PLAN

T4 Urban Residential Corridor (T4 RC) is intended to preserve, enhance and create urban residential corridors. T4 RC areas are located along prominent arterial-boulevard or collector-avenue corridors that are served by multiple modes of transportation and are designed and operated to enable safe, attractive and comfortable access and travel for all users. T4 RC areas provide high access management and are served by moderately connected street networks, sidewalks, and existing or planned mass transit.

Consistent with Policy?

Yes. The policy encourages the protection and preservation of historic features.



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REQUEST DETAILS

The Metro Historic Zoning Commission (MHZC) considered this application at its June 17, 2015, meeting and recommended approval. The following background information was provided by the Metro Historical Commission staff:

Welch Library (Neal-Grizzard House)

The Welch Library (Neal-Grizzard House), a French Eclectic style home, was not included in the National Register boundaries for the Richland-West End district because it faced West End Avenue, which had several intrusions. Nevertheless, the building contributes to the character and history of the district and was one of the first to be constructed in the area. The nomination describes the district as a “largely unaltered picture of the suburban building scene in Nashville during the first three decades of this century, ranging from the large comfortable homes of the upper middle class to the more modest bungalows.”

The Neal-Grizzard House has a long association with the medical community. Edward M. Neal, president of Spurlock-Neal Company constructed the home c. 1907 and lived there until 1911. Mr. Neal, originally from Lebanon, was one of the original owners of Spurlock-Neal Company, a “drug-house” formed in 1886. Prior to that, Mr. Neal ran a successful retail drug store, according to the 1903 Merck’s Report. The outbuilding, which was used as a carriage house and servant quarters, was likely constructed at the same time since it matches the home in materials.

Dr. Grizzard owned the home from the late 1920s to 1965. “Notable Men of Tennessee” describes him as “one of Nashville’s leading surgeons.” Dr. Grizzard was House Surgeon at Nashville General Hospital from 1906-1908. He was not only a graduate of Vanderbilt University, but also a member of the teaching and surgical staffs. At least as early as 1951, J. and Mildred Hardeman lived in the carriage house. Mr. Hardeman was a chauffeur and so may have worked as such for the Grizzard family. Free Will Baptist College purchased the home in 1965 as part of an expansion of the College that had been in the neighborhood since 1942. A newspaper report of the sale described the house as having 20 rooms and that the college planned to use the building for classrooms and teachers’ offices.

To be considered as an historic landmark a building, structure, site or object must meet one or more of the following criteria:

1. The historic landmark is associated with an event that has made a significant contribution to local, state or national history;
2. It is associated with the lives of persons significant in local, state or national history; or
3. It embodies the distinctive characteristics of a type, period or method of construction, or that represents the work of a master, or that possesses high artistic value; or
4. It has yielded or may be likely to yield archaeological information important in history or prehistory; or
5. It is listed or is eligible for listing in the National Register of Historic Places.

METRO HISTORIC ZONING COMMISSION RECOMMENDATION

On June 17, 2015, the Metro Historic Zoning Commission recommended approval of the Historic Landmark Overlay.



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PUBLIC WORKS RECOMMENDATION

No exception taken

STAFF RECOMMENDATION

Staff recommends approval of the Historic Landmark Overlay District.



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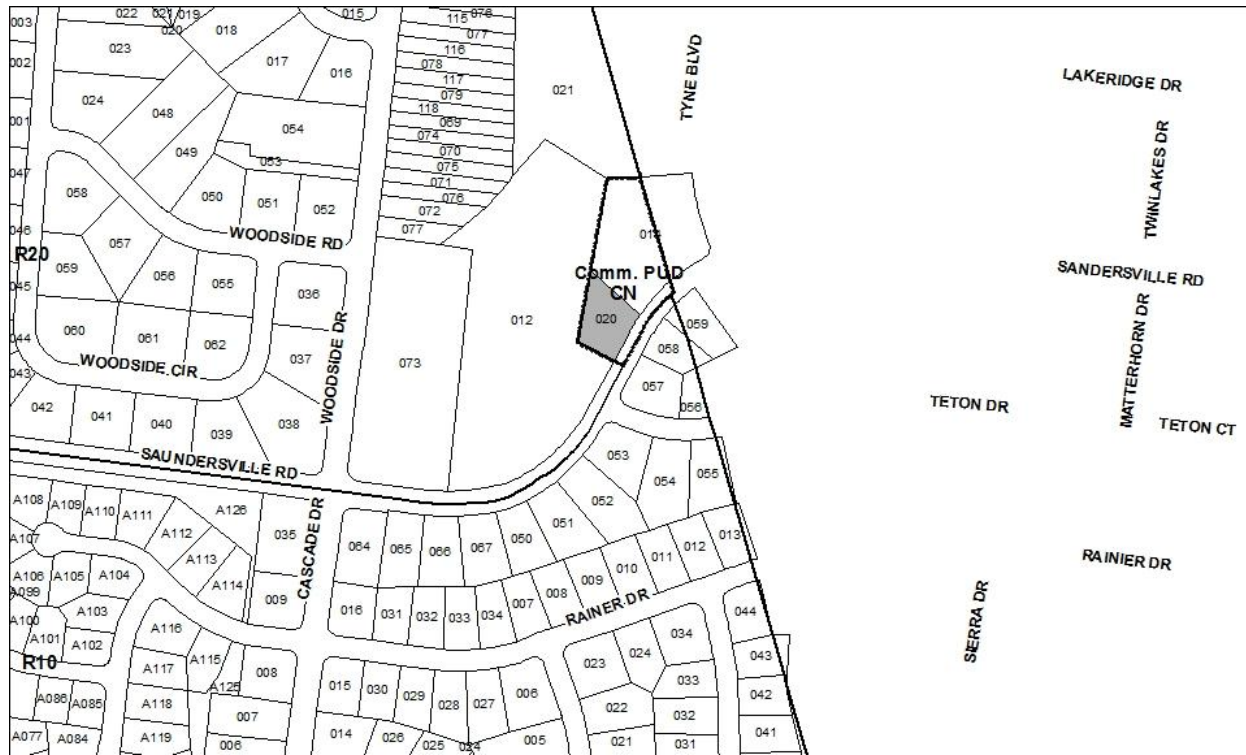


PLANNING COMMISSION ACTIONS

- **Planned Unit Developments**
- **Institutional Overlays**
- **Subdivision (Concept Plans)**
- **Subdivision (Final)**



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143-72P-001
SAUNDERSVILLE ROAD
Map 065, Parcel(s) 020
14, Donelson - Hermitage
11 (Larry Hagar)



Project No.	PUD Revision 143-72P-001
Project Name	Saundersville Road
Council District	11 – Hagar
School District	4 – Shepherd
Requested by	Stevens Design & consulting, applicant; 4331 Saundersville Rd, LLC, owner.
Staff Reviewer	Sajid
Staff Recommendation	<i>Approve with conditions.</i>

APPLICANT REQUEST

Revise preliminary plan and final site plan approval for a portion of the Saundersville Road PUD.

PUD Amendment

A request to revise the preliminary plan and for final site plan approval for a Commercial Planned Unit Development district located at 4331 Saundersville Road, approximately 800 feet east of Woodside Drive, zoned Commercial Neighborhood (CN) (.30 acres), to permit the addition of 616 square feet lease space to an existing building.

Existing Zoning

Commercial Neighborhood (CN) is intended for very low intensity retail, office, and consumer service uses which provide for the recurring shopping needs of nearby residential areas.

Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provision of an adequate roadway system or essential utilities and services. This PUD plan In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provision of essential utilities and streets. *This PUD is approved for commercial and office uses.*

CRITICAL PLANNING GOALS

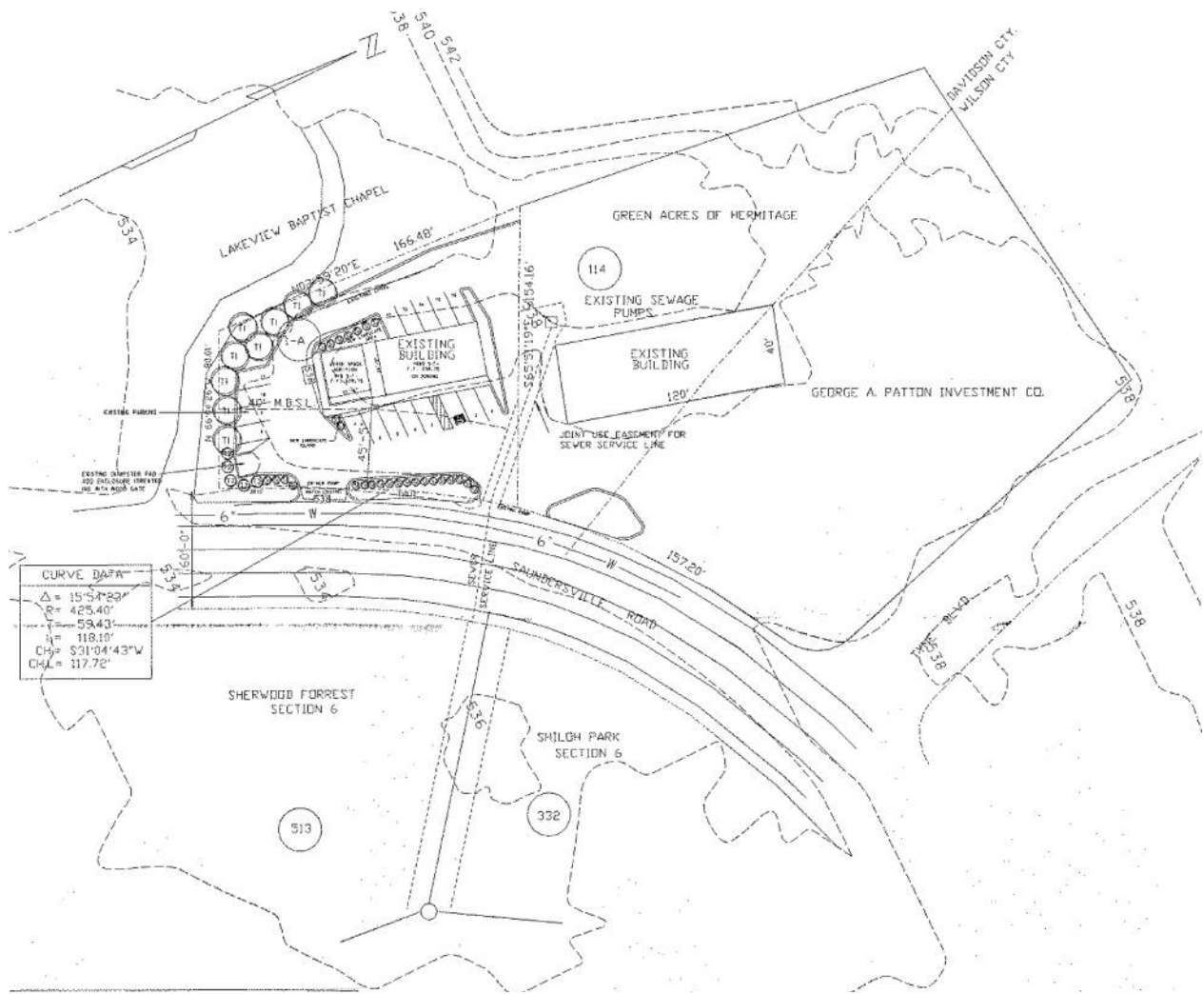
N/A

REQUEST DETAILS

The subject property is located northwest of the intersection of Saundersville Road and Tyne Boulevard, and includes property in both Davidson County and Wilson County. The proposal is for a revision to the preliminary plan and for final site plan approval to add 616 SF to the existing 1,680 square foot building. The site plan includes an existing single-story building. The plan complies with the Zoning Code requirements for parking for the existing and proposed retail uses and proposes to enclose the existing dumpster pad.



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Proposed Site Plan



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ANALYSIS

The Saundersville Road PUD was approved by Council in 1972 for 8,480 SF of office and commercial uses. The plan was revised in 2004 to permit a 772 square feet addition which, to date, has not been built.

The Zoning Code permits the Planning Commission to approve increases in floor area from what was approved by Council, as long as any increase does not exceed ten percent of the last Council approval. The requested revision proposes an increase of 616 square feet. The proposed addition is not in addition to the previously approved 772 SF addition which was not constructed. As a result, the proposal does not exceed ten percent of the Council approval plan.

No changes are being proposed that conflict with the concept of the Council approved plan. The addition is consistent with the concept of the PUD and is smaller than the previously approved addition that is unbuilt. Consequently, staff finds that the proposed revision is a minor modification.

Section 17.40.120.G permits the Planning Commission to approve “minor modifications” under certain conditions. Staff finds that the request is consistent with all the requirements of Section 17.40.120.G, and is provided below for review.

G. Status of Earlier Planned Unit Developments (PUDs). The following provisions shall apply to a planned unit development (PUD) approved under the authority of a previous zoning code and remaining a part of the official zoning map upon the enactment of this title.

1. The planned unit development (PUD) shall be recognized by this title according to the master development plan and its associated conditions specified in the PUD ordinance last approved by the metropolitan council prior to the effective date of the ordinance codified in this title.
2. The planning commission may consider and approve minor modifications to a previously approved planned unit development subject to the following limitations. All other modifications shall be considered by the planning commission as an amendment to the previously approved planned unit development and shall be referred back to the council for approval according to the procedures of Section 17.40.120(A)(5). That portion of a planned unit development master plan being amended by the council shall adhere to all provisions of this code:
 - a. In the judgment of the commission, the change does not alter the basic development concept of the PUD;
 - b. The boundary of the planned unit development overlay district is not expanded;
 - c. There is no change in general PUD classification (e.g. residential to any classification of commercial or industrial PUD; any change in general classification of a commercial PUD; or any change in general classification of an industrial PUD);
 - d. There is no deviation from special performance criteria, design standards, or other specific requirements made part of the enacting ordinance by the council;
 - e. There is no introduction of a new vehicular access point to an existing street, road or thoroughfare not previously designated for access;
 - f. There is no increase in the total number of residential dwelling units originally authorized by the enacting ordinance;



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- g. There is no change from a PUD approved exclusively for single-family units to another residential structure type;
- h. The total floor area of a commercial or industrial classification of PUD shall not be increased more than ten percent beyond the total floor area last approved by the council;
- i. If originally limited to office activities, the range of permitted uses in a commercial PUD shall not be expanded to broader classifications of retail, commercial or industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- j. If originally limited to office, retail and other general commercial activities, the range of permitted uses in a commercial PUD shall not be expanded to include industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- k. If originally limited to commercial activities, the range of permitted uses in a commercial PUD shall not be expanded to broader classifications of retail, commercial or industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- l. In the determination of the commission, the nature of the change will have no greater adverse impact on those environmentally sensitive features identified in Chapter 17.28 of this code than would have occurred had the development proceeded in conformance with the previous approval.

As the proposed revision keeps with the overall intent of the PUD and the final site plan is consistent with the Zoning Code, planning staff recommends approval of the request.

FIRE MARSHAL RECOMMENDATION

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review.

STORMWATER RECOMMENDATION

Approve

TRAFFIC & PARKING RECOMMENDATION

No exception taken



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WATER SERVICES RECOMMENDATION

Approve with conditions

- Approval does not apply to private water and sewer utility layout. These plans must be submitted for review and approval to Metro Water Permits before their construction may begin.

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- If sidewalks are required, then they should be shown on the plan per Public Works standards with the required curb and gutter.

No traffic table was prepared for this case, as it is not expected to generate additional traffic.

STAFF RECOMMENDATION

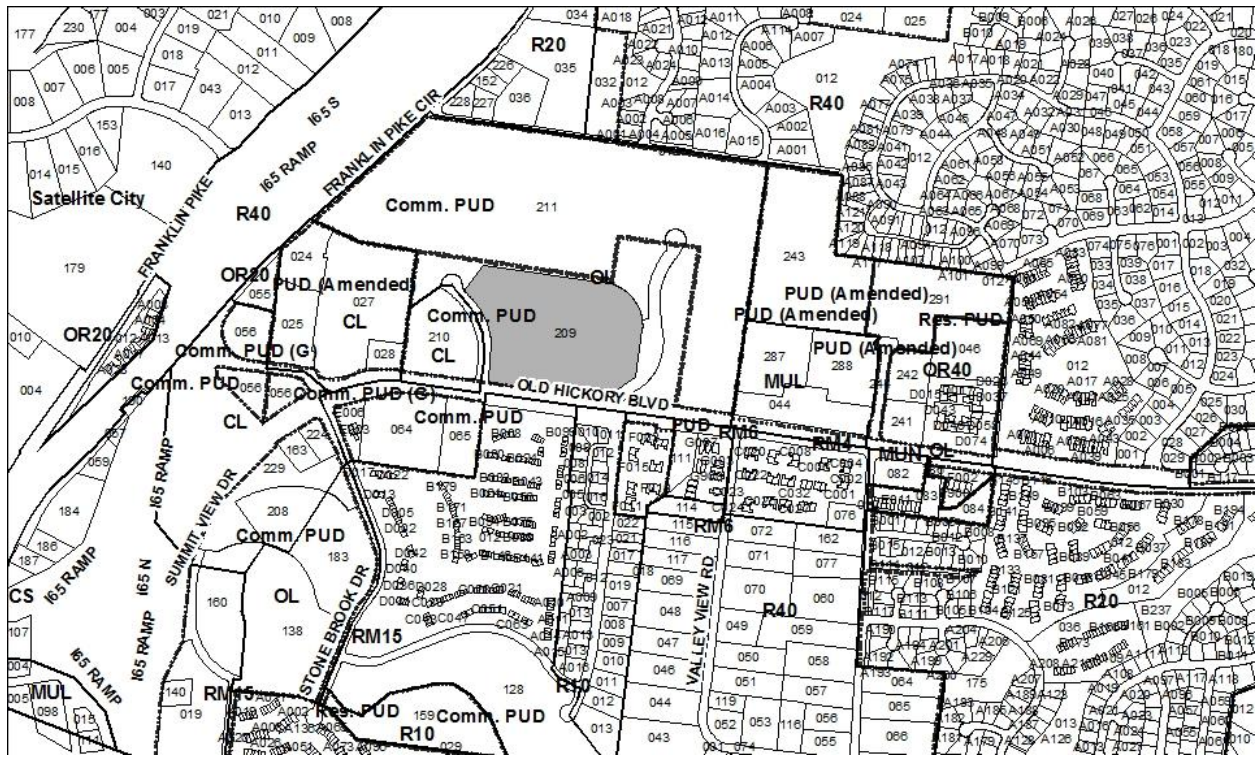
Staff recommends approval with conditions.

CONDITIONS

1. This approval does not include any signs. Signs in planned unit developments must be approved by the Metro Department of Codes Administration except in specific instances when the Metro Council directs the Metro Planning Commission to review such signs.
2. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
3. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Stormwater Management division of Water Services.
4. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Traffic Engineering Sections of the Metro Department of Public Works for all improvements within public rights of way.
5. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
6. Authorization for the issuance of permit applications will not be forwarded to the Department of Codes Administration until four additional copies of the approved plans have been submitted to the Metro Planning Commission.
7. The PUD final site plan as approved by the Planning Commission will be used by the Department of Codes Administration to determine compliance, both in the issuance of permits for construction and field inspection. Significant deviation from these plans may require reapproval by the Planning Commission and/or Metro Council.



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85-85P-002

BRENTWOOD COMMONS, LOT 1, PHASE 1

Map 160, Parcel(s) 209

12, Southeast

04 (Brady Banks)



Project No. 85-85P-002
Project Name Brentwood Commons, Lot 1, Phase 1
Council District 4 - Banks
School District 8 - Pierce
Requested by Kimley-Horn & Associates, applicant; Gateway Poplar, Inc., owner.

Staff Reviewer Thomas
Staff Recommendation *Defer unless a recommendation is received from all reviewing agencies prior to the Planning Commission meeting. If a recommendation is received, staff recommends approval with conditions.*

APPLICANT REQUEST

Revise a portion of a Planned Unit Development to permit the addition of one office building.

Revise PUD & Final Site Plan

A request to revise the preliminary plan and for final site plan approval for a portion of the Brentwood Commons Planned Unit Development Overlay District on property located at 750 Old Hickory Boulevard, at the corner of Old Hickory Boulevard and Brentwood Commons Way, zoned Office Limited (OL) (14.18 acres), to permit the development of one, five-story office building totaling 133,115 square feet.

Existing Zoning

Office Limited (OL) is intended for moderate intensity office uses.

Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provision of an adequate roadway system or essential utilities and services. In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provision of essential utilities and streets.

CRITICAL PLANNING GOALS

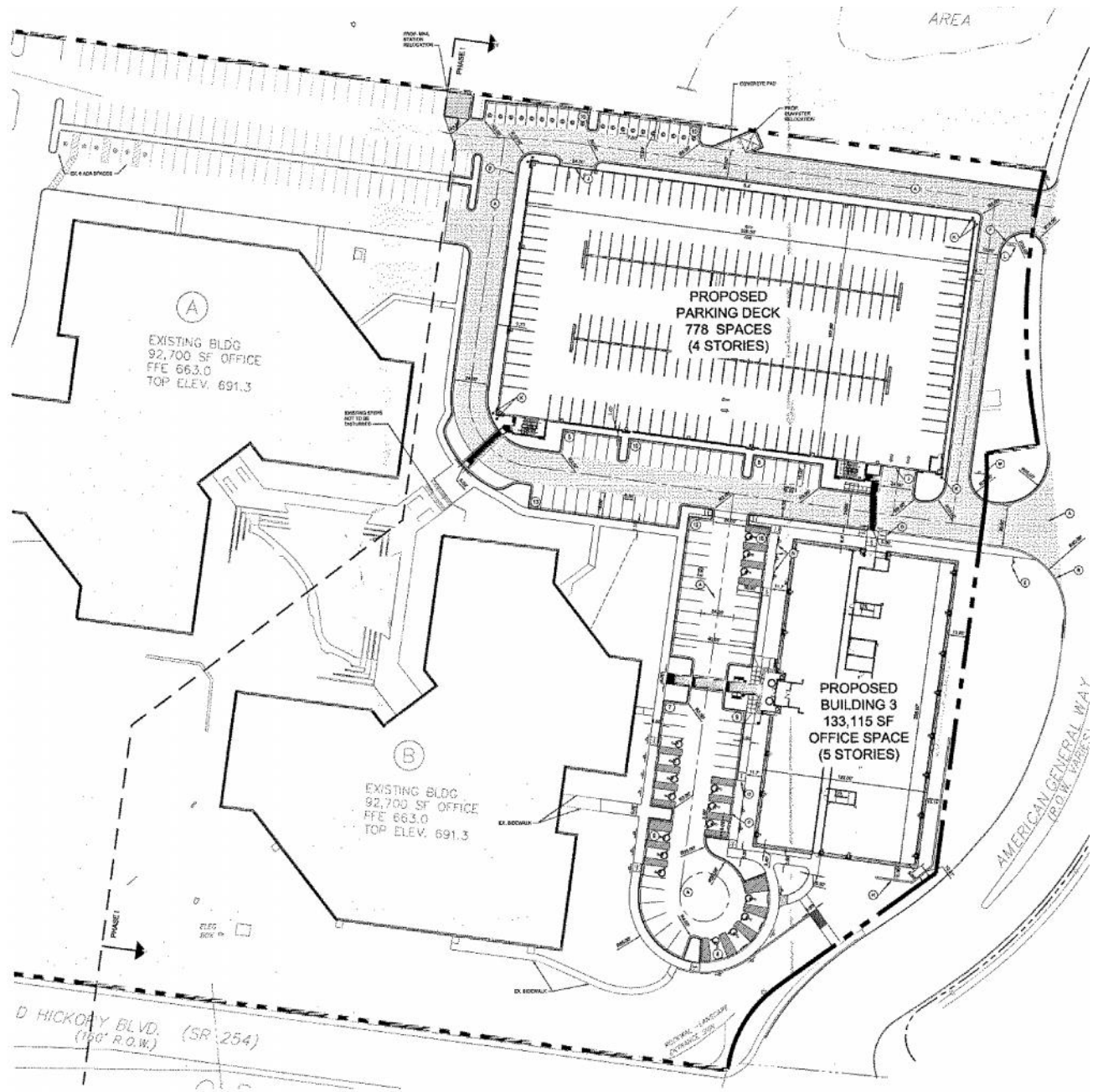
N/A

PLAN DETAILS

The subject PUD is located on the north side of Old Hickory Boulevard east of I-65. The site is developed and contains two office buildings totaling 185,400 square feet. The site is located in the Brentwood Commons PUD, which was approved in 1985 for a variety of office and associated uses.



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Proposed Site Plan



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Site Plan

The plan calls for one five-story office building along American General Way, east of the existing office buildings. The building is proposed to have 133,115 square feet of space. The plan also includes one four-story parking deck, immediately north of the proposed building, that includes 778 parking spaces.

ANALYSIS

The request is consistent with the overall concept of the Council approved plan. The Council approved plan was approved in 1985 for 1,307,553 square feet of office and associated uses. The PUD consists of two phases. Phase 1, which includes this site, was approved for 510,553 square feet of floor area. Phase 2 was approved for 797,000 square feet of floor area.

A revision to the preliminary plan for a portion of the Brentwood Commons PUD was approved in 2014 for two office buildings in Phase 1 totaling 250,000 square feet, which brought the overall floor area in Phase 1 to 562,400. The difference of 51,847 square feet approved with the preliminary plan did not increase the total floor area over 10% of what was originally approved by Council. The Zoning Code permits the Planning Commission to approve changes that do not increase the floor area over 10% of what was approved by Council.

This request increases the square footage of the five-story building by 8,115 square feet; however, the remaining available floor area in the PUD will be 582,793 square feet. This includes the remaining 511,000 square feet reserved for Phase 2, as well as the remaining 71,793 square feet of additional floor area permitted without requiring Council approval. The remaining 71,793 square feet is available to either Phase 1 or Phase 2.

Section 17.40.120.G permits the Planning Commission to approve “minor modifications” under certain conditions. Staff finds that the request is consistent with all the requirements of Section 17.40.120.G, which is provided below for review.

G. Status of Earlier Planned Unit Developments (PUDs). The following provisions shall apply to a planned unit development (PUD) approved under the authority of a previous zoning code and remaining a part of the official zoning map upon the enactment of this title.

1. The planned unit development (PUD) shall be recognized by this title according to the master development plan and its associated conditions specified in the PUD ordinance last approved by the metropolitan council prior to the effective date of the ordinance codified in this title.
2. The planning commission may consider and approve minor modifications to a previously approved planned unit development subject to the following limitations. All other modifications shall be considered by the planning commission as an amendment to the previously approved planned unit development and shall be referred back to the council for approval according to the procedures of Section 17.40.120(A)(5). That portion of a planned unit development master plan being amended by the council shall adhere to all provisions of this code:
 - a. In the judgment of the commission, the change does not alter the basic development concept of the PUD;
 - b. The boundary of the planned unit development overlay district is not expanded;



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- c. There is no change in general PUD classification (e.g. residential to any classification of commercial or industrial PUD; any change in general classification of a commercial PUD; or any change in general classification of an industrial PUD);
- d. There is no deviation from special performance criteria, design standards, or other specific requirements made part of the enacting ordinance by the council;
- e. There is no introduction of a new vehicular access point to an existing street, road or thoroughfare not previously designated for access;
- f. There is no increase in the total number of residential dwelling units originally authorized by the enacting ordinance;
- g. There is no change from a PUD approved exclusively for single-family units to another residential structure type;
- h. The total floor area of a commercial or industrial classification of PUD shall not be increased more than ten percent beyond the total floor area last approved by the council;
- i. If originally limited to office activities, the range of permitted uses in a commercial PUD shall not be expanded to broader classifications of retail, commercial or industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- j. If originally limited to office, retail and other general commercial activities, the range of permitted uses in a commercial PUD shall not be expanded to include industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- k. If originally limited to commercial activities, the range of permitted uses in a commercial PUD shall not be expanded to broader classifications of retail, commercial or industrial activities, unless such activities are otherwise permitted by the underlying base zone district. The permitted uses within the planned unit development shall be those specifically authorized by the council through the adopted master development plan, or by the existing base zone district beneath the overlay, whichever is more permissive.
- l. In the determination of the commission, the nature of the change will have no greater adverse impact on those environmentally sensitive features identified in Chapter 17.28 of this code than would have occurred had the development proceeded in conformance with the previous approval.
- m. In the judgment of the commission, the planned unit development or portion thereof to be modified does not meet the criteria for inactivity of Section 17.40.120.H.4.a.

FIRE MARSHAL'S OFFICE

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review.
- Fire hydrants to be located within 100' of the Fire Department Connections.



Metro Planning Commission Meeting of 07/23/2015

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Comply with MPW Traffic Engineer

TRAFFIC & PARKING RECOMMENDATION

Conditions if approved

- Construction documents for final PUD plan shall include off site road construction plans and signal plans.
- In accordance with the recommendations of the TIS to off-set the additional traffic generated from the proposed Lot 1 development, the following traffic improvements are required:
 - A. Developer of phase 1 building D and garage 2 shall construct and install the following roadway improvements:
 1. At Old Hickory Boulevard (SR-254) at Oakes Drive / American General Way
 - a. Provide an additional eastbound through lane along Old Hickory Boulevard, resulting in three (3) eastbound through lanes at this intersection. Construct this additional travel lane between Driveway / Brentwood Commons Way and Oakes Drive / American General Way.
 - b. Modify the existing EB Rt turn lane on Old Hickory Blvd. at the driveway opposite Brentwood Commons Way pavement markings to convert the existing right-turn lane into a through lane.
 - c. Provide pedestrian accommodations that are compliant with American with Disabilities Act (ADA) regulations on all four (4) approaches of this intersection. Install crosswalk pavement markings, curb ramps, truncated dome surfaces, pedestrian signal heads, and push buttons.
 - B. Developer of phase 2 building C and garage 1 shall construct and install the following roadway improvements:
 1. At Brentwood Commons Way
 - a. Install a sidewalk along the east side of Brentwood Commons Way between Old Hickory Boulevard and existing cul-de-sac.
 2. At Old Hickory Boulevard (SR-254) at Driveway / Brentwood Commons Way
 - a. Provide pedestrian accommodations that are compliant with Americans with Disabilities Act (ADA) regulations on all four (4) approaches of this intersection. Install crosswalk pavement markings, curb ramps, truncated dome surfaces, pedestrian signal heads, and push buttons.
 - b. Provide a westbound right-turn lane along Old Hickory Boulevard at Brentwood Commons Way with a minimum storage length of 150 feet and transitions per AASHTO standards.

STORMWATER RECOMMENDATION

Comments are anticipated; however, the application has not been reviewed by Stormwater at this time.



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WATER SERVICES RECOMMENDATION

Returned

Our original comments still apply to the latest plan revision (stamped received July 1, 2015):

- Please pay capacity fees.
- If the proposed hydrant is to be a public hydrant, than public construction plans must be submitted and approved prior to this Final Site plan approval.

STAFF RECOMMENDATION

Staff recommends deferral unless a recommendation of approval is received from all reviewing agencies prior to the Planning Commission meeting. If a recommendation is received, staff recommends approval with conditions.

CONDITIONS (if approved)

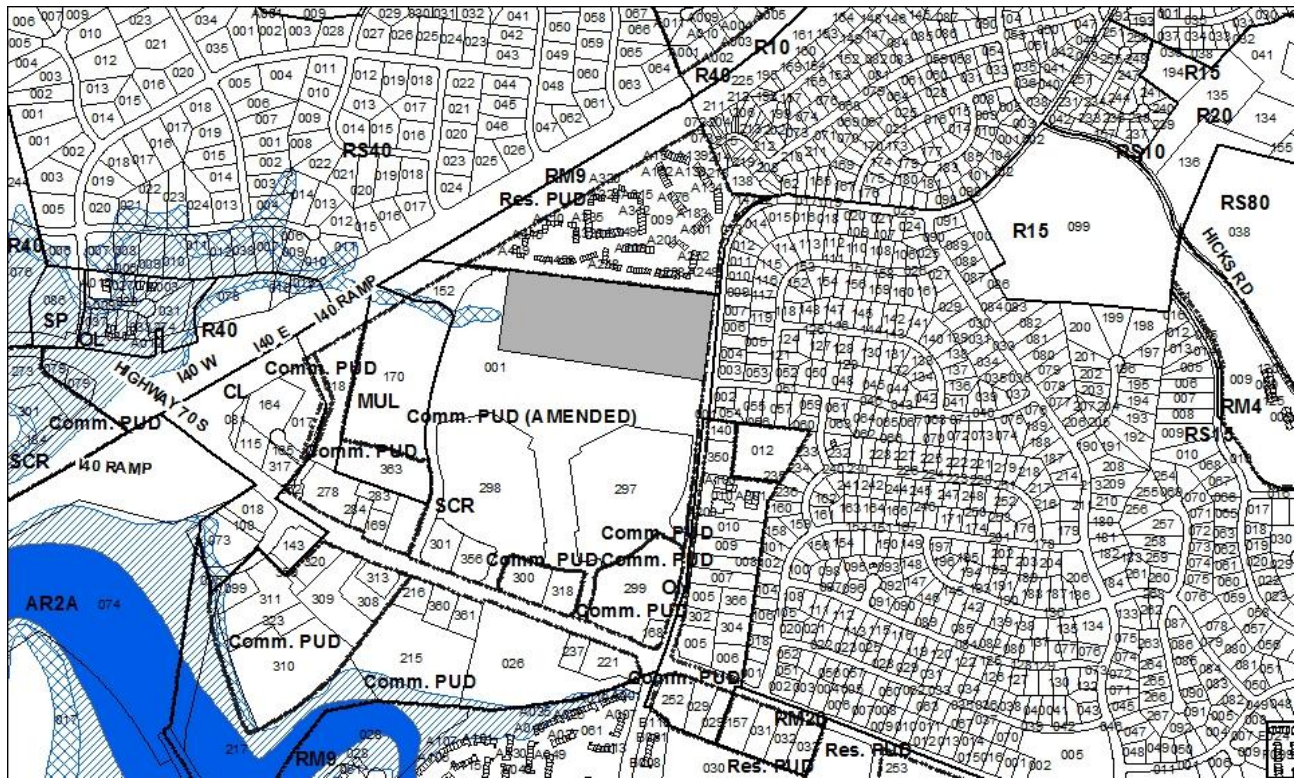
1. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Stormwater Management division of Water Services.
2. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Traffic Engineering Sections of the Metro Department of Public Works for all improvements within public rights of way.
3. This approval does not include any signs. Signs in planned unit developments must be approved by the Metro Department of Codes Administration except in specific instances when the Metro Council directs the Metro Planning Commission to review such signs.
4. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
5. Authorization for the issuance of permit applications will not be forwarded to the Department of Codes Administration until four additional copies of the approved plans have been submitted to the Metro Planning Commission.
6. The PUD final site plan as approved by the Planning Commission will be used by the Department of Codes Administration to determine compliance, both in the issuance of permits for construction and field inspection. Significant deviation from these plans may require reapproval by the Planning Commission and/or Metro Council.
7. A corrected copy of the PUD final site plan incorporating the conditions of approval by the Planning Commission shall be provided to the Planning Department prior to or with the final plat application or, when no final plat application is required, prior to the issuance of any permit for this property.



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Metro Planning Commission Meeting of 07/23/2015



94-71P-003
BELLEVUE CENTER (CRESCENT BELLEVUE)
Map 142, Part of Parcel(s) 001
06, Bellevue
22 (Sheri Weiner)



Project No.	Planned Unit Development 94-71P-003
Project Name	Bellevue Center (Crescent Bellevue)
Council District	22 – Weiner
School District	9 – Frogge
Requested by	Crescent Acquisitions, LLC, applicant; Bellevue Development, LLC, owner.
Staff Reviewer	Swaggart
Staff Recommendation	<i>Approve with conditions.</i>

APPLICANT REQUEST

Revise PUD and final site plan.

PUD Revision and Final Site Plan

A request to revise the preliminary plan and for final site plan approval for a portion of a Planned Unit Development Overlay District for a portion of property located at 7620 Highway 70 South, west of Sawyer Brown Road, zoned Shopping Center Regional (SCR) (16.47 acres), to permit a 355 unit multifamily development.

Existing Zoning

Shopping Center Regional (SCR) is intended for high intensity retail, office, and consumer service uses for a regional market area.

Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provision of an adequate roadway system or essential utilities and services. This PUD plan In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provision of essential utilities and streets.

CRITICAL PLANNING GOALS

N/A

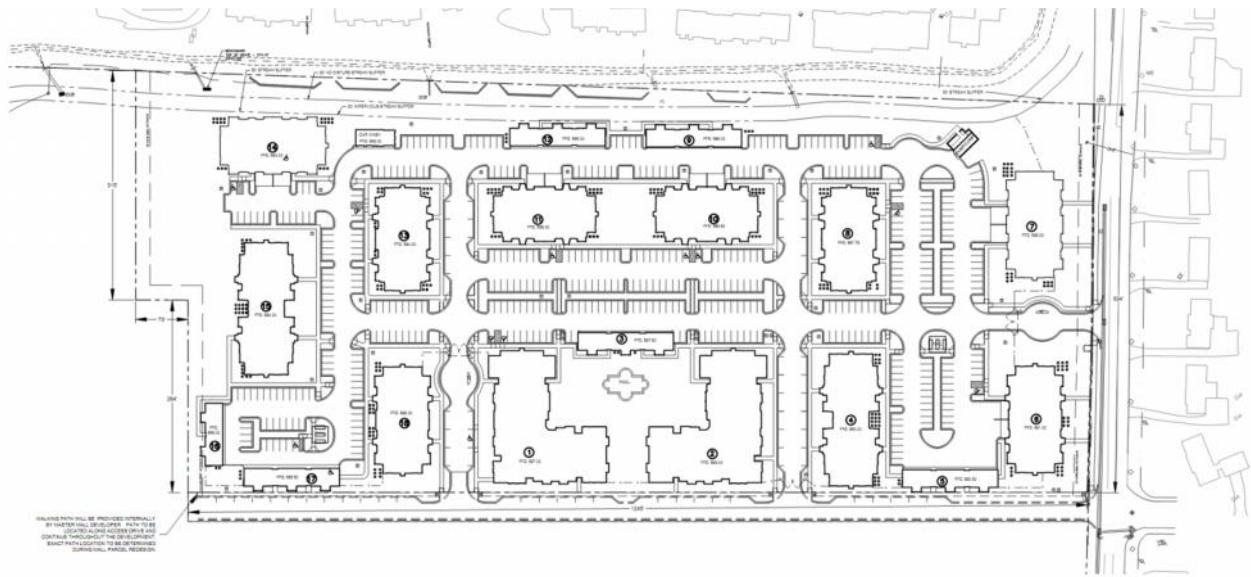
PLAN DETAILS

The approximately 20 acre site is part of the old Bellevue Mall Planned Unit Development (PUD). The overall PUD consists of approximately 102 acres and includes the subject site and several other parcels along HWY 70.

The original PUD was approved by Council in 1971. Since that time, the plan has been revised numerous times. The PUD was recently amended (BL2015-1028) to permit a mixed-use



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Proposed Site Plan



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development consisting of residential, office and commercial. The previous council approved plan did not permit residential. This proposal is the first phase of the amendment area and includes the only the residential portion if the PUD.

Site Plan

The plan calls for a total of 335 units with an overall density of 20 units per acre. Units are distributed in 18 individual buildings. A majority of the buildings are three stories in height and two buildings are two stories in height. Parking is provided per Metro Zoning Code and includes 548 spaces. The plan calls for an eight foot wide sidewalk and six foot planting strip along Sawyer Brown Road.

ANALYSIS

Staff finds that the proposed revision is consistent with the concept plan approved by Metro Council. It is also consistent with Zoning Code requirements. The primary change is to the layout, which is typical between preliminary and final. Since staff finds the plan consistent with the concept plan approved by Council and Zoning Code requirements, then staff is recommending approval with conditions.

FIRE MARSHAL'S OFFICE

Approve with conditions

- Fire Code issues for the structures will be addressed at permit application review.

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.

TRAFFIC AND PARKING

Approve with conditions

- Comply with all TIS requirements

STORMWATER RECOMMENDATION

Approved with conditions

Comply with minor changes to stormwater facilities as required.

HARPETH VALLEY UTILITY DISTRICT

No issues

STAFF RECOMMENDATION

Staff recommends approval with conditions. As proposed the plan is consistent with the concept plan approved by Council and Zoning Code requirements.

CONDITIONS

1. Building permits shall not be issued until the necessary demolition and improvements have been made to the neighboring phase.



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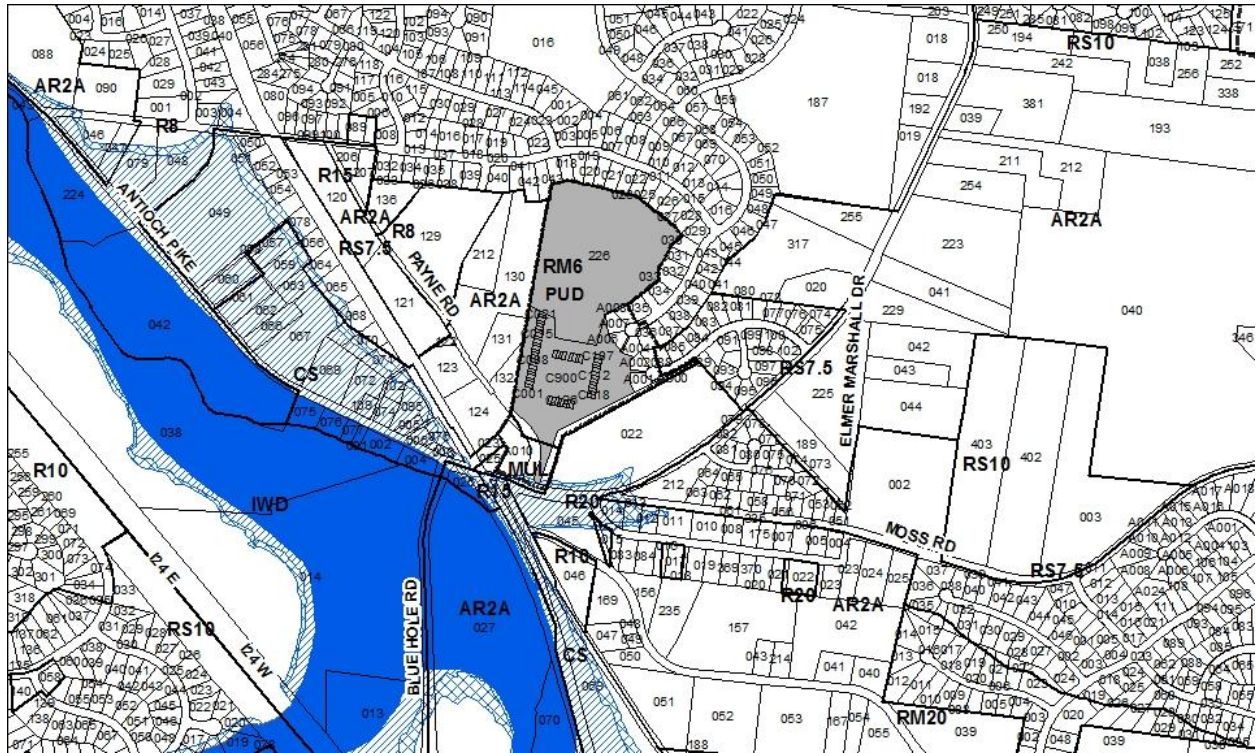
2. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Stormwater Management division of Water Services.
3. Prior to the issuance of any permits, confirmation of PUD final site plan approval of this proposal shall be forwarded to the Planning Commission by the Traffic Engineering Sections of the Metro Department of Public Works for all improvements within public rights of way.
4. This approval does not include any signs. Signs in planned unit developments must be approved by the Metro Department of Codes Administration except in specific instances when the Metro Council directs the Metro Planning Commission to review such signs.
5. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
6. Authorization for the issuance of permit applications will not be forwarded to the Department of Codes Administration until four additional copies of the approved plans have been submitted to the Metro Planning Commission.
7. The PUD final site plan as approved by the Planning Commission will be used by the Department of Codes Administration to determine compliance, both in the issuance of permits for construction and field inspection. Significant deviation from these plans may require reapproval by the Planning Commission and/or Metro Council.
8. A corrected copy of the PUD final site plan incorporating the conditions of approval by the Planning Commission shall be provided to the Planning Department prior to or with the final plat application or, when no final plat application is required, prior to the issuance of any permit for this property.



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Metro Planning Commission Meeting of 07/23/2015



2004P-004-001

CARROLTON STATION

Map 149, Parcel(s) 226

Map 149-13-0-A, Parcel(s) 001-008, 010

Map 149-13-0-C, Parcel(s) 001-021, 099-126, 900

13, Antioch - Priest Lake

28 (Duane A. Dominy)



Project No.	Planned Unit Development 2004P-004-001
Project Name	Carrolton Station
Council District	28 – Dominy
School District	6 – Hunter
Requested by	Civil Site Design Group, applicants for Jericho Properties, LLC, owner.
Staff Reviewer	Swaggart
Staff Recommendation	<i>Approve with conditions.</i>

APPLICANT REQUEST

Revise PUD and to permit 60 multi-family units.

Revise PUD & Final Site Plan

A request to revise the preliminary site plan for a portion of a Planned Unit Development located at Una-Antioch Pike unnumbered, 301, 303, 305, 307, 308, 309, 311, 313, 315, 317, 319, 321, 323, 325, 327, 329, 331, 333, 335, 337, 339, 341 Carrolton Station Drive, 558, 556, 554, 552, 550, 548, 546, 544, 542, 540, 538, 536, 534, 532, 530, 528, 526, 524, 522, 520, 514, 512, 510, 508, 506, 504, 502, 505 Adler Road abutting the north margin of Una-Antioch Pike opposite Hickory Hollow Parkway, zoned Multi-Family (RM6) and Mixed Use Limited (MUL), (20.93 acres), to permit 60 multi-family units were 126 are currently permitted.

Existing Zoning

Multi-Family (RM6) is intended for single-family, duplex, and multi-family dwellings at a density of six dwelling units per acre. The density in this PUD is controlled by the PUD.

Mixed Use Limited (MUL) is intended for a moderate intensity mixture of residential, retail, restaurant, and office uses.

Planned Unit Development Overlay District (PUD) is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by the conventional zoning provisions of this title. The PUD district may permit a greater mixing of land uses not easily accomplished by the application of conventional zoning district boundaries, or a framework for coordinating the development of land with the provision of an adequate roadway system or essential utilities and services. This PUD plan In return, the PUD district provisions require a high standard for the protection and preservation of environmentally sensitive lands, well-planned living, working and shopping environments, and an assurance of adequate and timely provision of essential utilities and streets.

CRITICAL PLANNING GOALS

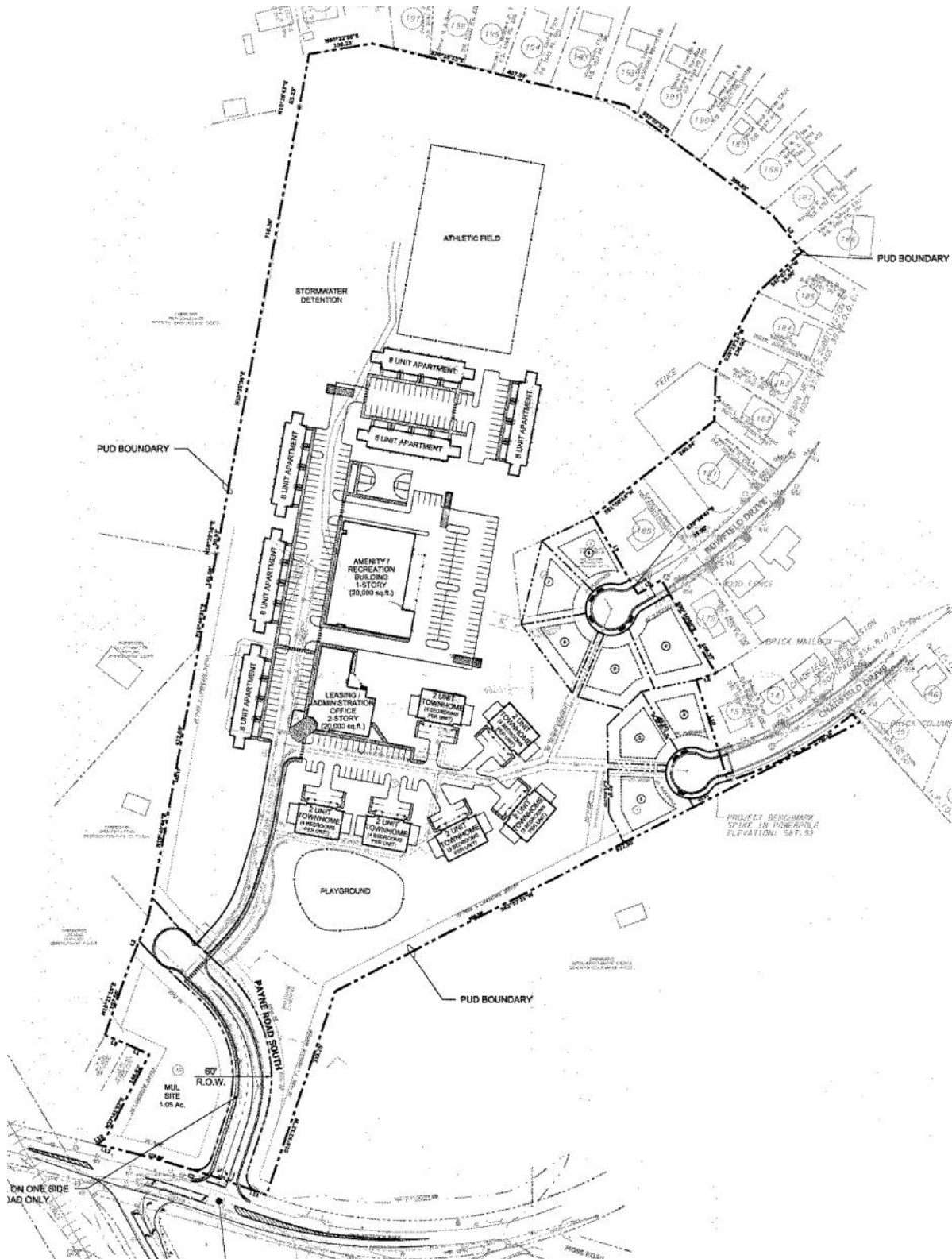
N/A

PLAN DETAILS

The subject PUD is located on the north side of Una-Antioch Pike across from where Hickory Hollow Parkway bisects Una-Antioch Pike. The PUD was originally approved in 2004 for a maximum of 16 single-family lots, 139 multi-family units and 14,000 square feet of commercial



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Proposed Site Plan



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uses (BL2004-161). In 2005, a final site plan was subsequently approved for 126 multi-family units and eight single-family lots. The portion of the PUD that is the subject of this revision was originally approved for 139 multi-family units and has final site plan approval for 126 multi-family units.

Site Plan

The plan calls for 60 multi-family units and amenity areas. Access into the site is proposed from Payne Road South which connects to Una-Antioch Pike to the south. Parking is provided on site and the number of spaces proposed is per Metro Code requirements. Access remains from the currently approved locations.

ANALYSIS

Staff finds that the proposed revision is consistent with the concept approved by Metro Council. As proposed the plan reduces the overall density of the PUD while maintaining the residential type (multi-family) currently approved. The request is also consistent with Metro Zoning Code requirements. Staff is also including applicable conditions from the previous approval.

FIRE MARSHAL'S OFFICE

Approve with Conditions

Fire Code issues for the structures will be addressed at permit application review.

PUBLIC WORKS RECOMMENDATION

No Exceptions Taken

TRAFFIC & PARKING RECOMMENDATION

Condition if approved

- Modify note on sheet C 2.00 addressing roadway improvements. Developer of residential land use shall construct left turn lane on Una- Antioch Pk at Payne Road and provide adequate sight distance when directed by Metro traffic engineer.
- Remaining roadway improvements on Hickory Hollow Parkway, per previous conditions of approval, shall be constructed by developer of MUL 1.05 acre site.

STORMWATER RECOMMENDATION

Approve

WATER SERVICES

Approved with conditions

Approved as a Preliminary PUD/Site Plan only. Revised public construction plans must be approved prior to Final SP approval.

STAFF RECOMMENDATION

Staff recommends approval with condition as it is consistent with the overall preliminary plan approved by Council and is consistent with zoning requirements.



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CONDITIONS

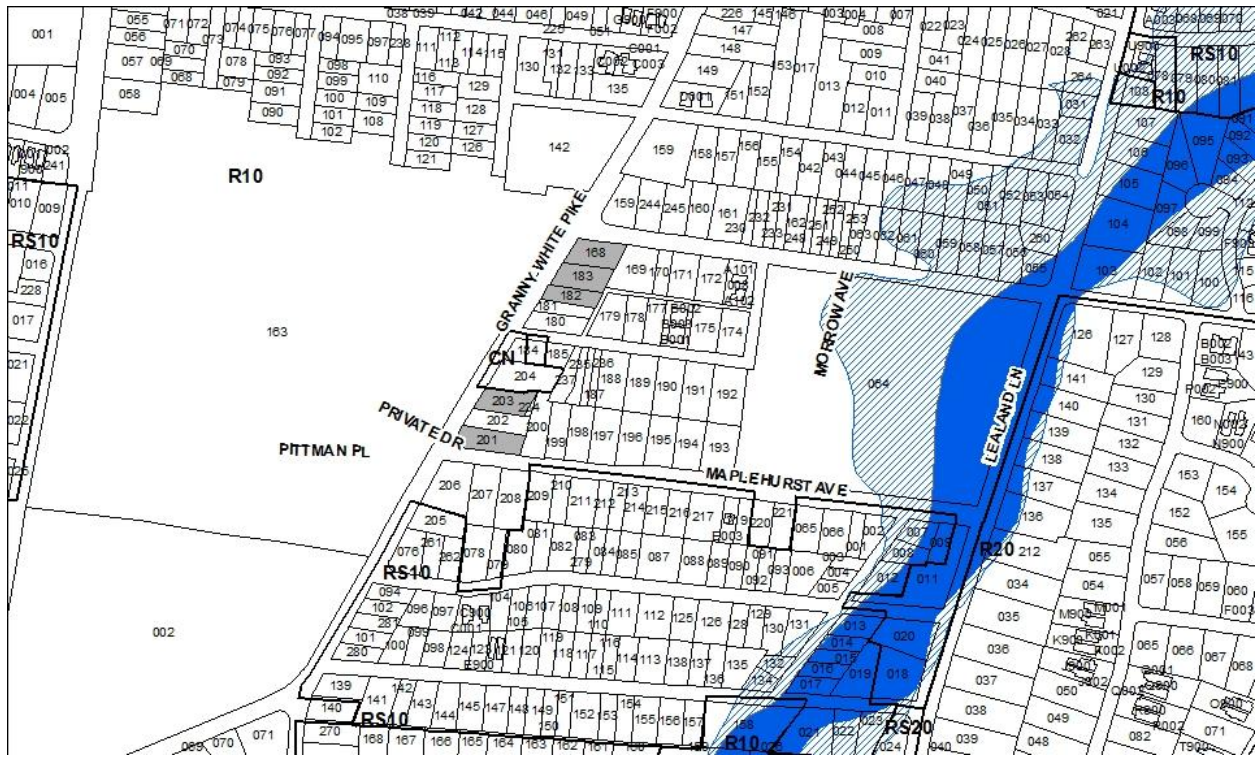
1. A Tree Preservation / Removal and Grading Boundary Plan (24x36) shall be submitted prior to, or in conjunction with, the submittal of the Final PUD application.
2. The applicant shall take all practical measures to protect all preserved trees and preserved hillside areas during construction. All trees proposed for preservation must be appropriately barricaded, must be kept free from debris and fill.
3. The three internal open space areas shall be planted / landscaped in a way that provides passive or active recreational areas for the residents of the townhouse community. These open space areas shall not be used for any part of the stormwater management system.
4. The perimeter of all stormwater detention areas shall be planted above and beyond the minimum Code requirement for landscaping amounts so as to limit access, as well as for aesthetic purposes.
5. All trash receptacles and dumpsters shall be located in inconspicuous areas of the development, but shall still be easily accessible to sanitation services.
6. The proposed sidewalk / path leading from the townhouse development to the two MUL parcels shall be lighted so as to provide safe pedestrian access to these areas at night.
7. This approval does not include any signs. Signs in planned unit developments must be approved by the Metro Department of Codes Administration except in specific instances when the Metro Council directs the Metro Planning Commission to review such signs.
8. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
9. If the PUD final site plan or final plat indicates that there is less acreage than what is shown on the approved preliminary plan, the final site plan shall be appropriately adjusted to show the actual total acreage, which may require that the total number of dwelling units or total floor area be reduced.



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Metro Planning Commission Meeting of 07/23/2015



2006IN-001-006
LIPSCOMB UNIVERSITY
Map 117-16, Parcel(s) 168, 182, 183, 201, 203
10, Green Hills - Midtown
D District - Major Institutional
25 (Sean McGuire)



Project No.	Institutional Overlay 2006IN-001-006
Project Name	Lipscomb University (Minor Modification)
Council District	25 – McGuire
School District	8 – Pierce
Requested by	Tuck-Hinton Architects, applicant, for David Lipscomb University, owner.
Staff Reviewer	Sajid
Staff Recommendation	<i>Approve with conditions.</i>

APPLICANT REQUEST

Clarify the use of 5 existing buildings

Institutional Overlay – Minor Modification to Preliminary Plan

A request for a minor modification of a portion of the preliminary plan for the Lipscomb University Institutional Overlay district for properties located at 3900, 3902, 3904, 4012 and 4020 Granny White Pike, south of Caldwell Lane (1.77 acres) zoned One and Two-Family Residential (R10), to clarify the use for the properties.

Existing Zoning

One and Two Family Residential (R10) requires a minimum 10,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 4.63 dwelling units per acre, including 25 percent duplex lots.

Institutional Overlay (IO) District is intended for colleges, universities, and other specialized community uses within or near residential areas.

CRITICAL PLANNING GOALS

N/A

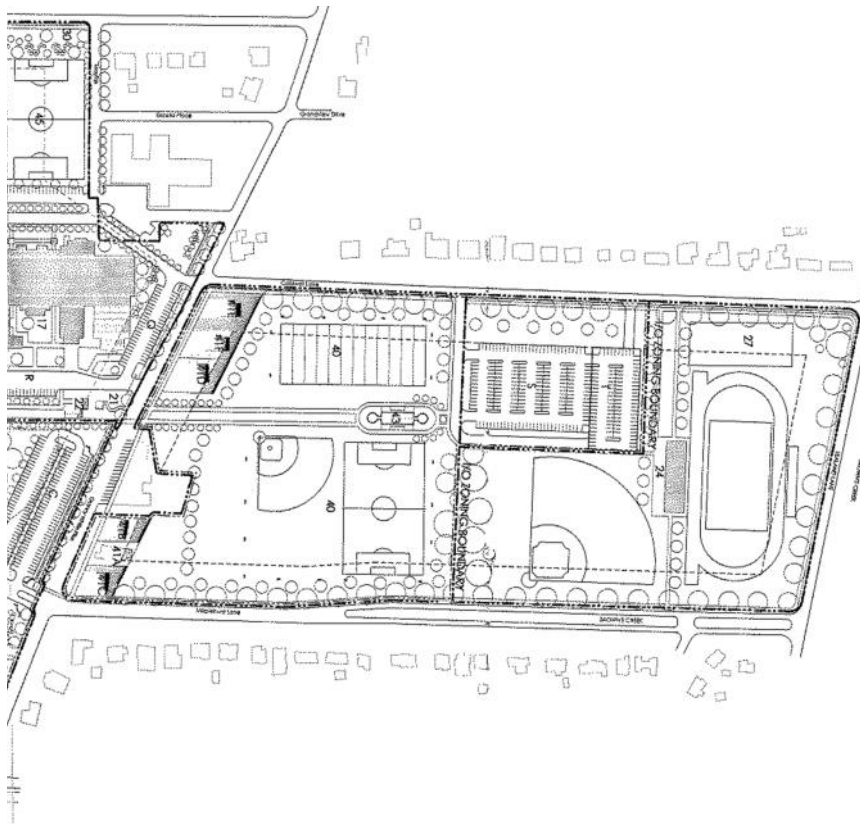
REQUEST DETAILS

The request is to reclassify the use of five existing residential structures that front Granny White Pike to offices, conference room and/or small classrooms to serve the university. The subject property is included in the Institutional Overlay, and the currently approved master plan does not specify the use of these structures. No changes to the building or parking are proposed with this minor modification.

The Zoning Code permits the Planning Commission to approve minor modifications to the approved campus master plan as long as the proposed changes do not include the following: an increase of more than 10% of the approved gross floor area, building setback, lot coverage, landscaping requirement, parking requirement, or dimensional requirement relating to fences or walls. The requested revision proposes to reclassify the use of structures that are part of the approved master plan. The modification does not include any building additions or parking lot additions or modifications.



Metro Planning Commission Meeting of 07/23/2015



Proposed Site Plan



Metro Planning Commission Meeting of 07/23/2015

FIRE RECOMMENDATION

N/A

STORMWATER RECOMMENDATION

Approved

PUBLIC WORKS RECOMMENDATION

Approve with conditions

- The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- Comply with MPW Traffic Engineer

TRAFFIC & PARKING RECOMMENDATION

Conditional if approved

- Refurbish crosswalk striping and install a pedestrian landing per ADA standards at Caldwell/Granny White intersection prior to use and occupancy of 41E,41F, and 41D buildings.

WATER SERVICES RECOMMENDATION

Approve

STAFF RECOMMENDATION

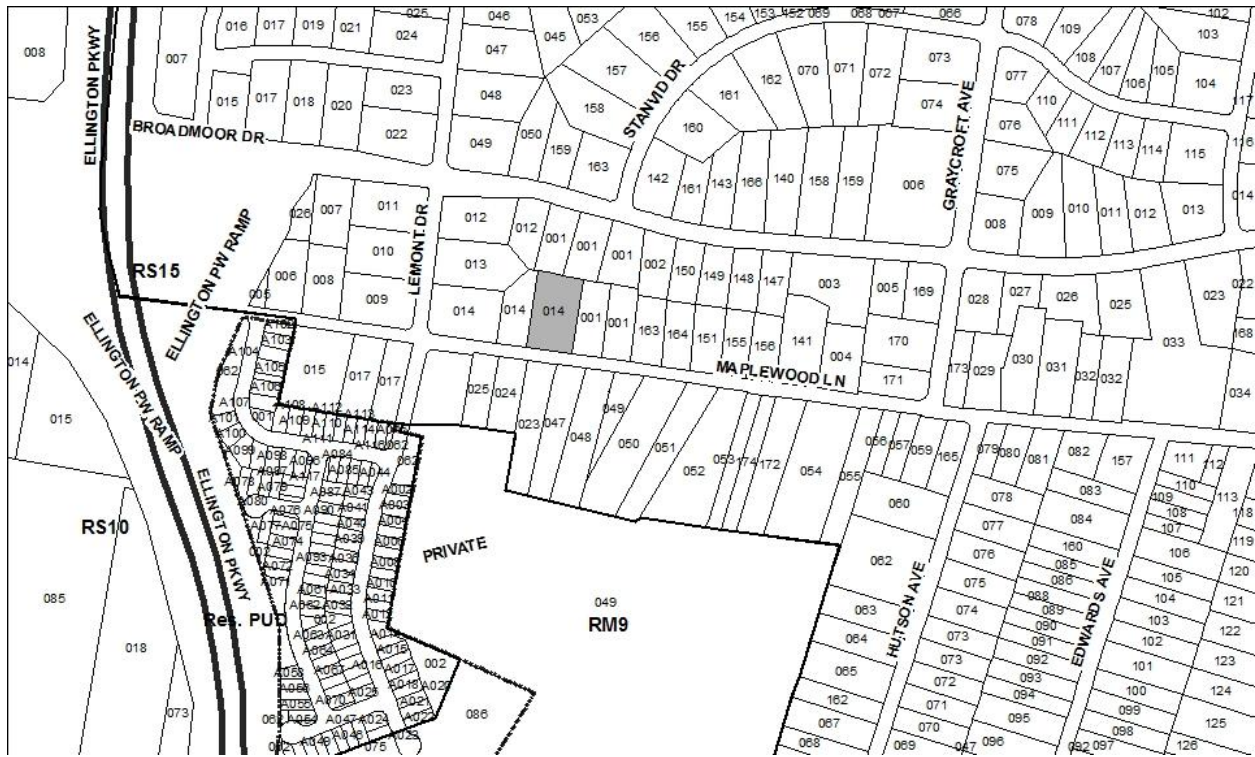
Staff recommends approval of the minor modification request as the approved master plan identifies these structures to remain, and the structures front on Granny White Pike which is classified as an arterial street on the Major and Collector Street Plan.

CONDITIONS

1. The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits.
2. Approval shall not include any changes to the existing parking layout.
3. Approval shall not include the addition any signage or lighting for the structures that are the subject of this minor modification.



Metro Planning Commission Meeting of 07/23/2015



2015S-081-001
H.C. PEARSON PROPERTY, RESUB TRACT B
Map 061-05, Parcel(s) 014
05, East Nashville
08 (Karen Bennett)



Project No.	2015S-081-001
Project Name	H. C. Pearson Property
Council District	8 - Bennett
School District	3 – Speering
Requested by	C & K Surveyors, LLC, applicant; Rosa Trevino-Alvarez, owner.
Staff Reviewer	Milligan
Staff Recommendation	<i>Disapprove.</i>

APPLICANT REQUEST

Create 2 lots.

Final Plat

A request for final plat approval to create two lots on property located at 535 Maplewood Lane, approximately 400 feet west of Lemont Drive, zoned Single-Family Residential (RS15) (0.94 acres).

Existing Zoning

Single-Family Residential (RS15) requires a minimum 15,000 square foot lot and is intended for single-family dwellings at a density of 2.47 dwelling units per acre. *RS15 would permit a maximum of 2 units.*

CRITICAL PLANNING GOALS

N/A

PLAN DETAILS

The request is for final plat approval to create 2 lots on property located at 535 Maplewood Lane. Section 3-5.2 of the Subdivision Regulations requires that newly created lots in areas that are previously subdivided and predominately developed must be comparable to surrounding lots in regards to area and frontage. Neither lot meets the compatibility requirement for area or frontage. The applicant requests approval under Section 3-5.2(f) of the Subdivision Regulations, under which the Planning Commission may grant approval of a subdivision that does not meet the compatibility criteria, if the subdivision can provide for harmonious development within the community.

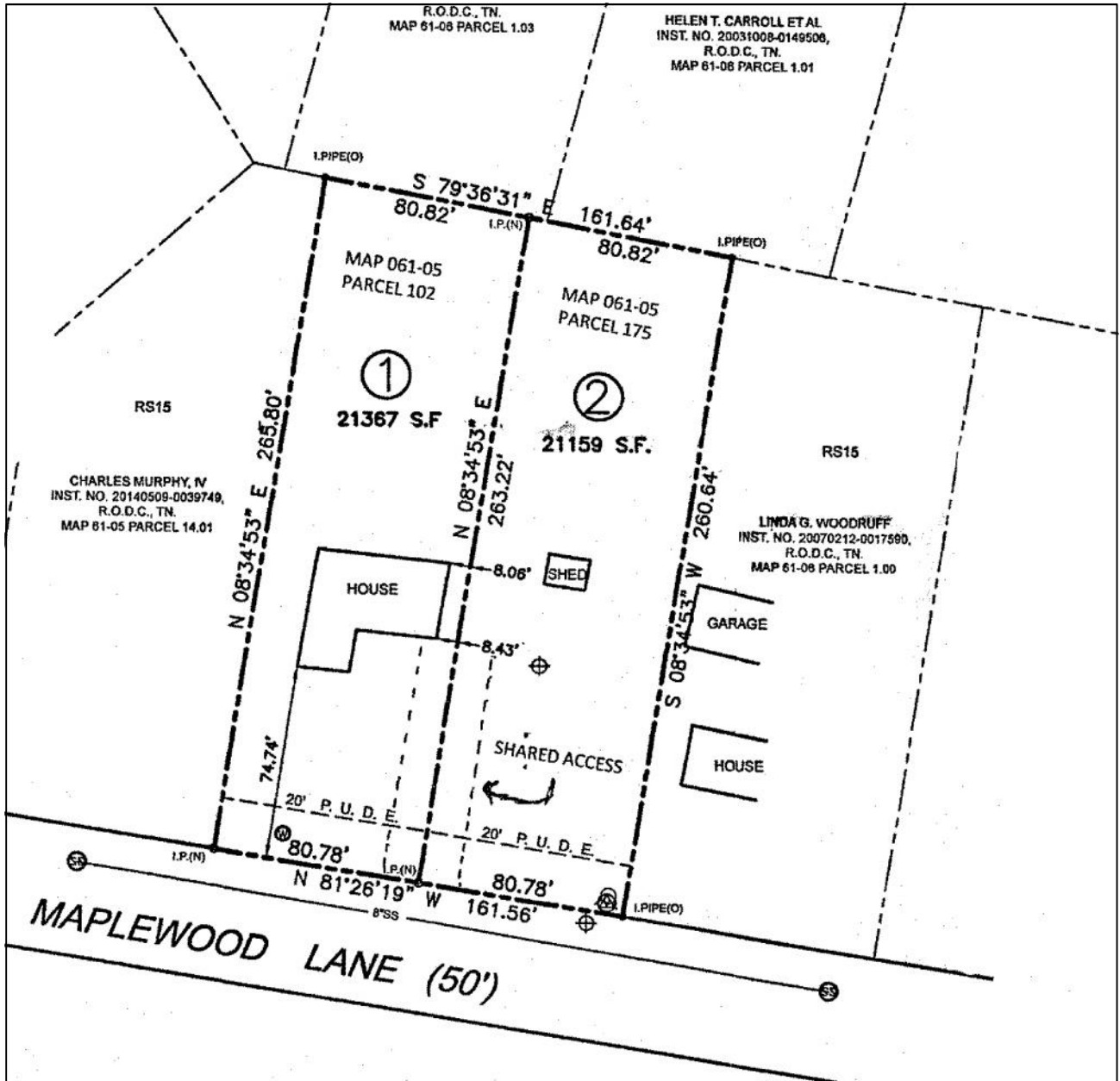
The existing lot is 42,526 SF acres is proposed to be subdivided into two lots with the following areas and street frontages:

- Lot 1: 21,367 Sq. Ft., (0.49 Acres), and 80.78 Ft. of frontage;
- Lot 2: 21,159 Sq. Ft., (0.49 Acres), and 80.78 Ft. of frontage.

The above frontages are based on the plat as drawn. The lot line will have to be shifted to provide for a 10’ side setback for the existing home so these frontages will change slightly. The plan proposes 2 lots, both fronting Maplewood Lane. The existing house on Lot 1 is to remain. Sidewalks are proposed and a note is included on the plat indicating that no building permit will be issued on any of the lots until the required sidewalk is constructed. Shared access is proposed.



Metro Planning Commission Meeting of 07/23/2015



Proposed Subdivision



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ANALYSIS

Lot Compatibility

Section 3-5.2 of the Subdivision Regulations outlines the criteria for reviewing infill subdivisions located within the Neighborhood Maintenance policy area. Staff reviewed the final plat against the following criteria as required by the Subdivision Regulations:

Zoning Code

Both lots meet the minimum standards of the RS15 zoning district.

Street Frontage

All lots have frontage on a public street.

Density

Urban Neighborhood Maintenance land use policy supports density up to 20 dwelling units per acre. The proposed infill subdivision provides a density of 2 dwelling units per acres which falls within the range supported by policy.

Community Character

1. Lot frontage: The proposed lots must have frontage either equal to or greater than 70% of the average frontage of surrounding parcels or equal to or greater than the surrounding lot with the least amount of frontage, whichever is greater. In this case, the lots must be equal to or greater than 98 ft which is the smallest lot frontage of the surrounding lots. Both lots have 80.78 ft of frontage and, therefore, do not meet the community character for lot frontage. These frontage calculations are based on the plat as drawn. The lot line will have to shift slightly to provide for a 10' side setback for the existing home.

Lot Frontage Analysis	
Minimum Proposed	80.78'
70% of Average	69.79'
Smallest Surrounding Parcel	98'

2. Lot size: The proposed lots must have lot area that is either equal to or greater than 70% of the lot size of the average size of surrounding parcels or equal to or larger than smallest surrounding lot, whichever is greater. In this case, the minimum lot area must be at least 21,908 square feet, which is the smallest lot area of the surrounding lots. Neither lot meets the requirement in regards to area. The lot sizes will change slightly based on the needed lot line shift.

Lot Size Analysis	
Minimum Proposed	21,159 SF
70% of Average	16,280 SF
Smallest Surrounding Parcel	21,908 SF



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3. Street setback: The existing house on Lot 1 is setback about 74 feet from Maplewood Lane. Per the Zoning Code, the street setback shall be a contextual setback that considers the minimum street of houses on surrounding lots on the same block face.

4. Lot orientation: Both lots are oriented toward Maplewood which is consistent with the existing lot pattern on the street.

Agency Review

Metro Water Services has not recommended approval at this time.

Harmony of Development

The proposed subdivision does not meet the Community Character criteria. However, the Planning Commission may grant approval if it determines that the subdivision provides for the harmonious development of the community. In this case, the applicant has proposed several conditions to attempt to meet this provision: limiting access to a shared driveway, prohibiting parking within the front setback and limiting the building height to 2 stories in 35 ft.

FIRE MARSHAL RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

Conditions if approved

- If sidewalks are required by Planning and the applicant chooses to construct rather than pay the in-lieu fee, they should be shown and labeled on the plan per Public Works standards. This includes curb and gutter, grass strip, sidewalk, and a minimum of 20 feet of street pavement width. Final construction plans must be submitted that address any related drainage improvements, grading, utility relocation(s), and tree removal, prior to commencing work.

STORMWATER RECOMMENDATION

Approved

TRAFFIC & PARKING RECOMMENDATION

Conditions if approved

- Note that a ramp/driveway permit from Public Works will be required prior to any new street connections.
- Any new driveways must provide a safe sight distance.

WATER SERVICES RECOMMENDATION

Returned for Corrections

- Show public water mains, as marked.
- List Minimum F.F.E.'s that ensure gravity sewer service, as marked.
- The required capacity fees must be paid for plat approval.

STAFF RECOMMENDATION

Staff recommends disapproval as the lot does not meet the compatibility requirement in regards to lot frontage or lot area.



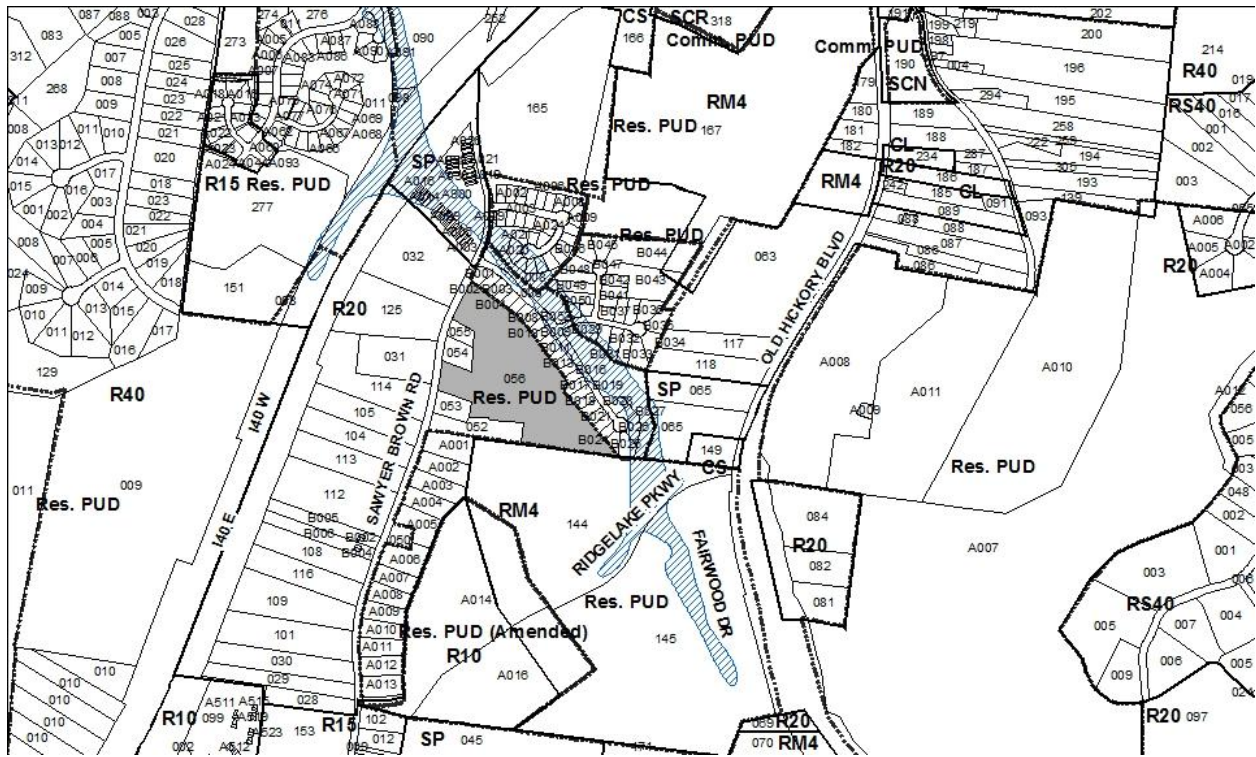
Metro Planning Commission Meeting of 07/23/2015

CONDITIONS (if approved)

1. Show and label the sidewalks on the plat per Public Works standards.
2. Submit revised plat that provides for the required 10' side setback for the existing home on Lot 1.



Metro Planning Commission Meeting of 07/23/2015



2015S-083-001
JOHN HILL PROPERTY
Map 128, Parcel(s) 056
06, Bellevue
22 (Sheri Weiner)



Project No. 2015S-083-001
Project Name John Hill Property
Council District 22-Weiner
School District 9-Frogge
Requested by K & A Land Surveying, applicant; John Robert Hill, owner.

Staff Reviewer Milligan
Staff Recommendation *Defer unless a recommendation is received from all reviewing agencies prior to the Planning Commission meeting. If a recommendation is received, staff recommends approval.*

APPLICANT REQUEST

Create 4 lots.

Final Plat

A request for final plat approval to create four lots on property located at 7650 Sawyer Brown Road, approximately 225 feet south of Williamsport Court, zoned One and Two-Family Residential (R20) (13.9 acres).

Existing Zoning

One and Two-Family Residential (R20) requires a minimum 20,000 square foot lot and is intended for single-family dwellings and duplexes at an overall density of 2.31 dwelling units per acre including 25 percent duplex lots. *R20 would permit a maximum of 30 lots with 7 duplex lots for a total of 37 units.*

CRITICAL PLANNING GOALS

- Preserves Sensitive Environmental Features

The property has areas of slope ranging from 15 to over 25%. The plat established building envelopes in areas where there is minimum to no slope allowing for the majority of the property to be undisturbed.

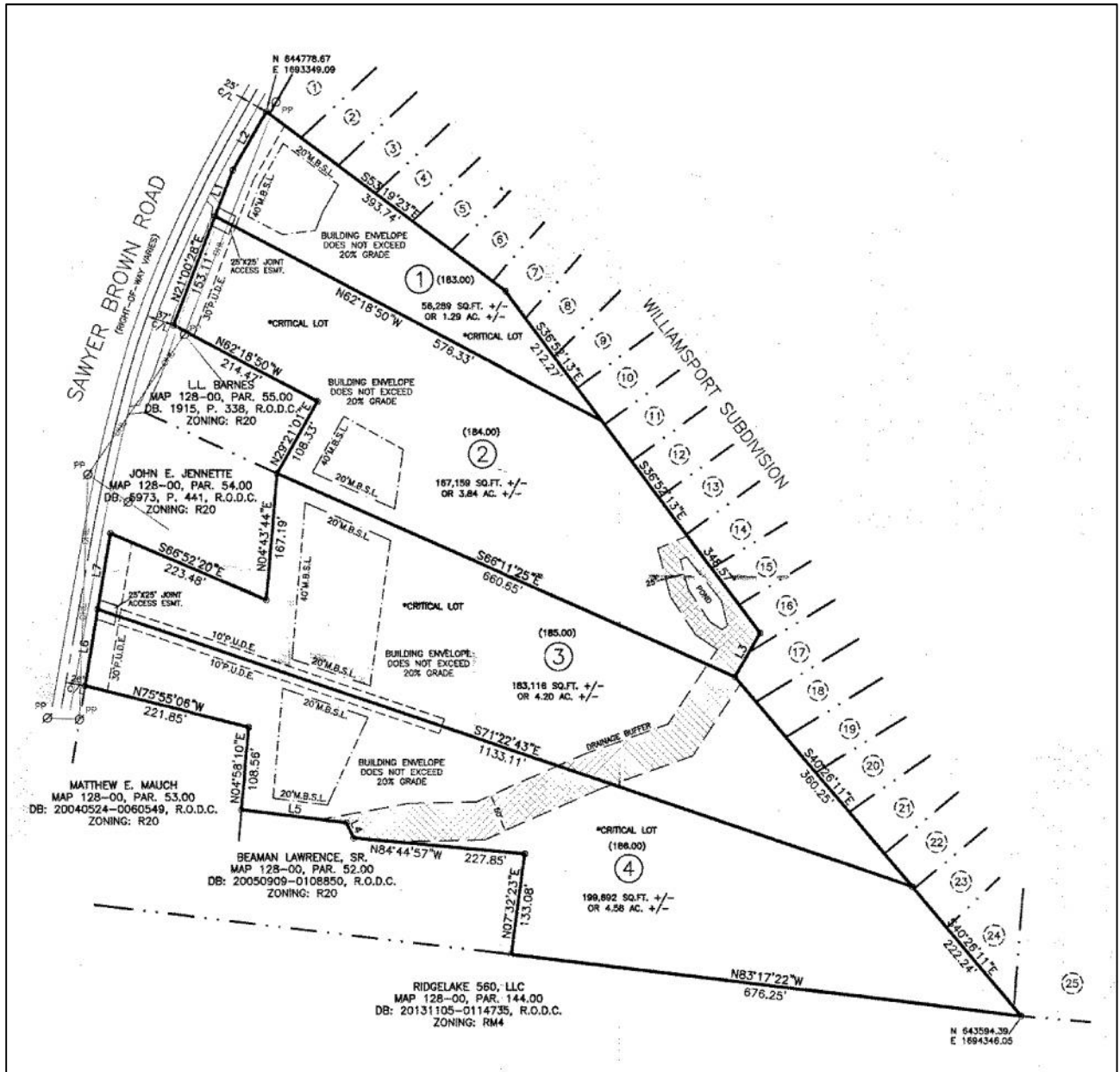
PLAN DETAILS

The request is for final plat approval to create 4 lots on property located at 7650 Sawyer Brown Road. The proposed lots range in size from 1.29 acres to 4.58 acres.

The plan proposes 4 lots all having frontage along Sawyer Brown Road. Sawyer Brown Road is a collector street which requires combined driveways. A joint access is proposed for Lots 1 & 2 and a joint access is proposed for Lots 3 & 4. The plat specifies the building envelope for each lot, allowing for the majority of the property to remain undisturbed which protects the slopes located on the property. Sidewalks are not required per the Subdivision Regulations because the subdivision is located with the General Services District and the Sidewalk Priority Index is less than 20.



Metro Planning Commission Meeting of 07/23/2015



Proposed Subdivision



Metro Planning Commission Meeting of 07/23/2015

ANALYSIS

By establishing the building envelopes on the portions of the property with the least amount of slope, the subdivision is providing for additional development while protecting the sensitive environmental areas on the property. Joint access easements are being provided to limit the driveways along Sawyer Brown Road.

FIRE MARSHAL RECOMMENDATION

Approved with conditions

- Hydrants shall be located within 500' of all parts of every structure via approved hard-surfaced roads.
- Grades are not to exceed 10% per code. The engineer shall provide the grade percentages. The Fire Marshal has allowed up to 12%. Any steeper grade will require the applicant to go to the Appeals Board.

PUBLIC WORKS RECOMMENDATION

Returned for corrections

- Show the ROW on both sides of Sawyer Brown Rd and in front of the Barnes and Jennette property.
- Show existing homes and driveways.

STORMWATER RECOMMENDATION

Approved

TRAFFIC & PARKING RECOMMENDATION

Conditions if approved

- Note that a ramp/driveway permit from Public Works will be required prior to any new street connections.
- Any new driveways must provide a safe sight distance.

HARPETH VALLEY UTILITY DISTRICT

Recommendation not yet received.

STAFF RECOMMENDATION

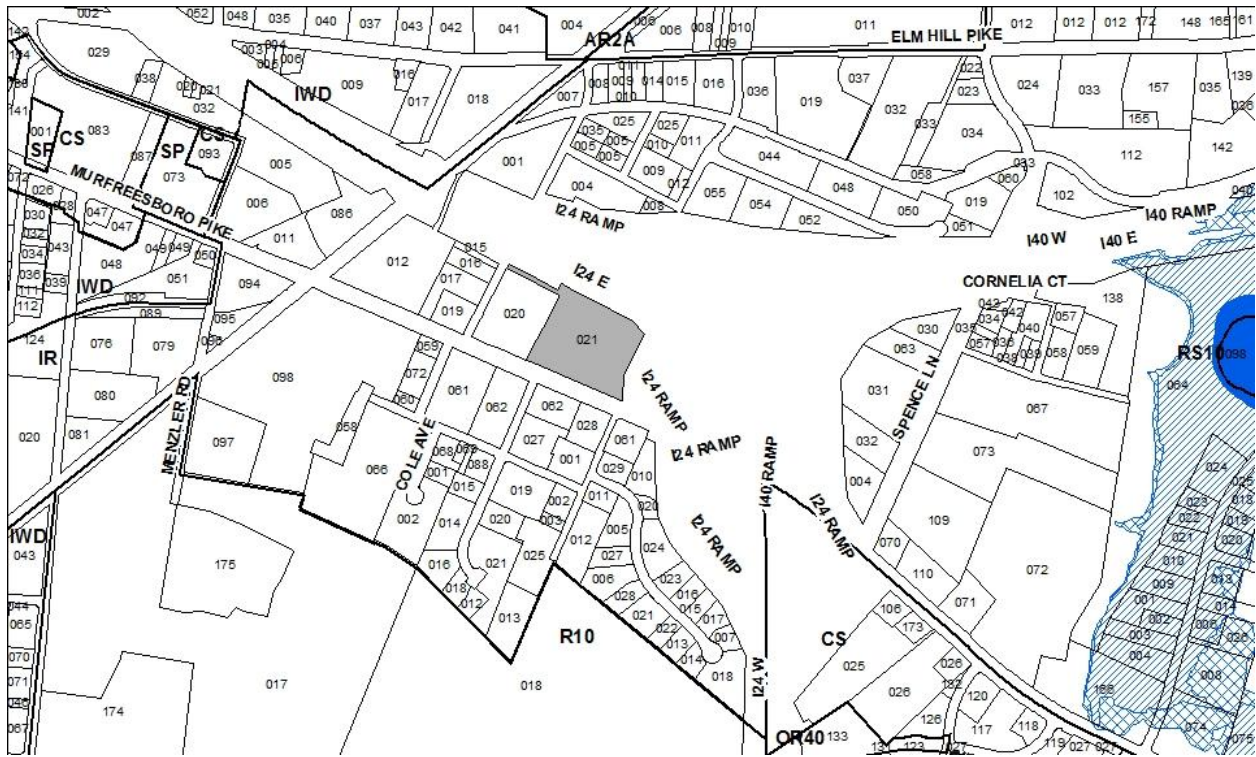
Staff recommends deferral unless a recommendation of approval is received from all reviewing agencies prior to the Planning Commission meeting. If a recommendation is received, staff recommends approval with a condition.

CONDITIONS (if approved)

1. Add the following note to the plat: Access from Sawyer Brown Road is limited to the designated joint access easements.



Metro Planning Commission Meeting of 07/23/2015



2015S-089-001

MICHAEL A. RIVALTO PROPERTY

Map 106-07, Parcel(s) 021

11, South Nashville

19 (Erica S. Gilmore)



Project No. 2015S-089-001
Project Name Michael A. Rivalto Property
Council District 19 - Gilmore
School District 5 - Kim
Requested by Arnold Consulting Engineering Services, Inc, applicant;
Michael A. Rivalto, owner.

Staff Reviewer Thomas
Staff Recommendation *Approve with conditions.*

APPLICANT REQUEST

Create 3 lots.

Final Plat

A request for final plat approval to create three lots on property located at 718 Murfreesboro Pike, approximately 485 feet east of Arlington Avenue, zoned Industrial Restrictive (IR) (6.38 acres).

Existing Zoning

Industrial Restrictive (IR) is intended for a wide range of light manufacturing uses at moderate intensities within enclosed structures.

CRITICAL PLANNING GOALS

N/A

PLAN DETAILS

The applicant requests final plat approval for a three lot subdivision of nonresidential property located at 718 Murfreesboro Pike. Each lot has frontage along Murfreesboro Pike; however, access is restricted to a single platted access easement on Lot 1. Lot 1 also has frontage along Arlington Avenue which could provide a second point of access for the owner of Lot 1.

The existing 273,589 square foot (6.28 acre) property is proposed to be subdivided into three lots with the following areas:

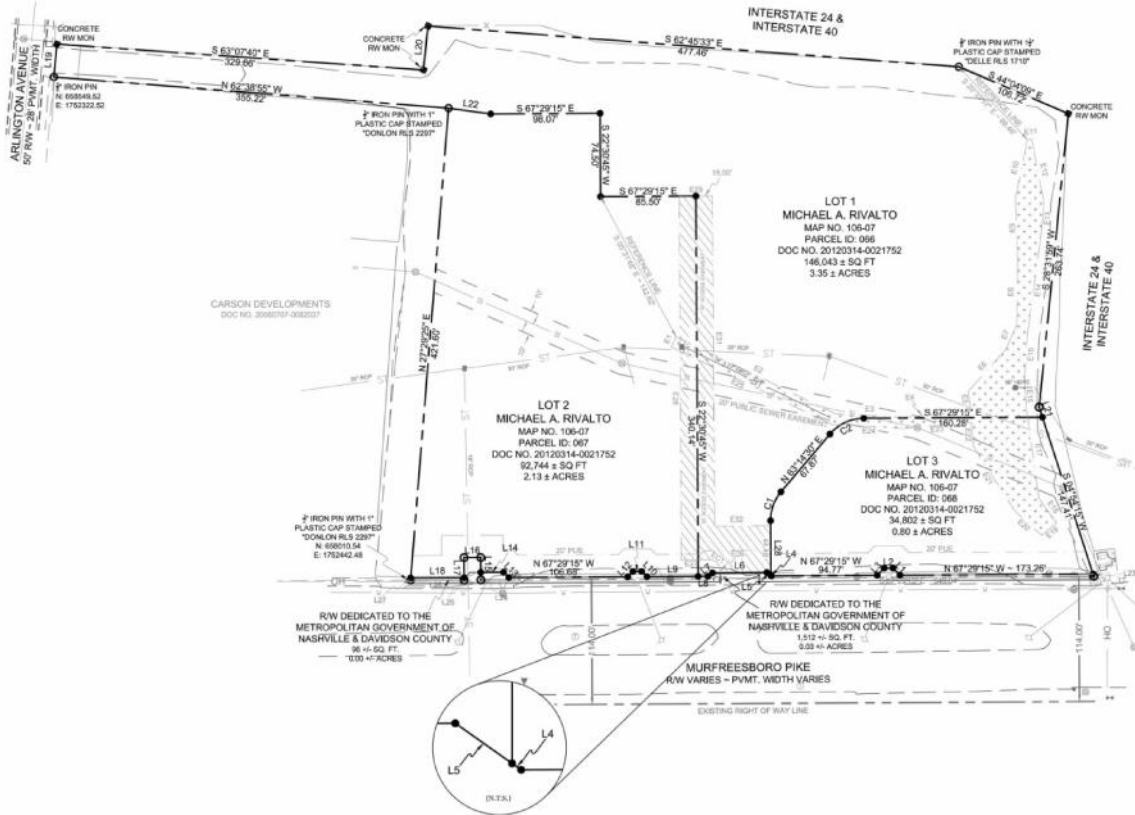
- Lot 1: 146,043 Sq. Ft., (3.35 acres)
- Lot 2: 92,744 Sq. Ft., (2.13 acres)
- Lot 3: 34,802 Sq. Ft., (0.80 acres)

ANALYSIS

All three lots meet the lot requirements of Section 3-4 of the Subdivision Regulations. None of the lots are required to be designated as critical lots. The IR zoning district does not require a minimum lot size. Access along Murfreesboro Pike is proposed to be restricted to a single platted access easement as depicted and noted on the plat. The applicant has coordinated drainage with Stormwater and a drainage easement is proposed on Lots 1 and 3.



Metro Planning Commission Meeting of 07/23/2015



Proposed Subdivision



Metro Planning Commission Meeting of 07/23/2015

FIRE MARSHAL RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

No exception taken

STORMWATER RECOMMENDATION

Approved

TRAFFIC & PARKING RECOMMENDATION

No exception taken

WATER SERVICES RECOMMENDATION

Approved

STAFF RECOMMENDATION

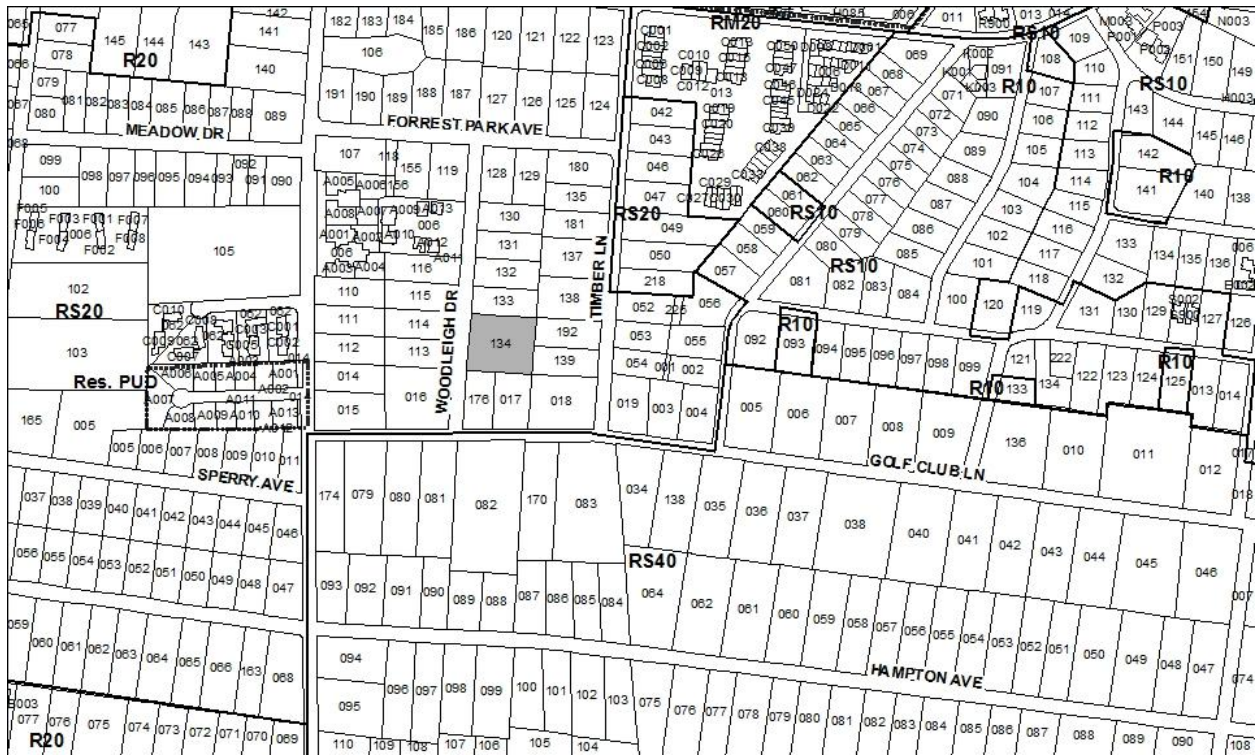
Staff recommends approval with a condition.

CONDITIONS

1. The final plat shall include the following revised language for General Note 15: "The platted access easement shall be the only location along Murfreesboro Pike where access shall be permitted."



Metro Planning Commission Meeting of 07/23/2015



2015S-096-001

714 WOODLEIGH

Map 117-01, Parcel(s) 134

10, Green Hills - Midtown

24 (Jason Holleman)



Project No.	2015S-096-001
Project Name	714 Woodleigh
Council District	24 – Holleman
School District	8 – Pierce
Requested by	Fulmer Engineering, applicant; Woodleigh Partners, LLC, owner.
Staff Reviewer	Sajid
Staff Recommendation	<i>Approve with conditions.</i>

APPLICANT REQUEST

Create 2 lots.

Subdivision

A request for final plat approval to create two lots on property located at 714 Woodleigh Drive, approximately 200 feet north of Golf Club Lane, zoned Single-Family Residential (RS20) (1.05 acres).

Existing Zoning

Single-Family Residential (RS20) requires a minimum 20,000 square foot lot and is intended for single-family dwellings at a density of 1.85 dwelling units per acre. *RS20 would permit a maximum of 2 units.*

CRITICAL PLANNING GOALS

- Supports Infill Development

The proposed SP creates an opportunity for infill housing in an area that is served by existing infrastructure. Locating development in areas served by existing, adequate infrastructure does not burden Metro with the cost of upgrading or building new infrastructure.

PLAN DETAILS

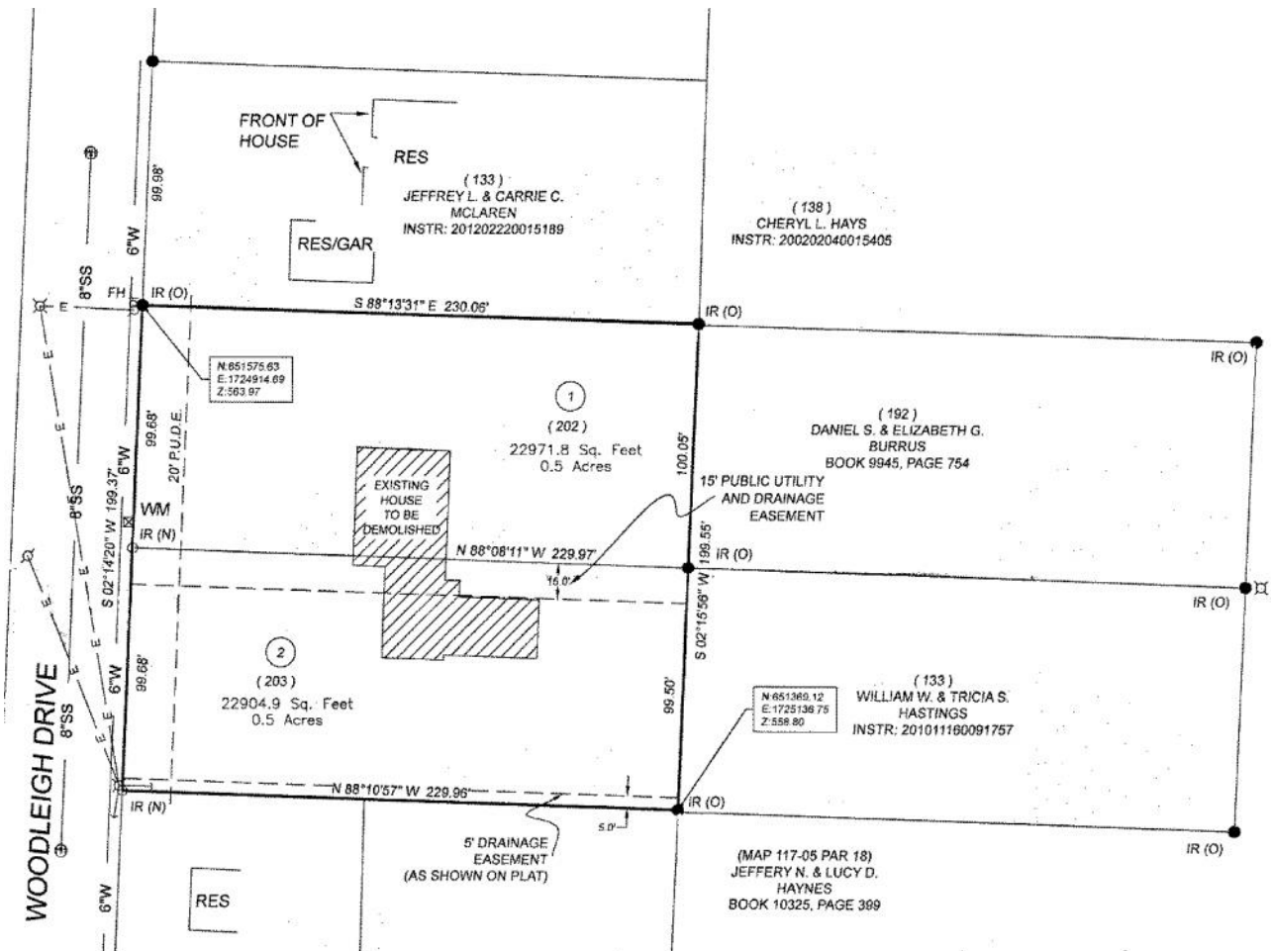
The applicant requests final plat approval for a two lot subdivision of property located at 714 Woodleigh Drive. While both lots meet the infill compatibility analysis that is outlined in Section 3-5.2 of the Subdivision Regulations for lot area, neither of the proposed lots meet for frontage. The applicant requests approval under Section 3-5.2(f) of the Subdivision Regulations; under this section, the Planning Commission may grant approval of a subdivision that does not meet the compatibility criteria, if the subdivision can provide for harmonious development within the community.

The existing lot is 45,876 SF acres is proposed to be subdivided into two lots with the following areas and street frontages:

- Lot 1: 22,971 Sq. Ft., (0.5 Acres), and 99.68 Ft. of frontage;
- Lot 2: 22,905 Sq. Ft., (0.5 Acres), and 99.68 Ft. of frontage.



Metro Planning Commission Meeting of 07/23/2015



Proposed Subdivision



Metro Planning Commission Meeting of 07/23/2015

The plan proposes two lots, both fronting Woodleigh Drive. The existing house on the site is to be demolished. Sidewalks are required with the subdivision as the site is not served by existing sidewalks, and the applicant has chosen to pay the contribution in-lieu of sidewalk construction fee.

ANALYSIS

Lot Compatibility

Section 3-5.2 of the Subdivision Regulations outlines the criteria for reviewing infill subdivisions located within the Suburban Neighborhood Maintenance policy area. Staff reviewed the final plat against the following criteria as required by the Subdivision Regulations:

Zoning Code

Both lots meet the minimum standards of the RS20 zoning district.

Street Frontage

Both lots have frontage on a public street.

Density

Suburban Neighborhood Maintenance areas are intended to be low to moderate density. The proposed infill subdivision provides a density of 1.9 dwelling units per acres which is consistent with the land use policy.

Community Character

1. Lot frontage: The proposed lots must have frontage either equal to or greater than 70% of the average frontage of surrounding parcels or equal to or greater than the surrounding lot with the least amount of frontage, whichever is greater. In this case, the lots must be equal to or greater than 100 ft which is the smallest lot frontage of the surrounding lots. Both lots have 99.68 ft of frontage and, therefore, do not meet the community character for lot frontage.

Lot Frontage Analysis	
Minimum Proposed	99.68'
70% of Average	70'
Smallest Surrounding Parcel	100'

2. Lot size: The proposed lots must have lot area that is either equal to or greater than 70% of the lot size of the average size of surrounding parcels or equal to or larger than smallest surrounding lot, whichever is greater. In this case, the minimum lot area must be at least 22,859 square feet, which is the smallest lot area of the surrounding lots. Both lots meet the lot area requirement.

Lot Size Analysis	
Minimum Proposed	22,905 SF
70% of Average	16,088 SF
Smallest Surrounding Parcel	22,859 SF



Metro Planning Commission Meeting of 07/23/2015

3. Street setback: Per the Zoning Code, the street setback shall be a contextual setback that considers the minimum street setback of houses on surrounding lots on the same block face.
4. Lot orientation: Both lots are oriented toward Woodleigh Drive which is consistent with the existing lot pattern on the street.

Agency Review

All review agencies recommend approval.

Harmony of Development

The proposed subdivision does not meet the Community Character criteria. However, the Planning Commission may grant approval if it determines that the subdivision provides for the harmonious development of the community. In this case, the applicant has proposed to limit the building height to 2 stories in 38 ft.

Staff finds that the conditions proposed by the applicant overcome the incompatibility of the proposed lots with regard to lot frontage to provide for the harmonious development of the community.

FIRE MARSHAL RECOMMENDATION

N/A

PUBLIC WORKS RECOMMENDATION

Conditional if approved

- If sidewalks are required by Planning, then they should be shown and labeled on the plan as 5 foot sidewalk, curb and gutter, 4 foot grass strip or as determined by Public Works, a minimum of 20 feet pavement on the street, and fully within the right of way.

STORMWATER RECOMMENDATION

Approved

TRAFFIC & PARKING RECOMMENDATION

Conditional if approved

- Note that a ramp/driveway permit from Public Works will be required prior to any new street connections.
- Any new driveways must provide a safe sight distance.

WATER SERVICES RECOMMENDATION

Approve



Metro Planning Commission Meeting of 07/23/2015

STAFF RECOMMENDATION

Staff finds that the proposed subdivision can provide for the harmonious development of the community and recommends approval with conditions.

CONDITIONS

1. The existing house shall be demolished and removed from the plat prior to recordation.
2. The applicant has chosen to pay the contribution in-lieu fee which shall be paid prior to recordation. Submit contribution in lieu of construction to the Planning Department. The rate of \$96 per linear foot of total frontage area will require a \$19,139 contribution to Pedestrian Benefit Zone 4-B.



SEE NEXT PAGE



EXHIBIT A



Richard T. Bowles
Michael P. Verna
Robert I. Westerfield
Richard A. Ergo
K.P. Dean Harper
Bradley R. Bowles
Kenneth B. McKenzie
David W. Trotter
Jason J. Granskog
Lawrence D. Goldberg
Cathleen S. Huang
Ethan K. Friedman
William T. Nagle
Michael P. Connolly
Nathaniel B. Duncan
Cheryl A. Noll
Michael T. Krueger
Deborah P. Cwalina
Mallory L. Homewood

Of Counsel
Bruce C. Paltenghi

July 10, 2015

Via E-mail

Doug Sloan
Metropolitan Nashville/Davidson County
Planning Department
Metro Office Building
800 Second Avenue South
P.O. Box 196300
Nashville, TN 37219-6300

**Re: Periodic Review of Nashville Highlands Planned Unit Development
Map 142, Parcel 021 and Map 128, Parcel 154)
Hearing Date: July 23, 2015**

Dear Mr. Sloan:

The undersigned acts on behalf of the owner of this project, Nashville Highlands, LLC, (“Nashville Highlands”).

You should be aware that a private appraisal of Nashville Highlands was performed by Boozer & Co. of Franklin, Tennessee on June 13, 2013 before we decided to list the property for sale rather than proceed with development ourselves. That appraised value was \$5,125,000. A copy of the summary report is attached.

Based on the contract we already have in hand with Alliance Realty Partners, LLC (“Alliance”) and the valuations for the remaining land from our broker, Marcus & Millichap, the fair market value of Nashville Highlands today is significantly higher.

Moreover, Alliance’s due diligence period has not yet expired, so they are free to void our contract if they are prevented from constructing units. Thus, cancelling or amending the PUD would be devastating to the value of Nashville Highlands’ property.

In addition, Nashville Highlands has expended a great deal of money already to develop and market the property. These expenses include:

Metropolitan Nashville/Davidson County Planning Department
July 10, 2015
Page 2

- \$1,111,572.43 for construction of a water tank, pump station, and water mains at the request of Harpeth Valley Utilities District (copy of MOU confirming this cost enclosed)
- \$830,000 est. for construction of 1850 linear feet of roadway, with utilities
- \$120,000 est. for installation of new traffic light on Old Hickory Blvd.
- \$100,000 est. for construction of 1800 linear feet of offsite gravity sanitary sewer to serve existing and remaining phases
- \$16,000 in site layout engineering costs to Civil Site Design Group in the last 18 months
- \$5,000 appraisal fee
- \$325,000 est. in brokerage commissions to Marcus & Millichap upon sale of the property

This adds up to over \$2.5 million of expenses to develop and market on top of more than \$5.125 million in a two year old appraised value that would be lost if the PUD were cancelled or significantly amended. This enormously negative financial impact on Nashville Highlands should be considered as part of the review process.

Thank you for your consideration.

Very truly yours,



Michael P. Verna

cc: Shawn R. Henry

**BOOZER &
COMPANY, P. C.**

SUMMARY OF CONCLUSIONS

Property Identification: Nashville Highlands Land Tract
Old Hickory Boulevard/Highway 70 S
Bellevue, Davidson County, TN 37221

Tax Map/Parcel(s): Map 128, Parcels 119 & 154
Map 142, Parcel 21

Current Ownership: Nashville Highlands, LLC

Effective Date of Appraisal: June 13, 2013

Date of Inspection: June 13, 2013

Date of Report: June 26, 2013

Interest Appraised: Fee Simple Estate

Value Requested: Market Value

Property Data

Multi-Family Land Area: 206.94 acres
Commercial Site: 4.91 acres
Total Land Area: 211.85 acres

Topography: The multi-family tract exhibits rolling to steep, heavily wooded topography. The commercial site features level terrain that has been cleared to allow for the construction of a 15,000 SF retail center.

Access: The multi-family tract is accessible via an unnamed entrance road that has been constructed along the northern boundary of the *Reserve at Eagle Ridge* townhouse development, which terminates at the subject's property line. The commercial site is also accessible from this entrance road, with two curb cuts in place.

Flood Map: 47037C-0306F & 0308F, which were last revised on April 20, 2001. Subject is **not** located within a flood hazard area.

Zoning: 206.94 acres zoned RESPUD (allows 864 multi-family units)
4.91 acres zoned CPUD (allows 15,000 SF retail building)

Highest and Best Use: Multi-family development on RESPUD zoned land over a 4-year absorption period. Commercial retail development on CPUD zoned land.

Exposure Time: 12 months or less

VALUATION ESTIMATES

Market Value of Multi-Family Land (206.94 acres).....\$4,210,000.00
Market Value of Commercial Site (4.91 acres).....\$ 915,000.00
Total "As Is" Market Value.....\$5,125,000.00

**MEMORANDUM OF UNDERSTANDING
CREDITS AND MISCELLANEOUS**

(For Harpeth Valley Utilities District Water and Sewer Service to Nashville Highlands Proposed Development)

It is agreed hereto by and between the District and the Developer:

For and in consideration of the mutual promises of the parties herein contained, and particularly taking into consideration the fact that the Developer is installing a storage reservoir, pump station and large trunk mains, all at great expense, in order that the area involved, among other areas, might be developed by the Developer and the community permitted to expand, and for further good and valuable considerations set forth hereinafter, the District and the Developer agree to the following:

1. For and in consideration of the construction of a water tank, booster station and water mains at a construction cost of \$1,111,572.43 for the Nashville Highlands Development, the District will allow tapping credits for the full amount of \$1,111,572.43.
 - a. These can be credited toward 50% water tapping fees of the Developer's Project
 - b. These can be sold and credited toward 50% water tapping fees of any Project/Development within the District
 - c. These credits are good for 15 years
 - d. The water line constructed by the Developer through Summit Oaks to be dedicated to the District shall be large enough to make water service available to residents of Summit Oaks upon payment of the required fees.

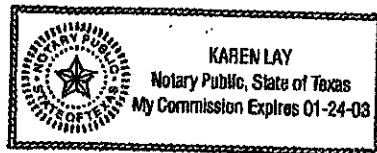
Proposed Development: Nashville Highlands

Developer: Nashville Highlands, LLC

By: _____
(Signature) *[Handwritten Signature]*

Harpeth Valley Utilities District

By: *[Handwritten Signature]*



7-21-00

CIVIL SITE
DESIGN GROUP
ENGINEERS • PLANNERS • LANDSCAPE ARCHITECTS

July 9, 2015

Mr. Dough Sloan
Metro Nashville Planning Department
800 Second Ave South
Nashville, TN 37219

RE: Nashville Highlands PUD
Review of the PUD Status
Map 142, Parcel 021 and Map 128 Parcel 154

Dear Mr. Sloan,

It is our understanding that the Planning Commission has been tasked with the request to review the status of the Planned Unit Development in accordance with Section 17.40.120.H. As considered under this section H.a.4.ii please find this letter herewith as proof that the property owner has been actively developing the remaining portions of the Nashville Highlands PUD over the past 12 months.

We were retained by the property owner in February of 2014 to conduct preliminary civil engineering studies pertaining to the remaining portions of the PUD. We prepared conceptual layouts for multifamily products, prepared conceptual grading plans, utility plans, and environmental plans. We then provided and actively worked with a large local residential construction firm to prepare preliminary construction cost estimates. We have had a number of meetings with this group, the multifamily developer, and the property owner to determine cost savings measures. With this information the multifamily developer has been able to determine approximate construction costs and thus a "per-door" cost of development. All of this information and coordination has been occurring since last June.

Also, please note that this property owner has already installed the necessary infrastructure to the boundaries or within the boundaries of this remaining phase.

- This property owner installed 1800 linear feet of offsite sanitary sewer main to and through this property for the existing phases as well as for the use of this remaining phase.
- This property owner has installed 1850 linear feet of roadway infrastructure to provide access to this remaining phase.
- This property owner has installed a \$1.1 million dollar regional water tower to serve the entire PUD including this remaining phase.
- This property owner has installed a traffic light on Old Hickory Blvd for the existing phases as well as this remaining phase.

So as you can see a significant amount of capital has been used for this PUD and particularly for this remaining phase. This action should satisfy Section H.3.a.ii.

With this information we respectfully request that staff provide a positive recommendation to the Planning Commission to keep the PUD in its current form. Thank you for your consideration.

Sincerely,



Ryan E. Lovelace, P.E.
Principal

CSDG No. 14-031-01

Metro Maps



DISCLAIMER: The user understands that the entities which comprise Metropolitan Government make no representations as to the accuracy of the information in the mapping data, but rather, provide said information as is. The user knowingly waives any and all claims for damages against any and all of the entities comprising Metropolitan Government that may arise from the mapping data. This is not a legal document!

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